ORDINARY COUNCIL

Wednesday 16 June 2021





Ordinary Council Meeting Wednesday, 16 June 2021

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Leadership and Governance

What we are trying to achieve

A community that works together in decision making that is defined as ethically, socially and environmentally responsible.

What the result will be

We will have:

- A community that has the opportunity to be involved in decision making
- Open, easy, meaningful, regular and diverse communication between the community and decision makers
- Partnerships and collaborative projects, that meet the community's expectations, needs and challenges
- Knowledgeable, skilled and connected community leaders
- Strong corporate management that is transparent

How we will get there

- 1.1 Inform and engage with the community about what Council does using varied communication channels
- 1.2 Maintain strong partnerships between all stakeholders local, state and federal so that they are affective advocates for the community
- 1.3 Demonstrate leadership
- 1.4 Use innovative, efficient and sustainable practices
- 1.5 Ensure strong corporate and financial management that is transparent and accountable



ORDINARY COUNCIL 16/06/2021



Mid North Coast Joint Organisation

Statement of Strategic Priorities 2021

What is the Mid North Coast Joint Organisation?

The Mid North Coast Joint Organisation (MNCJO) represents a new way for its member councils to work together for the benefit of the region. Established in 2018, the MNCJO has three members:

- Bellingen Shire Council
- Kempsey Shire Council
- Port Macquarie-Hastings Council.

Together the councils cover an area of 8,658 square kilometres and a population of approximately 125,000 people.

Joint Organisations (JOs) were created in NSW following changes to the *Local Government Act 1993 (NSW)* in late 2017 and represent a different approach for councils to work strategically with the NSW Government.

The MNCJO's primary role is to support a shared approach to the vision and long-term development of the Mid North Coast region. It builds on previous work undertaken by the councils as members of the former Mid North Coast Regional Organisation of Councils.

The MNCJO is overseen by a Board comprising the Mayors from each Member Council, supported by the General Managers of each member council... Senior Managers from Regional NSW, Destination NSW and Regional Development Australia (RDA) also attend and provide reports to the Board. The NSW OLG relationship manager also attends.

What is the timing for this plan?

This Statement of Regional Priorities (Statement) is an update to the Statement from September 2018 to September 2021. From September 2021, a new three year term of local government will come into effect, with the MDCJO Chair elected for a two year term.

What was the process for preparing this plan?

This 2021 Statement has been developed through a mix of research and stakeholder consultation. A wide range of relevant documents including regional plans and regional economic development strategies from member councils were reviewed and a workshop of the MNCJO Board was held on April 9 2021. During this workshop, performance against the 2018 Statement was assessed to understand progress to date and to determine appropriate actions and activities for the future to achieve the strategicgoals.

The 2021 Statement has been reviewed and updated throughout this process and was approved by the MNCJO Board on June 4, 2021.

When will this plan be reviewed?

This Statement will be reviewed annually and at the commencement of new terms of office for the MNCJO Board.

Vision for the Mid North Coast region

The MNCJO vision is to help create a vibrant, sustainable region underpinned by economic growth and a commitment to the social and environmental wellbeing of its communities.

Mission statement

To set priorities, lead and advocate for the Mid North Coast region and work collaboratively with the community, government and non-government organisations and business to achieve them.

Functions

The core functions of the MNCJO are:

- Strategic planning and priority setting based on robust processes for identifying regional goals, with the flexibility to respond to changing priorities and needs of member councils as required
- Intergovernmental collaboration building effective working relationships with other JOs, councils and the NSW and Australian Governments
- Regional leadership and advocacy creating a shared purpose and strategic direction for the region and a commitment to constructive and cooperative implementation by all member councils
- Building efficient and effective councils by examining opportunities for shared services and capacity building across the region.

Guiding principles

- Decisions are made with a whole of region focus Flexibility to respond to changing priorities and community needs
- Accountability to members and their communities of interest
- · Commitment to collaboration and good working relationships
- Clear communication between members and other stakeholders
- Commitment to make MNCJO work effectively and efficiently
- Evaluation and reporting to ensure targeted outputs and outcomes
- Strong governance, with robust processes, accountability and transparent decision-making.

Strategic goals and actions

This 2021 Statement builds on the Strategic Plan (Plan) which was prepared in 2018. This 2021 Statement contains six strategic goals (which relate to the social, environmental, economic, infrastructure, governance and advocacy goals for the region), linked to a number of high level actions and more specific individual activities under each strategic goal.

The key partners and regional documents are also highlighted to ensure strategic alignment. In addition to the regional documents noted, there are also a large number of relevant documents prepared by each member council, including Community Strategic Plans, Local Environmental Plans and a wide range of strategies, plans and policies.



1. Social, health and weilness Goal: an active, healthy, resilient and safe community

Кеу ра	irtners		Key regional documents	
1 • 1 • 1 • 1 •	ISW Department of Premier and Cabinet ISW Department of Education ISW Department of Communities and Justice ISW Department of Planning, Industry and invironment ISW Health and Mid North Coast Local Health District	NSW Land and Housing Corporation NSW Department of Primary Industries Australian Government Department of Social Services	North Coast Regional Plan 2036 (being 2021/22) North Coast Enabling Regional Adapta NSW) *Ageing Well in NSW: Seniors Strategy Community Strategic Plans prepared I Councils *Local Strategic Planning statements I Member Councils Local Strategic Planning Statements p member Councils NSW Housing Strategy 2041	tion (Adapt y 2021-2031?? by Member prepared by
No.	High level actions	Activities		Functional Area
1.1	Build capacity in the regional community to deal withsevere weather events and natural disasters	Strengthen the existing 'Resilience Partnership' with Charles and ensure sustainable community projects are developed in Engage with Resilience NSW to embed 'Preparedness Recov covered by the MNCJO. Ensure local communities have access to information and te- in the region	n the context of this partnership. ery and Resilience Strategies' in the area	Leadership
1.2	Develop plan on regional housing supply, demand and housing affordability	MNCJO councils to collaborate a round LSPSs to influence zo	ning and unlock land supply	Planning, collaboration
		MNCJO to advocate to NSW government on regional housing	g issues	Collaboration
		Seek funding to Investigate the feasibility of a community la	nd trust in the region	Planning, collaboration
1.3	Collaborate with landholders (property, land and caravan parks) to understand crisis accommodation issues	Collaborate with RDA to finalise evidence base and bring bac	k to the MNCJO	Colla boration

2. Environmen

Goal: enhance natural and cultural heritage

Key partners	Key regional documents
Local Aboriginal Land Councils	North Coast Regional Plan 2036 (being reviewed in 2021/22)
NSW Department of Primary Industries	 North Coast Enabling Regional Adaptation (Adapt NSW)
NSW Department of Planning, Industry and Environment	 NSW State Environmental Planning Policy (Koala Habitat Protection) 2021
NSW Environmental Protection Authority	 North Coast Enabling Regional Adaptation report 2019
NSW Environment, Energy and Science Group	 North Coast Regional Emergency Management Plan 2019
North Coast Local Land Services	 NSW Natural Disaster Resilience Program (Community Resilience Innovation Program)
	 NSW Floodplain Development Manual 2005
	Koala SEPP 2021
	Community Strategic Plans
	 Local Strategic Planning Statements

No.	High level actions	Activities	Functional Area
2.1	Preserve strategic biodiversity and identify land with high environmental value and biodiversity corridors across the region	Deliver Regional Biodiversity Offsetting and Carbon Project (funded by OLG Financial Sustainability Project) with appointed external consultants	Planning, collaboration
bi		Build on the success of the Koala Recovery Partnership to improve koala conservation across the MNCJO Region	Leadership, collaboration
2.2	Maintain focus on koala protection and habitat conservation and build on the Koala Recovery Project	Build koala priorities into council plans	Planning, research, collaboration
		Create shared priorities for koala protection with MNCJO, RDA and Local Land Services (LLS)	Collaboration
		Use evidence base established through Great Koala National Park and biodiversity work to advocate to other councils and NSW government	Collaboration, research
		Actively engage the timber industry in a conversation about the future of the region and develop an evidence base with the timber industry to get the best value out of this resource	Collaboration, research

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2.3	Develop improved solutions for waste and water at a regional level	MNCJO to form working group with regional waste providers and all councils to develop a strategic waste plan	Leadership, collaboration, planning
		MNCJO build on discussion paper on water and work with other JOs to investigate opportunities for water authorities to link to JOs to collaborate in strategic water planning	Leadership, collaboration, planning
2.4	Develop a plan aligned to Adapt North Coost to facilitate actions for climate change adaptation at a regional level	MNCJO to align with priorities in the regional adaptation plan and source funding to implement initiatives on a regional basis, including microgrids and other adaption strategies	Planning, collaboration

3. Economy

Goal: a diverse and robust economy which strengthens existing and creates new industries and educational opportunities

No.	High level actions	Activities	Functional Area
3.1	Actively position the Mid North Coast as a great place to live, work, invest and play.	MNCJO in association with Destination NSW to lead Mid North Coast regional brand strategy across all councils	Planning, collaboration
		Identify opportunities for nature-based and indigenous tourism	Research, planning, collaboration
3.2	Support research to better understand responses to regional skills shortage	MNCJO to support the RDA work and its advocacy in this area	Research, collaboration
3.3	Support research for development and manufacture of assistive technologies, especially for aged care	MNCJO to support the RDA work and its advocacy in this area	Research, collaboration

4. Infrastructure

Goal: vibrant, connected cities and centres

Key partners	Key regional documents
Regional Development Australia – Mid North Coast	 North Coast Regional Plan 2036 (being reviewed in 2021/22)
NSW Department of Regional NSW	 North Coast Enabling Regional Adaptation (Adapt NSW)
 NSW Department of Planning, Industry and 	 A 20-Year Economic Vision for Regional NSW 2021
Environment Infrastructure NSW	 Hastings Macleay Regional Economic Development Strategy 2018-2022
Transport for NSW	Coffs Coast Regional Economic Development Strategy 2018-
Property Council Australia	2022
Urban Development Institute of Australia	 Bellingen Shire Council Economic Development and Tourism Plan 2015-2020
North Coast Local Land Services	Mid North Coast SMART Region Strategy 2018
	Local Land Services North Coast Local Strategic Plan 2016-2021
	Coffs Harbour Regional City Action Plan 2036
	Port Macquarie Draft Regional City Action Plan 2036
	Community Strategic Plans
	Local Strategic Planning Statements

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No.	High level actions	Activities	Functional Area
4.1 Restore and renew regional roads and bridges		Continue to support member councils with roll out of MNCJO bridges project	Planning, collaboration
		Continue joint discussions regarding a regional approach other transport infrastructure.	Planning, collaboration
4.2	Design and create safe public places and preserve the unique regional/local character of cities, towns and villages	MNCJO councils to continue to actively seek grant funding to upgrade tourism infrastructure through funds such as Stronger Country Communities, BLERF, Regional Tourism Fund etc.	Planning, collaboration

5. Regional leadership

Goal: effective and efficient governance and regional leadership

Key partners Key regional documents				
• R • N • N	legional Leadership Executive legional Development Australia – Mid North Coast ISW Department of Regional NSW ISW Office of Local Government ISW Department of Premier and Cabinet Other Joint Organisations High level actions Develop consistency across the region with all councils having preparedness/recovery/resilience officers to enable LGA and regional response to disaster and embed the function into the core business of councils.	Local Government NSW & Local Government Procurement NSW Local Government Professionals NSW Resilience NSW Activities MNCJO to identify needs for each council and engag these resources	North Coast Regional Plan 2036 (being review North Coast Enabling Regional Adaptation (Ad Community Strategic Plans prepared by Memi Local Strategic Planning Statements RDA Mid North Coast Regional Plan	lapt NSW)
5.2	Improve financial sustainability of local governments in the MNCJO	MNCJO to undertake research to develop an eviden would lead to increased skills/jobs etc.	ce base to evaluate how much % increase in FAGs	Leadership, research, collaboration
		MNCJO to advocate to LG NSW and federal governme	ient on FAGs	Leadership, collaboration
		Identify opportunities for shared procurement to im	prove efficiency and effectiveness of service delivery	Planning, collaboration
5.3		Continue to engage with neighbouring Local Govern membership of the MNCJO	ment Areas about possible membership/associate	

6. Advocacy

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Goal: effective relationships with NSW and federal governments to ensure the long term sustainability of local government in the region

y partners	Key regional documents	
Regional Leadership Executive NSW Department of Regional NSW NSW Office of Local Government NSW Department of Premier and Cabinet Other Joint Organisations Local Government NSW & Local Government Procurement NSW Local Government Professionals NSW NSW Department of Housing Transport for NSW	 Australian Department of Infrastructure, Transport, Regional Development and Communications Department of Agriculture, Water and Australian Department of Environment and Department of Industry, Science, Energy and Resources Australian Government Department of Social Services 	 North Coast Regional Plan 2036 (being reviewed in 2021/22) Community Strategic Plans prepared by Member Councils Local Strategic Planning Statements prepared be Member Councils RDA Mid North Coast Regional Plan Funding agreements MNCJO Communications Plan

No.	High level actions	Activities	Functional Area
6.1	Improve coverage, speed and connectivity issues in the region	MNCJO to advocate to NBN and phone companies to improve bandwidth, availability and consistency, generally and during natural disasters	Leadership, collaboration

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Funding Profile by Major Projects and Operations

Table 1: Hastings-Macleay Koala Recovery Project

Income 2020/2021	\$	Forecast Expenses 2020/2021	\$
Grant (DPIE)	150,000	Expenditure to	348,856
Port Macquarie & Kempsey Councils	110,000	01/04/21. Governance	35,000
		Habitat Creation PMHC	25,502
		Habitat Creation Kempsey	25,502
		Conservation Partners	28,690
		Koala Karaoke Program	11,250
		Administration	7,500
Income 2021/2022		Forecast Expenses 2021/2022	
Port Macquarie & Kempsey Councils	110,000	Governance (Inc salaries)	100,000
Carry-over income (DPIE Grants, Council Contributions)	432,634	Habitat Creation PMHC	76,507
Koala Hospital	65,000	Habitat Creation Kempsey	76,507
		Conservation Partners	86,070
		Koala Karaoke Program	33,750
		Community Events	5,000
		Administration	7,500
	<u>867,634</u>		<u>867,634</u>

The underspend of the Koala Recovery Project relates to the late start of the project; funding of North Coast Local Land Services, which picked up projects previously assigned to the Koala Recovery Project and the impact of Covid 19 on community engagement. Funding on Habitat Creation and Conservation Partners is proposed (2020/21 and 2021/22) and has the support of the funding body (DPIE). The project is subject to regular reporting to the MNCJO. In addition, there is a separate report to this meeting regarding extending the project.

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Income 2021/2022	\$	Forecast Expenses 2021/2022	\$
Grant (Resilience NSW) -70%	157,944	Simtable	46,665
		Project Officer	70,184
		Workshops	4,752
		Additional Resources	23,760
		Administration	14,715
Income 2022/2023		Forecast Expenses 2022/2023	
Grant (resilience NSW) – 30%	67,691	Project Officer	36,155
		Workshops	2,448
		Resources	12,240
		Administration	14,716
	225.635		225,635

Table 2: Bushfire Community Resilience and Recovery Project

Table 3: Capacity Building (Biodiversity Stewardship) Project

Income 2020/2021	\$	Expenses 2020/2021	\$
Grant (Office of Local	150,000	Consultant reporting on	75,000
Government)		Opportunities and constraints re	
		Biodiversity Stewardship & the	
		Carbon Market	
		Administration	7,500
		Forecast Expenses 2021/2022	
		Project implementation	60,000
		Administration	7,500
	150,000		<u>150,000</u>

Table 4: Increasing Resilience to Climate Change

Income 2020/2021	\$	Expenses 2020/2021	\$
Grant (Local Government NSW) 80% Member Councils (\$6K x 3)		Project Initiation Charles Sturt University Administration	4,200 42,000 5,200
Income 2021/2022		Forecast Expenses 2021/2022	
Grant (Local Government NSW) -20%	14,600	Resilience Projects	30,000
		Project Evaluation	5,000
		Developing a Toolkit	2,500
		Administration	2,100
	<u>91,000</u>		91,000

Table 5: MNCJO Operations

Income 2020/2021	\$	Expenses 2020/2021	\$
Admin contributions from funding bodies:		Expenditure to 01/04/21 Project Manager (31K) Audit (6K)	49,000
Koala Recovery 15,000 Disaster Preparedness 12,918 Biodiversity 7,500 Increasing Resilience 5,200		Accounting (9K)/ Other (3K) Forecast expenditure – 06/21 Project Manager (14K)/ Local Govt Elections (10K)/ Accounting (3K)/ Uni of Newcastle (5K)	32,000
Income 2021/2022		Forecast Expenses 2021/2022	
	04.045		70.000
Admin contributions from	24,315	Audit (10K)/ Project Manager	78,000
funding bodies: Biodiversity 7,500		(45K)/ MNCJO Board (8K) Office Expenses (5K)/ Rebuild website (5K)/ Contingency (5K)	
Increasing Resilience 2,100			

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Simtable 14,715			
Income 2022/2023		Forecast expenses to January 2023	
Admin contributions from	14,716	Project Management	39,000
funding bodies		Contingency	17,054
Simtable (Admin contribution)			
Total contributions from funding bodies	79,649		
Plus: Unexpended funds	135,405	1	
	<u>215.054</u>		215.054

Notes relating to Table 5

Unexpended funds are those remaining from the establishment grant provided by the Office of Local Government. These funds were used to fund the 'Why Local Government Matters' research, the initial Statement of Strategic Priorities and salaries and on costs for the Executive Officer position.

Member Council contributions relating to establishment and the 'Why Local Government' research and establishment costs are fully expended.

Anticipated operational costs for 2020/2021 is expected to be in the order of \$86,000. If current arrangements remain the same, expected operational costs for 2021/2022 are expected to be about \$78,000. On this basis, there is funding available to sustain the JO until January 2023.

Except for audit costs, which are known (\$10K), expenditure detailed for 2021/2022 is estimated on the basis of expenditure realised in 2020/2021. It includes an assumption that funding for on-going project management will still be required.



Attachment 1



Schedule of Fees and Charges 2021-2022



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Schedule of Fees and Charges 2021 – 2022

Pricing Policy

In determining the appropriate fees to be charged for Council services and facilities in 2021-2022, the basic principle applied was that charges should be considered fair and equitable to the general community. Individual members of the community should not be precluded from essential services because of economic circumstances.

The range of services provided by Council to the community is diverse and require different considerations when determining the associated fee or charge. The level of the fee or charge was determined having regard to the following categories (which have been included in this document next to the relevant fee or charge):

Code	Description
А	Economic Cost
	Total cost of providing services for private good.
	Community Service
В	Services considered to have a level of benefit to the community. Generally benefits are not solely
	confined to users. Partially funded by rates.
с	Regulated Charges
Ū	Federal or State Government set charges.
	User Pays Principal
D	Services under this category are such that individual costs can be determined and met by the user
	of the service.
	Market Forces
Е	Services that Council operates in a competitive market and needs to fix charges similar to other
	providers.
F	Cost Plus
•	Services provided on a commercial basis with an amount of risk profit included.
	Developer Contributions - Section 7.11, Section 7.12 & Development Servicing Plans (DSP)
G	Charges are determined in accordance with Council's adopted plans. Charges are indexed
Ũ	quarterly in line with movements in the Consumer Price Index (All Groups, Sydney). Current charges
	are available from Council's website.

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Schedule of Fees and Charges 2021 – 2022

Goods and Services Tax

Introduction

The Goods and Services Tax (GST) passed through legislation on 30 June 1999 and received Royal Ascent on the 8 July 1999. The introduction of the Goods and Services Tax meant that certain fees and charges levied by Port Macquarie - Hastings Council attract GST.

Transparency

Council has gone to great lengths to ensure it has been totally transparent in reporting the applicability of the GST to its fees and charges. This document has been set out in such a way so that you, as the user, will know exactly where the GST applies to fees and charges. Three columns in particular within this document relate to GST:

- Charge (excl. GST): This is the actual fee or charge without GST applied to it.
- GST: If GST is applicable, this column will show the value of the GST to be applied (for one unit) and is rounded to
 the nearest cent. If we are unable to determine the cost of a good or service at the time of printing (eg the cost may
 vary during the year), "calculate" will appear in the fee line and GST will be calculated once the fee is known. If the
 column is blank yet the Fee Amount field contains a fee, it indicates GST is not applicable.
- Charge (incl. GST): This column should only be used for indicative purposes only (refer to "GST Calculations" below). The column displays the Total Charge for one unit with GST included.

GST Calculations

In most cases Council has been able to calculate the GST charge and report it as a dollar figure. This amount is based on one unit. For example, the charge for a copy of a Deposited Plan is \$20.00, with GST of \$2.00, bringing the Charge incl GST to \$22.00.

If Council was unable to calculate the unit charge at the time of printing, the word "calculate" will be shown in the fee line. In this case, GST will be calculated once the Fee Amount is known.

GST Amendments

While the fee amount in this document will not alter without a resolution of Council, followed by a subsequent period on public exhibition, the applicability of the GST to the fees and charges may, depending on the interpretations and rulings as the GST legislation evolves.

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Port Macquarie-Hastings Council

Leadership & Governance

Financial Management

Certificates

Section 603 Certificate	\$85.00	\$0.00	\$85.00	Each			
This is a statutory charge determined yearly by the Office of Local Government							
Urgency fee Section 603 Certificate	\$85.00	\$0.00	\$85.00	Each			
Fee to issue Section 603 Certificate on the spot - in addition to the normal Section 603 certificate fee							

Rate Information

Rate Information – Per hour	\$64.20	\$0.00	\$64.20	Per 1 Hour
Minimum Charge 1 hour				
Copies of Rates & Water Notices – current notice and previous financial year			No Charge	
Copies of Rates & Water Notices – all notices earlier than current notice or previous financial year	\$10.40	\$0.00	\$10.40	Each

Interest Rate on Overdue Rates & Charges

As per the rate determined by the NSW Office of Local Government

Broadwater Special Rate	6%	
General Rates	6%	
Sanctuary Springs Special Rate	6%	

Ordinary Rates

Base amount by Category

Business – Defined Urban Centres	\$626.60	\$0.00	\$626.60	Per Annum
Business – Other	\$526.60	\$0.00	\$526.60	Per Annum
Business – Port Macquarie CBD	\$626.60	\$0.00	\$626.60	Per Annum
Farmland	\$647.30	\$0.00	\$647.30	Per Annum
Residential – Defined Urban Centres	\$626.60	\$0.00	\$626.60	Per Annum
Residential - Other	\$526.60	\$0.00	\$526.60	Per Annum

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Fee GST Fee Unit	
(excl. GST) (incl. GST)	

Rate in \$ by Category

Business – Defined Urban Centres	0.54547c	Per \$ Valuation
Business – Other	0.47540c	Per \$ Valuation
Business – Port Macquarie CBD	2.40857c	Per \$ Valuation
Farmland	0.28078c	Per \$ Valuation
Residential – Defined Urban Centres	0.25172c	Per \$ Valuation
Residential - Other	0.28082c	Per \$ Valuation
Sanctuary Springs Special Rate	0.13382c	Per \$ Valuation
Broadwater Special Rate	0.06277c	Per \$ Valuation

Lease Fees – Council Land

Lease Application

Administration Fee - Not elsewhere indicated - No Report to Council	\$209.09	\$20.91	\$230.00	Each		
Non-refundable						
Can be used for investigations / requests to lease new locations involve lease application fee if lease proceeds.	es research / site	e visits / meetir	ngs. Could be de	educted from		
Administration Fee - Not elsewhere indicated - Report to Council	\$290.91	\$29.09	\$320.00	Each		
Non-Refundable						
Can be used for investigations/requests to lease new locations involves research/site visits/meetings. Could be deducted from Lease application fee if lease proceeds						
Commercial Leases up to 5 Years	\$781.82	\$78.18	\$860.00	Each		
Application and Administration Fee						
Commercial Leases over 5 Years	\$1,072.73	\$107.27	\$1,180.00	Each		
Application and Administration Fee						
Commercial – Renewal or Assignment of Lease Not Requiring Amendment	\$336.36	\$33.64	\$370.00	Each		
Application and Administration Fee						
Community – Application and Administration Fee	\$700.00	\$70.00	\$770.00	Each		
Application and Administration Fee						
Community – Renewal or Assignment of Lease Not Requiring Amendment	\$290.91	\$29.09	\$320.00	Each		
Application and Administration Fee						
Survey Fee			At Cost	Each		
Valuation Fees			At Cost	Each		
Legal Fees			At Cost	Each		
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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Lease Application [continued]

Advertising			At Cost	Each
Bond – Community Lease/Licence	\$500.00	\$0.00	\$500.00	Each
Bond – Commercial Lease – Monthly Rental up to \$5,000	\$2,000.00	\$0.00	\$2,000.00	Each
Bond - Commercial Lease - Monthly Rental \$5,001 and Over	\$5,000.00	\$0.00	\$5,000.00	Each

Lease Rental

Commercial	Set by market valuation			Each
Community	\$545.45	\$54.55	\$600.00	Per Annum
Agistment		Set by ma	arket valuation	Each

Outdoor Dining and Outdoor Trading on Council Roads and Footpaths

Application and Administration Fee	\$400.00	\$0.00	\$400.00	Each
Non-refundable				
Supply, Installation, Removal or Relocation of Markers to Identify the Footway Dining Area.			At Cost	Each
Rental for Outdoor Dining – Port Macquarie CBD – per sqm	\$150.00	\$0.00	\$150.00	Per m2/Annum
Rental for Outdoor Dining – Non Port Macquarie CBD – per sqm	\$65.00	\$0.00	\$65.00	Per m2/Annum
Wauchope, Lake Cathie, Lighthouse Beach, Laurieton, Bonny Hills, etc				
Rental for Outdoor Dining – Villages – per sqm	\$42.00	\$0.00	\$42.00	Per m2/Annum
Telegraph Point, Beechwood, etc				
Footpath Bond	\$500 Bond or one (1) month's rental whichever is the greater			Each

Statutory Property Fees

Property Management

Administration Fee – Sales of land with a value of between \$0 and \$20,000	\$509.09	\$50.91	\$560.00	Each
Administration Fee – Sales of land with a value of between $20,001$ and $100,000$	\$981.82	\$98.18	\$1,080.00	Each
Non-refundable				
Administration Fee - Sale of land with a value of over \$100,000	\$1,463.64	\$146.36	\$1,610.00	Each
Non-refundable				
Based on administration time to co-ordinate, report etc				
Administration Fee – Not elsewhere indicated	\$209.09	\$20.91	\$230.00	Each
No report to Council, Non-refundable				

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit	
Property Management [continued]					
Administration Fee – Not elsewhere indicated	\$300.00	\$30.00	\$330.00	Each	
Report to Council, Non-refundable					
Investigation Fee, Search Fee	\$345.45	\$34.55	\$380.00	Each	
Sale of Council Owned Land			At Cost	Each	
Should the sale be undertaken following a reclassification of the land which was funded by the applicant, no additional investigation and administration fees will be charged. Should Council instigate the sale of the lot, no investigation, administration, valuation, survey or legal fees will be charged. Should the purchaser engage any external parties this will be at the purchaser's cost.					
Survey, Valuation, Legal			At Cost	Each	
Statutory Property Transactions					
Hourly Rate for Statutory Property Transactions and Advice	\$127.27	\$12.73	\$140.00	Per Hour	
Non-refundable					
For transactions including road closures, easements, land transfers, bo fee	oundary adjustm	ents etc where	not covered by	an existing	

Naming & Renaming of Roads and Reserves

Naming / Renaming Council Public Road / Reserve	\$760.00	\$0.00	\$760.00	Each
Application Fee - Non-refundable				
Naming / Renaming of a Crown Reserve	\$950.00	\$0.00	\$950.00	Fach
Numing / Nonaming of a crown Neserve	0000.00	\$0.00	\$000.00	

Road Closing Application Fees

Council Road Closure – Investigation Fee	\$660.00	\$0.00	\$660.00	Each
Non-refundable				
This fee will be deducted from the Administration Fee if road closure pr	oceeds.			
Crown Road Closure Application	\$2,000.00	\$0.00	\$2,000.00	Each
Non-refundable				
Council Road Closure Stage 1 – Administration Fee	\$1,300.00	\$0.00	\$1,300.00	Each
Non-refundable				
Council Road Closure Stage 2 – Administration Fee	\$1,800.00	\$0.00	\$1,800.00	Each
Non-refundable				
Survey, Valuation, Legal			At Cost	Each
Fees to Other Authorities, Government Departments			At Cost	Each
Non-refundable				

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Covenants and Easements				
Extinguishment / Variation to Restrictive Covenant			At Cost	Each
Non-refundable				
Requests for New / Variation to Easements Non-refundable			At Cost	Each
Survey, Valuation, Legal			At Cost	Each
Fees to Other Authorities, Government Departments Non-refundable			At Cost	Each
Land Register				
Land Register Extract – Multiple Properties	\$110.00	\$0.00	\$110.00	Each
Credit Card Surcharge		A 0	0.5%	Per Transactio n
Cheque Dishonour Administration Fee	\$36.00	\$0.00	\$36.00	Each
Direct Debit Dishonour Administration Fee	\$26.00	\$0.00		
	\$51.00	\$0.00	\$26.00 \$51.00	Each
New Owners Administration Fee Rates/Water Overpayment Refund Fee	\$51.00 \$26.00	\$0.00 \$0.00	\$26.00 \$51.00 \$26.00	
New Owners Administration Fee Rates/Water Overpayment Refund Fee Fee for Bank Trace on Unidentified Payment			\$51.00	Each Each
Rates/Water Overpayment Refund Fee Fee for Bank Trace on Unidentified Payment	\$26.00	\$0.00	\$51.00 \$26.00	Each Each Each
Rates/Water Overpayment Refund Fee Fee for Bank Trace on Unidentified Payment Document Sales	\$26.00	\$0.00	\$51.00 \$26.00	Each Each Each
Rates/Water Overpayment Refund Fee Fee for Bank Trace on Unidentified Payment Document Sales Other Documents	\$26.00	\$0.00	\$51.00 \$26.00	Each Each Each
Rates/Water Overpayment Refund Fee Fee for Bank Trace on Unidentified Payment Document Sales Other Documents	\$26.00	\$0.00	\$51.00 \$26.00	Each Each Each
Rates/Water Overpayment Refund Fee Fee for Bank Trace on Unidentified Payment Document Sales Other Documents Refer Appendix 1 Council's Drainage Code	\$26.00 \$35.00	\$0.00 \$0.00	\$51.00 \$26.00 \$35.00	Each Each Each Each
Rates/Water Overpayment Refund Fee Fee for Bank Trace on Unidentified Payment Document Sales Other Documents Refer Appendix 1	\$26.00 \$35.00	\$0.00 \$0.00	\$51.00 \$26.00 \$35.00	Each Each Each Each

Government Information (Public Access) Act 2009 (GIPA)

Subpoena to produce documents

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Access to Records

Personal Records

Application Fee	\$30.00	\$0.00	\$30.00	Each
Processing Charge – hourly rate	\$30.00	\$0.00	\$30.00	Per Hour
after first 20 hours				
Internal Review Fee	\$40.00	\$0.00	\$40.00	Each

Formal Applications – Non Personal Records

Application Fee	\$30.00	\$0.00	\$30.00	Each	
Processing Charge – hourly rate	\$30.00	\$0.00	\$30.00	Per Hour	
First hour free					
A 50% reduction in fees may apply to the processing charge for customers suffering financial hardship or of special benefit to the public generally					
Internal Review Fee	\$40.00	\$0.00	\$40.00	Each	

Informal and all other Applications

Application Fee			No Charge	
Processing Charge – hourly rate	\$30.00	\$0.00	\$30.00	Per Hour
Adjoining Owners				
Application for Adjoining Owners Details	\$17.00	\$0.00	\$17.00	Each
Subpoenas – Conduct Money				

Where Council is not a party to the proceedings

Subpoena to produce documents

Conduct Money	\$69.00	\$0.00	\$69.00	Each
Conduct Money for subpoenas served under seven (7) days	\$138.50	\$0.00	\$138.50	Each
Searching and compiling documents - per 15 mins	\$22.50	\$0.00	\$22.50	1/4 hour
Per 1/4 hour of part thereof. Plus copying, printing, postage or courier charges				

Subpoena to attend court

Conduct money where production required at a Court of tribunal	\$71.00	\$0.00	\$71.00	Each
Staff attendance and travel time - Hourly rate			At Cost	Per Hour
Travel, accommodation and meal expenses			At Cost	

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Digital Technology

Certified Copy

Refer Appendix 1

Certified Copy (for evidence)

Certified Copy of Document, Map or Plan	\$53.00	\$0.00	\$53.00	Each
EP&A Act S.10.8 (2) & EP&A Reg 2000 cl.262				

Printing & Laminating

Refer Appendix 1

Printing

Prints 1-50	\$1.05	\$0.10	\$1.15	Per Print	
Prints 51+	\$0.29	\$0.03	\$0.32	Each	
This refers to each print in excess of 50. First 50 prints are at \$1.00 any additional prints over 50 are at 0.30c					

Collating

A4	\$0.15	\$0.02	\$0.17	Per Sheet
A3	\$0.22	\$0.02	\$0.24	Per Sheet
Stapling	\$0.22	\$0.02	\$0.24	Per Sheet

Comb Binding

1 – 50 pages	\$3.86	\$0.39	\$4.25	Each
51 – 100 pages	\$4.50	\$0.45	\$4.95	Each
101 – 150 pages	\$5.23	\$0.52	\$5.75	Each

Laminating

A4 sheet	\$2.86	\$0.29	\$3.15	Per Sheet
A3 sheet	\$5.64	\$0.56	\$6.20	Per Sheet

		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Your Business and Industry

Economic Development

The Hub Pop-Up Business and Co-Working Space

Co-Working – Full Day (up to 8 hours)	\$30.00	\$3.00	\$33.00	Per Day
Co-Working - Full Time (up to 40 hours)	\$90.00	\$9.00	\$99.00	Per Week
Co-Working Monthly Subscription – Part Time (up to 2 days per week)	\$90.00	\$9.00	\$99.00	per month
Co-Working Monthly Subscription – Full Time (Up to 5 days per week)	\$180.91	\$18.09	\$199.00	per month
Meeting Room Hire	\$17.27	\$1.73	\$19.00	Per Hour
Venue Hire – After Hours Week Days	\$30.00	\$3.00	\$33.00	Per Hour
Venue Hire – After Hours Weekends	\$50.00	\$5.00	\$55.00	Per Hour
Closed Office Hire – Daily	\$90.00	\$9.00	\$99.00	Per Day
Closed Office Hire – Weekly	\$180.91	\$18.09	\$199.00	Per Week
Closed Office Hire – Monthly	\$271.82	\$27.18	\$299.00	Per Month

Tourism Marketing

Co-operative Marketing		POA		
Advertising				
Advertising				
Other Organisations				
Electronic Direct Mail Advertising	\$72.73	\$7.27	\$80.00	
Visitor Guide Advertisement	\$481.82	\$48.18	\$530.00	
Destination Website Promotion Panel	\$740.91	\$74.09	\$815.00	Per Month
Not for P rofit				
Electronic Direct Mail Advertising	\$36.36	\$3.64	\$40.00	
Visitor Guide Advertisement	\$237.27	\$23.73	\$261.00	
Destination Website Promotion Panel	\$369.09	\$36.91	\$406.00	Per Month
Image Library				
Photographic Digital Images	\$33.64	\$3.36	\$37.00	Each
Cost per digital photographic image (for commercial purposes)				
No Charge if not for profit				
Video	\$110.91	\$11.09	\$122.00	Per Module
Module				

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Airport

Port Macquarie Airport

Refer Appendix 2

Regular Public Transport (RPT) Services

Passenger Landing Fee	\$21.00	\$2.10	\$23.10	Per Passenger	
Applies to all RPT aircraft, as well as any charter aircraft, including helic	copters, using the	RPT apron			
Passenger Departure Fee	\$21.00	\$2.10	\$23.10	Per Passenger	
Applies to all RPT aircraft, as well as any charter aircraft, including helic	copters, using the	RPT apron			
Security Screening Fee	PO.	A - per departin	g passenger	POA – per departing passenger	
Security screening charges are charged on a cost recovery basis, apportioned per passenger on Screened Air Services* and					

calculated monthly. Any additional security screening costs that are directly incurred as a result of other air services operating on the RPT apron may be cost recovered from that operator.

Screened Air Services refer to government mandated "screened air service" as defined in the Aviation Transport Security Regulations 2005.

General Aviation (GA) Services

Airport Usage Charge – Practice Approaches			No Charge	Per practice approach
Applies to all aircraft per practice approach				
Exempt from landing charges				
Airport Usage Charge (Annual) – For Commercial Purposes	\$1,233.00	\$123.30	\$1,356.30	Per Annum per Aircraft
Applies to airport based commercial operators with less than five (5) air	craft - refer Appe	ndix 2		
Excludes RPT and freight aircraft operators				
Airport Usage Charge (Annual) – For Commercial Purposes	\$1,233.00	\$123.30	\$1,356.30	Per Annum per Aircraft
Applies to Helicopter / Microlight / Ultralight aircraft operators with less	than five (5) aircr	aft - refer Appe	endix 2	
Hangar lessee will be charged the annual fee for a commercial ultraligh	t, microlight or si	milar, per aircr	aft	
Airport Usage Charge (Annual) – For Private Purposes	\$616.50	\$61.65	\$678.15	Per Annum per Aircraft
Applies to airport based private operators				
Airport Usage Charge (Annual) – For Private Purposes	\$616.50	\$61.65	\$678.15	Per Annum per Aircraft

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
General Aviation (GA) Services [continued]				
Airport Usage Charge (Annual) – For Private Purposes	\$616.50	\$61.65	\$678.15	Per Annum per Aircraft
Applies to Helicopter / Microlight / Ultralight aircraft				
Hangar lessee will be charged the annual fee for a private ultralight, min	crolight or simila	r, per aircraft		
Airport Usage Charge - Weight Based Movement Charge	\$11.82	\$1.18	\$13.00	Per Tonne pro rata
Applies to all aircraft per landing, excluding RPT aircraft and aircraft com Minimum charge \$13.00 (incl. GST) per landing.	vered by the anr	ual usage cha	rges listed abov	e.
Airport Usage Charge – Training Circuits	\$11.82	\$1.18	\$13.00	Per Tonne pro rata
Applies to all aircraft per landing, excluding RPT aircraft and aircraft com Minimum charge of \$13.00 (incl. GST) per landing.	vered by the anr	ual usage cha	rges listed abov	e.
Airport Usage Charge – Training Circuits (out of hours operations – refer Appendix 2)	\$17.73	\$1.77	\$19.50	Per Tonne pro rata
Applies to all aircraft per landing, excluding RPT aircraft and aircraft com Minimum charge \$19.50 (incl. GST) per landing.	vered by the anr	iual usage cha	rges listed abov	e.
Aircraft Parking – RPT Apron	\$62.73	\$6.27	\$69.00	Per hour
Applies to aircraft parked on RPT apron				
RPT aircraft exempt				
Aircraft Parking – GA Apron	\$9.55	\$0.95	\$10.50	Per hour after first 2 hours, per aircraft
Applies to aircraft parked on GA sealed surfaces in defined areas in ac excludes the width of hangar door openings	cordance with co	onditions set ou	ut on adjacent s	gnage and
Non-Operational and Abandoned Aircraft	\$13.18	\$1.32	\$14.50	Per Day
Applies to aircraft without a current Certificate of Airworthiness or current fee applies after first 30 days	nt Maintenance	Release parke	d on any part of	the airport -
Exemption: aircraft that are accommodated within a leased boundary /	premises			
Charitable not for profit organisations providing aeromedical services only		Based	on application	Per Aircraft
Applies to charitable not for profit aeromedical services only on a case-	by-case basis			
Exempt from landing charges based on application				

Airport Meeting Rooms

Meeting Room 1 OR 2 Hire (1/2 Day)	\$90.91	\$9.09	\$100.00	1/2 Day
Includes electricity, A/C, cleaning, tables and chairs Monday to Friday, max 4 hours Max 4 hours				
Meeting Room 1 OR 2 Hire (Full Day)	\$136.36	\$13.64	\$150.00	Full Day
Includes electricity, A/C, cleaning, tables and chairs				

continued on next page ...

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Name Fee GST Fee Unit (excl. GST) (incl. GST)			Year 21/22		
(excl. GST) (incl. GST)	Name	Fee	GST	Fee	Unit
		(excl. GST)		(incl. GST)	

Airport Meeting Rooms [continued]

Meeting Room 1 OR 2 Hire (Full Day)	\$136.36	\$13.64	\$150.00	Full Day
Includes electricity, A/C, cleaning, tables and chairs Monday to Friday, max 8 hours				
8 hours				

Other Miscellaneous Fees

Hire Car Operators (Desk & Car Parking)			Market value	POA – Per Annum
Adjusted in June each year in line with CPI				
Hangar and Ground Lease Rental			Market value	POA – Per M2 per Annum
Determined by valuation.				
Airside Activities Charges	\$60.45	\$6.05	\$66.50	Per Hour, Each
For Airport Safety / Security Officer supervision				
Cost recovery for airside activities				
Lost Gate Key	\$272.73	\$27.27	\$300.00	Each

Airport Parking Fees

Car Parking Charges: 0-30 minutes			FREE	per stay
Car Parking Charges: 30-60 minutes	\$2.73	\$0.27	\$3.00	Per Hour
Terminal Public Car Park				
First 30 mins free				
Car Parking Charges: 1-2 hours	\$5.45	\$0.55	\$6.00	per hour
Car Parking Charges: 2-3 hours	\$8.18	\$0.82	\$9.00	per hour
Car Parking Charges: 3-4 hours	\$10.91	\$1.09	\$12.00	per hour
Car Parking Charges: 4-5 hours	\$13.64	\$1.36	\$15.00	per hour
Car Parking Charges: Full Day (5 to 24 hours from time of entry)	\$13.64	\$1.36	\$15.00	Per Day
Terminal Public Car Park				
Discounted fees apply for longer term parking				
Car Parking Charges: 2 days	\$27.27	\$2.73	\$30.00	per stay
Car Parking Charges: 3 days	\$40.91	\$4.09	\$45.00	per stay
Car Parking Charges: 4 days	\$50.00	\$5.00	\$55.00	per stay
Car Parking Charges: 5 days	\$59.09	\$5.91	\$65.00	per stay
Car Parking Charges: 6 days	\$68.18	\$6.82	\$75.00	per stay
Car Parking Charges: 7 days	\$77.27	\$7.73	\$85.00	per stay
Car Parking Charges: Greater than 7 days	\$85.00 p	olus \$5.00 per	day thereafter	per stay
Lost Parking Ticket	\$77.27	\$7.73	\$85.00	per ticket

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Item 09.05 Attachment 1 Page 34 Name

Unit

Year 21/22 GST

Fee

Fee

Name	(excl. GST)	001	(incl. GST)	
Airport Parking Fees [continued]				
Car Parking Charges – Credit Card Service Fee	2.5% Charge a	applies to pay	ments made by credit card	Each
Environmental Testing				
Environmental Testing				
Water Analysis				
Minimum Invoice/report fee will be \$50 (incl GST). Pric undertaken.	e may vary in acc	ordance wi	th various pro	grams
Activated Sludge Analysis	\$118.91	\$11.89	\$130.80	each
UV Absorbance/UV Transmission	\$13.73	\$1.37	\$15.10	each
Administration Fee	\$38.00	\$3.80	\$41.80	per sampl batch
Extensive Report Fee	\$75.09	\$7.51	\$82.60	per sampl batch
Re-issue of Reports	\$38.00	\$3.80	\$41.80	per samp batch
Sample Preparation Fee For Complex Matrix	\$28.27	\$2.83	\$31.10	each
Urgent Analysis (if possible) Incurs a Surcharge		Price	e on Application	POA
Weekend Analysis Incurs a Surcharge		Price	e on Application	POA
Alkalinity	\$23.45	\$2.35	\$25.80	each
Ammonia (includes filtration charge)	\$23.45	\$2.35	\$25.80	each
Total BOD5	\$66.27	\$6.63	\$72.90	each
Biovolume (literature and or historical values only)	\$28.27	\$2.83	\$31.10	each
Biovolume (literature and or historical + measurement)	\$56.55	\$5.65	\$62.20	each
Chlorophyll – a	\$60.45	\$6.05	\$66.50	each
Colour (apparent)	\$14.64	\$1.46	\$16.10	each
Colour (true)	\$16.55	\$1.65	\$18.20	each
Dissolved Oxygen	\$13.73	\$1.37	\$15.10	each
Electrical Conductivity	\$13.73	\$1.37	\$15.10	each
Enterococci (confirmed)	\$46.73	\$4.67	\$51.40	each
Thermotolerant Coliforms (presumptive)	\$46.73	\$4.67	\$51.40	each
Thermotolerant Coliforms (confirmed)	\$60.45	\$6.05	\$66.50	each
Fluoride	\$28.27	\$2.83	\$31.10	each
Microscopic Identification	\$53.64	\$5.36	\$59.00	each
Nitrite – N (includes filtration charge)	\$23.45	\$2.35	\$25.80	each
Nitrate – N (calculated from NOx and Nitrite)	\$35.09	\$3.51	\$38.60	each
NOx – N (includes filtration charge)	\$35.09	\$3.51	\$38.60	each
Nitrate (if NOx and Nitrite also reported)	\$5.91	\$0.59	\$6.50	each
Total Suspended Solids	\$23.45	\$2.35	\$25.80	each
Filterable Reactive Phosphorus (includes filtration charge)	\$23.45	\$2.35	\$25.80	each
Other Services			By quotation	each
pH	\$13.73	\$1.37	\$15.10	each
		4		

\$17.64

\$1.76

\$19.40

continued on next page ...

Redox

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each

Item 09.05 Attachment 1 Page 35

ATTACHMENT

ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Water Analysis [continued]				
Salinity	\$13.73	\$1.37	\$15.10	each
Sample Collection (water)	\$140.27	\$14.03	\$154.30	per hour
1 person on run				
Sample Collection (gas)	\$113.00	\$11.30	\$124.30	per hour
1 person on run				
Sample Containers		Included	in testing fees	
Total Chlorine	\$20.45	\$2.05	\$22.50	each
Free Chlorine	\$20.45	\$2.05	\$22.50	each
Total Dissolved Solids (Calculation)	\$13.73	\$1.37	\$15.10	each
Total Kjeldahl Nitrogen – N	\$76.00	\$7.60	\$83.60	each
Total Kjeldahl Nitrogen – N (Calculated if TN and NOx also reported)	\$5.91	\$0.59	\$6.50	each
Total Organic Nitrogen – N (Calculated TKN – NH3)	\$28.27	\$2.83	\$31.10	each
Total Phosphorous (persulphate digestion)	\$35.09	\$3.51	\$38.60	each
Total Nitrogen (persulphate digestion)	\$41.00	\$4.10	\$45.10	each
Turbidity	\$13.73	\$1.37	\$15.10	each
Cyanobacteria Identification & Enumeration	\$118.91	\$11.89	\$130.80	each
Potentially Toxic Cyanobacteria Identification & Enumeration	\$79.91	\$7.99	\$87.90	each
Total Phytoplankton Identification & Enumeration	\$172.45	\$17.25	\$189.70	each
Total Coliforms & E. coli (MPN)	\$66.27	\$6.63	\$72.90	each
Chemical Oxygen Demand	\$45.82	\$4.58	\$50.40	each
Heterotrophic Plate Count (HPC)	\$36.18	\$3.62	\$39.80	each

The Glasshouse

Glasshouse Port Macquarie

Refer also to Conditions of Hire (available on website www.glasshouse.org.au)

Refer to Appendix 10

Performance Spaces

Full Theatre Hire (hire per performance or event)	\$354.55	\$35.45	\$390.00	per hour			
As per standard rates as costed or 10% of Gross Box Office taking (whichever is the greater). Includes A/C, cleaning, access to standard in-house lighting, 1 x Venue Supervisor / 1 x Theatre Technician 4 hour minimum hire							
Theatre Hire Stalls Only	\$309.09	\$30.91	\$340.00	per hour			
Includes A/C, cleaning, access to standard in-house lighting, 1 x Venue Supervisor / 1 x Theatre Technician							
4 hour minimum hire							
Theatre Hire Rehearsal Rate (per hour)	\$272.73	\$27.27	\$300.00	per hour			
Includes 1 x Venue Supervisor and 1 x Technician. Excludes access to	auditorium sea	ting and theatre	e is under work	lights only.			

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Performance Spaces [continued]				
Theatre Hire Rehearsal Rate (per hour)	\$272.73	\$27.27	\$300.00	per hour
Includes 1 x Venue Supervisor and 1 x Technician. Excludes access to	auditorium seati	ing and theatre	e is under work	lights only.
4 hour minimum hire				
Studio Hire – Performance/Seminar	\$81.82	\$8.18	\$90.00	per hour
Includes A/C, cleaning, access to standard in-house lighting and sound	l rig, 1 x Venue S	Supervisor		
4 hour minimum hire				
Studio Hire Bare Walls Hire combined with Theatre Hire. Flat rate of \$180, then \$30/hr thereafter	\$163.64	\$16.36	\$180.00	\$180 Flat Rate + \$30 Per Hour
Includes electricity, A/C, cleaning and event support staff. Room set to	client specificatio	ons.		
4 hour minimum hire				
Studio Hire Bare Walls. Flat fee of \$200 per day. Available Monday to Thursday 9.00am – 5.00pm. Not available after hours.	\$181.82	\$18.18	\$200.00	\$200.00 Flat Rate Per Day
Includes electricity, AC, cleaning, work lights.				,
No event staff included (space subject to availability)				
Studio Hire (1/2 Day) – Meeting Only During Business Hours, Monday to Friday 9am – 5.30pm	\$227.27	\$22.73	\$250.00	1/2 Day
Includes electricity, A/C, cleaning, table, chairs. AV Operator pre rig, se	et OR operation a	at additional co	ost.	
1/2 Day (max 4 hours)				
Studio Hire (Full Day) – Meeting Only During Business Hours, Monday to Friday 9am – 5.30pm	\$318.18	\$31.82	\$350.00	Full Day
Includes electricity, A/C, cleaning, table, chairs. AV Operator pre rig, se	et OR operation a	at additional co	ost.	
Full Day (8 hours)				
Glasshouse Podium Hire Package (Charge is Per Hour)	\$54.55	\$5.45	\$60.00	Per Hour
Includes 1 x Venue Supervisor for the duration of the booking. Hire and cables) and access to 240v power (space is subject to availability). Minimum 3 hour hire	l set up of equipr	ment (up to 20	chairs, music s	tands, power
Glasshouse Podium Hire Only (Charge is Per Hire)	\$54.55	\$5.45	\$60.00	\$60.00 Flat Rate Per Hire
No event staff, equipment or power included (space is subject to availa	bility).			
Mezzanine Theatre Presentation Package	\$186.36	\$18.64	\$205.00	Per Hour
Only available in conjunction with hire of the mezzanine foyer. Includes maximum 100 pax in the theatre dress circle. Please Note: additional s				
3 hour minimum hire				

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Conference Spaces				
Meeting Room 1 OR 2 Hire (1/2 Day)	\$136.36	\$13.64	\$150.00	1/2 Day
Includes electricity, A/C, cleaning, tables, chairs				
Max 4 hours				
Meeting Room 1 OR 2 Hire (Full Day)	\$181.82	\$18.18	\$200.00	Full Day
Includes electricity, A/C, cleaning, tables, chairs				
8 hours				
Meeting Room 1 OR 2 Hire – After Hours (0 – 70 people, after business hours access inclusive of 1 staff). After Hours = 5.30pm – 11pm weekdays or 9am – 11pm Saturdays. Staff surcharges apply on Sundays, Public Holidays and after 11pm	\$72.73	\$7.27	\$80.00	Per Hour
Includes electricity, A/C, cleaning, tables, chairs				
minimum 3 hours / price per hour	4007 of	400 TO	\$050.00	4/0 5
Meeting Room 1 and 2 Combined Hire (1/2 Day) Includes electricity, A/C, cleaning, tables, chairs	\$227.27	\$22.73	\$250.00	1/2 Day
Max 4 hours				
Meeting Room 1 and 2 Combined Hire (Full Day) Includes electricity, A/C, cleaning, tables, chairs	\$318.18	\$31.82	\$350.00	Full Day
8 hours				-
Meeting Room 1 and 2 Combined Hire – After Hours (71 – 140 people, after business hours access inclusive of 1 staff). After Hours = 5.30pm – 11pm weekdays or 9am – 11pm Saturdays. Staff surcharges apply on Sundays, Public Holidays and after 11pm	\$90.91	\$9.09	\$100.00	Per Hour
Includes electricity, A/C, cleaning, tables, chairs				
minimum 3 hours / price per hour				
Art Lab Room Hire 1/2 Day (Not available after hours)	\$113.64	\$11.36	\$125.00	1/2 Day
Includes electricity, A/C, cleaning, tables, chairs				
Max 4 hours				
Art Lab Room Hire Full Day (Not available after hours)	\$159.09	\$15.91	\$175.00	Full Day
Includes electricity, A/C, cleaning, tables, chairs				
8 hours				
Boardroom 1 Hire (Level 2) – 1/2 Day	\$100.00	\$10.00	\$110.00	1/2 Day
16 people maximum. Includes electricity, A/C, deaning, tables, chairs				
Max 4 hours				
Boardroom 1 Hire (Level 2) – Full Day	\$145.45	\$14.55	\$160.00	Full Day
16 people maximum. Includes electricity, A/C, deaning, tables, chairs				
8 hours				
Boardroom 2 (Level 2) – 1/2 Day	\$90.91	\$9.09	\$100.00	1/2 Day

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Name	Fee	Year 21/22 GST	Fee	Unit
	(excl. GST)		(incl. GST)	
Conference Spaces [continued]				
10 people maximum. Includes electricity, A/C, cleaning, tables, chairs				
Max 4 hours				
Boardroom 2 (Level 2) – Full Day	\$136.36	\$13.64	\$150.00	Full Day
10 people maximum. Includes electricity, A/C, cleaning, tables, chairs				
8 hours				
Boardroom 3 (Level 2) – Full Day	\$54.55	\$5.45	\$60.00	Full Day
6 people maximum, Includes electricity, A/C, cleaning, tables, chairs, V	VI-FI			
8 hours				
Boardroom 3 (Level 2) – Weekly Hire	\$200.00	\$20.00	\$220.00	Per Week
6 people maximum, Includes electricity, A/C, cleaning, tables, chairs, V	VI-FI			
Monday to Friday, max 8 hours per day				
Regional Gallery Hire (Top Floor OR Ground Floor)	\$90.91	\$9.09	\$100.00	Per Hour
Access to floor space subject to availability and exhibition restrictions - equipment rates	any set up requ	ired will be cha	rged at applicat	le staff and
3 hour minimum hire				
Foyer Hire (for event)	\$90.91	\$9.09	\$100.00	Per Hour
Access to floor space - any set up required will be charged at applicable	e staff and equip	ment rates		
3 hour minimum hire				
Foyer Hire (for set up by Hirer)	\$45.45	\$4.55	\$50.00	Per Hour
Access to floor space by Hirer - any equipment set up required will be o	charged at applie	able staff and	equipment rates	6
1 hour minimum hire				
AV Package (Mezzanine Foyer)	\$309.09	\$30.91	\$340.00	Per Day
Includes hire of a data projector, lectern and microphone OR radio mic (Operator is extra and charged at the published hourly rate IF required)		PA system, L	ED lights, tabs a	nd screen.
Whole of Venue: Includes access to Theatre, Studio, Meeting Room 1 & 2, Small Function Room, Mezzanine Foyer, 3 x staff			By quotation	Per Day
Subject to availability				
Additional dressing room activation	\$181.82	\$18.18	\$200.00	Per Hire/Event
Only available when combined with theatre hire				
Includes electricity, A/C, cleaning				

Staff Rates

Staff working conditions include the following - Minimum 3 hr call, plus staff must be allowed a meal break no later than 5 hours after commencement, all missed meal breaks will incur A/H charges until meal break is taken.

Rates are per hour.

continued on next page ...

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By quotation

By quotation

By quotation

\$50.00

Agreement

Per Day

10% of total sales. Fixed Percentage

\$4.55

Prices as per Glasshouse Venue Hire

\$45.45

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Staff Rates [continued]				
Venue Supervisor / Technician / Duty Manager / Gallery Technician / Editing Technician (Monday – Friday)	\$50.00	\$5.00	\$55.00	Per Hour
Venue Supervisor / Technician / Duty Manager / Gallery Technician (Saturday)	\$52.73	\$5.27	\$58.00	Per Hour
Usher / Tour Guide / Merchandise Seller (Monday – Friday)	\$45.45	\$4.55	\$50.00	Per Hour
Usher / Tour Guide / Merchandise Seller (Saturday)	\$48.18	\$4.82	\$53.00	Per Hour
Venue Supervisor / Technician / Duty Manager / Gallery Technician (Sunday)	\$61.82	\$6.18	\$68.00	Per Hour
Usher / Tour Guide / Merchandise Seller (Sunday)	\$57.27	\$5.73	\$63.00	Per Hour
Venue Supervisor / Technician / Duty Manager / Gallery Technician (Public Holiday)	\$90.91	\$9.09	\$100.00	Per Hour
Usher / Tour Guide / Merchandise Seller (Public Holiday)	\$90.91	\$9.09	\$100.00	Per Hour
Miscellaneous Fees				
Function Package			By quotation	
Customised Package				

Only available during business hours: 8.30 am to 5.30 pm, catering as per set menu, minimum numbers apply, contact Glasshouse for further information.

1/2 Day

Marketing Packages

Merchandising

Wedding Ceremony Packages

Technical Specifications Late Fee

Venue Hire Cancellation Fees

Business Sense Packages

Photography Packages (access to non-public venues)

Commission on any goods sold within the premises

Includes Meeting Room 1 & 2 combined, technical assistance on arrival, morning OR afternoon tea, Glasshouse working lunch, free wi-fi, whiteboard, data projector, lectern & microphone, water & mints, room set to clients requirements.

Business Sense Package (minimum 20 delegates)	\$40.00	\$4.00	\$44.00	Per Person
Includes: conference from hire set to clients requirements, technical as and microphone, whiteboard, water and mints on tables. Catering included: arrival tea and coffee, morning tea, working lunch w		, , ,	tor, laptop, Wi-l	Fi, lectern

Full Day

Includes Meeting Room 1 & 2 combined, technical assistance on arrival, morning tea, afternoon tea, Glasshouse working lunch, free wi-fi, whiteboard, data projector, lectern & microphone, water & mints, room set to clients requirements.

continued on next page ...

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Item 09.05 Attachment 1

		Year 21/22			
Name	Fee (excl. GST)	GST	Fee (incl. GST)	Unit	
Full Day [continued]					

Business Sense Package (minimum 20 delegates)	\$43.64	\$4.36	\$48.00	Per Person
Includes: conference from hire set to clients requirements, technical as and microphone, whiteboard, water and mints on tables. Catering included: arrival tea and coffee, morning tea OR afternoon tea		, , ,	, , ,,	Fi, lectern

Live Stream Packages

Specialised Web Streaming & Recording Technology for Online/Live Video Conferencing. Supported by GoTo Meetings.

Package A	\$227.27	\$22.73	\$250.00	Per Day	
Includes equipment, set up/down, testing, data allowance and livestream platform. Technician to operate is an additional charge at published hourly rate.					
Package B			By quotation		

Venue Set Up Items

Theatre PA	\$109.09	\$10.91	\$120.00	Per Hire
Dance Floor (Tarkett) Theatre	\$181.82	\$18.18	\$200.00	Per Hire
Dance Floor (Tarkett) Studio	\$70.00	\$7.00	\$77.00	Per Hire
Theatre Forestage / Pit	\$350.00	\$35.00	\$385.00	Per Hire
Radio Microphone – Hand held OR lapel	\$50.00	\$5.00	\$55.00	Per Day
Radio Microphone – Headset (Radio Mic + DPA Microphone)	\$90.91	\$9.09	\$100.00	Per Day
Data Projector – Meeting Rooms & Studio	\$136.36	\$13.64	\$150.00	Per Day
Data Projector + Screen – Theatre	\$281.82	\$28.18	\$310.00	Per Day
Small PA	\$72.73	\$7.27	\$80.00	Per Day
Corded Microphone & Speaker				
Medium PA	\$118.18	\$11.82	\$130.00	Per Day
Includes corded microphone (and lectern where required), CD player,	mixing desk, left	& right speake	rs and small st	age
Large PA	\$181.82	\$18.18	\$200.00	Per Day
Includes 1 x radio microphone, 1 x corded microphone (and lectern wh speakers, 2 foldback sends. Requires operator at relevant hourly staff		CD players, m	ixing desk, left	& right
Colour Filters for Lighting	\$20.00	\$2.00	\$22.00	Per 1/2 Sheet
Non standard lighting rig				
Lectern with Microphone (Wired)	\$50.00	\$5.00	\$55.00	Per Day
Screen (8ft x 12ft)	\$90.91	\$9.09	\$100.00	Per Day
Other Miscellaneous Technical or Event Items			By negotiation	
Subject to equipment & availability				
Arcus Picture Hanging System	\$181.82	\$18.18	\$200.00	Por Hiro

Arcus Picture Hanging System	\$181.82	\$18.18	\$200.00	Per Hire
Preview Monitor	\$18.18	\$1.82	\$20.00	Per Day
White Board			No charge	
Laser Pointer	\$10.00	\$1.00	\$11.00	Per Hire

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Venue Set Up Items [continued]				
Presentation Mouse Orchestral Chairs 40 available	\$15.00	\$1.50	\$16.50 No charge	Per Hire
Music Stands 50 available (short & tall)			No charge	
Grand Piano – Yamaha C7 Does not include Tuning	\$254.55	\$25.45	\$280.00	Per Hire
Digital Piano – Clarinova Baby Grand Piano Tuning	\$136.36 \$209.09	\$13.64 \$20.91	\$150.00 \$230.00	Per Hire Per Request
Rostrum / Risers 10 units available for hire	\$9.09	\$0.91	\$10.00	Per Unit Per Hire
Mirror Ball Smoke / Haze Machine Round Table Cloths	\$50.00 \$68.18 \$6.36	\$5.00 \$6.82 \$0.64	\$55.00 \$75.00 \$7.00	Per Hire Per Hire Each
Rectangle Table Cloths Laptop 2 units available for hire (running Windows 7)	\$5.91 \$90.91	\$0.59 \$9.09	\$6.50 \$100.00	Each Per Unit Per Day
Apple MAC 2 units available for hire	\$90.91	\$9.09	\$100.00	Per Unit Per Day
Modem	\$45.45	\$4.55	\$50.00	Per Modem Per Day
Light It Up Package 240v Power (Podium) Kiosk (self service) power only available for events located on the Glass	\$88.18 \$38.18 shouse Podium	\$8.82 \$3.82	\$97.00 \$42.00	Per Day
3-Phase Power (Loading Dock) Requires Technician to provide access. All leads must be tested and ta	\$76.36 agged.	\$7.64	\$84.00	Per Day

Video Recording Packages

Event/Show Recording for Full Show Quality	\$318.18	\$31.82	\$350.00	Per Event/Sho w		
2 x cameras, audio feed, no editing. This service requires an operator for the show call which will be an additional charge at the published hourly rate.						
Staff charges would be per day for the duration of the event if recording required on multiple days.						

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Transaction Fees				
Glasshouse Membership Program Fee – Adult	\$44.55	\$4.45	\$49.00	Each
Glasshouse Membership Program Fee – Adult Double	\$71.82	\$7.18	\$79.00	Per Couple
Glasshouse Membership Program Fee – Concession/Under 21/Student	\$40.00	\$4.00	\$44.00	Each
Proof of age/concession required				
Internet Transaction Fee	\$3.18	\$0.32	\$3.50	Per Transactio n
Inclusive of postage fee when tickets are purchased more than 14 days	s prior to the eve	ent.		
Counter Transaction Fee	\$2.73	\$0.27	\$3.00	Per Transactio n
Telephone Transaction Fee	\$5.00	\$0.50	\$5.50	Per Transactio n
Inclusive of postage fees when tickets are purchased more than 14 day	ys prior to the ev	ent.		
Member Transaction Fee		Charge - Availa e members (e:		Per Transactio n
Ticket Re-issue Fee (non-members)	\$0.91	\$0.09	\$1.00	Per Ticket
Ticket Re-issue Fee (members)			No Charge	Per Ticket
Exchange Fee (non-members)	\$0.91	\$0.09	\$1.00	Per Ticket
Exchange Fee (members)			No Charge	Per Ticket
Administration Fee	\$4.55	\$0.45	\$5.00	Per Transactio n
Membership Processing Fee			No charge	Per Transactio n
Applicable for counter and telephone membership sales. Online membership sales incur standard internet transaction fees				
Donations Processing Fee			No charge	Per Transactio n
Booking Fees				
Ticket Price \$0.01 - \$25.00	\$2.27	\$0.23	\$2.50	Per Ticket Printed
Ticket Price \$25.01 - \$50.00	\$3.18	\$0.32	\$3.50	Per Ticket Printed
Ticket Price \$50.01 – \$75.00	\$4.09	\$0.41	\$4.50	Per Ticket Printed
Ticket Price \$75.01 – \$100.00	\$5.00	\$0.50	\$5.50	Per Ticket Printed
Ticket Price \$100.01 +			on - From \$6.00	POA
Cancellation Fee Ticket Price \$0.01 - \$25.00	\$2.27	\$0.23	\$2.50	Per Ticket
Cancellation Fee Ticket Price \$25.01 – \$50.00	\$3.18	\$0.32	\$3.50	Per Ticket

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Booking Fees [continued]				
Cancellation Fee Ticket Price \$50.01 – \$75.00	\$4.09	\$0.41	\$4.50	Per Ticket
Cancellation Fee Ticket Price \$75.01 – \$100.00	\$5.00	\$0.50	\$5.50	Per Ticket
Cancellation Fee Ticket Price \$100.01 +	Price	e on applicatio	on - From \$6.00	POA
Complimentary Tickets	\$0.91	\$0.09	\$1.00	Per Ticket
Bulk Ticket Print	\$1.36	\$0.14	\$1.50	Per Ticket
Returned Bulk Tickets	\$0.45	\$0.05	\$0.50	Per Returned Ticket
External Ticketing Event Set-up Fee	\$54.55	\$5.45	\$60.00	Per Booking
This will be redeemable to the promoter after the sale of 25 tickets.				
Ticketing Event Amendment Fee	\$54.55	\$5.45	\$60.00	Per Hour
Relates to changes in event ticketing requirements post on sale date				
Transport Tickets	\$9.09	\$0.91	\$10.00	Per Booking
Maximum of 6 tickets				5
Miscellaneous Ticket / Merchandise Packages		Price	on Application	Per Ticket
Event and Conference Registration		Price	on Application	Per Delegate
Visitor Information Centre Miscellaneous				
Glasshouse Brochure Display Program	\$227.27	\$22.73	\$250.00	Each
Accommodation				
Booking Commission			10% of booking	Per Booking
Cancellation – Within 7 days prior to arrival			Non refundable	Per Booking
Tour Bookings				
Booking Commission			10% of booking	Each
Cancellation – Within 7 days prior to departure – unless tour is cancelled by the operator			Non refundable	Per Booking
Strategic Landuse				

Development Control Plans

Where Council has to prepare or assess

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Development Control Plans [continued]				
Preparation and / or review of a DCP	Fee determine	d by quotation	n based on full recovery	Estimate
Full cost recovery of external consultant fees and / or Council co-ordina	ation / review cos	ts		

Local Environment Plans

Where Council has to prepare or assess. Note: LEP amendments will be prioritised in accordance with Council policy and adopted strategy. There is no guarantee of the outcomes from the LEP process and fees are non-refundable.

Administrative LEP Amendments

E.g. Mapping anomalies and corrections

No Fee	No Charge	

Minor LEP Amendments and Reclassifications

No significant support studies for Council review

Minor LEP amendments and reclassifications	\$12,930.00	\$0.00	\$12,930.00	Each
50% refundable if proposal is not supported by Council or the Gateway	Review Panel			

Other LEP Amendments

E.g. where a significant support study is required

Stage 1: Lodgement and review	\$16,160.00	\$0.00	\$16,160.00	Each
1) To be paid at the time of lodgement, 2) Charged for each party or gr proposal, 3) \$1,000 discount on fee if pre-lodgement has been held in	oup of parties repr relation to the prop	esented sepa osal	arately in the pla	anning
Stage 2: Exhibition and finalisation	\$10,770.00	\$0.00	\$10,770.00	Each
Stage 2. Exhibition and indisation	\$10,770.00	Φ0.00	\$10,770.00	Each

Council co-ordination of specialist study preparation or peer review

External consultant fees and Council co-ordination costs	Fee determined by quotation based on full recovery	Estimate
Public Hearing		
Where required or requested		
Full cost recovery	Fee determined by quotation based on full recovery	Each
	locovory	

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Deferred Developer Contributions

Development contributions are levied under various Contribution Plans in accordance with s7.11 of the *Environmental Planning & Assessment Act 1979.* Details of current development servicing plans and charges are available from Council's website or by contacting Council's Development Contributions Section on 6581 8686.

Development Contributions are indexed quarterly in line with movements in the CPI.

Preparation of Contributions Deferral Deed (Standard)	\$500.00	\$50.00	\$550.00	per Deed
Variation of a Contributions Deferral Deed or associated dealings including a review of Caveator's Consent or a request to vary the deferral deed template.	Fee determine Recov		based on full charge \$250<	per Deed
Withdrawal of caveat associated with Contributions Deferral Deeds per lot created in the subdivision (Excluding lots to be dedicated to Council). Paid at the time of preparation of the Contributions Deferral Deed.	\$90.91	\$9.09	\$100.00	per lot

Works-in-Kind Agreements

Development contributions are levied under various Contributions Plans in accordance with s7.11 of the *Environmental Planning & Assessment Act 1979*. Works-in-Kind (WIK) Agreements are used to facilitate the delivery of works as part of a Contribution Plan or of Material Public Benefit. Details of current development servicing plans and charges are available from Council's website or by contacting Council's Development Contributions Section on 6581 8686.

Preparation of a Standard Works-in-Kind Agreement	\$500.00	\$50.00	\$550.00	Per Agreement
Preparation of Non-Standard Works-in-Kind Agreement or a Request to Vary the Standard Agreement		nined by quota very. Minimum		Per Agreement

Section 64 Development Servicing Plan Fees & Charges

Water Supply & Sewerage Headworks charges are levied under Development Servicing Plans prepared under the provisions of the *Water Management Act (NSW) 2000* pursuant to Section 64 of the *Local Government Act (NSW) 1993*.

Notes: Equivalent Tenement (ET) is defined as per unit cost for a standard residential 450 sqm to 2,000 sqm lot. Fees & Charges defined as of 1 February 2021.

Development Servicing Plan for Water and Supply and Sewerage 2014

Water Supply Headworks Charge	\$10,375.00	\$0.00	\$10,375.00	Per ET
Sewerage Services Headworks Charge	\$3,936.00	\$0.00	\$3,936.00	Per ET

s7.11 Development Contributions Fees & Charges

Contribution charges are levied under Council's Contribution Plans prepared under the provisions of Section 7.11 of the *Environmental Planning and Assessment Act* 1979.

Notes: Equivalent Tenement (ET) is defined as per unit cost for a standard residential 450 sqm to 2,000 sqm lot. Fees & Charges defined as of 1 February 2021.

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Port Macquarie-Hastings Administration Building Contributions Plan 2007

All Areas	\$926.00	\$0.00	\$926.00	Per ET

Port Macquarie-Hastings Administration Levy Contribution Plan

All Areas	2.20%	% of s7.11 charges
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Port Macquarie-Hastings Community, Cultural and Emergency Services Facilities 2005

Port Macquarie	\$4,705.00	\$0.00	\$4,705.00	Per ET
Innes Peninsula	\$4,705.00	\$0.00	\$4,705.00	Per ET
Sancrox Thrumster	\$5,311.00	\$0.00	\$5,311.00	Per ET
Wauchope	\$4,819.00	\$0.00	\$4,819.00	Per ET
Kings Creek	\$4,819.00	\$0.00	\$4,819.00	Per ET
Lake Cathie / Bonny Hills	\$5,357.00	\$0.00	\$5,357.00	Per ET
Camden Haven	\$4,530.00	\$0.00	\$4,530.00	Per ET
Rural (Not included in other areas)	\$4,530.00	\$0.00	\$4,530.00	Per ET
Bushfire (All Areas)	\$514.00	\$0.00	\$514.00	Per ET

Kings Creek Contribution Plan Version 2.5

Kings Creek Area	\$9,553.00	\$0.00	\$9,553.00	Per ET	

Port Macquarie-Hastings Major Roads Contributions Plan

Port Macquarie	\$7,777.00	\$0.00	\$7,777.00	Per ET
Innes Peninsula (Includes Innes Peninsula Contribution Plan)	\$9,581.00	\$0.00	\$9,581.00	Per ET
Sancrox Thrumster	\$8,259.00	\$0.00	\$8,259.00	Per ET
Thrumster Area 13 (Includes Area 13, 14 & 15 Local Roads Plan)	\$14,024.00	\$0.00	\$14,024.00	Per ET
Camden Haven	\$8,601.00	\$0.00	\$8,601.00	Per ET
Camden Haven Area 15 (Includes Area 13, 14 & 15 Local Roads Plan)	\$16,258.00	\$0.00	\$16,258.00	Per ET
Lake Cathie/Bonny Hills	\$5,982.00	\$0.00	\$5,982.00	Per ET
Lake Cathie/Bonny Hills Area 14 (Includes Area 13, 14 & 15 Local Roads Plan)	\$13,491.00	\$0.00	\$13,491.00	Per ET
Wauchope	\$7,268.00	\$0.00	\$7,268.00	Per ET
Remaining Rural Areas	\$5,873.00	\$0.00	\$5,873.00	Per ET
Regional Component	\$5,873.00	\$0.00	\$5,873.00	Per ET

Port Macquarie-Hastings Open Space Contributions Plan 2018

Port Macquarie	\$5,730.00	\$0.00	\$5,730.00	Per ET
Innes Peninsula	\$6,409.00	\$0.00	\$6,409.00	Per ET
Sancrox Thrumster	\$6,784.00	\$0.00	\$6,784.00	Per ET

continued on next page ...

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Item 09.05 Attachment 1

		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Port Macquarie-Hastings Open Space Contributions Plan 2018 [continued]

Camden Haven	\$6,112.00	\$0.00	\$6,112.00	Per ET
Lake Cathie/Bonny Hills	\$6,861.00	\$0.00	\$6,861.00	Per ET
Wauchope	\$5,331.00	\$0.00	\$5,331.00	Per ET
Yippin Creek (Hastings S.94 Open Space Contribuitions Plan – Version 2.2 applies)	\$5,078.00	\$0.00	\$5,078.00	Per ET
Remaining Rural Areas	\$4,930.00	\$0.00	\$4,930.00	Per ET
Regional Component	\$4,930.00	\$0.00	\$4,930.00	Per ET

Port Macquarie-Hastings Contributions Plan 1993 (Part C – Car Parking)

Port Macquarie	\$23,176.00	\$0.00	\$23,176.00	Per Space
Gordon Street	\$9,897.00	\$0.00	\$9,897.00	Per Space
Wauchope	\$6,484.00	\$0.00	\$6,484.00	Per Space
Laurieton	\$9,133.00	\$0.00	\$9,133.00	Per Space
North Haven	\$10,839.00	\$0.00	\$10,839.00	Per Space

North Haven Neighbourhood Business Precinct Contribution Plan V1.2

North Haven	\$100.06	\$0.00	\$100.06	Per m2

Hastings River Drive Contribution Plan for Traffic Facility Works V1.2

Hastings River Drive3(s) Area	\$24.69	\$0.00	\$24.69	Per sqm developed for use
Hastings River Drive4(t) Area	\$7.61	\$0.00	\$7.61	Per sqm developed for use

The Bain Dairy Contributions Plan V1.4

\$2,440.00	\$0.00	\$2,440.00	Per ET
\$3,708.00	\$0.00	\$3,708.00	Per ET
ns Plan			
\$501.00	\$0.00	\$501.00	Per Trip
ibutions Plan	Areas 13	14 and 15 2	2014
	\$3,708.00 IS Plan \$501.00	\$3,708.00 \$0.00 Is Plan \$501.00 \$0.00	\$3,708.00 \$0.00 \$3,708.00

Thrumster (Area 13)	\$8,151.00	\$0.00	\$8,151.00	Per ET
Lake Cathie/Bonny Hills (Area 14)	\$7,618.00	\$0.00	\$7,618.00	Per ET
Camden Haven (Area 15)	\$10,385.00	\$0.00	\$10,385.00	Per ET

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

s7.12 Development Contributions Fees & Charges

Contribution charges are levied under Council's Contribution Plans prepared under the provisions of Section 7.12 of the *Environmental Planning and Assessment Act* 1979.

Notes: Fees & Charges defined as of 1 February 2021.

Port Maquarie-Hastings Council Section 94A Levy Contributions Plan 2007

Cost of works up to and including \$100,000	Nil	% of Cost
Cost of works more than \$100,000 and up to and including \$200,000	0.50%	% of Cost
Cost of works more than \$200,000	1.00%	% of Cost

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Your Community Life

Community Participation

Halls/Community Centres

Alderman CC "Mac" Adams Music Centre

Miscellaneous

Hastings Municipal Band	\$860.00	\$86.00	\$946.00	Per Annum
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	

Main Hall

Commercial & Other	\$20.91	\$2.09	\$23.00	Per Hour
Private Functions			Not Available	
Not for Profit	\$14.55	\$1.45	\$16.00	Per Hour

Practice Rooms

Commercial & Other	\$13.64	\$1.36	\$15.00	Per Hour
Private Functions			Not Available	
Not for Profit	\$8.18	\$0.82	\$9.00	Per Hour

Beechwood School of Arts

Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.00	\$1.00	\$11.00	Per Hour
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	

Bonny Hills Community Hall

Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.91	\$1.09	\$12.00	Per Hour
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election

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ORDINARY COUNCIL 16/06/2021

Name	Fee	Year 21/22 GST	Fee	Unit
	(excl. GST)		(incl. GST)	
Ponny Hills Community Hall [configured]				
Bonny Hills Community Hall [continued]				
Council Activities & Elections			No Charge	
Dunbogan Jubilee Hall				
Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.91	\$1.09	\$12.00	Per Hour
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	LIECTON
			Ū	
Emerald Downs Community Centre				
-				
Hall				
Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$11.82	\$1.18	\$13.00	Per Hour
Meeting Room				
Alcohol Not Permitted		A 4 55	* 47.00	D 11
Commercial & Other	\$15.45	\$1.55	\$17.00	Per Hour
Private Functions	\$11.82	\$1.18	\$13.00	Per Hour
Not for Profit	\$9.09	\$0.91	\$10.00	Per Hour
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Lake Cathie Community Hall				
Hall				
Commercial & Other	\$22.73	\$2.27	\$25.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.91	\$1.09	\$12.00	Per Hour
Maating Doom				
Meeting Room				
Commercial & Other	\$18.18	\$1.82	\$20.00	Per Hour
Private Functions	\$13.64	\$1.36	\$15.00	Per Hour
Not for Profit	\$9.09	\$0.91	\$10.00	Per Hour

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Other Fees				
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	
Laurieton School of Arts Hall Hall				
Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.91	\$1.09	\$12.00	Per Hour
Other Fees				
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	
Lorne Recreation Centre				
Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.00	\$1.00	\$11.00	Per Hour
Other Fees				
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	
North Haven Community Hall Hall				
Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$21.02	\$2.10	\$24.00	Per Hour
Not for Profit	\$10.91	\$1.09	\$19.00	Per Hour
Hottor From	ψ10.01	ψ1.00	ψ12.00	1 of Hour

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Other Fees				
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	
Pappinbarra Recreation Reserve				
Tennis Court & Facilities				
Commercial & Other	\$11.82	\$1.18	\$13.00	Per Hour
Private Functions	\$7.27	\$0.73	\$8.00	Per Hour
Not for Profit	\$7.27	\$0.73	\$8.00	Per Hour
General Bond	\$219.00	\$0.00	\$219.00	Each
Pembrooke Hall Hall Commercial & Other	¢01.00	ድጋ 10	¢24.00	Dor Hour
Commercial & Other Private Functions	\$21.82 \$17.27	\$2.18	\$24.00 \$10.00	Per Hour Per Hour
Not for Profit	\$10.00	\$1.73 \$1.00	\$19.00 \$11.00	Per Hour
NOTOFFOR	\$10.00	Q1.00	ψ11.00	1 of Flour
Other Fees				
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	
Port Macquarie Seniors' Facility				
Les Crisp Auditorium				
Commercial & Other	\$32.73	\$3.27	\$36.00	Per Hour
Private Functions	\$24.55	\$2.45	\$27.00	Per Hour
Not for Profit	\$18.18	\$1.82	\$20.00	Per Hour
The Pioneer Room				
Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$11.82	\$1.18	\$13.00	Per Hour

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Marjorie "Nikki" Adams Lounge				
Commercial & Other	\$17.27	\$1.73	\$19.00	Per Hour
Private Functions			Not Available	Per Hour
Not for Profit	\$10.00	\$1.00	\$11.00	Per Hour
Other Fees				
Use of Tea Making Facilities	\$9.09	\$0.91	\$10.00	Per Function
Kitchen (use of facilities)	\$17.27	\$1.73	\$19.00	Per Function
Stove, fridge, bain-marie				
PMQ Senior's Club	\$1,063.64	\$106.36	\$1,170.00	Per Month
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	Liocaon
Hall Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.91	\$1.09	\$12.00	Per Hour
Other Fees				
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	
Wauchope Rotary Youth Centre				
Hall				
Commercial & Other	\$21.82	\$2.18	\$24.00	Per Hour
Private Functions	\$17.27	\$1.73	\$19.00	Per Hour
Not for Profit	\$10.00	\$1.00	\$11.00	Per Hour
Other Fees				
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Other Fees [continued]

Council Activities & Elections	No Charge	

Historic Court House

Courtroom

Other	\$30.91	\$3.09	\$34.00	Per Hour
Commercial not permitted				
Private Functions	\$23.64	\$2.36	\$26.00	Per Hour
Not for Profit	\$10.91	\$1.09	\$12.00	Per Hour
General Admission – Adult	\$3.64	\$0.36	\$4.00	Each
General Admission – Pensioners and School Children (including School Excursions)	\$2.73	\$0.27	\$3.00	Each

Hire of Grounds

Other	\$300.00	\$30.00	\$330.00	Per Day
Commercial not permitted				
Private Functions	\$199.09	\$19.91	\$219.00	Per Day
Not for Profit	\$110.00	\$11.00	\$121.00	Per Day
General Bond	\$219.00	\$0.00	\$219.00	Each
State & Federal Elections	\$319.09	\$31.91	\$351.00	Per Election
Council Activities & Elections			No Charge	

Library

Hire of Rooms

Access Fee – Outside Library Hours

Applies to Both Rooms	Cost recove	ery - set by see	curity company	Each
Meeting Room				
Commercial Organisations	\$18.18	\$1.82	\$20.00	Per Hour
Hastings Schools & Community Organisations	\$9.09	\$0.91	\$10.00	Per Hour
Library / Council			No Charge	Each
Technology Training Room				
Commercial Organisations	\$36.36	\$3.64	\$40.00	Per Hour
Hastings Schools & Community Organisations	\$13.64	\$1.36	\$15.00	Per Hour
Hasungs Schools & Community Organisations	\$13.04	\$1.30	\$15.00	Per Hour

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

	_			
Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Technology Training Room [continued]				
Library / Council			No Charge	
Hire of Both Rooms				
Commercial Organisations	\$54.55	\$5.45	\$60.00	Per Hour
Hastings Schools & Community Organisations	\$18.18	\$1.82	\$20.00	Per Hour
Library / Council			No Charge	
Other Library Charges				
Computer Bookings				
Library Members			No Charge	Per Hour
Visitors			No Charge	Per Hour
Photocopying Coin Slot Operation				
Colour Copies	\$0.91	\$0.09	\$1.00	Per Page
A4 & B4 page	\$0.18	\$0.02	\$0.20	Per Page
A3 page	\$0.45	\$0.05	\$0.50	Per Page
Other				
3D Printer – Prints	\$0.91	\$0.09	\$1.00	per hour
Canvas Library Bags	\$1.82	\$0.18	\$2.00	each
Inter-Library Loans – Application	\$13.64	\$1.36	\$15.00	Each
Payable on application				
Inter-Library Loans – Urgent		Cost Rec	overy + \$12.00	Each
Sent within 24 hours				
Fine at Account Stage	\$0.00	\$0.00	\$0.00	Per Item
After 2nd notice				
Visitors Charges – Joining Fee – Adults/Children	\$3.00	\$0.00	\$3.00	Each
Visitors Charges – Temporary Membership (20 items)	\$10.00	\$0.00	\$10.00	Each
Lost or Damaged Books Beyond Repair		(Cost of the Item	Each
Replacement of Lost Cards	\$2.60	\$0.00	\$2.60	Each
Sale of Flash Drives	\$9.09	\$0.91	\$10.00	Each
Headphones	\$3.64	\$0.36	\$4.00	Each
Commercial Reference Service	\$72.73	\$7.27	\$80.00	Per Hour or Part Thereof

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Sports & Recreation

Beach & Boating

Beach Driving Permit – 4WD Vehicles – Visitor	\$34.00	\$0.00	\$34.00	Each
Beach Driving Permit Yearly – 4WD Vehicles	\$68.00	\$0.00	\$68.00	Per Annum
Beach Driving Permit – 4WD Vehicles – Pensioner	\$34.00	\$0.00	\$34.00	Per Annum
Use of Wharves by Commercial Operators	\$18.18	\$1.82	\$20.00	Per Day

Commercial Activities on Council Managed Land

Parks, Reserves, Sports Fields and Beaches

New Application Fee	\$160.00	\$0.00	\$160.00	Per Annum
Application Renewal Fee	\$85.00	\$0.00	\$85.00	Per Annum
Temporary Licence (up to 2 nominated locations)	\$500.00	\$0.00	\$500.00	Per Annum
Temporary Licence (per additional location)	\$120.00	\$0.00	\$120.00	Per Additional Location
Applies if the applicant does not pay a business rate directly related to	the Temporary L	icence activity	,	
Temporary Licence (per additional location)	\$60.00	\$0.00	\$60.00	Per Additional Location
Applies if the applicant pays a business rate directly related to the Tem	norary Licence a	activity		

Applies if the applicant pays a business rate directly related to the Temporary Licence activity

Sporting Complexes

Camden Haven District Sport & Recreation Management Committee

Ground Levy

Little Athletics	\$110.91	\$11.09	\$122.00	Each
Bonny Hills Cricket Club	\$213.64	\$21.36	\$235.00	Each
Camden Haven Cricket	\$213.64	\$21.36	\$235.00	Each
Camden Haven Bombers Junior AFL Club	\$213.64	\$21.36	\$235.00	Each
Netball	\$145.45	\$14.55	\$160.00	Each
Junior Rugby League	\$213.64	\$21.36	\$235.00	Each
Soccer	\$213.64	\$21.36	\$235.00	Each
Tennis	\$145.45	\$14.55	\$160.00	Each
Touch Football	\$213.64	\$21.36	\$235.00	Each

Lights

Soccer Field (each pole)	\$5.55	\$0.55	\$6.10	Per Hour
continued on next page			I	Page 44 of 140

ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Lights [continued]				
Support Field (each pole)	\$5.55	\$0.55	\$6.10	Per Hour
Oval	\$23.64	\$2.36	\$26.00	Per Hour
Canteen Fee				
Outside groups	\$25.45	\$2.55	\$28.00	Per Day
Plus Bond				
Outside groups – Bond	\$65.00	\$0.00	\$65.00	Each
Refundable if left clean and undamaged				
Casual users – Per Day	\$7.27	\$0.73	\$8.00	Per Day
Casual users – Per Season	\$59.09	\$5.91	\$65.00	Per Season
Lank Bain Sporting Complex				
Group 2 Competition Matches	\$1,100.00	\$110.00	\$1,210.00	Per Year
Hastings League Club Use – Field 1	\$663.64	\$66.36	\$730.00	Per Year
Hastings League Club Use – Field 2	\$381.82	\$38.18	\$420.00	Per Year
High School Use	\$281.82	\$28.18	\$310.00	Per Year
Primary School Use	\$281.82	\$28.18	\$310.00	Per Year
Cricket Club Use	\$281.82	\$28.18	\$310.00	Per Year
Hastings League Controlled Fixtures	\$110.00	\$11.00	\$121.00	Per Day
Junior Cricket	\$281.82	\$28.18	\$310.00	Per Year
Lighting	\$6.18	\$0.62	\$6.80	Per Pole Per Hour
Other Sporting Fields				
Lighting	\$6.27	\$0.63	\$6.90	Per Pole Per Hour
Amenity Use – 1 canteen + 2 changerooms	\$42.73	\$4.27	\$47.00	Per Day
Amenity Use - 1 canteen + 2 changerooms - 4 hours	\$20.91	\$2.09	\$23.00	Half Day
Lighting callout fee (user error)	\$63.64	\$6.36	\$70.00	Each
Ground Fee – Commercial	\$418.18	\$41.82	\$460.00	Per Day
Ground Fee – Commercial	\$86.36	\$8.64	\$95.00	Per Hour
Unlock/Lock facilities fee	\$61.82	\$6.18	\$68.00	Each
Wauchope Sporting Fields				
Blackbutt Reserve – Lighting – 8 lights on pole	\$8.64	\$0.86	\$9.50	Per Pole Per Hour
Blackbutt Reserve – Lighting – 4 lights on pole	\$6.00	\$0.60	\$6.60	Per Pole Per Hour
Landrigan Park – Lighting	\$6.27	\$0.63	\$6.90	Per Pole Per Hour

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		Year 21/22		
Name	Fee (excl. GST)	GST	Fee (incl. GST)	Unit
Wauchope Sporting Fields [continued]				
Fairmont Gardens – Lighting	\$6.27	\$0.63	\$6.90	Per Pole Per Hour
				1 of flour
Parks & Reserves				
Circuses, Carnivals & Major Events				
Cround Eco. Non Holidova				
Ground Fee – Non Holidays				
Performance Days	\$418.18	\$41.82	\$460.00	Per Day
Bump in/Bump out	\$368.18	\$36.82	\$405.00	Per Day
Ground Hire Fees – School & Public Holidays				
-				
Performance Days	\$577.27	\$57.73	\$635.00	Per Day
Bump in/Bump out	\$463.64	\$46.36	\$510.00	Per Day
Bonds				
Performance Bond	\$6,000.00	\$0.00	\$6,000.00	Each
P				
Power				
Power Use				
At Cost			At Cost	Per Day
Power – Access Fee				
Single Phase	\$39.09	\$3.91	\$43.00	Per Day/Per Outlet
Three Phase	\$78.18	\$7.82	\$86.00	Per
				Day/Per Outlet
General Use				
Application for Private Ceremony	\$116.36	\$11.64	\$128.00	Per Application
2 hours maximum				
Ground Fee – General Use	\$172.73	\$17.27	\$190.00	Per Day
Ground Fee – Commercial Use	\$418.18	\$41.82	\$460.00	Per Day
Ground Fee – Charitable			No Charge	Per Day
Performance Bond	\$2,000 - \$6,0	000 Determine	d according to event	Per Event

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

BadmintonS7.73\$0.77\$8.50Each / Per HourBadminton – Seniors\$7.73\$0.77\$8.50Each / Per HourEquipment HireBalls & Racquets\$3.18\$0.32\$3.50EachShuttles\$1.82\$0.18\$2.00EachCourt HireLocal Clubs and Associations Peak TimeBadminton\$20.00\$2.00\$22.00Basketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourBadminton\$19.55\$1.95\$21.50Badminton\$19.55\$1.95\$21.50Per Hour	Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Application for Filming on Public Lands S60.00 S0 0.00 S0 0.00 Paplication Access of Reserve for Private Works – Performance Bond \$2,000.00 \$0 0.00 \$2,000.00 Per Application Electrical Call Out Fee At Cost Per Hour At Cost Per Hour Stadiums Stadium Hire Costs Stadium Hire Costs Stadium Hire Costs Stadium Hire Costs Monthly Ticket Stadium NA \$24.09 \$2.41 \$26.50 Each / Per Hour Badminton N/A \$24.09 \$2.41 \$26.50 Each / Per Hour Badminton N/A \$24.09 \$2.41 \$26.50 Each / Per Hour Badminton N/A \$24.09 \$2.41 \$26.50 Each / Per Hour Badminton N/A \$24.09 \$2.41 \$26.50 Each / Per Hour Badminton \$3.18 \$0.32 \$3.50 Each / Per Hour Badminton \$1.62 \$0.18 \$2.000 \$2.000 Each Equipment Hire \$1.82 \$0.18 \$2.000 Each Each Court Hire	General Use [continued]				
Application for Filming on Public Lands\$60.00\$0.00\$60.00Per ApplicationAccess of Reserve for Private Works – Performance Bond\$2,000.00\$0.00\$2,000.00Per ApplicationElectrical Call Out FeeAt CostPer HourPer HourStadiumsStadium Hire CostsStadium Hire CostsStadium Hire CostsMonthly Ticket\$24.09\$2.41\$26.50EachBasketball, Volleyball, Futsal, Netball, Other\$3.18\$0.32\$3.50Each / Per HourBadminton\$7.73\$0.77\$8.50Each / Per HourBadminton – Seniors\$3.18\$0.32\$3.50Each / Per HourEquipment HireStadium Stadium\$1.82\$0.18\$2.00Each / Per HourCourt HireStadius And Associations Peak Time\$20.00\$2.00\$2.00Per HourBadminton\$20.00\$2.00\$2.00Per Hour\$2.00\$2.00Per HourCourt HireStadius And Associations Off Peak Times\$3.18\$1.95\$1.95\$2.1.50Per HourBadminton\$20.00\$2.00\$2.00\$2.00\$2.00\$2.00Per HourStadius And Associations Off Peak Times\$3.19.5\$1.95\$1.95\$2.1.50Per Hour	Application to Stage an Event – All Applicants	\$128.00	\$0.00	\$128.00	
Access of Reserve for Private Works – Performance Bond\$2,000.00\$0.00\$2,000.00Per AccessAt CostPort Macquarie Indoor StadiumStadium Hire CostsMonthly TicketBadminton N/A\$24.09\$2.41\$26.50EachCasual PlayBasketball, Volleyball, Futsal, Netball, Other\$3.18\$0.32\$3.50Each / Per HourBadminton\$7.73\$0.77\$8.50Each / Per HourBadminton\$7.73\$0.77\$8.50Each / Per HourBadminton - Seniors\$3.18\$0.32\$3.50Each / Per HourEquipment Hire\$1.82\$0.18\$2.00EachCourt Hire\$1.82\$0.18\$2.00EachBadminton\$20.00\$2.00\$2.00Per HourCourt Hire\$2000\$2.00\$2.00Per HourEadminton\$2000\$2.00\$2.00Per HourLocal Clubs and Associations Off Peak Times\$1.95\$1.95\$21.50Per HourBadminton\$1.95\$1.95\$21.50Per Hour	Application for Filming on Public Lands	\$60.00	\$0.00	\$60.00	Per
Electrical Call Out Fee At Cost Per Hour Stadiums Port Macquarie Indoor Stadium Stadium Hire Costs Stadium Hire Costs Monthly Ticket Badminton NVA \$24.09 \$2.41 \$26.50 Each Badminton NVA \$24.09 \$2.41 \$26.50 Each Casual Play Badsetball, Volleyball, Futsal, Netball, Other \$3.18 \$0.32 \$3.50 Each / Per Hour Badminton \$7.73 \$0.77 \$8.50 Each / Per Hour Badminton - Seniors \$7.73 \$0.77 \$8.50 Each / Per Hour Equipment Hire \$3.18 \$0.32 \$3.50 Each / Per Hour Court Hire \$3.18 \$0.32 \$3.50 Each Local Clubs and Associations Peak Time \$43.64 \$4.36 \$48.00 Per Hour Badminton \$20.00 \$2.00 \$22.00 Per Hour Per Hour Local Clubs and Associations Off Peak Times \$43.64 \$4.36 \$48.00 Per Hour Badminton \$19.55 \$1.95 \$21.50 Per	Access of Reserve for Private Works – Performance Bond	\$2,000.00	\$0.00	\$2,000.00	Per
Port Macquarie Indoor Stadium Stadium Hire Costs Monthly Ticket Badminton N/A \$24.09 \$2.41 \$26.50 Each Casual Play Badsketball, Volleyball, Futsal, Netball, Other \$3.18 \$0.32 \$3.50 Each / Per Hour Badminton \$7.73 \$0.77 \$8.50 Each / Per Hour Badminton - Seniors \$7.73 \$0.77 \$8.50 Each / Per Hour Equipment Hire \$3.18 \$0.32 \$3.50 Each / Per Hour Balls & Racquels \$3.18 \$0.32 \$3.50 Each / Per Hour Court Hire \$1.82 \$0.18 \$2.00 Each Badminton \$20.00 \$2.00 \$2.00 Per Hour Badminton \$3.44 \$4.36 \$48.00 Per Hour Local Clubs and Associations Off Peak Times \$3.95 \$2.00 \$2.00 \$2.00 Per Hour Badminton \$3.95.5 \$19.55 \$19.5 \$21.50 Per Hour	Electrical Call Out Fee			At Cost	
Stadium Hire Costs Monthly Ticket \$24.09 \$2.41 \$26.50 Each Badminton NVA \$24.09 \$2.41 \$26.50 Each Casual Play \$3.18 \$0.32 \$3.50 Each / Per Hour Badminton \$7.73 \$0.77 \$8.50 Each / Per Hour Badminton \$7.73 \$0.77 \$8.50 Each / Per Hour Badminton - Seniors \$7.73 \$0.77 \$8.50 Each / Per Hour Equipment Hire \$3.18 \$0.32 \$3.50 Each / Per Hour Shuttles \$3.18 \$0.32 \$3.50 Each / Per Hour Court Hire \$3.18 \$0.32 \$3.50 Each Shuttles \$3.18 \$0.32 \$3.50 Each Court Hire \$3.182 \$0.18 \$2.00 Each Badminton \$20.00 \$22.00 \$22.00 Per Hour Badminton \$3.64 \$4.36 \$48.00 Per Hour Local Clubs and Associations Off Peak Times \$31.9	Stadiums				
Monthly Ticket Badminton N/A \$24.09 \$2.41 \$26.50 Each Casual Play \$3.18 \$0.32 \$3.50 Fach / Per Badminton \$7.73 \$0.77 \$8.50 Each / Per Badminton \$7.73 \$0.77 \$8.50 Each / Per Badminton \$7.73 \$0.77 \$8.50 Each / Per Badminton - Seniors \$7.73 \$0.77 \$8.50 Each / Per Bals & Racquets \$7.73 \$0.77 \$8.50 Each / Per Balls & Racquets \$1.82 \$0.18 \$2.00 Each Shuttles \$1.82 \$0.18 \$2.00 Each Court Hire Local Clubs and Associations Peak Time \$43.64 \$4.36 \$48.00 Per Hour Badminton \$20.00 \$2.00 \$22.00 Per Hour Per Hour Local Clubs and Associations Off Peak Times \$43.64 \$4.36 \$48.00 Per Hour Badminton \$19.55 \$1.95 \$21.50 Per Hour <td>Port Macquarie Indoor Stadium</td> <td></td> <td></td> <td></td> <td></td>	Port Macquarie Indoor Stadium				
Badminton NVA\$24.09\$2.41\$26.50EachCasual PlayBasketball, Volleyball, Futsal, Netball, Other\$3.18\$0.32\$3.50Each / Per HourBadminton\$7.73\$0.77\$8.50Each / Per HourBadminton - Seniors\$7.73\$0.77\$8.50Each / Per HourBalls & Racquets Shuttles\$3.18\$0.32\$3.50EachCourt Hire\$1.82\$0.18\$2.00EachBadminton\$20.00\$2.00\$22.00Per HourBadminton\$20.00\$2.00\$22.00Per HourLocal Clubs and Associations Off Peak Times\$19.55\$1.95\$21.50Per Hour	Stadium Hire Costs				
Badminton NVA\$24.09\$2.41\$26.50EachCasual PlayBasketball, Volleyball, Futsal, Netball, Other\$3.18\$0.32\$3.50Each / Per HourBadminton\$7.73\$0.77\$8.50Each / Per HourBadminton - Seniors\$7.73\$0.77\$8.50Each / Per HourBalls & Racquets Shuttles\$3.18\$0.32\$3.50EachCourt Hire\$1.82\$0.18\$2.00EachBadminton\$20.00\$2.00\$22.00Per HourBadminton\$20.00\$2.00\$22.00Per HourLocal Clubs and Associations Off Peak Times\$19.55\$1.95\$21.50Per Hour	Monthly Ticket				
Casual PlayBasketball, Volleyball, Futsal, Netball, Other\$3.18\$0.32\$3.50Each / Per HourBadminton\$7.73\$0.77\$8.50Each / Per HourBadminton – Seniors\$7.73\$0.77\$8.50Each / Per HourBadminton – Seniors\$7.73\$0.77\$8.50Each / Per HourBalls & Racquets\$3.18\$0.32\$3.50EachShuttles\$1.82\$0.18\$2.00EachCourt HireLocal Clubs and Associations Peak TimeBadminton\$20.00\$2.00\$22.00Basketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Local Clubs and Associations Off Peak TimesBadminton\$19.55\$1.95\$21.50Per HourLocal Clubs and Associations Off Peak Times\$19.55\$1.95\$21.50Per Hour	-	\$24.00	ድጋ / 1	\$26.50	Fach
Basketball, Volleyball, Futsal, Netball, Other\$3.18\$0.32\$3.50Each / Per HourBadminton\$7.73\$0.77\$8.50Each / Per HourBadminton - Seniors\$7.73\$0.77\$8.50Each / Per HourEquipment Hire\$3.18\$0.32\$3.50EachBalls & Racquets\$3.18\$0.32\$3.50EachShuttles\$1.82\$0.18\$2.00EachCourt HireBadminton\$20.00\$2.00\$2.00Equipment Associations Peak TimeBadminton\$43.64\$4.36\$48.00Per HourBadminton\$20.00\$2.00\$2.00Per Hour\$43.64\$4.36\$48.00Per HourLocal Clubs and Associations Off Peak TimesBadminton\$19.55\$1.95\$21.50Per HourBadminton\$19.55\$1.95\$21.50Per Hour	Bauminton NA	\$24.09	ΨΖ. 4 Ι	\$20. 0 0	Lacii
BadmintonS7.73\$0.77\$8.50Each / Per HourBadminton – Seniors\$7.73\$0.77\$8.50Each / Per HourEquipment HireBalls & Racquets\$3.18\$0.32\$3.50EachShuttles\$1.82\$0.18\$2.00EachCourt HireLocal Clubs and Associations Peak TimeBadminton\$20.00\$2.00\$22.00Basketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourBadminton\$19.55\$1.95\$21.50Badminton\$19.55\$1.95\$21.50Per Hour	Casual Play				
Badminton – SeniorsHour Each / Per HourEquipment Hire\$7.73\$0.77\$8.50Each / Per HourBalls & Racquets Shuttles\$3.18 \$1.82\$0.32 \$0.18\$3.50 \$2.00Each EachCourt Hire\$1.82 \$0.18\$0.18 \$2.00\$2.00 \$22.00Per Hour Per HourBadminton Basketball, Volleyball, Soccer (Indoor), Netball\$20.00 \$43.64\$2.00 \$4.36\$22.00 \$44.36Per Hour Per HourLocal Clubs and Associations Off Peak Times\$19.55 \$1.95\$1.95 \$21.50Per Hour	Basketball, Volleyball, Futsal, Netball, Other	\$3.18	\$0.32	\$3.50	Each / Per Hour
Equipment HireHourBalls & Racquets Shuttles\$3.18 \$1.82\$0.32 \$1.82\$3.50 	Badminton	\$7.73	\$0.77	\$8.50	Each / Per Hour
Balls & Racquets\$3.18\$0.32\$3.50EachShuttles\$1.82\$0.18\$2.00EachCourt HireLocal Clubs and Associations Peak TimeBadminton\$20.00\$2.00\$22.00Basketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourLocal Clubs and Associations Off Peak TimesBadmintonBadminton\$19.55\$1.95\$21.50Per Hour\$19.55\$1.95\$21.50Per Hour	Badminton – Seniors	\$7.73	\$0.77	\$8.50	Each / Per Hour
Balls & Racquets\$3.18\$0.32\$3.50EachShuttles\$1.82\$0.18\$2.00EachCourt HireLocal Clubs and Associations Peak TimeBadminton\$20.00\$2.00\$22.00Basketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourLocal Clubs and Associations Off Peak TimesBadmintonBadminton\$19.55\$1.95\$21.50Per Hour\$19.55\$1.95\$21.50Per Hour	Equipment Hire				
Shuttles\$1.82\$0.18\$2.00EachCourt HireLocal Clubs and Associations Peak TimeBadminton\$20.00\$2.00\$22.00Per HourBasketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourLocal Clubs and Associations Off Peak TimesBadminton\$19.55\$1.95\$21.50Per Hour		\$3.18	\$0.32	\$3.50	Fach
Local Clubs and Associations Peak Time\$20.00\$2.00\$22.00Per HourBadminton Basketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourLocal Clubs and Associations Off Peak TimesBadminton\$19.55\$1.95\$21.50Per Hour					
Local Clubs and Associations Peak Time\$20.00\$2.00\$22.00Per HourBadminton Basketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourLocal Clubs and Associations Off Peak TimesBadminton\$19.55\$1.95\$21.50Per Hour	Court Hire				
Badminton\$20.00\$2.00\$22.00Per HourBasketball, Volleyball, Soccer (Indoor), Netball\$43.64\$4.36\$48.00Per HourLocal Clubs and Associations Off Peak TimesBadminton\$19.55\$1.95\$21.50Per Hour					
Basketball, Volleyball, Soccer (Indoor), Netball \$43.64 \$4.36 \$48.00 Per Hour Local Clubs and Associations Off Peak Times \$19.55 \$1.95 \$21.50 Per Hour	Local Clubs and Associations Peak Time				
Local Clubs and Associations Off Peak Times Badminton \$19.55 \$1.95 \$21.50 Per Hour					
Badminton \$19.55 \$1.95 \$21.50 Per Hour	Basketdall, Volleydall, Soccer (Indoor), Netball	\$43.64	\$4.36	\$48.00	Per Hour
	Local Clubs and Associations Off Peak Times				
Basketball, Volleyball, Soccer (Indoor), Netball \$33.64 \$3.36 \$37.00 Per Hour	Badminton	\$19.55	\$1.95	\$21.50	Per Hour
	Basketball, Volleyball, Soccer (Indoor), Netball	\$33.64	\$3.36	\$37.00	Per Hour

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Other Groups				
Badminton	\$24.09	\$2.41	\$26.50	Per Hour
Basketball, Volleyball, Soccer (Indoor), Netball	\$48.18	\$4.82	\$53.00	Per Hour
Hire of Stadium				
Cleaning				
Extra Cleaning	\$29.09	\$2.91	\$32.00	Per Hour
Inclusive				
6 Courts	\$3,118.18	\$311.82	\$3,430.00	Per Day
3 Courts	\$1,736.36	\$173.64	\$1,910.00	Per Day
Restricted				
6 Courts	\$290.91	\$29.09	\$320.00	Per Hour
3 Courts	\$154.55	\$15.45	\$170.00	Per Hour
Schools				
Supervision Required	\$5.00	\$0.50	\$5.50	Each
Supervision NOT Required	\$4.09	\$0.41	\$4.50	Each
School Holiday Program				
8.30am to 12.30pm	\$19.09	\$1.91	\$21.00	Per Half Day
8.30am to 5pm – Including morning tea	\$29.09	\$2.91	\$32.00	Per Session
<i>After School Care</i> Per 12 children				
On Site	\$57.73	\$5.77	\$63.50	Per Hour
Multi Sport Competition				
Nomination	\$145.45	\$14.55	\$160.00	Per Team
18 Weeks				
Plus Weekly Game Fee	\$52.73	\$5.27	\$58.00	Per Team
18 Weeks				

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Volleyball Competition				
Nomination	\$96.36	\$9.64	\$106.00	Per Team
Plus Weekly Game Fee	\$43.18	\$4.32	\$47.50	Per Team
Ultimate Disc Competition				
Nomination	\$96.36	\$9.64	\$106.00	Per Team
plus Weekly Game Fee	\$43.18	\$4.32	\$47.50	Per Team
Mixed Netball/Fast 5 Competition				
Nomination	\$38.64	\$3.86	\$42.50	Per Team
18 Weeks				
plus Weekly Game Fee 18 Weeks	\$38.64	\$3.86	\$42.50	Per Team
Birthday Parties Supervised 2 hour booking Up to 20 children	\$145.45	\$14.55	\$160.00	Per 2 hour booking
Unsupervised (private) 2 hours	\$96.36	\$9.64	\$106.00	Per 2 hour booking
Up to 20 children				
Meeting Rooms				
Room 1	\$24.09	\$2.41	\$26.50	Per Hour
Room 2	\$14.55	\$1.45	\$16.00	Per Hour
Port Macquarie Regional Sports Stadium				
Special Event / Social			% gate takings	
Charity (on application)	\$86.36	\$8.64	\$95.00	Per Day
Junior Sporting Groups	\$122.73	\$12.27	\$135.00	Per Use

Junior Sporting Groups	\$122.73	\$12.27	\$135.00	Per Use
Hastings League	\$122.73	\$12.27	\$135.00	Per Use
Group 2 / 3 Rugby League	\$236.36	\$23.64	\$260.00	Per Use
Casual Users	\$436.36	\$43.64	\$480.00	Per Day
Lighting Fields 1 and 2	\$38.18	\$3.82	\$42.00	Per Hour

Refundable Bonds and Deposits

Cleaning	\$500-\$5,000	Each

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
			(1101:001)	
Refundable Bonds and Deposits [continued]				
Ground damage	\$2,000 - \$6,0	000 Determine	ed according to event	Each
Property Loss			\$500-\$5,000	Each
Removal of rubbish			\$500-\$5,000	Each
Structural damage			\$500-\$5,000	Each
Regular users – Annual Bond	\$800.00	\$0.00	\$800.00	Each
Regular users – Key Deposits	\$70.00	\$0.00	\$70.00	Each
Regular users – Schools/Charity	\$29.00	\$0.00	\$29.00	Each
Refund Subject to Application				
Council reserves the right to deduct all cleaning costs				
Wauchope Indoor Stadium				
Main Stadium (all courts)				
Commercial, private or other	\$45.45	\$4.55	\$50.00	Per Hour
Not for profit, sporting groups, community groups	\$23.64	\$2.36	\$26.00	Per Hour
Downstairs Studio				
Commercial, private or other	\$15.36	\$1.54	\$16.90	Per Hour
Not for profit, sporting groups, community groups	\$7.73	\$0.77	\$8.50	Per Hour
Upstairs Studio				
Commercial, private or other	\$0.00	\$0.00	\$0.00	Per Hour
Not for profit, sporting groups, community groups	\$0.00	\$0.00	\$0.00	Per Hour
Whole Stadium				
Non sporting				
Per Hour	\$109.09	\$10.91	\$120.00	Per Hour
Per Day	\$827.27	\$82.73	\$910.00	Per Day
Refundable Bonds and Deposits				
Performance Bond	\$2,000 minim	um determine	ed according to event	Per Event
Cleaning			\$500-\$5,000	Each
Regular users – Key Deposits	\$70.00	\$0.00	\$70.00	Each

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Swimming Pools

Kendall, Laurieton and Wauchope

Spectator Admission	\$1.64	\$0.16	\$1.80	Each
General Admission	\$4.18	\$0.42	\$4.60	Each
Concession Admission	\$3.64	\$0.36	\$4.00	Each
Book of 20 Tickets	\$71.36	\$7.14	\$78.50	Each
Book of 50 Tickets	\$154.55	\$15.45	\$170.00	Each
School Groups	\$3.82	\$0.38	\$4.20	Per Person
Teachers and carers free. No lane hire				
School Carnivals	\$4.18	\$0.42	\$4.60	Per Person
Teachers and carers free. No lane hire				
Daycare and Disability Groups	\$4.18	\$0.42	\$4.60	Per Person
Carers free. No lane hire				
Swim Club	\$4.18	\$0.42	\$4.60	Per Person
No Lane Hire Fees				
Lane Hire	\$30.00	\$3.00	\$33.00	Per Hour/per 50 metres of lane space
Per 50 Metres of Lane Space - Per Hour				

Port Macquarie

Spectator Admission	\$1.64	\$0.16	\$1.80	Each
General Admission	\$4.64	\$0.46	\$5.10	Each
Concession Admission	\$4.09	\$0.41	\$4.50	Each
Book of 20 Tickets	\$78.18	\$7.82	\$86.00	Each
Book of 50 Tickets	\$181.82	\$18.18	\$200.00	Each
School Groups	\$4.27	\$0.43	\$4.70	Per Person
Teachers and carer free. No lane hire				
School Carnivals	\$4.64	\$0.46	\$5.10	Per Person
Teachers and carers free. No lane hire				
Daycare and Disability Groups	\$4.64	\$0.46	\$5.10	Per Person
Carers Free. No lane hire				

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Port Macquarie [continued]				
Swim Club	\$4.64	\$0.46	\$5.10	Per Person
No lane hire fees				
Masters Swim Club	\$5.91	\$0.59	\$6.50	Per Person
Out of hours. No lane hire fees.				
Lane Hire – 1x lane of 50m pool	\$30.00	\$3.00	\$33.00	Per Hour/per 50 metres of lane space
Per hour, per 50 metres of lane space				

Cemeteries

Port Macquarie Hastings Cemeteries

Refer to Appendix 11

Inurnment of Ashes

Wall Niche Inurnment	\$1,298.18	\$129.82	\$1,428.00	Each
Wall Niche Reservation	\$743.64	\$74.36	\$818.00	Each
Plaques for above	\$554.55	\$55.45	\$610.00	Each

Extras

Family Ash Placement by Appointment		on Application	Each			
Surcharge for Grave Digging		Each				
Applies to requests outside of normal working hours for religious reason	ns					
Administration Fee – Transferring Rights of Burial	\$160.00	\$0.00	\$160.00	Each		
Ash Removal and Packaging	\$227.27	\$22.73	\$250.00	Each		
Late Fee	\$172.73	\$17.27	\$190.00	Each		
Applies to services which cannot be completed within normal business hours (9am to 3pm Monday to Friday)						
Application for Exhumation	Exhumation requires approval from NSW Health. Price subject to quotation.			Each		
Saturday Surcharge	\$454.55	\$45.45	\$500.00	Each		

Burial

All General Cemeteries

Port Macquarie General Digging Fee	\$2,018.18	\$201.82	\$2,220.00	Each
Grave Digging Fee	\$1,500.00	\$150.00	\$1,650.00	Each
Grave Digging Fee – Child up to 12 years	\$966.36	\$96.64	\$1,063.00	Each

continued on next page ...

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Name Fee	GST	Fee	1 Insi4
	001	Fee	Unit
(excl. GST)	(incl. GST)	

All General Cemeteries [continued]

Open General Cemeteries - Site only or Reservation Fee	\$2,268.18	\$226.82	\$2,495.00	Each
Open General Cemeteries – Site only (Child up to 12 years)	\$816.36	\$81.64	\$898.00	Each
Single Headstone Application	\$130.00	\$0.00	\$130.00	Each
Double Headstone Application	\$190.00	\$0.00	\$190.00	per application

Ash Replacement

All General Cemeteries

Placement fee per lot of ash in an occupied gravesite (up to 4) – Open cemeteries	\$627.27	\$62.73	\$690.00	Each
Placement of ash in a gravesite (up to 4)				
Placement fee per lot of ash in an occupied gravesite (up to 4) – Closed cemeteries	\$627.27	\$62.73	\$690.00	Each
New Site – Right of Burial Site Fee	\$2,281.82	\$228.18	\$2,510.00	Each
Administration Fee for Family History Enquiries	\$65.00	\$0.00	\$65.00	Per 30 Minutes or Part Thereof

Communications

CBD Flags

CBD Flags – Installation (42 Flags)	\$1,118.18	\$111.82	\$1,230.00	Per Installation
CBD Flags – Installation (21 Flags)	\$559.09	\$55.91	\$615.00	Per Installation

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Your Natural & Built Environment				
Development Assessment				
Approvals under Environmental Planning and A	Assessme	nt Act (El	P&A Act)	
Subdivision Works Certificate				
Subdivision and Infrastructure Works				
Works Certificate (Infrastructure) Application – Dual Occupancy (Torrens or Strata)	\$280.91	\$28.09	\$309.00	Each
Base Administration Fee - applicable to ALL applications with constructi (includes AUSPEC admin & maintenance)	on of public infra	astructure wor	ks and/or subdi	vision works
Includes Roads Act (s138) application when works application works are	e on existing put	olic roads		
Works Certificate (Infrastructure) Application, Building Related or Subdivision Works With No New Public Roads	\$280.91	\$28.09	\$309.00	Each
Base Administration Fee - applicable to ALL applications with constructi without the creation of new public roads (includes AUSPEC admin & ma		astructure wor	ks and/or subdi	vision works
Works Certificate (Infrastructure) Application, Subdivision Works with New Public Roads	\$280.91	\$28.09	\$309.00	Each
Base Administration Fee - applicable to ALL applications with constructi works with the creation of new public roads (includes AUSPEC admin &		astructure wor	ks and / or subo	division
Includes Roads Act (s138) application when works in application are on	existing public r	oads		
Works Certificate (Infrastructure) Application Amendments	\$118.18	\$11.82	\$130.00	Each
Base administration fee for additional amendment				
First amendment included in initial application				
Engineering Review Fees – Dual Occupancy (Torrens)	\$750.00	\$75.00	\$825.00	Each
Engineering review fees for dual occupancy Works Certificate (Infrastructure)	applications			
Engineering Review Fees – Dual Occupancy (Strata)	\$500.91	\$50.09	\$551.00	Each
Engineering review fees for dual occupancy Works Certificate (Infrastructure)	applications			
Engineering Review Fees – Minor Works	\$251.82	\$25.18	\$277.00	Review Area
Engineering review fees for Works Certificate (Infrastructure) application	ns with minor wo	rks and no ne	w public roads	
Minimum fee \$405				
Engineering Review Fees – Major Works	\$358.18	\$35.82	\$394.00	Review Area
Engineering review fees for Works Certificate (Infrastructure) application	ns with major wo	rks and no ne	w public roads	
Minimum fee \$623				
	\$266.36	\$26.64	\$293.00	Per Lot
Engineering Review Fees – Subdivision Development (with new public roads)	φ200.00			
		Vorks with Ne	w Public Roads	5

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Subdivision Works Certificate [continued]				
Engineering Review Fees – Amendments	\$54.55	\$5.45	\$60.00	Review Area
Engineering review fees for additional amendments, excludes first ame Minimum fee \$94	endment			
Development Inspection Fees – Dual Occupancy (Torrens)	\$363.64	\$36.36	\$400.00	Each
Development inspection fees for dual occupancy Works Certificate (Infrastrue	cture) applications	•		
Development Inspection Fees – Dual Occupancy (Strata)	\$243.64	\$24.36	\$268.00	Each
Development inspection fees for dual occupancy certificate Works Certificate	(Infrastructure) a	pplications		
Development Inspection Fees – Minor Works	\$122.73	\$12.27	\$135.00	Review Area
Development inspection fees for Works Certificate (Infrastructure) appl works associated with building or subdivision development Minimum fee \$176	ications with mir	nor public infra	structure and su	Ibdivision
Development Inspection Fees – Major Works	\$233.64	\$23.36	\$257.00	Review Area
Development inspection fees for Works Certificate (Infrastructure) appl works associated with building or subdivision development Minimum fee \$353	ications with ma	jor public infra	structure and su	Ibdivision
Subdivision Inspection Fee – Residential Roads	\$22.00	\$0.00	\$22.00	Per Metre
New roads (residential) Minimum fee \$399				
Subdivision Inspection Fee – Rural Roads	\$10.91	\$1.09	\$12.00	Per Metre
New roads (rural) Minimum fee \$399				
Subdivision Inspection Fee – Industrial Roads	\$24.55	\$2.45	\$27.00	Per Metre
New roads (industrial)				
Minimum fee \$399				
Subdivision Inspection Fee – Rural Residential Roads New roads (rural residential)	\$18.18	\$1.82	\$20.00	Per Metre
Minimum fee \$399				
Additional Inspection Fee	\$122.73	\$12.27	\$135.00	Per Hour
For infrastructure works associated with building developments (non re	fundable)			
Minimum 1 hour				
Works Certificate (Infrastructure) Modification – Minor Works	\$122.73	\$12.27	\$135.00	Review Area
Applicable to modify existing Works Certificate (Infrastructure) after ap	proval. Base app	olication fees a	lso apply.	
Fees for engineering reviews associated with proposed changes				

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Subdivision Works Certificate [continued]				
Works Certificate (Infrastructure) Modification – Major Works	\$193.09	\$19.31	\$212.40	Review Area
Applicable to modify existing Works Certificate (Infrastructure) after ap	proval. Base app	lication fees a	lso apply.	
Fees for engineering reviews associated with proposed changes				
Outstanding Works Bond Fees & Charges	See "S	ecurity Bond" s	section (below)	
Approvals under Roads Act 1993				
Section 138 Application Works, activities, events in, on and above a public road				
Single Residential Dwelling Driveway and Utility Connection Application (All Works on Public Road Reserve)	\$369.00	\$0.00	\$369.00	Each
Note: Upon Roads Act approval - vehicle crossings must be constructed contractor listed with Council must be used with public liability insurance by Council (s138)				
Includes base administration fee and inspection				
Dual Occupancy without Subdivision (All works on Public Road Reserve)	\$309.00	\$0.00	\$309.00	Each
Note: Upon Roads Act approval - vehicle crossings must be constructed contractor listed with Council must be used with public liability insurance by Council (s138)				
Does not include inspection and review fees				
Industrial Driveway Application (All works on Public Road Reserve)	\$309.00	\$0.00	\$309.00	Each
Note: Upon Roads Act approval - vehicle crossings must be constructe contractor listed with Council must be used with public liability insurance by Council (s138)				
Does not include inspection and review fees				
Roads Act Application for Construction Works on Road Reserve (Section 138 Application)	\$309.00	\$0.00	\$309.00	Each
Base administration fee (applies to all Section 138 applications other th	an residential d	riveway applica	ations)	
Roads Act Application Amendments	\$129.00	\$0.00	\$129.00	Each
Base administration fee for additional amendments after the first amen	dment			
First amendment included in initial application				
Engineering Review Fees – Dual Occupancy without Subdivision (All works on Public Road Reserve)	\$549.00	\$0.00	\$549.00	Each
Engineering review fees for Dual Occupancy without Subdivision applied	ations			
Engineering Review Fees – Industrial Driveway (All works on Public Road Reserve)	\$551.00	\$0.00	\$551.00	Each
Engineering review fees for Industrial Driveway applications				
Engineering Review Fees – Minor Works	\$277.00	\$0.00	\$277.00	Review Area

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit		
Section 138 Application [continued]						
Engineering Review Fees – Minor Works	\$277.00	\$0.00	\$277.00	Review Area		
Engineering review fees for Roads Act applications with minor infrastru	cture works on p	oublic roads				
Minimum fee \$405						
Engineering Review Fees – Major Works	\$394.00	\$0.00	\$394.00	Review Area		
Engineering review fees for Roads Act applications with major infrastructure works on public roads						
Minimum fee \$623						
Engineering Review Fees – Amendments	\$60.00	\$0.00	\$60.00	Review Area		
Engineering review fees for additional amendments, excludes first ame	ndment					
Minimum fee \$94						
Development Inspection Fees – Dual Occupancy without Subdivision (All works on Public Road Reserve)	\$269.00	\$0.00	\$269.00	Each		
Development Inspection fees for Dual Occupancy without Subdivision a	applications					
Development Inspection Fees – Industrial Driveway (All works on Public Road Reserve)	\$269.00	\$0.00	\$269.00	Each		
Development Inspection fees for Industrial Driveway						
Development Inspection Fees – Minor Works	\$135.00	\$0.00	\$135.00	Review Area		
Development inspection fees for Roads Act applications with minor infra	astructure and r	oad works on p	oublic roads			
Minimum fee \$176						
Development Inspection Fees – Major Works	\$257.00	\$0.00	\$257.00	Review Area		
Development inspection fees for Roads Act applications with major infra	astructure and r	oad works on p	oublic roads			
Minimum fee \$353						
Additional Inspection Fee	\$135.00	\$0.00	\$135.00	Per Hour		
For infrastructure works associated with building developments (non re	fundable)					
Minimum 1 hour						
Roads Act Approval Modification – Minor Works	\$137.00	\$0.00	\$137.00	Review Area		
Applicable to modify existing Roads Act after approval. Base administration	ation fees also a	pply.				
Fees for engineering reviews associated with proposed changes						
Roads Act Approval Modification – Major Works	\$194.00	\$0.00	\$194.00	Review Area		
Applicable to modify existing Roads Act after approval. Base administration	ation fees also a	pply.				
Fees for engineering reviews associated with proposed changes						
Outstanding Works Bond Fees & Charges	See "S	ecurity Bond" s	ection (below)			

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Name	Year 21/22 Fee GST Fee			Unit
	(excl. GST)	631	(incl. GST)	
ecurity Bonds				
Application for Temporary Occupation of Footways and Road Reserve for Hoardings and/or Work Zones – CBD or Shopping Precinct	\$100,000.00	\$0.00	\$100,000.00	
Security bond for infrastructure damage associated with building develo Council satisfaction upon completion of development.	opments. Refund	able upon le	ased area being	left to
This bond not required if applicant has submitted security for damage to developments to be determined on merit by Council.	o infrastructure. N	Maximum \$1	00,000 bond for 1	minor
Application for Temporary Occupation of Footways and Road Reserve for Hoardings and/or Work Zones – Other (Not CBD or Shopping Precinct)	\$50,000.00	\$0.00	\$50,000.00	
Security bond for infrastructure damage associated with building develor Council satisfaction upon completion of development.	opments. Refund	able upon le	ased area being	left to
This bond not required if applicant has submitted security for damage to developments to be determined on merit by Council.	o infrastructure. N	Maximum \$5	0,000 bond for m	inor
Development Application Fees				
Application Fees				
Application Fees (AII)	A	As per statuto	ory scale of fees	Estimate
EP&A Regulation 2000 refer cl 246B				
dvertising				
Vhere advertising under ss.79 or 79A is compulsory				
Designated Development	\$2,220.00	\$0.00	\$2,220.00	Each
EP&A Regulation 2000 CI 252 (maximum charge)	.,			
This fee is in addition to the application fee mentioned above and unex application	pended fees will	be refunded	after determinati	on of
Where Notification is Required by Council Policy	\$168.00	\$0.00	\$168.00	Each
etters to adjoining & adjacent owners. EP&A Regulation cl 252 maxim	um fee \$1,105			
Where Notification and Advertising is Required by Council Policy – Advertising in Local Paper	\$479.00	\$0.00	\$479.00	Each
EP&A Regulation cl 252 maximum fee \$1,105				
odgement Fees				
Electronic Lodgement	\$15.00	\$0.00	\$15.00	Each
-	\$15.00	\$0.00	\$15.00	Each
Electronic Lodgement Form and supporting documents provided in PDF as un-protected files Paper Lodgement	\$15.00 \$165.00	\$0.00 \$0.00	\$15.00 \$165.00	Each Each

Scanning and archive costs for applications lodged in paper only

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Archive Fee	\$43.00						
	0.0 \$10						
Electronic Application	\$45.00	\$0.00	\$43.00	Each			
All documents submitted in PDF as un-protected files							
Paper Application	\$190.00	\$0.00	\$190.00	Each			
Scanning and archive costs for paper based applications							
Review of Determination of DA							
Review of Determination of DA	1	As per statutor	y scale of fees	Estimate			
EP&A Regulation 2000 cl 257							
Modification of DA or Consent							
Correction to Minor Error, Misdescription or Miscalculation	As per sta	tutory scale of	fees in EP&A Regulation	Each			
Modification Involving Minimal Environmental Impact	As per sta	tutory scale of	fees in EP&A Regulation	Estimate			
Other Modifications	As per sta	tutory scale of	fees in EP&A Regulation	Estimate			
Integrated Development							
Processing Fee for Council	As per sta	tutory scale of	fees in EP&A Regulation	Each			
For Each Relevant Authority (Separate Cheque Forwarded by Council)	As per sta	tutory scale of	fees in EP&A Regulation	Each Authority			
When Concurrence is Required							
Processing Fee for Council	As per sta	tutory scale of	fees in EP&A Regulation	Each			
For Each Relevant Authority (Separate Cheque Forwarded by Council)	As per sta	tutory scale of	fees in EP&A Regulation	Each Authority			
Approvals under Environmental Planning Assessment Act (EP&A Act)							
Subdivision Works Certificate – Earthworks & Clearing ONLY – Application Fee							
Base administration fee (applicable to ALL applications)	\$646.36	\$64.64	\$711.00	Each			

Subdivision Works Certificate – Earthworks & Clearing ONLY – Engineering Plan Checking

				-
VOW up to \$10,000	\$340.00	\$34.00	\$374.00	Each
VOW \$10,001 - \$20,000	\$479.09	\$47.91	\$527.00	Each
VOW \$20,001 - \$50,000	\$655.45	\$65.55	\$721.00	Each

continued on next page ...

Add engineering plan checking fees for Value of Work (VOW)

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Subdivision Works Certificate – Earthworks & Clearing ONLY – Engineering Plan Checking [continued]

VOW \$50,001 - \$100,000	\$1,303.64	\$130.36	\$1,434.00	Each
VOW \$100,001 - \$150,000	\$1,563.64	\$156.36	\$1,720.00	Each
VOW \$150,001 - \$300,000	\$2,102.73	\$210.27	\$2,313.00	Each
VOW \$300,001 - \$800,000 PLUS per \$1,000 above \$800,000 fee (as below)	\$2,915.45	\$291.55	\$3,207.00	Each
Per \$1,000 above \$800,000 fee	\$6.36	\$0.64	\$7.00	Each

Compliance Certificates for Subdivision Works

Compliance Certificate Application - Subdivision Development	\$309.00	\$0.00	\$309.00	Each
Application to provide concurrence from relevant authorities or Council	sections for cor	struction of ne	w assets.	
This application does not permit construction of infrastructure. A separate Construction Certificate for subdivision works or Construction Certificate (Infrastructure) required to permit construction. Note: all subdivision inspection fees still apply.				
General Review – Subdivision Development	\$60.00	\$0.00	\$60.00	Per Lot
Compliance Certificate review fees for one Council review area (i.e lan	dscaping)			
Minimum Fee \$134				
Water Authority Review - Subdivision Development	\$118.00	\$0.00	\$118.00	Per Lot
Water Authority Compliance Certificate Review Fees				
Minimum Fee \$269				

Refunds

Development Applications & Construction Certificates

Cancelled / Withdrawn After 7 days With No Assessment by Council Officer (Minimum)	\$58.18	\$5.82	\$64.00	Each
Administration fee and PCA component deducted				
All Other Cases Where No Work Has Commenced			No Charge	Each
DA's				
Cancelled / Withdrawn within 48 Hours – CC's (Infrastructure), S138, Subdivision Certificate Fees and Bonds		Administration	n fee deducted	Each

Security Bonds

Bond Administration

Bond Administration Fee	\$284.00	\$0.00	\$284.00	Each
Base Administration Fee (applicable to ALL bonds)				
Non Refundable				

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Outstanding Works Bonds

Bond is security for completion of engineering works on public roads or infrastructure to be paid prior to approval

Outstanding Works Bond (Single Dwelling) - Complex Applications	\$2,000.00	\$0.00	\$2,000.00	Each
Applies to complex residential dwelling applications based upon Counc	il discretion. Bond	d refundable.		
Outstanding Works Bond (Dual Occupancy)	\$5,000.00	\$0.00	\$5,000.00	Each
Applies to 2 dwellings or units ONLY. Bond refundable.				
Outstanding Works Bond (Industrial Driveway)	\$8,000.00	\$0.00	\$8,000.00	Each
Bond is security for completion of engineering works on public roads or	infrastructure to	be paid prior t	o approval	
Applies to industrial driveway applications.				
Outstanding Works Bond (General)	Calculation can be used for any case based upon 130% contract cost inclusive of GST. Bond refundable.			Contract Cost

Defects Liability Bonds

Bond is security for new Council assets prior acceptance into a defects liability period

Defects Liability Bond (Dual Occupancy)	\$2,000.00	\$0.00	\$2,000.00	Each	
Bond applies to dual-occupancy Torrens Title subdivisions					
Defects Liability Bond (General)		Calculation can be used for any case based upon 10% assets to be dedicated to Council. Bond refundable.			

Protection of Public Infrastructure Bonds

Security deposit for damage to public infrastructure. Minimum \$10,000 (minor building works) minimum \$100,000 (major building works and CBD development). Refundable upon completion of development to Council satisfaction.

Security Bond for Infrastructure Works and Infrastructure Damage	Calculate	Each
Associated with Building Developments		

Tree Preservation Bonds

Council can require lodgement at subdivision (or construction) certificate stage of a deposit to secure retention of trees on public land subject to application

Refundable where Council satisfied of no loss

Tree Preservation Bond – Specific Species (Any Development)	\$11,956.00	\$0.00	\$11,956.00	Per Tree
Council Accreditation Charges				
-				
Authorised Contractors Annual Registration Fee	\$170.00	\$0.00	\$170.00	Each

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Subdivisions

Subdivision Certificates (Torrens)

Application for Subdivision Certificate (Torrens)	\$236.00	\$0.00	\$236.00	Per Lot
Base Administration Fee				
Minimum fee \$444 (includes consolidation)				
Resubmitted Linen Plan	25% of origin	al fee or \$25	5 whichever is lesser	Estimate
Application Fee for Council to Release, Vary or Modify Real Property Documents	\$211.00	\$0.00	\$211.00	Each
Under General Manager delegation (s377 Local Govt Act)				
Modification of real property instruments				
Application Fee for Council to Release, Vary or Modify Real Property Documents	\$419.00	\$0.00	\$419.00	Each
Under Council seal following report to Council				
Modification of real property instruments				
Re-signing of Subdivision Plans/Post Cadastral Correction	\$211.00	\$0.00	\$211.00	Each
Endorsement of Plans (For Example: S88B Instruments, Transfers, etc) – Council's Legal Costs are in Addition to This Fee	\$211.00	\$0.00	\$211.00	Each
Lighting – Non Standard – "Prestige"		Levy -	see Policy R5	Each

Subdivision Certificates (Torrens) and Occupation Certificates (Strata)

Work as Executed – Strata and Torrens Title	\$656.00	\$0.00	\$656.00	Each		
Submission of wax detail NOT in CAD format - minor subdivision works only - up to 3 lots and any building development works						

Search Fee – Miscellaneous

Minimum 1 hour	\$63.00	\$0.00	\$63.00	Per Hour
Aus-spec #1				
Refer Appendix 1				
Construction Specification				
Design specifications one off hard copy	\$291.00	\$0.00	\$291.00	Per Set
Construction specifications one off hard copy	\$359.00	\$0.00	\$359.00	Per Set
Hard copy individual specifications	\$32.00	\$0.00	\$32.00	Each

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Certificates

Environmental Planning & Assessment Act 1979

Section 10.7(2) Planning Certificate	\$53.00	\$0.00	\$53.00	Each
Fixed by EPA Reg 2000 cl 259				
Section 10.7(5) Planning Certificate	\$80.00	\$0.00	\$80.00	Each
Maximum under EPA Reg 2000 cl 259				
Expedition Charge (24 Hr Response)	\$110.00	\$0.00	\$110.00	Each
Section 6.26 Building Certificate		As per statutor	y scale of fees	Each
Refer EPA Reg 2000 cl 260-261				
Strata Certificate				
Strata Certificate	\$156.36	\$15.64	\$172.00	Per Lot
Bushfire Attack Level				
Bushfire Attack Level (BAL) Certificate	\$351.82	\$35.18	\$387.00	Each
Bushfire Attack Level (BAL) Certificate Associated With Complying Development Certificate Lodged With PMHC	\$201.82	\$20.18	\$222.00	Each
Conveyancing Act 1919 No. 6				
Section 88G Certificate		As per statutor	y scale of fees	Per Lot
Inspection of Land Required		As per statutor	y scale of fees	Per Lot
Plan Copy Charges				
Refer Appendix 1				
A3 & A4 Plans	\$4.20	\$0.00	\$4.20	Per Shee
A2 Sheet	\$14.00	\$0.00	\$14.00	Per Shee
A1 Sheet	\$16.00	\$0.00	\$16.00	Per Shee
A0 Sheet	\$20.00	\$0.00	\$20.00	Per Shee
Over Size (Sewer Plans)	\$22.00	\$0.00	\$22.00	Per Shee
Or per Lineal Metre	\$18.00	\$0.00	\$18.00	Per L/Mtr
Development Records				
Copy of Building Certificate		As per statutor	y scale of fees	Each
EPA Regulation 261				
Written Report on Dwelling Entitlement or Zoning Matter	\$176.00	\$0.00	\$176.00	Each
Plus \$80.00 inspection fee if required				

continued on next page ...

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Development Records [continued]

Urgent (Response in 5 Days) – Written Report on Dwelling Entitlement or Zoning Matter	\$293.00	\$0.00	\$293.00	Each
Copy of Application Documents (i.e. Consent, Plan, Submission or Supporting Report)	\$44.00	\$0.00	\$44.00	Each
Before making a request check www.pmhc.nsw.gov.au/applicationtracker - wh	nat you need may	already be avai	lable free. Any	

documents requested will be emailed. Searches for some historic records are more involved and additional fees may apply; we will discuss this with you.

Waste Management

Under section 88 of the *Protection of the Environment Operations Act 1997* (POEO Act), Council is required to pay the Waste & Environment Levy. This levy is considered when setting the fees and charges for waste management services.

Interest Rate

Interest Rate on overdue rates & charges

As per the rate determined by the NSW Office of Local Government

Domestic Waste Management Services	6%	
Other Waste Management Services	6%	

Domestic Waste Management Services

Annual Charge

Minimiser service - 140 Litre fortnightly general waste	\$428.00	\$0.00	\$428.00	Per Annum
Standard service - 240 Litre fortnightly general waste	\$488.00	\$0.00	\$488.00	Per Annum
Excess service – 240 Litre weekly general waste	\$721.00	\$0.00	\$721.00	Per Annum
Half minimiser service – 140 Litre fortnightly general waste	\$239.00	\$0.00	\$239.00	Per Annum
Multi-unit dwellings only				
Half standard service - 240 Litre fortnightly general waste (Shared)	\$268.00	\$0.00	\$268.00	Per Annum
Multi-unit dwellings only				
Half excess service - 240 Litre weekly general waste (Shared)	\$386.00	\$0.00	\$386.00	Per Annum
Multi-unit dwellings only				
On application only special weekly service - 140 Litre weekly general waste	\$603.00	\$0.00	\$603.00	Per Annum
Half special weekly service (shared)	\$328.00	\$0.00	\$328.00	Per Annum
Multi-unit dwellings only				

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Annual Charge [continued]				
Additional weekly garbage service - 240 Litre general waste	\$373.00	\$0.00	\$373.00	Per Annum
Additional weekly garbage service - 140 Litre general waste	\$283.00	\$0.00	\$283.00	Per Annum
Multi-unit dwellings only				
Additional organics service - 240 Litre weekly	\$98.00	\$0.00	\$98.00	Per Annum
Additional recycling service – 240 Litre fortnightly	\$76.00	\$0.00	\$76.00	Per Annum
Minimum Waste Charge – Single Dwelling	\$428.00	\$0.00	\$428.00	Per Annum
Availability charge on vacant land	\$79.00	\$0.00	\$79.00	Per Annum
Availability charge on land not receiving full charge	\$143.00	\$0.00	\$143.00	Per Annum
Multi-unit dwellings only				

Other Waste Management Services

Annual Charge

Waste Management Access Charge	\$31.00	\$0.00	\$31.00	Per Annum
Commercial weekly service - 140 Litre general waste	\$594.00	\$0.00	\$594.00	Per Annum
Commercial excess service - 240 Litre weekly general waste	\$709.00	\$0.00	\$709.00	Per Annum
Commercial half weekly service - 140 Litre general waste (Shared)	\$322.00	\$0.00	\$322.00	Per Annum
Commercial half excess service – 240 Litre weekly general waste (Shared)	\$380.00	\$0.00	\$380.00	Per Annum
Commercial additional weekly garbage service – 240 Litre general waste	\$372.00	\$0.00	\$372.00	Per Annum
Commercial additional weekly garbage service – 140 Litre general waste	\$283.00	\$0.00	\$283.00	Per Annum
Commercial additional organics service - 240 Litre weekly	\$98.00	\$0.00	\$98.00	Per Annum
Commercial additional recycling service - 240 Litre fortnightly	\$76.00	\$0.00	\$76.00	Per Annum
Commercial availability charge on land not receiving full charge	\$79.00	\$0.00	\$79.00	Per Annum
Business Properties				
Availability charge on vacant land	\$79.00	\$0.00	\$79.00	Per Annum
Business Properties				

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Other Services				
On Request Kerbside Collection	\$39.00	\$0.00	\$39.00	Per Service
Replacement Bin	\$59.00	\$0.00	\$59.00	Per Unit
Supply of Bins – New Service	\$149.00	\$0.00	\$149.00	Per Service
Stolen Bin – First Replacement Only			No Charge	Per Unit
Stolen Bin – Subsequent Replacements	\$59.00	\$0.00	\$59.00	Per Unit
Bin Size Adjustment Fee	\$59.00	\$0.00	\$59.00	Per Unit
Red or yellow bin				
Bin Reinstatement Fee	\$59.00	\$0.00	\$59.00	Per Unit
Charge for replacement of organic/recycle bin, due to suspended servi	ce			
Collection of Missed Service	\$15.00	\$0.00	\$15.00	Per Unit
Weighbridge Ticket	\$50.91	\$5.09	\$56.00	Each
Waste Adminstration Fee (Unpaid Waste Fees)	\$20.00	\$2.00	\$22.00	Each
Contamination Reloading Fee	\$227.27	\$22.73	\$250.00	Per Incident
Charge for management and reloading any waste incorrectly declared a site	at weighbridge/	gatehouse or o	lisposed in incor	rect area at

Waste Depot Charges

Cairncross Waste Depot

General Waste

Calculated by Nett Weight

Transfer Station General Waste	\$229.09	\$22.91	\$252.00	Per Tonne
Minimum Charge - \$10				
All Mattresses or Mattress Bases	\$25.45	\$2.55	\$28.00	Each
Building Waste (Landfill)	\$229.09	\$22.91	\$252.00	Per Tonne
Includes contaminated soil classified as general solid waste				
Minimum charge - \$10				
Asbestos	\$355.45	\$35.55	\$391.00	Per Tonne
Minimum charge - \$10				
Mixed Solid Waste	\$229.09	\$22.91	\$252.00	Per Tonne
Minimum charge - \$10				
Clean fill / VENM	\$100.00	\$10.00	\$110.00	Per Tonne
Minimum charge - \$10				
Bricks / Concrete	\$27.27	\$2.73	\$30.00	Per Tonne
Minimum charge - \$10				

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
General Waste [continued]				
Green / Wood Waste	\$60.00	\$6.00	\$66.00	Per Tonne
Includes lawn clippings, garden waste, branches & leaves, trees & tree $\hfill\square$	loppings & untr	eated timber		
Special Wastes			By Negotiation	
Subject to the nature of the material & ability to receive				
Recyclable materials			No Charge	
Clean glass, paper cardboard, oil in drop-off area				
Metal & White goods			No Charge	
Clean and Separated				

Tyres

Motorcycle	\$3.64	\$0.36	\$4.00	Each
Car	\$3.64	\$0.36	\$4.00	Each
Car on rim	\$9.09	\$0.91	\$10.00	Each
4x4	\$7.27	\$0.73	\$8.00	Each
Light Truck	\$7.27	\$0.73	\$8.00	Each
Light Truck/4x4 on rim	\$18.18	\$1.82	\$20.00	Each
Heavy Truck	\$16.36	\$1.64	\$18.00	Each
Heavy Truck on rim	\$31.82	\$3.18	\$35.00	Each
Super Singles	\$35.45	\$3.55	\$39.00	Each
Super Singles on rim	\$72.73	\$7.27	\$80.00	Each
Tractor – Small/Medium	\$40.00	\$4.00	\$44.00	Each
Tractor – Large	\$59.09	\$5.91	\$65.00	Each
Other (Tractor Earthmoving)			By Assessment	Each

Animals

Small Domestic	\$10.00	\$1.00	\$11.00	Each
Small Livestock (sheep, goat, calf, etc)	\$39.09	\$3.91	\$43.00	Each
Large Livestock (cattle, horses, etc)	\$62.73	\$6.27	\$69.00	Each

Recovered Aggregate

Sale of Recycled Crushed Concrete Aggregate (Various Sizes)	Price on Application	Per Tonne			
In accordance with the NSW Environment Protection Authority (EPA) Recovered Aggregate Resource Recovery Order 2					

Port Macquarie Waste Depot

Commercial Waste loads are limited to Passenger Vans, Utilities, Small Trailers up to 8' x 5' size with no side extensions.

Name	Fee	Year 21/22 GST	Fee	Unit
Naille	(excl. GST)	631	(incl. GST)	
General Waste				
Transfer Station General Waste	\$229.09	\$22.91	\$252.00	Per Tonne
Minimum Charge - \$10				
All Mattresses or Mattress Bases	\$25.45	\$2.55	\$28.00	Each
Green / Wood Waste	\$60.00	\$6.00	\$66.00	Per Tonne
Includes lawn clippings, garden waste, branches & leaves, trees & tree $\hfill\square$	e loppings & untre	eated timber		
Recyclable Materials			No Charge	
Clean glass, paper cardboard, oil in drop-off area				
Metal & White Goods			No Charge	
Clean and Separated				
Special Wastes			By Negotiation	
Subject to nature of the material & ability to receive				

Tyres

Motorcycle	\$3.64	\$0.36	\$4.00	Each
Car	\$3.64	\$0.36	\$4.00	Each
Car on rim	\$9.09	\$0.91	\$10.00	Each
4x4	\$7.27	\$0.73	\$8.00	Each
Light Truck	\$7.27	\$0.73	\$8.00	Each
Light Truck/4x4 on rim	\$18.18	\$1.82	\$20.00	Each
Heavy Truck	\$16.36	\$1.64	\$18.00	Each
Heavy Truck on rim	\$31.82	\$3.18	\$35.00	Each
Super Singles	\$35.45	\$3.55	\$39.00	Each
Super Singles on rim	\$72.73	\$7.27	\$80.00	Each
Tractor – Small/Medium	\$40.00	\$4.00	\$44.00	Each
Tractor – Large	\$59.09	\$5.91	\$65.00	Each
Other (Tractor Earthmoving)		E	By Assessment	

Wauchope, Kew & Comboyne Waste Depots

Household Loads Only - Council's Waste Depot Attendants calculate charges per size of load at entry.

General Waste

Domestic

Cars/Station Wagons	\$10.00	\$1.00	\$11.00	Per Load
Passenger Vans, Utilities, Small Trailers up to 8' x 5' size with no side extensions	\$17.27	\$1.73	\$19.00	Per Load
Large trailers and all trailers with side extensions, Vans & 1 Ton Utilities	\$29.09	\$2.91	\$32.00	Per Load

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Commercial				
Cars/Station Wagons	\$75.45	\$7.55	\$83.00	Each
Passenger Vans, Utilities, Small Trailers up to 8' x 5' size with no side extensions	\$75.45	\$7.55	\$83.00	Per Load
Accepted load size limited to 0.5 metre in height				
Greenwaste				
Domestic				
Cars/Station Wagons	\$9.09	\$0.91	\$10.00	Per Load
Passenger Vans, Utilities, Small Trailers up to 8' x 5' size with no side extensions	\$14.55	\$1.45	\$16.00	Per Load
Large trailers and all trailers with side extensions, Vans & 1 Ton Utilities	\$25.45	\$2.55	\$28.00	Per Load
Commercial				
Cars/Station Wagons	\$22.73	\$2.27	\$25.00	Each
Passenger Vans, Utilities, Small Trailers up to 8' x 5' with no side extensions	\$22.73	\$2.27	\$25.00	Per Load
Tyres				
Motorcycle	\$3.64	\$0.36	\$4.00	Each
Car	\$3.64	\$0.36	\$4.00	Each
Car on rim	\$9.09	\$0.91	\$10.00	Each
4x4	\$7.27	\$0.73	\$8.00	Each
Light Truck	\$7.27 \$18.18	\$0.73 \$1.82	\$8.00 \$20.00	Each
Light Truck/4x4 on rim Heavy Truck	\$16.36	\$1.64	\$20.00	Each Each
Heavy Truck on rim	\$10.30	\$3.18	\$15.00	Each
		\$3.55	\$39.00	Each
Super Singles	11747			Each
Super Singles	\$35.45 \$72.73		\$80.00	
Super Singles on rim	\$72.73	\$7.27	\$80.00 \$44.00	
Super Singles on rim Tractor – Small/Medium	\$72.73 \$40.00	\$7.27 \$4.00	\$44.00	Each
Super Singles on rim	\$72.73	\$7.27 \$4.00 \$5.91		
Super Singles on rim Tractor – Small/Medium Tractor – Large Other (Tractor Earthmoving)	\$72.73 \$40.00	\$7.27 \$4.00 \$5.91	\$44.00 \$65.00	Each Each
Super Singles on rim Tractor – Small/Medium Tractor – Large Other (Tractor Earthmoving)	\$72.73 \$40.00	\$7.27 \$4.00 \$5.91	\$44.00 \$65.00	Each Each
Super Singles on rim Tractor – Small/Medium Tractor – Large Other (Tractor Earthmoving) Other Waste	\$72.73 \$40.00 \$59.09	\$7.27 \$4.00 \$5.91	\$44.00 \$65.00 By Assessment	Each Each Each
Super Singles on rim Tractor – Small/Medium Tractor – Large Other (Tractor Earthmoving) Other Waste All Mattresses	\$72.73 \$40.00 \$59.09	\$7.27 \$4.00 \$5.91	\$44.00 \$65.00 By Assessment \$28.00	Each Each Each
Super Singles on rim Tractor – Small/Medium Tractor – Large Other (Tractor Earthmoving) Other Waste All Mattresses Recyclable Materials	\$72.73 \$40.00 \$59.09	\$7.27 \$4.00 \$5.91	\$44.00 \$65.00 By Assessment \$28.00	Each Each Each

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

All Waste Facilities

Other Waste

Domestic (> 20 It or kg) and Commercial (any amount)

Vehicle Batteries			No Charge	
Single Use Batteries			No Charge	
Rechargeable Batteries			No Charge	
Fluorescent Tubes and Lighting Fittings	\$0.45	\$0.05	\$0.50	Per Unit
Smoke Detectors	\$0.91	\$0.09	\$1.00	Per Unit
Paint	\$1.82	\$0.18	\$2.00	Per Litre
Minimum charge - \$5				
Hazardous By-Catch Materials	\$1.82	\$0.18	\$2.00	Per Litre
By-catch material includes Flammable liquid other than paint, Toxic, Ox does not include radioactive substances $\hfill\square$	xidising agents,	Corrosives - ac	ids, Corrosives	- alkalis;

Waste Collection Service

Available 7 days per week Port Macquarie area; 6 days per week Wauchope/Camden Haven area

Commercial Premises – 240 Litre General Waste Mobile Bin – per bin	\$10.00	\$1.00	\$11.00	Each Service
Commercial Premises - 660 Litre General Waste Mobile Bin - per bin	\$31.82	\$3.18	\$35.00	Each Service

Asset Management

Geographical Information Systems

Refer Appendix 1

Sale of Maps

Cost estimates provided based on 15 minute intervals

AO		Price	on Application	Each
A1		Each		
A2		Each		
A3		Each		
A4		Price	on Application	Each
Map Customizing (Charged in 15 Minute Modules)	\$100.00	\$0.00	\$100.00	Per Hour
Applies to all map sizes Charged in 15 minute modules				
Minimum charge of \$25 applied to all maps				
Priority Charge	\$200.00	\$0.00	\$200.00	Per Hour
By quotation and based on an hourly rate - Request required within 2 w	vorking days of ac	ceptance of c	uote by both pa	arties

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ORDINARY COUNCIL 16/06/2021

	_			
Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
ALS 2005 Height Data				
ALS 2005 Height Data Extraction Costs	\$100.00	\$0.00	\$100.00	Per Hour
Minimum 1/2 hour				
ALS 2005 Data / Contours	\$130.00	\$0.00	\$130.00	Per 10 Ha
Minimum Area 10Ha up to 100Ha				
\$110/10Ha				
ALS 2005 Data / Contours			By quotation	Each
Areas > 100Ha or 1km2				
All other Digital GIS Data				
Digital Data	\$100.00	\$0.00	\$100.00	Per Hour
Extraction costs				
Digital Data		Price	e on Application	Per Layer
Cost estimates provided based on 15 minute intervals				
Some data has restrictions on access				
Postage and Handling		Price	e on Application	Each
Aerial Photography Imagery extraction process	\$100.00	\$0.00	\$100.00	Per Hour
Minimum 1/2 hour				
Consultancy Provision of specialised GIS services	\$270.00	\$0.00	\$270.00	Per Hour
Minimum 1 hour				
Compliance				
Companion Animals Act				
Registration Fees				
Under Companion Animals Regulation 2018 Cl. 18				
Desexed animal	As per sta	atutory scale o Compan	of fees from the ion Animals Act	Each
Desexed animal – Eligible pensioner	As per sta		of fees from the ion Animals Act	Each
Non – Desexed Animal	As per sta		of fees from the ion Animals Act	Each
continued on next page			Р	age 71 of 14

ORDINARY COUNCIL 16/06/2021

Name Fee	GST	Fee	Unit
(excl. GST)		(incl. GST)	

Registration Fees [continued]

Non – Desexed Animal – Recognised Breeder	As per statutory scale of fees from the Companion Animals Act			Each
Animal Registration Fees – 28 Day Late Fee	\$15.00	\$0.00	\$15.00	As per statutory scale of fees

Annual Permit Fees

Under Companion Animals Regulation 2018 Cl. 27

Annual Permit – Cat Not Desexed	\$80.00	\$0.00	\$80.00	As per statutory scale of fees
Annual Permit – Dangerous Dog	\$195.00	\$0.00	\$195.00	As per statutory scale of fees
Annual Permit – Restricted Dog	\$195.00	\$0.00	\$195.00	As per statutory scale of fees

Other Companion Animal Fees

Companion Animals Act – Dangerous Enclosure Compliance Certificate	\$150.00	\$0.00	\$150.00	As per statutory scale of fees
Under Companion Animals Regulation 2018 CI. 36				
Microchipping	\$36.00	\$0.00	\$36.00	Each
Microchipping – Promotion	\$20.00	\$0.00	\$20.00	Each
Microchipping – Microchipping Day			No Charge	
Animal Pound – Vet Costs			Cost + 10%	
Animal Pound – Merchandise			Cost + 10%	
Release of dog/cat from Pound	\$63.00	\$0.00	\$63.00	Each
Plus maintenance				
Contact of dog/cat owner from registration service of notice	\$47.00	\$0.00	\$47.00	Each
Disposal of dog/cat surrendered to the pound by owner	\$89.00	\$0.00	\$89.00	Each
Hire of feral animal trap	\$15.45	\$1.55	\$17.00	Per Week
Hire of vermin traps	\$13.64	\$1.36	\$15.00	Per Week
Maintenance fee for dogs/cats	\$49.00	\$0.00	\$49.00	Per Day
Sale of doggy leash bags	\$4.55	\$0.45	\$5.00	
Sale of refill bags for doggy leash bags	\$0.91	\$0.09	\$1.00	
Adoption Fee – Cat	\$181.82	\$18.18	\$200.00	Plus registration if required
Adoption Fee – Dog	\$363.64	\$36.36	\$400.00	Plus registration if required

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Item 09.05 Attachment 1

ORDINARY COUNCIL 16/06/2021

		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Impounding

Animals

Sustenance Fee	\$49.00	\$0.00	\$49.00	Refer Comments
Per head, per day or part thereof				
Conveyance Fee	\$49.00	\$0.00	\$49.00	Each
Administration Fee (Plus Actual Cost)				
Conveyance Fee			At Cost	Estimate
Lost or Damaged Admin Fee	\$49.00	\$0.00	\$49.00	Each
Administration Fee (Plus Actual Cost)				
Lost or Damaged Fee			At Cost	Estimate
Notice of Impounding – Administration Fee	\$44.00	\$0.00	\$44.00	Each
Impounding Admin Fee	\$64.00	\$0.00	\$64.00	Each
Administration Fee (Plus Actual Cost)				
Impounding Fee			At Cost	Estimate
Actual Cost of Impounding Item				

Other Articles

Storage Fee	\$49.00	\$0.00	\$49.00	Refer Comments
Per article, per week or part thereof				
Conveyance Admin Fee	\$49.00	\$0.00	\$49.00	Each
Administration Fee (Plus Actual Cost)				
Conveyance Fee			Cost + 10%	Estimate
Lost or Damaged Admin Fee	\$49.00	\$0.00	\$49.00	Each
Administration Fee (Plus Actual Cost)				
Lost or Damaged Fee			Cost + 10%	Estimate + 10%
Notice of Impounding – Administration Fee	\$44.00	\$0.00	\$44.00	Each
Impounding Admin Fee	\$64.00	\$0.00	\$64.00	Each
Administration Fee (Plus Actual Cost)				
Impounding and Recovery Fee			At Cost	Estimate
Actual Cost of Impounding and Recovering Item				

Approvals Under Section 68 Local Government Act

Refer also to Local Government (General) Regulation 2005.

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Section 68				
Approval for septic to sewer conversions for small town sewerage	\$252.00	\$0.00	\$252.00	Each
schemes and other				
Part A – Structures or places of public entertainment				
Notice of completion of a manufactured home, removable dwelling or associated structure on land – inspection fee.	\$186.40	\$0.00	\$186.40	Each
Notice of completion of a manufactured home, removable dwelling or associated structure on land – re-inspection fee.	\$110.00	\$0.00	\$110.00	Each
Application to install a manufactured home, removable dwelling on land or associated structure	\$102.00	\$0.00	\$102.00	Each

Part B - 2. Section 68 Application

Approval to undertake water supply, storm water and/or sewerage works associated with building construction works (Includes Completion Certificate) (i) Application not accompanied by verification certification.

Estimated cost of project

Up to \$15,000	\$240.00	\$0.00	\$240.00	Each
\$15,001 - \$50,000	\$479.00	\$0.00	\$479.00	Each
\$50,001 - \$200,000	\$629.00	\$0.00	\$629.00	Each
\$200,001 - \$500,000	\$1,126.00	\$0.00	\$1,126.00	Each
\$500,001 - \$1,000,000	\$1,661.00	\$0.00	\$1,661.00	Each
\$1,000,001 - \$5,000,000	\$4,296.00	\$0.00	\$4,296.00	Each
\$5,000,001 - \$10,000,000	\$6,848.00	\$0.00	\$6,848.00	Each
> \$10,000,001	\$8,936.00	\$0.00	\$8,936.00	Each

Other

Maximum water and stormwater fee for dwellings not on reticulated sewerage – on-site sewage management fee still applicable where relevant	\$479.00	\$0.00	\$479.00	Each	
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Part B - 2. Section 68 Application

Approval to undertake private storm water works only

Estimated cost of building work

Up to \$50,000	\$181.00	\$0.00	\$181.00	Each
\$50,000 - \$200,000	\$240.00	\$0.00	\$240.00	Each
> \$200,000	\$356.00	\$0.00	\$356.00	Each

Part B - 3. Section 68 Application

Approval to undertake water supply, storm water and/or sewerage works associated with subdivision

continued on next page ...

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Part B – 3. Section 68 Application [continued]

developments (Includes Completion Certificate) (i) Application not accompanied by verification certification.

Estimated cost of project

Up to \$50,000	\$700.00	\$0.00	\$700.00	Each
\$50,001 - \$100,000	\$895.00	\$0.00	\$895.00	Each
\$100,001 - \$150,000	\$1,164.00	\$0.00	\$1,164.00	Each
\$150,001 - \$200,000	\$1,431.00	\$0.00	\$1,431.00	Each
\$200,001 - \$500,000	\$2,498.00	\$0.00	\$2,498.00	Each
\$500,001 - \$1,000,000	\$3,367.00	\$0.00	\$3,367.00	Each
\$1,000,001 - \$5,000,000	\$4,296.00	\$0.00	\$4,296.00	Each
\$5,000,001 - \$10,000,000	\$6,848.00	\$0.00	\$6,848.00	Each
> \$10,000,000	\$8,936.00	\$0.00	\$8,936.00	Each

Part B – 8. Section 68 Application

Approval of the installation of a water meter

Meter size

Multiple services – sizes larger than 20mm		Actual cost	plus \$150.00	Each
New single 20mm service	\$508.00	\$0.00	\$508.00	Each

Part B – 10. Other Services Fees and Charges

Pre-Purchase Inspection – Onsite Sewage Management Facility: Domestic – Standard Service	\$285.00	\$0.00	\$285.00	Each
Includes approval to operate if sale proceeds				
Pre-Purchase Inspection – Onsite Sewage Management Facility: Domestic – Urgent Service	\$427.00	\$0.00	\$427.00	Each
Includes approval to operate if sale proceeds				
Pre-Purchase Inspection – Onsite Sewage Management Facility: Commercial / Industrial – Standard Service	\$435.00	\$0.00	\$435.00	Each
Includes approval to operate if sale proceeds				
Pre-Purchase Inspection – Onsite Sewage Management Facility: Commercial / Industrial – Urgent Service	\$638.00	\$0.00	\$638.00	Each
Includes approval to operate if sale proceeds				
Follow-Up Inspection – Onsite Sewage Management Facility: Domestic	\$153.00	\$0.00	\$153.00	Each
Follow-Up Inspection – Onsite Sewage Management Facility: Commercial / Industrial	\$178.00	\$0.00	\$178.00	Each
Written Report - Septic System Registration	\$51.00	\$0.00	\$51.00	Each
Septic to Sewer Conversion	\$276.00	\$0.00	\$276.00	Each
Consultancy / Advisory Services	\$121.00	\$0.00	\$121.00	Per Hour

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Part B – Section 68 Application – Manhole Adjustment				
Engineering Review & Inspection Fees (Manhole Adjustment)	\$277.00	\$0.00	\$277.00	Each
Fee to be included with Section 68 (Local Government Act) applications i level adjustment	f single resident	ial dwelling re	quires manhole	e surface
Fees in addition to application fee and includes inspections				
Part C – Section 68 Approval Approval to Install, Construct, Alter Sewage Management Fa	cility			
Install, construct and operate a new sewage management facility (Domestic premises)	\$565.00	\$0.00	\$565.00	Each
Includes assessment, 3 x inspections, 1st approval to operate				
Alter an existing sewage management facility (Domestic premises)	\$275.00	\$0.00	\$275.00	Each
Amend or upgrade an existing approval for sewage management facility Includes reassessment and 1 extra inspection				
Operate sewage management facility (Domestic premises)	\$169.00	\$0.00	\$169.00	Each
Approval to operate a sewage management system including change of Includes assessment and 1 inspection	ownership and s	ystem upgrad	e or modificati	on
Install, construct and operate a new sewage management facility (Commercial / Industrial premises)	\$791.00	\$0.00	\$791.00	Each
Includes assessment, 3 x inspections, 1st approval to operate				
Alter an existing sewage management facility (Commercial / Industrial premises)	\$396.00	\$0.00	\$396.00	Each
Amend or upgrade an existing approval for sewage management facility				
Includes reassessment and 1 extra inspection				
Operate sewage management facility (Commercial / industrial premises)	\$210.00	\$0.00	\$210.00	Each
Approval to operate a sewage management system including change of	ownership and s	ystem upgrad	e or modificati	on
Includes assessment and 1 inspection				

Part D – Community Land

Application for an activity on public / community land	\$124.00	\$0.00	\$124.00	Each
Application for A-Frame Sign – 1 Year Licence	\$119.00	\$0.00	\$119.00	Each
Application for A-Frame Sign – 3 Year Licence	\$235.00	\$0.00	\$235.00	Each

Part E – Public Roads

Applications for approval	Refer to the following section of this document - Roads, Bridges and Transport - Section 138 Certificate (Works on Public Road)
	. today

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Part F – Other Activities				
Register a caravan park or manufactured home estate – Minimum Fee PLUS per site	\$116.00	\$0.00	\$116.00	Each
Register a caravan park or manufactured home estate – Per site	\$8.00	\$0.00	\$8.00	Each
Amended application to register a Caravan Park or manufactured home estate – Minimum fee plus per amended/altered site fee	\$126.00	\$0.00	\$126.00	Each
Amended application to register a Caravan Park or manufactured home estate- Per amended/altered site - PLUS:	\$5.08	\$0.00	\$5.08	Each
Caravan Park – Annual Inspection	\$180.00	\$0.00	\$180.00	Each
Inspection of amusement devices	\$52.00	\$0.00	\$52.00	Each
Charitable/Non-Profit Organisations excluded				
Application to burn under POEO	\$150.00	\$0.00	\$150.00	Each
Application - other miscellaneous Section 68 activities	\$126.00	\$0.00	\$126.00	Each

Certification

PCA (where CC/CDC issued by private certifier) – All values	G	ouotation as per	fee calculator	Each
PCA (where CC/CDC issued by private certifier) – Re-inspection fee / minimum inspection fee	C	ouotation as per	fee calculator	Each
PCA Site Signage	C	uotation as per	fee calculator	Each
Certificate of Compliance – associated structure	\$160.91	\$16.09	\$177.00	Each
Certificate of Compliance - sewer connection per site	\$178.00	\$0.00	\$178.00	Each

Complying Development Certificate

Occupation Certificate	Quotation as per fee calculator	Each
All values	Quotation as per fee calculator - discounts available for online lodgement	Each
Amended applications	Quotation as per fee calculator	Each

Construction Certificate (Building)

Class	1	and	10
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All Values	Quotation as per fee calculator	Each
Class 2 – 9		
All Values	Quotation as per fee calculator	Each
Applying to all Part 4A or complying development certi	ficate	

Lodgement Fee	Each
This is a statutory fee under S7.44 of the EP&A Regulation 2000 s263	

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		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Amended Construction Certificate

Minor Change	Quotation as per fee calculator	Each			
Major Change – Original application fee < \$1,000	25% of applied fee	Each			
Major Change – Original application fee > \$1,000	50% of applied fee	Each			
Alternative solutions	See comments	Each			
Fees as per Class 2 - 9 building plus cost for assessment of alternative solution					

Information Services

Advice to building professionals	\$159.09	\$15.91	\$175.00	Each			
Advice regarding compliance with development conditions or construction requirements.							
BCA Advisory service / reports As per fee schedule Each							

Refunds

Development Applications & Construction Certificates

All other cases where no work has commenced	CC's (Building work) Compliance PCA & occupation certificate fees & any non engineering bonds	Each
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Building Certificates

Cancelled / withdrawn with no inspection	\$42.00	\$0.00	\$42.00	Each
Minimum administration fee deducted				
Cancelled / withdrawn prior to determination after inspection			No refund	Each

All other Environment Applications (Including S68)

Cancelled / withdrawn within 48 hours	\$70.00	\$0.00	\$70.00	Each
Administration fee deducted				
50% or \$62 - whichever is greater				
Cancelled / withdrawn prior to inspection			See comments	Each
Inspection fees to be refunded				
All other cases			No refund	Each
Information Services (Minimum)	\$15.00	\$0.00	\$15.00	Each

Food Businesses

Low Risk Premises	\$138.00	\$0.00	\$138.00	Each						
Proprietor will be notified of risk assessment and methodology (determined	ned by Council's	adopted risk	assessment me	ethodology)						
Medium Risk Premises	\$205.00	\$0.00	\$205.00	Each						
				Proprietor will be notified of risk assessment and methodology (determined by Council's adopted risk assessment methodology)						

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Food Businesses [continued]				
High Risk Premises	\$273.00	\$0.00	\$273.00	Each
Proprietor will be notified of risk assessment and methodology (determ	ined by Council	s adopted risk	assessment me	thodology)
Large Commercial Premises	\$396.00	\$0.00	\$396.00	Each
Reinspection		i0% of original the property ri m		Each
Pre-Registration Fee	\$218.00	\$0.00	\$218.00	Each
Professional advice and progress inspections of new businesses				
Includes assessment and site inspections				
Pre-Purchase Inspection Report for Food Handling Business	\$329.00	\$0.00	\$329.00	Each
Written application and current owner consent				
Temporary Event Involving Food (1-3 days p.a maximum)	\$67.00	\$0.00	\$67.00	Per Event
Charities and non-profit organisations exempt unless certificate is requ	ired			
Temporary Event Food Premises (4-50 events p.a)	\$163.00	\$0.00	\$163.00	Per Calendar Year
Mobile Food Vending Vehicle – Annual Approval	\$235.00	\$0.00	\$235.00	Per Calendar Year
Registration of Food Handling Premises	\$67.00	\$0.00	\$67.00	Each
Notification cost per single food premise				
Refer Food Act 2003				
Food Improvement Notice Fee – Regulated	\$330.00	\$0.00	\$330.00	Each

Public Health

Businesses that have a public health impact e.g. skin penetration services, public and semi-public pools, regulated heating and cooling systems, brothels, barbers, beauty salons, etc.

Legionella Annual Administration Fee	\$101.50	\$0.00	\$101.50	each		
Notification of Cooling Tower/Warm Water System	\$116.50	\$0.00	\$116.50	per notification		
Pre-Purchase Public Health Inspection	\$344.00	\$0.00	\$344.00	Each		
Written application and current owners consent						
Notification / Registration of Public Health Business	\$102.00	\$0.00	\$102.00	Each		
A one-off fee for notification/registration of a business that has a public health impact						
Public Health Inspection: Reinspection			Calculate	Each		
Charge is 50% of the original inspection fee plus the cost of relevant la	boratory testing					
Improvement Notice: Regulated System	\$560.00	\$0.00	\$560.00	Each		
Improvement Notice: Other	\$270.00	\$0.00	\$270.00	Each		
Prohibition Order: Regulated System	\$560.00	\$0.00	\$560.00	Each		
Prohibition Order: Other	\$270.00	\$0.00	\$270.00	Each		

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Public Health [continued]				
				-
Prohibition Order: Reinspection	Statutory fee. Maximum cl		ge is 1/2 hour. rs at \$250 per hour	Per Hour
Public Health Inspection				
Proprietor will be notified of risk assessment and metho	doloav.			
Low Risk Premises	\$191.00	\$0.00	\$191.00	Each
Plus relevant laboratory testing costs	¢101.00	\$0.00	¢101.00	Luon
Medium Risk Premises	\$254.00	\$0.00	\$254.00	Each
Plus relevant laboratory testing costs		+		
High Risk Premises	\$307.00	\$0.00	\$307.00	Each
Plus relevant laboratory testing costs		+	•••••	
Swimming Pools Initial Inspection of Swimming Pool Barrier	\$227.27	\$22.73	\$250.00	per
Initial inspection of Swithining Pool Damer	φΖΖΙ.ΖΙ	<i>ΨΖΖ.15</i>	\$250.00	inspection
\$100 refunded if pool passes first inspection				
Subsequent Inspection of Swimming Pool Barrier	\$90.91	\$9.09	\$100.00	per inspection
Fee to Register a Swimming Pool	\$9.09	\$0.91	\$10.00	Each
Application for Exemption	\$250.00	\$0.00	\$250.00	Each
Fire Safety				
Fire Safety Inspection – request by owner Minimum	\$229.09	\$22.91	\$252.00	Each
Annual Fire Safety Inspection	\$143.64	\$14.36	\$158.00	Per Hour
Preparation of fire safety report			Calculate	Each
Assessment of fire safety report - Minimum	\$95.45	\$9.55	\$105.00	Each
Assessment of fire safety report	\$110.00	\$11.00	\$121.00	Per Hour
Fire Safety Statement	\$74.55	\$7.45	\$82.00	Each
Refer EPA Reg 2000 - REG 177				
Environment				
Environmental Inspection Fee	\$250.00	\$0.00	\$250.00	per inspection
Local Government Act Order (OSM) Administration Fee	\$200.00	\$0.00	\$200.00	per order
Overgrown Land – according to LGA (Clause 124)	\$138.00	\$0.00	\$138.00	Each
A desiriate the fee DLUC Classing of land by sector step (where and	E			

Administration fee PLUS Clearing of land by contractor (where applicable)

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Environment [continued]

Overgrown Land			Cost + 10%	Each
Clearing of land by contractor (where applicable)				
Issue of Environmental Protection Notices	\$591.00	\$0.00	\$591.00	As per statutory scale of fees
Protection of the Environment Operations (General) Regulation 2009				
As per statutory scale of fees				

Certificates

Outstanding notices	\$145.00	\$0.00	\$145.00	Each	
Section 735A Certificate (Local Govt Act) and Section 121ZP Certificate (EPA Act)					
Application for Building Certificate (including 1st inspection)	\$250.00	\$0.00	\$250.00	Each	
In accordance with Clause 260 of the Environmental Planning & Assess	sment Regulatio	n (2000)			
Re-inspection Fee – Building Certificate Application	\$90.00	\$0.00	\$90.00	Each	
In accordance with Clause 260 of the Environmental Planning & Assessment Regulation (2000)					
Copy of Building Certificate	\$13.00	\$0.00	\$13.00	Each	
Refer EPA Reg 2000 cl 261					

Development Records

House Drainage Plan	\$38.00	\$0.00	\$38.00	Each			
	A diagram of the pipes within the house and yard to the service connection point. If you want a sewer junction plan showing the location of the connection point to the Council sewer main, please refer to the Water Section.						

Natural Resource Management

Tree Management (Private Land)

Application for permit to remove or prune trees	\$95.00	\$0.00	\$95.00	Per Application
1 - 5 trees				
Application for permit to remove or prune trees – accompanied by qualified Arborist's report	\$40.00	\$0.00	\$40.00	Per Application
Application for permit to remove or prune trees in conjunction with erection of a building	\$110.00	\$0.00	\$110.00	Per Application
Combination Tree Pruning / Removal				
Compensatory Tree Offset Planting – Residential Zones	\$29.00	\$0.00	\$29.00	Per Tree
Tree replacement on public lands				

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Tree Management (Private Land) [continued]				
Compensatory Tree Offset Planting - Other Zones	Based		ation for trees ed for removal	Per Tree
Tree replacement on public lands				
Certificates				
Weed Notice / Expenses Certificate	\$200.00	\$0.00	\$200.00	As per statutory scale of fees
Under S28(2) Schedule 7 of the Biosecurity Act				

Roads, Bridges & Transport

Approvals under Roads Act 1993

Events within the Glasshouse Forecourt and Town Square

Application Under Section 138 must be made

Glasshouse Forecourt Bollard Key Deposit	\$50.00	\$0.00	\$50.00	Per Application
Deposit for the use of the Bollard Key held by Infrastructure Planning G	Froup. The depos	sit will be refur	nded on return of	f the key.
Special Event Glasshouse Hay Street Reserve Usage Fee – General Use	\$172.73	\$17.27	\$190.00	Per Day
Special Event Glasshouse Hay Street Reserve Usage Fee – Commercial Use	\$418.18	\$41.82	\$460.00	Per Day
Special Event Glasshouse Hay Street Reserve Usage Fee – Charitable			No Charge	Per Day
Event Town Square Reserve Usage Fee – No Closure or Quarter Use – General Use	\$172.73	\$17.27	\$190.00	Per Day
Event Town Square Reserve Usage Fee – Half Closure – General Use	\$190.00	\$19.00	\$209.00	Per Day
Event Town Square Reserve Usage Fee – Full Closure – General Use	\$198.64	\$19.86	\$218.50	Per Day
Event Town Square Reserve Usage Fee – No Closure or Quarter Closure – Commercial Use	\$418.18	\$41.82	\$460.00	Per Day
Event Town Square Reserve Usage Fee – Half Closure – Commercial Use	\$460.00	\$46.00	\$506.00	Per Day
Event Town Square Reserve Usage Fee – Full Closure – Commercial Use	\$480.91	\$48.09	\$529.00	Per Day
Event Town Square Reserve Usage Fee – No Closure or Quarter Closure – Charitable			No Charge	Per Day
Event Town Square Reserve Usage Fee - Half Closure - Charitable			No Charge	Per Day
Event Town Square Reserve Usage Fee - Full Closure - Charitable			No Charge	Per Day
Occupation Bond	\$0 - \$2,000 de	etermined acc	ording to event	Per Event

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
REF for Works within Council Public Road				
Section 138 REF in Relation to Works within Council Public Road(s)	\$437.00	\$0.00	\$437.00	Per REF
Where works are to be undertaken by a party other than Council that recommencement - for example vegetation clearing of road reserve along fences.				pairing stock
Section 138 – Cattle Grid/Gates within a Public Road				
Application Fee, Pre-Approval Inspection Fee and Post-Approval Inspection Fee	\$501.00	\$0.00	\$501.00	Per Application
Traffic Management				
Refer Appendix 9				
Application Fees				
Base application fee (plus calculated additional occupancy fees)	\$193.00	\$0.00	\$193.00	Each
All Occupancies				
Minimum				
Road occupancy / opening < 1 week	\$193.00	\$0.00	\$193.00	Each
Road occupancy / opening > 1 week	\$431.00	\$0.00	\$431.00	Each
Standing plant < 1 week	\$193.00	\$0.00	\$193.00	Each
Standing plant > 1 week	\$431.00	\$0.00	\$431.00	Each
Work zone	\$431.00	\$0.00	\$431.00	Each
Occupancy of Car Park	\$193.00	\$0.00	\$193.00	Each
<i>Occupancy Fees</i> Minimum 1 Day				
Footpath / Nature Strip (Other Than for Vehicle Crossing)	\$2.55	\$0.00	\$2.55	Per Metre Per Day
Non residential zone (more than 3 days) Minimum 6 metres				
Parking Lane – Residential Zone (Other Than Vehicle Crossing)	\$2.55	\$0.00	\$2.55	Per Metre Per Day
With no parking restriction (more than 1 week) Minimum 6 metres				
Parking Lane – Residential Zone (Other Than Vehicle Crossing)	\$2.55	\$0.00	\$2.55	Per Metre Per Day
With parking restrictions Minimum 6 metres				
Parking Lane – Industrial and CBD Areas	\$2.60	\$0.00	\$2.60	Per Metre Per Day
No parking restrictions (less than 3 days) Minimum 6 metres				

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Item 09.05 Attachment 1 Page 96

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Occupancy Fees [continued]				
Parking Lane – Industrial and CBD Areas	\$4.90	\$0.00	\$4.90	Per Metre Per Day
No parking restrictions (more than 3 days) Minimum 6 metres				
Parking Lane – Industrial and CBD Areas	\$4.90	\$0.00	\$4.90	Per Metre Per Day
With parking restriction Minimum 6 metres				
Trafficable Lane Occupancy	\$10.40	\$0.00	\$10.40	Per Metre Per Day
Minimum 6 metres				
Standing Plant (Skips, Concrete Pumps, Cranes, etc)	\$2.60	\$0.00	\$2.60	Per Metre Length of Plant Per Day
Minimum 5 metres				
Work Zone (CBD and Shopping Precincts)	\$27.20	\$0.00	\$27.20	Per Metre Per Day
Standing plant fees additional				
Work Zone (Others Area)	\$14.05	\$0.00	\$14.05	Per Metre Per Day
Standing plant fees additional				

Security Bonds

Traffic Management Bond Administration Fee	\$279.00	\$0.00	\$279.00	Per Bond	
Applicable to all bonds					
Security Bond – For Damage to Public Infrastructure		inimum \$2,000. week, calculate	Each		
Plant and Equipment Occupation (Standing Plant) within Road Reserve (including CBD) Bond	\$2,000.00	\$0.00	\$2,000.00	Per Application	
Note this is different from physical works on the road reserve.					
Works on Road Reserve – Located within CBD – General Bond	\$10,000.00	\$0.00	\$10,000.00	Per Application	
This bond is applied where works require the use of heavy plant, equipment and or scaffolding within the road reserve within the CBD					
Works within Road Reserve – Location other than CBD – General Bond	\$2,000.00	\$0.00	\$2,000.00	Per Application	
This bond may be applied where works require the use of heavy plant, equipment and or scaffolding within the road reserve outside of the CBD					
Large events, and large events within Glasshouse Forecourt and Town Green Parking Areas – General Bond Area	\$2,000.00	\$0.00	\$2,000.00	Per Application	
This bond may be applied where a bump in and out requires the use of possibility of Council assets being damaged	large vehicles/fo	rk lifts etc. ar	nd/or where there	e is a	

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		Year 21/22		
Name	Fee	GST	Fee (incl. GST)	Unit
	(excl. GST)		(Incl. GST)	

Street Banner Poles

Street Banner Poles – Banner Change Over Fee	\$70.00	\$0.00	\$70.00	per banner
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Directions Signage

Community, Service & Tourism Signage

Application and Sign Fee for New Community, Service and Tourism Signage

One sign	\$210.00	\$21.00	\$231.00	Per Application
Includes application fee and supply & installation of one sign				
Two or three signs	\$354.55	\$35.45	\$390.00	Per Application

Private Works – Restoration of Road Openings in Public Roads Maintained by Council

Refer to Appendix 5

Restoration of Road Openings up to 10m2

Application Administration Fee – Restoration of Damaged Assets	\$500.50	\$0.00	\$500.50	Per Application
Fees relating to internal costs incurred by Council including inspection of to Council's Traffic & Stormwater Network Assets.	of works and org	anisation of C	ouncil staff to re	pair damage

Type of Road Pavement

Asphaltic concrete with cement concrete base	\$425.00	\$0.00	\$425.00	Per m2
Tar and bitumous surfaces on all classes of base other than cement concrete	\$157.50	\$0.00	\$157.50	Per m2
Earth and gravel, waterbound macadam and all other classes of unsealed pavement or shoulders	\$66.00	\$0.00	\$66.00	Per m2

Kerbing, Guttering Construction

Contribution from land owners for Kerb & Guttering

Frontage	\$83.00	\$0.00	\$83.00	Per Lineal Metre
Side and/or Rear	\$51.00	\$0.00	\$51.00	Per Lineal Metre

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Street Stalls				
Street Stalls			No Charge	Per Day
Management and Investigation of Street Lighti Application Fee Approved Essential Energy Glare Shield Installation	ng Glare Is \$171.82 \$509.09	\$ SUE \$17.18 \$50.91	\$189.00 \$560.00	Each Each
Essential Energy tariff (\$506.56) + 10%				
Detailed Street Lighting Design Fee	\$1,818.18	\$181.82	\$2,000.00	Each
Required for V category lighting on high traffic volume (collector/sub ar levels) areas needing assessment. Accrediated electrical/lighting desig including Essential Energy authorisation costs.				
New Ilumanire (Light) as per Design		Essential	Energy + 10%	Each
Note this is where no approved glare shield is applicable.				

Ferries

Ferry Services

Refer Appendix 3

Standard Full Fee

Cars/Utilities Annual Ticket	\$909.09	\$90.91	\$1,000.00	Annually
Unlimited use				
Fee will be quarterly pro-rated - pass issued for financial year				
Cars/Utilities Monthly Ticket	\$90.91	\$9.09	\$100.00	Monthly
Unlimited use				
Ticket is issued for the calendar month				
Cars/Utilities Weekly Ticket	\$36.36	\$3.64	\$40.00	Weekly
Unlimited use				
Ticket is issued for the week, Monday to Sunday				
Car/Utilities	\$4.55	\$0.45	\$5.00	Per Trip
Single Trip				
Caravans/Trailers	\$4.55	\$0.45	\$5.00	Per Trip
Extra Charge per Trip				
Trucks > 4t GVM Approx. 2t Payload	\$9.09	\$0.91	\$10.00	Per Trip
Single Trip				
Semi-Trailers	\$13.64	\$1.36	\$15.00	Per Trip
Single Trip				

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Standard Full Fee [continued]				
Motorcycles	\$1.82	\$0.18	\$2.00	Per Trip
Single Trip				
Resident/Ratepayer Concession Holders				
Annual Concession Stickers	\$50.00	\$5.00	\$55.00	Each
Based on a financial year				
Fee will be quarterly pro-rated				
Concession Annual Pass – Motorcycle	\$159.09	\$15.91	\$175.00	Annually
Unlimited use				
Includes cost for annual concession sticker				
Concession Annual Pass – Cars/Utilities	\$318.18	\$31.82	\$350.00	Annually
Unlimited use				
Includes cost for annual concession sticker				
Concession Annual Pass – Trucks > 4t GVM Approx. 2t Payload	\$954.55	\$95.45	\$1,050.00	Annually
Unlimited use				
Includes cost for annual concession sticker				
Concession Annual Pass – Semi-Trailer, Dog-Trailer, Articulated Vehicle	\$1,272.73	\$127.27	\$1,400.00	Annually
Unlimited use				
Includes cost for annual concession sticker				
Concession Monthly Ticket – Cars/Utilities	\$27.27	\$2.73	\$30.00	Monthly
Unlimited use	Ψ <u></u> ΓΙ. <u></u> ΓΙ	Q2.10	<i>Q00.00</i>	monuny
Ticket is issued for the calendar month				
Concession Monthly Ticket – Trucks > 4t GVM Approx. 2t Payload	\$81.82	\$8.18	\$90.00	Monthly
Unlimited use				·····,
Ticket is issued for the calendar month				
Concession Monthly Ticket – Semi-Trailer, Dog-Trailer, Articulated Vehicle	\$109.09	\$10.91	\$120.00	Monthly
Unlimited use				
Ticket is issued for the calendar month				
Concession Monthly Ticket – Motorcycles	\$13.64	\$1.36	\$15.00	Monthly
Unlimited use				
Ticket is issued for the calendar month				
Concession Weekly Ticket – Cars/Utilities	\$9.09	\$0.91	\$10.00	Weekly
Unlimited use				

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Resident/Ratepayer Concession Holders [continued]				
Concession Weekly Ticket – Cars/Utilities	\$9.09	\$0.91	\$10.00	Weekly
Unlimited use				
Ticket is issued for the week, Monday to Sunday				
Concession Weekly Ticket – Trucks > 4t GVM Approx. 2t Payload Unlimited use	\$27.27	\$2.73	\$30.00	Weekly
Ticket is issued for the week, Monday to Sunday				
Concession Weekly Ticket - Semi-Trailer, Dog-Trailer, Articulated Vehicle	\$36.36	\$3.64	\$40.00	Weekly
Unlimited use				
Ticket is issued for the week, Monday to Sunday				
Concession Weekly Ticket – Motorcycles Unlimited use	\$4.55	\$0.45	\$5.00	Weekly
Ticket is issued for the week, Monday to Sunday				
Concession Pre-Paid Booklets – 25 Tickets per booklet	\$18.18	\$1.82	\$20.00	Per Booklet
Per Booklet				
Pre-paid tickets must be used for single concession trips				
Concession Car/utilities	\$0.73	\$0.07	\$0.80	1 pre-paid ticket, per trip
Single Trip				
1 pre-paid ticket, handed to ferry operator				
Concession Caravans/trailers	\$0.73	\$0.07	\$0.80	1 pre-paid ticket, per trip
Extra Charge per Trip				
1 pre-paid ticket, handed to ferry operator				
Concession Trucks > 4t GVM approx. 2t payload	\$2.18	\$0.22	\$2.40	3 pre-paid tickets, per trip
Single Trip				
3 pre-paid tickets, handed to ferry operator				
Concession Semi-Trailer, Dog-Trailer, Articulated Vehicle	\$2.91	\$0.29	\$3.20	4 pre-paid tickets, per trip
Single Trip				
4 pre-paid tickets, handed to ferry operator				

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Resident/Ratepayer Concession Holders [continued]				
Concession Motorcycles	\$0.36	\$0.04	\$0.40	1/2 pre-paid ticket, per trip
Single Trip 1/2 pre-paid ticket, handed to ferry operator				
Private Works				
Refer to Appendix 4				
General Fund				
Emergency Works				
Emergency Works		Actu	al Costs + 25%	
Private Works (pre-paid)				
Up to \$20,000			uotation + 18%	
\$20,000 - \$50,000			uotation + 12%	
\$50,000 & above		(Quotation + 6%	
Government Department & Other Local Government In	strumentalit	ies		
Up to \$20,000		Q	uotation + 18%	
\$20,000 - \$50,000			uotation + 12%	
\$50,000 & above			Quotation + 6%	
Sewerage Services				
Interest Rate				
Interest Rate on overdue rates & charges				
Sewer Annual Charges			6%	
As per the rate determined by the Division of Local Government				
Wastewater Annual Charges & Usage Fees				
Section 501, 502 & 608 Local Government Act, 1993				
Annual Charges				
Connected or available other than churches and halls	\$923.70	\$0.00	\$923.70	Per Annum
continued on next page			Ρ	age 89 of 140

ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Annual Charges [continued]				
Non Rateable - Section 555 churches & halls	\$592.80	\$0.00	\$592.80	Per Annum

Non Rateable - Section 555 Other than churches and halls	\$735.40	\$0.00	\$735.40	Per Annum
Non Rateable - Section 556	\$923.70	\$0.00	\$923.70	Per Annum
Unconnected – Section 548A	\$592.80	\$0.00	\$592.80	Per Annum
Pressure Sewer	\$893.70	\$0.00	\$893.70	Per Annum

Usage Fees

Caravan Parks, Retirement Homes, Motels, Clubs, Hotels, Schools & Backpackers Hostels, Flats, Granny Flats, Secondary Dwellings, Multi Dwelling Housing, Dual Occupancy (where not on separate	\$1.37	\$0.00	\$1.37	C per kl
sewer connections), Major Regional Shopping Centres				

On-site Sewerage Management System

Annual Charges

Operate on-site sewage management system

Residential

Low Risk System	\$39.80	\$0.00	\$39.80	Each
Medium Risk System	\$78.50	\$0.00	\$78.50	Each
High Risk System	\$112.20	\$0.00	\$112.20	Each

Commercial

Small	\$224.40	\$0.00	\$224.40	Each
Large	\$279.50	\$0.00	\$279.50	Each

Recycled Water

Port Macquarie Urban Area Business/Commercial/Council Facilities Only (Usage Charge - Refer to Appendix 8 for Usage Allowance)

Annual Availability Charge

Charge Per Meter

50% of Potable Water Charge

Usage Fees

50% of Potable Water Charge

		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Residential Properties to be Serviced by Recycled Water (i.e. Dual Scheme with Two Meters per Property)

Section 501, 502 & 556 of the Local Government Act, 1993

Annual Availability Charge

Charge Per Meter

80% of Potable Water Charge

Usage Charges

80% of Potable Water Charge

Approvals Under Section 68 Local Government Act, 1993

Refer also to Local Government (Approvals) Regulation 1999.

Part B - 7. Approval to Discharge Waste to a Council Sewer

Treatment Facility Type/Development Type

Application requiring State Government concurrence	\$587.00	\$0.00	\$587.00	Each
Application not requiring State Government concurrence	\$321.00	\$0.00	\$321.00	Each

Septic Sewage Fees

Pans & Effluent	\$2.54	\$0.00	\$2.54	Per kl
Sludge (septic)	\$26.00	\$0.00	\$26.00	Per kl

Trade Waste Charges

Refer also Policy T2

Annual Inspection Charge Category 1 & 2	\$208.00	\$0.00	\$208.00	Each
Annual Inspection Charge Category 3	\$638.00	\$0.00	\$638.00	Each
Biochemical Oxygen Demand (BOD)	\$0.83	\$0.00	\$0.83	Per kg
Total Dissolved Solids (TDS)	\$1.12	\$0.00	\$1.12	Per kg
Total Suspended Solids (TSS)	\$0.96	\$0.00	\$0.96	Per kg
Oil & Grease	\$1.40	\$0.00	\$1.40	Per kg
Re-inspection Fee	\$111.00	\$0.00	\$111.00	Each
Operating Trade Waste Charge - Volume with Pre-Treatment	\$1.84	\$0.00	\$1.84	Per kl
Operating Trade Waste Charge - Volume with No Treatment	\$16.80	\$0.00	\$16.80	Per kl
pH Coefficient	\$0.47	\$0.00	\$0.47	Constant
refer to PMHC Trade Waste Policy				

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ORDINARY COUNCIL 16/06/2021

		Year 21/22		
Name	Fee	GST	Fee	Unit
	(excl. GST)		(incl. GST)	

Private Works - Sewer Fund

Emergency Works

Emergency Works	Actual Costs + 25%	
Emorgoney frome	/ total 00010 * 2070	

Private Works (pre-paid)

Up to \$20,000	Quotation + 18%	
\$20,000 - \$50,000	Quotation + 12%	
\$50,000 & above	Quotation + 6%	

Government Department & Other Local Government Instrumentalities

Up to \$20,000	Quotation + 18%	
\$20,000 - \$50,000	Quotation + 12%	
\$50,000 & above	Quotation + 6%	

Miscellaneous Sewerage Fees and Charges

Sewerage Pumping Station Commissioning	\$372.00	\$0.00	\$372.00	Per pump station
Effluent Reuse	\$0.15	\$0.00	\$0.15	Per kl
Not recycled quality				
Camera investigation - of sewer drainage	\$192.00	\$0.00	\$192.00	Per Hour
Minimum 2 Hours				
Sewer Main Pressure Testing	\$146.00	\$0.00	\$146.00	Per Hour
Hourly rate for all time spent setting up and pressure testing main.				
Sewer Junction Location – Mailed	\$38.00	\$0.00	\$38.00	Each
Sewer Junction Location - Letter to Customer Advising not Available	\$38.00	\$0.00	\$38.00	Each
Sewer Junction Location – Expedition Charge – Response Required for 24 Hour Pick Up	\$51.00	\$0.00	\$51.00	Each

Stormwater & Drainage

Interest Rate

Interest Rate on overdue rates & charges

Stormwater Management Annual Charge	6%	
As per the rate determined by the Division of Local Government		

Annual Stormwater Charge

Section 496A of Local Government Act, 1993

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Business				
Between 0 & 350m2	\$25.00	\$0.00	\$25.00	Per Annum
Between 351m2 & 700m2	\$50.00	\$0.00	\$50.00	Per Annum
Between 701m2 & 1,000m2	\$75.00	\$0.00	\$75.00	Per Annum
Between 1,001m2 & 1,400m2	\$100.00	\$0.00	\$100.00	Per Annum
Between 1,401m2 & 1,750m2	\$125.00	\$0.00	\$125.00	Per Annum
Between 1,751m2 & 2,100m2	\$150.00	\$0.00	\$150.00	Per Annum
Between 2,101m2 & 2,450m2	\$175.00	\$0.00	\$175.00	Per Annum
Between 2,451m2 & 2,800m2	\$200.00	\$0.00	\$200.00	Per Annum
Between 2,801m2 & 3,200m2	\$225.00	\$0.00	\$225.00	Per Annum
Between 3,201m2 & 3,500m2	\$250.00	\$0.00	\$250.00	Per Annum
Between 3,501m2 & 3,850m2	\$275.00	\$0.00	\$275.00	Per Annum
Between 3,851m2 & 4,200m2	\$300.00	\$0.00	\$300.00	Per Annum
Between 4,201m2 & 4,600m2	\$325.00	\$0.00	\$325.00	Per Annum
Between 4,601m2 & 4,900m2	\$350.00	\$0.00	\$350.00	Per Annum
Between 4,901m2 & 5,200m2	\$375.00	\$0.00	\$375.00	Per Annum
Between 5,201m2 & 5,600m2	\$400.00	\$0.00	\$400.00	Per Annum
Between 5,601m2 & 5,950m2	\$425.00	\$0.00	\$425.00	Per Annum
Between 5,951m2 & 6,300m2	\$450.00	\$0.00	\$450.00	Per Annum
Between 6,301m2 & 6,600m2	\$475.00	\$0.00	\$475.00	Per Annum
Greater than 6,600m2	\$500.00	\$0.00	\$500.00	Per Annum
Business Strata				
Business Strata	\$12.50	\$0.00	\$12.50	Per Annum
Residential				
Residential	\$25.00	\$0.00	\$25.00	Per Annum

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Residential Strata				
Residential Strata	\$12.50	\$0.00	\$12.50	Per Annum
Stormwater Asset Investigations				
Camera Investigation of Stormwater Drainage Assets	\$16.36	\$1.64	\$18.00	Per Lineal Metre
Water Supply				
Interest Rate				
Interest Rate on overdue rates & charges				
Water Annual Charges As per the rate determined by the Division of Local Government			6%	
Water Charges & Essa				
Water Charges & Fees Annual Availability Charge				
Section 501, 502 & 556 of Local Government Act, 1993				
Community Service Organisations			No Charge	Per
Section 555				Annum
Equivalent Tenements	\$171.00	\$0.00	\$171.00	Per Annum
Strata Units	\$171.00	\$0.00	\$171.00	Per Annum
Not individually metered				
Vacant Land	\$171.00	\$0.00	\$171.00	Per Annum
Other than land that complies with Section 548A				
20 mm Meter	\$171.00	\$0.00	\$171.00	Per Annum
25 mm Meter	\$272.00	\$0.00	\$272.00	Per Annum
32 mm Meter	\$453.00	\$0.00	\$453.00	Per Annum
				-

40 mm Meter	\$714.00	\$0.00	\$714.00
50 mm Meter	\$1,124.00	\$0.00	\$1,124.00
80 mm Meter	\$2,889.00	\$0.00	\$2,889.00

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Per Annum

Per Annum Per Annum

ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Annual Availability Charge [continued]				
100 mm Meter	\$4,517.00	\$0.00	\$4,517.00	Per Annum
150 mm Meter	\$10,185.00	\$0.00	\$10,185.00	Per Annum
200 mm Meter	\$18,110.00	\$0.00	\$18,110.00	Per Annum
Usage Fees				
Refer Appendix 6 for Usage Allowance				
Consumption below usage allowance				
Water Consumption – Tier 1	\$3.45	\$0.00	\$3.45	Per kl
Water Consumption – Tier 2	\$6.90	\$0.00	\$6.90	Per kl
Fire Service Levy Annual Charges Charge per Meter Size 50% of the potable water charge				
32 mm Meter	\$226.50	\$0.00	\$226.50	Per Annum
40 mm Meter	\$357.00	\$0.00	\$357.00	Per Annum
50 mm Meter	\$562.00	\$0.00	\$562.00	Per Annum
80 mm Meter	\$1,444.50	\$0.00	\$1,444.50	Per Annum
100 mm Meter	\$2,258.50	\$0.00	\$2,258.50	Per Annum
150 mm Meter	\$5,092.50	\$0.00	\$5,092.50	Per Annum
200mm Meter	\$9,055.00	\$0.00	\$9,055.00	Per Annum

Recycled Water Charges

Port Macquarie Urban Area Business/Commercial/Council Facilities Only

Annual Availability Charge

20 mm Meter	\$85.50	\$0.00	\$85.50	Per Annum
25 mm Meter	\$136.00	\$0.00	\$136.00	Per Annum
32 mm Meter	\$226.50	\$0.00	\$226.50	Per Annum

continued on next page ...

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Annual Availability Charge [continued]				
40 mm Meter	\$357.00	\$0.00	\$357.00	Per Annum
50 mm Meter	\$562.00	\$0.00	\$562.00	Per Annum
80 mm Meter	\$1,444.50	\$0.00	\$1,444.50	Per Annum
100 mm Meter	\$2,258.50	\$0.00	\$2,258.50	Per Annum
150 mm Meter	\$5,092.50	\$0.00	\$5,092.50	Per Annum
200 mm Meter	\$9,055.00	\$0.00	\$9,055.00	Per Annum
Usage Fees				
Refer to Appendix 7				
Recycled Water Consumption – Tier 1	\$1.72	\$0.00	\$1.72	Per kl
i.e. Dual Scheme with Two Meters per Property Included in Annual Water Charge		No Ad	ditional Charge	
		110710	ulional onarge	
Private Works – Water Fund				
Emergency Works				
Emergency Works		Actu	al Costs + 25%	
Private Works (pre-paid)				
Up to \$20,000			uotation + 18%	
\$20,000 - \$50,000			uotation + 12%	
\$50,000 & above			Quotation + 6%	
Government Department & Other Local Government I	nstrumentaliti	ies		
Up to \$20,000		Q	uotation + 18%	
\$20,000 - \$50,000		Q	uotation + 12%	
\$50,000 & above			Quotation + 6%	
Miscellaneous Water Fees and Charges				
Service Fee to be charged upon the issue of a Final Notice of Intention to Disconnect Water	\$64.00	\$0.00	\$64.00	Each
Disconnection/Reconnection of Water Supply	\$127.00	\$0.00	\$127.00	Each

continued on next page ...

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Viscellaneous Water Fees and Charges [contin	nued]			
Supervision of Interconnection to existing main or Supervision of Pressure Testing	\$127.00	\$0.00	\$127.00	Per Hour
For each connection, includes notification and supervision of interconr to be reapplied if site is not available within 1/2 hour of agreed time Recovering actual cost	nection or pressu	e testing by c	ontractor. NOTI	E: This fee is
Water Main Pressure Testing	\$146.00	\$0.00	\$146.00	Per Hour
Hourly rate for all time spent setting up and pressure testing main.	ψ1 1 0.00	ψ0.00	ψ1 4 0.00	T CI TIOUI
Sale of in-house water meters	\$100.91	\$10.09	\$111.00	Each
Remote water meter transmitter hire	\$120.00	\$0.00	\$120.00	Each
Available areas only				
Consultancy – Investigation and Design	\$178.18	\$17.82	\$196.00	Per Hour
Vacuum Excavation Truck	\$225.45	\$22.55	\$248.00	Per Hour
Includes Plant & Operator cost				
Tanker Truck	\$104.55	\$10.45	\$115.00	Per Hour
Includes Plant and Operator cost				
Water Usage Printout – Standard Delivery	\$40.00	\$0.00	\$40.00	Each
Water Usage Printout – Urgent Delivery	\$40.00	\$0.00	\$40.00	Each
Disinfection of Water Mains Minimum Fee	\$932.00	\$0.00	\$932.00	Each
Lineal Metre rate	\$8.00	\$0.00	\$8.00	Lineal Metre
Re-establishment fee	\$468.00	\$0.00	\$468.00	Each
Fee to be applied if site not available within 1/2 hour of agreed time				
Tanker Hire for Disinfection of Water Mains	\$115.00	\$0.00	\$115.00	Per Hour
_eakages				
Water Leakage Investigations	\$127.27	\$12.73	\$140.00	Per Hour
Hire of equipment plus operator				
Minimum 1 hour				
Swimming Pools				
Water directly from main to fill Residential Swimming Pool	\$656.00	\$0.00	\$656.00	
esting of Water Meters				
20 mm	\$396.00	\$0.00	\$396.00	Each
Deposit, if the meter is found to be faulty and over-registering, this dep	posit will be refun	ded		
ontinued on next page			I	Page 97 of 14

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Testing of Water Meters [continued]				
Greater than 25 mm			Actual Cost	Each
Registration of Backflow Prevention Devices	\$70.00	\$0.00	\$70.00	Each
Testing of Backflow Prevention Devices	\$133.00	\$0.00	\$133.00	Each
Water Meter Readings				
Additional Water Meter Reading Requests - Standard Delivery	\$70.00	\$0.00	\$70.00	Each
Additional Water Meter Reading Requests – Urgent Delivery	\$140.00	\$0.00	\$140.00	Each
Private water meter readings	\$2.50	\$0.00	\$2.50	Per Meter
Water from Mains				
Water Carter's Licence Fee – Potable	\$1,021.00	\$0.00	\$1,021.00	Per Annum
Private Water Carters Licence Fee	\$642.00	\$0.00	\$642.00	Per Annum
Water Meter Installation	¢409.00	¢0.00	¢400.00	
Lowering existing 20mm Water Meter into a Non-Trafficable Pit	\$498.00	\$0.00	\$498.00	per service
Replacement of a Stolen 20mm Water Meter New single 20 mm Potable Water Meter	\$143.24 \$556.80	\$0.00 \$0.00	\$143.24 \$556.80	per meter Each
New single 20 mm Potable Water Meter	\$556.80	\$0.00	\$556.80	Each
20mm Short Service Additional – Potable/Recycled	\$719.24	\$0.00	\$719.24	Each
All services terminate prior to 90 bend to raise out of ground.	Ψ/ 13.2 4	ψ0.00	Ψ/13.2 4	Lach
20mm Long Service Additional – Potable/Recycled	\$2,307.54	\$0.00	\$2,307.54	Each
All services terminate prior to 90 bend to raise out of ground.	Ψ2,507.54	.00.00	Ψ2,007.04	Lach
20mm RPZ Backflow Prevention Device	\$356.77	\$0.00	\$356.77	Each
Does not include labour.				
20mm DCV Backflow Prevention Device	\$306.80	\$0.00	\$306.80	Each
Does not include labour.				
25mm Short Service – Potable/Recycled (Hydraulic Plans to Support)	\$804.59	\$0.00	\$804.59	Each
All services terminate prior to 90 bend to raise out of ground.				
25mm Long Service – Potable/Recycled (Hydraulic Plans to Support)	\$2,671.38	\$0.00	\$2,671.38	Each
All services terminate prior to 90 bend to raise out of ground.				
25mm Water Meter – Potable/Recycled (Hydraulic Plans to Support)	\$671.30	\$0.00	\$671.30	Each
All meters include cost of meter frame beginning at the 90 bend.				
25mm RPZ Backflow Prevention Device	\$397.03	\$0.00	\$397.03	Each
Does not include labour.				

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ORDINARY COUNCIL 16/06/2021

Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Vater Meter Installation [continued]				
25mm DCV Backflow Prevention Device	\$326.23	\$0.00	\$326.23	Each
Does not include labour.				
32mm Short Service – Potable/Recycled (Hydraulic Plans to Support)	\$906.59	\$0.00	\$906.59	Each
All services terminate prior to 90 bend to raise out of ground.				
32mm Long Service – Potable/Recycled (Hydraulic Plans to Support)	\$2,810.36	\$0.00	\$2,810.36	Each
All services terminate prior to 90 bend to raise out of ground.				
32mm Water Meter– Potable/Recycled (Hydraulic Plans to Support) All meters include cost of meter frame beginning at the 90 bend.	\$1,306.06	\$0.00	\$1,306.06	Each
32mm RPZ Backflow Prevention Device	\$590.00	\$0.00	\$590.00	Each
Does not include labour.		\$0.00		_ 10.7
32mm DCV Backflow Prevention Device	\$512.26	\$0.00	\$512.26	Each
Does not include labour.	WOTZ.20	ψ0.00	\$ \$12.25	Luch
40mm Short Service – Potable/Recycled (Hydraulic Plans to Support)	\$994.48	\$0.00	\$994.48	Each
All services terminate prior to 90 bend to raise out of ground.				
40mm Long Service – Potable/Recycled (Hydraulic Plans to Support)	\$2,940.75	\$0.00	\$2,940.75	Each
All services terminate prior to 90 bend to raise out of ground.				
40mm Water Meter – Potable/Recycled (Hydraulic Plans to Support)	\$1,408.08	\$0.00	\$1,408.08	Each
All meters include cost of meter frame beginning at the 90 bend.				
40mm RPZ Backflow Prevention Device	\$649.58	\$0.00	\$649.58	Each
Does not include labour.				
40mm DCV Backflow Prevention Device	\$567.79	\$0.00	\$567.79	Each
Does not include labour.				
50mm Short Service – Potable/Recycled (Hydraulic Plans to Support)	\$1,335.81	\$0.00	\$1,335.81	Each
All services terminate prior to 90 bend to raise out of ground.				
50mm Long Service – Potable/Recycled (Hydraulic Plans to Support)	\$3,366.93	\$0.00	\$3,366.93	Each
All services terminate prior to 90 bend to raise out of ground.				
50mm Water Meter – Potable/Recycled (Hydraulic Plans to Support)	\$3,226.84	\$0.00	\$3,226.84	Each
All meters include cost of meter frame beginning at the 90 bend.				
50mm RPZ Backflow Prevention Device	\$744.10	\$0.00	\$744.10	Each
Does not include labour.				
50mm DCV Backflow Prevention Device	\$634.43	\$0.00	\$634.43	Each
Does not include labour.				
80mm Short Service – Potable/Recycled (Hydraulic Plans to Support)	\$2,083.69	\$0.00	\$2,083.69	Each
All services terminate prior to 90 bend to raise out of ground.				

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Name	Fee (excl. GST)	Year 21/22 GST	Fee (incl. GST)	Unit
Water Meter Installation [continued]				
80mm Water Meter – Potable/Recycled (Hydraulic Plans to Support) All meters include cost of meter frame beginning at the 90 bend.	\$4,149.62	\$0.00	\$4,149.62	Each
80mm RPZ Backflow Prevention Device	\$4,300.53	\$0.00	\$4,300.53	Each
Does not include labour.				
80mm DCV Backflow Prevention Device Does not include labour.	\$2,904.15	\$0.00	\$2,904.15	Each
100mm Short Service – Potable/Recycled (Hydraulic Plans to Support)	\$2,696.53	\$0.00	\$2,696.53	Each
All services terminate prior to 90 bend to raise out of ground.				
100mm Long Service – Potable/Recycled (Hydraulic Plans to Support)	\$4,772.12	\$0.00	\$4,772.12	Each
All services terminate prior to 90 bend to raise out of ground.				
100mm Water Meter (Hydraulic Plans to Support) All meters include cost of meter frame beginning at the 90 bend.	\$4,879.58	\$0.00	\$4,879.58	Each
100mm SDC with Bypass Meter + 2 x Rising Spindle Valves Includes cost to raise service up from end of service.	\$4,125.18	\$0.00	\$4,125.18	Each
100mm RPZ Backflow Prevention Device Does not include labour.	\$4,668.91	\$0.00	\$4,668.91	Each
100mm DCV Backflow Prevention Device	\$3,589.49	\$0.00	\$3,589.49	Each
Does not include labour.	\$5,569.49	\$0.00	\$ 3, 309.49	Lacii
150mm Short Service – Potable/Recycled (Hydraulic Plans to Support)	\$3,525.67	\$0.00	\$3,525.67	Each
All services terminate prior to 90 bend to raise out of ground.				
150mm Long Service – Potable/Recycled (Hydraulic Plans to Support)	\$5,745.00	\$0.00	\$5,745.00	Each
All services terminate prior to 90 bend to raise out of ground.				
150mm Water Meter (Hydraulic Plans to Support) All meters include cost of meter frame beginning at the 90 bend.	\$10,102.73	\$0.00	\$10,102.73	Each
150mm SDC with Bypass Meter + 2 x Rising Spindle Valves	\$7,500.21	\$0.00	\$7,500.21	Each
Includes cost to raise service up from end of services.				
150mm RPZ Backflow Prevention Device	\$7,418.85	\$0.00	\$7,418.85	Each
Does not include labour.				
150mm DCV Backflow Prevention Device Does not include labour.	\$5,722.62	\$0.00	\$5,722.62	Each
Hydrant Relocation up to 150mm Water Main	\$2,971.33	\$0.00	\$2,971.33	Each
Valve Relocation up to 150mm Water Main	\$3,014.99	\$0.00	\$3,014.99	Each
Single Panel Footpath Removal and Restoration to Access Water Main	\$1,044.19	\$0.00	\$1,044.19	Each

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Nevee	F	Year 21/22	E	11-:4
Name	Fee (excl. GST)	GST	Fee (incl. GST)	Unit
Water Pressure Certificates				
Hose reel installation	\$99.00	\$0.00	\$99.00	Each
Fire hydrant installation	\$196.00	\$0.00	\$196.00	Each
Fire sprinkler and / or wall drencher installation	\$285.00	\$0.00	\$285.00	Each
Canal Management				
ound managomont				
Private Works on Canals – Maintenance Permit	\$159.00	\$0.00	\$159.00	Each
Boating Structure Licence Fee	\$138.00	\$0.00	\$138.00	Per
				Annum

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Appendices

Appendix 1 – Documents & Publications

Single Copies

The public should be encouraged to use coin-operated machines in the Library except where it is a Council document that is to be copied (e.g. Minutes). In this case, the office copiers are to be used for single jobs.

50 Copies or Greater per Original

This service will be provided on the basis that it is not an "on demand" service. If the copy cannot be done immediately a time will be mutually allocated.

A concession of 50% off the normal charge will apply to the following type of organisations:

- (a) Government and/or Council sponsored non-profit organisations concerned primarily with the welfare and or safety of the community and whole complement of workers consist mainly of volunteers, e.g. SES, Neighbourhood Centre, and Volunteer Fire Brigade.
- (b) Organisations directly sponsored by Council, consisting wholly of volunteers concerned with special projects, the promotion of which being subject to the declared support of Council, i.e. Friends of Kooloonbung, and that in all other cases the adopted scale of charges apply.

GST Exemption

Income from the sale of the following documents is GST Exempt: the production of, copying of, certifying of, including documents or data produced as a result of the searching or extracting of information from a data file. Examples include Policy documents, DCP's, LEP's, Council Minutes, mapping information, valuations, statistical data etc. The exemption does not extend to documents produced for commercial sale, such as tourist guides.

Publicly Available Information

Many items are available, at least in one form, free of charge as required by section 6 of the *Government Information* (*Public Access*) Act 2009. Access provided in alternative ways may be charged for.

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Appendices (continued)

Appendix 2 - Port Macquarie Airport

- RPT Regular Public Transport
- GA General Aviation
- Definition of private aircraft used solely for private purposes where no income is generated from the aircraft use by the operator.
- Definition of commercial aircraft any aircraft operation where income is received by the operator (excluding regular passenger / airline / freight services), including for example though not limited to, pilot training, charter, surveying, fire-fighting, joy/adventure flights, skydiving, etc
- Airport Usage Charge (Annual) For Commercial Purposes Applies to all airport based Commercial Operators with less than five (5) aircraft.

'Commercial Operator' means:

(a) a natural person to whom a general aviation aircraft using the airport is registered,

(b) a body corporate to which a general aviation aircraft using the airport is registered,

(c) two or more natural persons to whom general aviation aircraft using the airport are registered and who are associated for a common business purpose in relation to the operation of those aircraft,

(d) two or more bodies corporate to which general aviation aircraft using the airport are registered and which are associated for a common business purpose in relation to the operation of those aircraft,

(e) two or more related bodies corporate within the meaning of section 50 of the Corporations Act 2001 (Cth) to which general aviation aircraft using the airport are registered,

(f) two or more bodies corporate each of which has at least one director in common to which general aviation aircraft using the airport are registered,

(g) two or more bodies corporate each of which has at least one shareholder in common holding at least 10% of the shares in each of the bodies corporate to which general aviation aircraft using the airport are registered.'

- Council may choose to waive or reduce a fee for an aircraft operator pursuant to s610E(1) of the Local Government Act 1993 by written agreement entered into between the General Manager, under delegated authority granted by the Council, and the aircraft operator by reference to the categories of hardship or other categories appearing below:
 - Regular Public Transport (RPT) Services
 - General Aviation (GA) Services.
- Defined pilot circuit training hours:
 - Monday Friday 7am to 10pm October March (7am to 9pm April to September)
 - Saturday Sunday 8am to 8pm

A surcharge applies for any circuit training movements conducted outside of these hours.

Operators wishing to pay the annual fee are required to apply in writing outlining their aircraft registration number, confirming that it is parked and operated out of Port Macquarie Airport and describing the purposes for which the aircraft is used (i.e. commercial or private use).

An additional fee applies where any flying machine not registered on the "VH" register is provided with storage/hangar in a leased area. The hangar lessee will be charged the annual fee for a commercial ultra-light, micro-light or similar, for each

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Appendices (continued)

Appendix 2 - Port Macquarie Airport (continued)

such aircraft, or the 'private fee' where the lessee declares such machine as a private aircraft (describing any external markings for identification).

A movement charge will be levied based on broadcasts over the CTAF(R) (radio) Frequency as mandated by Civil Aviation Safety Regulations and the Aeronautical Information Publication if:

- (a) An on ground aircraft broadcasts the intention to taxi and enter runway 03/21
- (b) An airborne aircraft broadcasts the intention to join the circuit or broadcasts the intention to undertake a straight approach to runway 03/21

The Port Macquarie Airport Car Park is a pay parking area. Conditions of Parking and the parking fees applicable are displayed at the entry to the car park. A 'lost ticket' fee shall apply for any lost ticket. The 'lost ticket' fee shall be determined by the General Manager or delegate as appropriate to suit operational circumstances.

Port Macquarie is a Security Controlled Airport.

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Appendices (continued)

Appendix 3 - Ferry Services

All residents and rate payers of the Port Macquarie-Hastings Council Local Government Area are entitled to purchase a concession sticker. The concession annual pass prices are inclusive of the concession sticker fee and the annual pass only will be issued.

When concession stickers or annual passes are purchased, they are to be affixed to the driver's side of the windscreen of the vehicle to be valid for traveling or making a concession ticket purchase. Concession stickers and annual passes are not transferrable. A concession sticker or annual pass is required to purchase all concession booklets, concession weekly tickets and concession monthly tickets.

Weekly Tickets are issued for the week from Monday to Sunday, monthly tickets are issued for the calendar month and pre-paid concession booklets have no expiry date (they will only become invalid should the ticketing system be altered).

A concession sticker must be affixed to the vehicle using a pre-paid single trip concession ticket. A concession sticker is not required on a vehicle when using a weekly or monthly ticket. Weekly and monthly tickets are transferrable. Concession single trips can only be made using pre-paid tickets from the booklet - single trip tickets will only be available for purchase at the standard full fare rate.

Annual passes and concession stickers will only be available for purchase from Council.

All other tickets including pre-paid concession ticket booklets are to be purchased from the ferry operator. Weekly and monthly tickets do not cover the cost of a caravan or trailer (including dog-trailer). An additional cost is payable for the caravan or trailer - for concession holders this will be the use of 1 pre-paid ticket.

Concession travelers will hand the ferry operator the required number of pre-paid tickets when boarding the ferry for each single trip. The number of tickets required for each class of vehicle for a single trip is as follows:

- Car single trip = 1 ticket
- Motorcycle single trip = ½ ticket
- Trucks > 4t GVM (approx. 2 ton payload) = 3 tickets
- Semi-trailers, Dog-trailers, articulated vehicles and the like = 4 tickets
- Caravan / Trailer is an additional cost = 1 ticket

Charitable not for profit organisations who undertake charitable work on the North Shore are eligible to apply for 1 annual pass to be affixed to a nominated vehicle. Application is to be made in writing to the Group Manager Infrastructure Operations for consideration - specific information relating to the charity, its registration as a not for profit organisation and the type of work undertaken will be required.

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Appendices (continued)

Appendix 4 - Private Works

The Local Government Act 1993 enables Council to carry out the following works for residents and organisations on private property.

- Paving and road marking (GST applies)
- Kerbing and guttering (GST exempt)
- Demolition and excavation (GST applies)
- Water, sewerage and drainage connections (GST applies)
- Land clearing and tree felling (GST applies)
- Tree planting and tree maintenance (GST applies)
- Fencing and ditching (GST applies)

Appendix 5 - Restoration of Road Openings in Public Roads Maintained by Council

- (a) The rate per m² is to be interpreted as meaning a minimum charge / deposit on final cost.
- (b) The rate per m² is a minimum charge for restoration of road openings up to 10m².
- (c) Where actual restoration costs exceed, by more than \$200, the charge calculated by applying the appropriate rate per m², actual may be charged and any amount received in accordance with the above scale rates would be regarded as a deposit on final cost.
- (d) Actual costs are to be charged for restoration or road openings over 10m².
- (e) Several openings made at the one time less than 50m apart may be grouped as one, unless otherwise determined by the Council's Director of Infrastructure.
- (f) Where earth and gravel shoulders exist adjacent to pavements and restoration by the Council is necessary to the shoulders, the charge shown for "earth and gravel, waterbound macadam and all other classes of unsealed pavement or shoulders" is to be made additional to the charge for pavement. Applicants will be required to complete an "Applicants Information Check Sheet" in all road-closing matters prior to initiation or road closing action.

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Appendices (continued)

Appendix 6 - Annual Water Charge - Usage Allowance

Usage allowance - the per annum consumption level where consumption (usage) charges increase from Tier 1 to Tier 2

- 20mm Meter 270kL/pa
- 25mm Meter 422kL/pa
- 32mm Meter 691kL/pa
- 40mm Meter 1080kL/pa
- 50mm Meter 1688kL/pa
- 80mm Meter 4320kL/pa
- 100mm Meter 6750kL/pa
- 150mm Meter 15188kL/pa
- 200mm Meter 27000kL/pa

Refer to body of document for relevant water consumption charges.

Appendix 7 – Recycled Water Charge – Usage Allowance

Usage allowance – the per annum consumption level where consumption (usage) charges increase from Tier 1 to Tier 2. Port Macquarie Recycled Water Scheme users currently exempt from Tier 2 charges.

- 20mm Meter 270kL/pa
- 25mm Meter 422kL/pa
- 32mm Meter 691kL/pa
- 40mm Meter 1080kL/pa
- 50mm Meter
 1688kL/pa
- 80mm Meter 4320kL/pa
- 100mm Meter 6750kL/pa

Refer to body of document for relevant water consumption charges.

Appendix 8 – Traffic Management

The Traffic Management Section replaces the "Temporary Occupation of Footways & Road Reserve" fees from previous years. The fees have now been divided into separate application & occupation sections. Users should find that they are more equitable and short term occupancies are not penalised. This fee is only applicable to development building sites during construction / reconstruction / renovating.

This fee does not include the leasing costs imposed by Council for renting public roadway area.

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Appendices (continued)

Appendix 9 - Council Facilities / Halls

Explanation of Fee Categories:

- Commercial & Other includes activities staged by individuals, groups, organisations that *are not* incorporated under the Associated Incorporations Act NSW (2009) were revenue generated by use of the building goes back to these individuals, groups, organisations. This rate enables Council to take a full cost recovery approach to this use. Example – Local dance school.
- Private Functions includes activities that have admission restricted to an identifiable group. Example Birthday
 Party, Wedding Reception. This rate recognises that no financial gain will be gained from activity and that significant
 discount is applied.
- Not for Profit Rate includes activities provided by community groups, clubs and organisations for the physical, social, cultural, intellectual development or welfare of the local community. Organisations eligible for this rate should be incorporated under the Associated Incorporations Act NSW (2009). This rate recognises the benefit these activities provide to the community and therefore a major discount applies, while allowing users to make a contribution to the cost facility. E.g. Local Quilting Group.
- General Bonds Refundable Please note that refunds will only be granted if the hall, room, key or equipment is
 returned in good and clean condition. Council or the Management Committee / Booking Officer has the rights to deduct
 appropriate recovery costs.
- Alcohol Bond Refundable Please note that refunds will only be granted if the hall, room, key or equipment is
 returned in good and clean condition. Council or the Management Committee / Booking Officer has the rights to deduct
 appropriate recovery costs. It is the responsibility of the hirer to obtain the necessary approvals from Police to
 undertake an event involving alcohol. Refer to hire conditions for each of the facilities.

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Appendices (continued)

Appendix 10 - Glasshouse

- After Hours = 5.30pm 11pm weekdays or 9am 11pm Saturdays.
- Staff surcharges apply on Sundays, Public Holidays and after 11pm.
- Full Theatre = Stalls seating + Balcony seating.
- Studio Bare Walls = This mode only available when combined with Theatre Hire. No seating, fluoro LX, no audio, no staff. Includes access, cleaning & electricity.
- Whole of Venue = Price on Application. Includes access to the Theatre, Studio, Meeting Rooms 1 & 2, Small Function Room, Mezzanine Foyer and minimum 3 staff, does not include Art Lab or Regional Gallery spaces. Staff charges to set-up and pack down at additional charge and based on event requirements in consultation with Glasshouse staff. Does not include Sunday, Public Holidays, after 11pm or Audio Visual services & equipment.

Glasshouse Discounted Rates

- 50% discount is available to incorporated not-for-profit groups, incorporated community groups, registered charities
 and schools. Eligibility is limited to organisations that are based in the Port Macquarie-Hastings local government area
 and that are not already receiving support via Port Macquarie Hastings Council's Community Grants or similar funding
 programs. Proof of Incorporation, ABN and registrations must be provided. Please note the 50% discount only applies
 to venue hire between Monday and Thursday inclusive (i.e. not including Friday, Saturday or Sunday) unless otherwise
 noted below.
- In addition, not-for-profit groups and community groups must have a form of organisational permanence, be nongovernment; and not be operating for the profit or gain of its individual members. The Venue Manager, Glasshouse will have responsibility for determining the eligibility status of groups.
- Discounted rates only to apply to the following fees and charges categories (with all other fees and charges to be charged as per commercial rates such as Staff Rates):-
 - Theatre Hire
 - Studio Hire discount available 7 days per week, subject to availability
 - Meeting Room Hire
 - Art Lab Hire
 - Small Function Room Hire
 - Podium discount available 7 days per week, subject to availability
 - Other Venue Hire
 - Venue Set Up and audio visual Items (equipment only)
- 30% discount is available for Studio Hire only to local performing arts organisations and local performance artists. Eligibility is limited to organisations and artists that are based in the Port Macquarie Hastings local government area and that are not already receiving support via Port Macquarie Hastings Council's Community Grants or similar funding programs. "Local" is defined as organisation having a principal place of business and artists having a principal place of residence (not being a PO Box) that is located in the Port Macquarie Hastings local government area, and that the organisation/artist has operated/resided within the Port Macquarie Hastings local government area for a minimum period of three (3) months. The onus is on the performing arts organisation and performance artist to provide the necessary supporting evidence.

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Appendices (continued)

Appendix 10 - Glasshouse (continued)

• Please contact the Glasshouse Event Services Team for a comprehensive quote tailor made to suit your function requirements.

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Appendices (continued)

Appendix 11 - Cemeteries

- Services which cannot be completed within normal business hours (9am to 3pm Monday to Friday) will incur a Late Fee.
- Burial services booked outside of normal business hours for religious reasons will be subject to an additional surcharge.
- Reservations without payment for burial sites and ash inurnment will be held for one (1) calendar month only after which full payment will be required.
- · Council cannot accept part payments / deposits for burial services.
- Requests for refunds on unused reserved sites will be issued at the rate of the original amount paid less the current Administration Fee. Exclusive rights must be relinquished. No refunds on used sites.
- Family Ash Placements by appointment can be made at Council's Customer Services offices and subject to fees
 depending on location of the cemetery.
- Ash removal on request by the original applicant or Executor may be approved and subject to the relevant fee. A
 Statutory Declaration signed by all next of kin is required for ash removal requests in situations where the original
 Applicant or Executor are not contactable.

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PORT MACQUARIE HASTINGS COUNCIL

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Item 09.05 Attachment 1



Attachment 2 REVENUE POLICY 2021-2022



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Statutory Requirements

In accordance with the Local Government Act 1993 the following statutory requirements are included in this document:

- Statement containing a detailed estimate of the council's income and expenditure;
- Statement of ordinary and special rates;
- Rating structure;
- Statement of charges to apply to rateable and non-rateable properties;
- Statement of fees to be charged and pricing policy of goods and services;
- Statement of borrowings;
- Statement of private works; and
- Statement of business and commercial activities.

Estimate of Council's Income and Expenditure

	2021-22 Budget
Continuing Operations Movements	
Income	
Rates and annual charges	108,389,830
User charges and fees	39,587,808
Interest received	5,454,710
Grants and contributions	60,845,445
Other operating receipts	5,457,657
Expenses	
Employee costs	(55,268,240)
Materials and contracts	(45,200,921)
Depreciation	(50,236,200)
Interest paid	(2,157,754)
Other operating payments	(15,351,600)
Loss on disposal of assets	(3,000,000)
Operating Result from Continuing Operations	48,520,735
Restricted Asset Movements	
Receipts	
Transfer from restricted assets	38,796,355
Payments	
Transfer to restricted assets	(55,465,759)
Net Revenue (Used in)/Provided by Restricted Assets	(16,669,404)
Property, Plant and Equipment Movements	
Receipts	
Sale of fixed assets/real estate development	-
Payments	
Purchase of assets	(67,573,068)
Developer provided assets	(6,500,000)
Net Revenues (Used in)/Provided by Property, Plant & Equipment	(74,073,068)
Financing Movements	
Receipts	
Proceeds from borrowings	0
Repayment of borrowings and advances	(11,014,463)

Payments	
Net Revenues (Used in)/Provided by Financing Activity	(11,014,463)
Net Result (including depreciation)	(53,236,200)
Add back: Non-cash items	53,236,200
Budget Surplus/(Shortfall)	0

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Statement of Ordinary and Special Rates

Strategy – ordinary rates

Council will continue to levy ordinary rates using a structure comprising a base amount to which an ad valorem (rate in the dollar) component is added. The Base Amount will vary, depending on rating category/sub-category combination, up to the maximum allowed (50%) for defined urban areas. All rateable properties within each category / sub-category, regardless of their land value, are levied the base amount. The balance of income for ordinary rates is derived by multiplying the land value of a property by a rate in dollar for the relevant category / sub-category, which is determined by Council. The amount payable by ratepayers under this component is dependent on the land value of the property. In addition, the total amount collected for ordinary rates each year will take account of any approved increase advised by the Minister.

The following is the structure of Council's rates:

Туре	Category	Sub-Category	Comments
Ordinary	Residential	Defined Urban Centres	All properties categorised as residential within the localities of Bonny Hills, Camden Head, Dunbogan, Kendall, Kew, Lake Cathie, Lakewood, Laurieton, North Haven, Port Macquarie, Wauchope and West Haven as defined by the "Designated Urban Centre" maps following.
Ordinary	Residential	Other Hastings	All other properties classified as residential within the Council area and are not located within the areas defined as "Designated Urban Centres" above.
Ordinary	Farming		Eligibility determined in accordance with Local Government Act 1993.
Ordinary	Mining		Eligibility determined in accordance with Local Government Act 1993.
Ordinary	Business	Port Macquarie CBD	All business properties within the Port Macquarie CBD which are identified on the map following, and as adopted by Council for the purpose of identifying the Port Macquarie CBD for Business Sub-Category rating.
Ordinary	Business	Defined Urban Centres	All business properties within the "Designated Urban Centres" are identified on the maps following, and as adopted by Council for the purpose of identifying the "Designated Urban Centre" for Business Sub-Category rating. The locality of Port Macquarie does not include the area defined as "Port Macquarie CBD".
Ordinary	Business	Other Hastings	All business properties within the Council area other than those in the CBD of Port Macquarie area and "Designated Urban Centres" as identified on the maps following and as adopted by Council.

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Rating Structure

Ordinary rates under section 494 of the *Local Government Act* 1993 and special rates under section 495 of the *Local Government Act* 1993. The Minister's allowable increase for the 2021-22 financial year has been set at 2.0% and has been applied in full. Council is using land valuations as at 1 July 2019 for the rating calculations.

Category	Sub-Category	General Base Amount	Environ- mental Base	Base Amount % of Total Levy	Plus	Ad Valorem Rate: Cents in \$	Estimated Gross Yield	% of Total	Average Rates
Residential	Defined Urban Centre	\$601.50	\$25.10	49.61%		0.25172	\$37,911,199	68.52	\$1,263
Residential	Other Hastings	\$501.50	\$25.10	39.66%		0.28082	\$5,985,275	10.82	\$1,328
Farmland		\$622.20	\$25.10	30.72%		0.28078	\$2,480,170	4.49	\$2,107
Business	Port Macquarie CBD	\$601.50	\$25.10	5.19%		2.40857	\$3,319,469	6.00	\$12,071
Business	Defined Urban Centre	\$601.50	\$25.10	20.09%		0.54547	\$5,127,970	9.27	\$3,119
Business	Hastings Other	\$501.50	\$25.10	30.82%		0.47540	\$355,408	0.64	\$1,709
The Broadwater Special Rate						0.06277	\$111,601	0.20	\$333
Sanctuary Springs Special Rate						0.13382	\$31,191	0.06	\$346
Allowance for Growth							\$288,329		
Sub-Total							\$55,610,612	100.00	
Pensioner Rebat	es						(\$1,536,600)		
Postponed Rates							(\$50,000)		
Abandoned Other	r/Appeals						(\$134,500)		
Economic Develo	pment						(\$329,370)		
Tourism							(\$572,800)		
The Broadwater S	Special Rate						(\$111,601)		
Sanctuary Spring	s Special Rate						(\$31,191)		
Environmental Ba	ise						(\$953,197)		
Port Macquarie C Improvement	BD Town						(\$1,240,600)		
General Net Inc	ome						\$50,650,753		

The table below includes the 2.0% rate pegging limit for 2021-22.

Rates and Charges Hardship Assistance Policy

Council has a current Rates and Charges Hardship Assistance Policy. This policy was adopted by Council on the 16 August 2017. If you would like to get more information on this policy, please contact Council's customer services section on 6581 8111.

Environmental and community infrastructure levy expenditure: 2021-22

Project	Amount
Flying Fox Camp Management - Implement actions from Management Plan	\$45,000
Undertake Biosecurity (Weed Management) program to restore and conserve the natural environment of the Mid North Coast	\$122,900
Koala Road Strike - Feasibility Study for engineering solutions and initial concept design work	\$150,000
Coastal Management Plan development - CMP Stage 2 - Hastings River Drive	\$50,000
Wild Deer Management in the Council area	\$25,000
Koala Recovery Partnership Program - Support	\$90,000
Vince Inmon Sporting Field - Sporting facility renewal	\$111,487
Sporting Infrastructure Renewals	227,731
McInherney Park - Contribution to public amenities in new boat storage facility	\$106,080
Development of Illegal Tracks Identification	\$10,000
Total	\$938,198

Statement of Fees and Charges

Water charges under Sections 501 & 502 of the Local Government Act 1993

With water an increasingly scarce resource both locally and globally, it is appropriate that councils focus on influencing water demand through increasing emphasis on usage based pricing. Best practice water supply pricing requires councils to provide strong pricing signals to encourage efficient water use. With this in mind the water usage charge has been increased and the water access charge has been reduced over three years commencing 2019-20 with the goal that 75% of water income will be derived from usage charges by 2021-22, in line with best practice recommendations.

The total yield from water charges in 2021-22 has been increased in line with the IPART approved general fund rate peg of 2.0%, however a change in the pricing structure as detailed above has been included. The tier 1 water usage charge has been increased by 14c per kl from \$3.31 in 2020-21 to \$3.45 in 2021-22 and the access charge for a 20mm meter has reduced from \$178.00 in 2020-21 to \$171.00 in 2021-22.

If you require specific information on these charges please contact Council on 6581 8111.

The following total revenue is to be derived from the 2021-22 Water Pricing Tariff.

Description	Annual Charge	Services Charged	Total Annual Income	Total Usage Charge	Total Usage Income	Total Income
20mm Water Meter (Including Vacant Land)	\$171.00	33,511	\$5,730,381.00			
25mm Water Meter	\$272.00	1,013	\$275,536.00			
32mm Water Meter	\$453.00	259	\$117,327.00			
40mm Water Meter	\$714.00	177	\$126,378.00			
50mm Water Meter	\$1,124.00	132	\$148,368.00			
80mm Water Meter	\$2,889.00	6	\$17,334.00			
100mm Water Meter	\$4,517.00	43	\$194,231.00			
150mm Water Meter	\$10,185.00	5	\$50,925.00			
200mm Water Meter	\$18,110.00	0	\$0.00			
Equivalent Tenants	\$171.00	2,736.00	\$467,856.00			
Fire Service: 32mm Water Meter	\$226.50	1	\$226.50			
Fire Service: 40mm Water Meter	\$357.00	2	\$714.00			
Fire Service: 50mm Water Meter	\$562.00	8	\$4,496.00			
Fire Service: 80mm Water Meter	\$1,444.50	0	\$0.00			
Fire Service: 100mm Water Meter	\$2,258.50	160	\$361,360.00			
Fire Service: 150mm Water Meter	\$5,092.50	23	\$117,127.50			

Description	Annual Charge	Services Charged	Total Annual Income	Total Usage Charge	Total Usage Income	Total Income
Fire Service: 200mm Water Meter	\$9,055.00	2	\$18,110.00			
Gross Income from Annual Charges			\$7,630,370,00			\$7,630,370.00
Charges for Consumption: Tier 1 Tier 2				\$3.45 \$6.90	\$18,842,793.00 \$4,297,506.00	
Gross Income from Consumption Charges					\$23,140,299.00	\$23,140,299.00
Total Gross Income						\$30,770,669.00
Less: Pension Rebates						(\$670,100.00)
Less: Other Rebates						(\$410,000.00)

Net Income \$29,690,569.00

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Sewer charges under Sections 501 & 502 of the Local Government Act 1993

All properties connected or capable of connection to the sewerage system will be charged an annual charge. In addition, properties such as major regional shopping centres, caravan parks, flats, retirement homes, motels, clubs, hotels, schools and backpacker hostels will be required to pay an additional charge based on the amount of water consumed. Council intends to increase the sewer annual and usage charges by 2.0% for the 2021-22 financial year which is in line with the general fund rate pegging allowance that has been determined by IPART.

The projected income for sewerage for 2021-22 is as follows:

Description	Annual Charge	Assessments Charged	Total Annual Income	Usage Charge	Expected Water Usage (kls)	Total Usage Income
Available	\$923.70	32,263	\$29,801,333.10			
Available Pump Out	\$893.70	428	\$382,503.60			
Unconnected (Section 548a)	\$592.80	0	\$0.00			
Non-rateable (other than Churches & Halls s555)	\$735.40	79	\$58,096.60			
Non-rateable (Churches & Halls s555)	\$592.80	22	\$13,041.60			
Non-rateable (Other s556)	\$923.70	154	\$142,249.80			
Additional Charge				\$1.37	978,102.19	\$1,340,000.00
Gross Income			\$30,397,224.70			\$1,340,000.00
Less: Pension Rebates			(\$635,000.00)			
Less: Other						(\$10,000.00)
Net Income from Sewerage Annual and Usage Charges			\$29,762,224.70			\$1,330,000.00

On-site sewerage management system annual charges

Properties with an on-site sewerage management system will be charged an annual charge based on the risk of the system. Council intends to increase the on-site sewerage management annual charge by 2.0% for the 2021-22 financial year which is in line with the general fund rate pegging allowance that has been determined by IPART.

The 2021-22 projected income for onsite effluent is as follows:

Description	Annual Charge	Assessments Charged	Total Annual Income
Low Risk System	\$39.80	1,738	\$69,172.40
Medium Risk System	\$78.50	2,282	\$179,137.00
High Risk System	\$112.20	550	\$61,710.00
Small Commercial	\$224.40	92	\$20,644.80
Large Commercial	\$279.50	12	\$3,354.00
Gross Income		4,674	\$334,018.20

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Recycled water charges

Recycled water for non-residential properties will continue to be charged at 50% of the water charge. For residential properties the charge for recycled water will be 80% of the water charge. The 2021-22 projected income for recycled water is as follows:

Description	Annual Charge	Assessments Charged	Total Annual Income	Usage Charge	Expected Water Usage (kls)	Total Usage Income
Non-Residential						
20mm Meter	\$85.50	6	\$513.00			
25mm Meter	\$136.00	20	\$2,720.00			
32mm Meter	\$226.50	5	\$1,132.50			
40mm Meter	\$357.00	7	\$2,499.00			
50mm Meter	\$562.00	8	\$4,496.00			
80mm Meter	\$1,444.50	3	\$4,333.50			
100mm Meter	\$2,258.50	1	\$2,258.50			
150mm Meter	\$5,092.50	1	\$5,092.50			
200mm Meter	\$9,055.00	0	\$0.00			
Charges for Consumption						
Tier 1				\$1.72	203,488	\$350,000.00
Gross Income		51	\$23,045.00			\$350,000.00

Description	Annual Charge	Assessments Charged	Total Annual Income	Usage Charge	Expected Water Usage (kls)	Total Usage Income
Residential						
20mm Meter	\$136.80	0	\$0.00			
25mm Meter	\$217.60	0	\$0.00			
32mm Meter	\$362.40	0	\$0.00			
40mm Meter	\$571.20	0	\$0.00			
50mm Meter	\$899.20	0	\$0.00			
80mm Meter	\$2,311.20	0	\$0.00			
100mm Meter	\$3,613.60	0	\$0.00			
150mm Meter	\$8,148.00	0	\$0.00			
200mm Meter	\$14,488.00	0	\$0.00			
Charges for Consumption						
Tier 1				\$2.76	0	\$0.00
Gross Income		0	\$0.00			\$0.00

Stormwater charges - annual charge under Local Government Amendment (Stormwater) Act 2005 and the Local Government (General) Amendment (Stormwater) Regulation 2006

All urban properties except vacant land that are in the residential and business categories for rating purposes, for which Council provides a stormwater management service will be charged a stormwater levy. The base stormwater levy has remained unchanged since it was introduced in 2006-07.

The projected income for stormwater in 2021-22 is as follows:

Description	Charge	Number of Assessments	Total Annual Income
Business - 0 to 350m2	\$25.00	59	\$1,475.00
Business - 351m2 to 700m2	\$50.00	195	\$9,750.00
Business - 701m2 to 1,000m2	\$75.00	191	\$14,325.00
Business - 1,001m2 to 1,400m2	\$100.00	140	\$14,000.00
Business - 1,401m2 to 1,750m2	\$125.00	73	\$9,125.00
Business - 1,751m2 to 2,100m2	\$150.00	55	\$8,250.00
Business - 2,101m2 to 2,450m2	\$175.00	49	\$8,575.00
Business - 2,451m2 to 2,800m2	\$200.00	35	\$7,000.00
Business - 2,801m2 to 3,200m2	\$225.00	23	\$5,175.00
Business - 3,201m2 to 3,500m2	\$250.00	18	\$4,500.00
Business - 3,501m2 to 3,850m2	\$275.00	20	\$5,500.00
Business - 3,851m2 to 4,200m2	\$300.00	28	\$8,400.00
Business - 4,201m2 to 4,600m2	\$325.00	17	\$5,525.00
Business - 4,601m2 to 4,900m2	\$350.00	8	\$2,800.00
Business - 4,901m2 to 5,200m2	\$375.00	12	\$4,500.00
Business - 5,201m2 to 5,600m2	\$400.00	6	\$2,400.00
Business - 5,601m2 to 5,950m2	\$425.00	9	\$3,825.00
Business - 5,951m2 to 6,300m2	\$450.00	10	\$4,500.00
Business - 6,301m2 to 6,600m2	\$475.00	6	\$2,850.00
Business - Greater than 6,600m2	\$500.00	136	\$68,000.00
Business Strata	\$12.50	631	\$7,887.50
Residential	\$25.00	22,923	\$573,075.00
Residential Strata	\$12.50	6,553	\$81,912.50
Gross Income		31,197	\$853,350.00

Stormwater levy expenditure: Council has allocated the full amount of the Stormwater Levy to maintenance and running of the current stormwater system. This allocation is over and above previous allocations for stormwater maintenance. Urban land (not vacant) that is categorised for rating purposes as residential or business with a stormwater service will be charged the levy as allowed for in the legislation.

Pursuant to Section 403(1) of the *Local Government Act* 1993 the following statement relates to stormwater management services to be funded by the Stormwater Levy.

The Stormwater Levy is used to fund operational stormwater infrastructure maintenance and remedial works across all urban areas of the LGA. Stormwater Levy and General Fund resources for 2021-22 will be allocated as follows:

Purpose	Funding Source	Amount
Operational Maintenance and Remedial Works	General Fund Revenue	\$1,805,630
Operational Maintenance and Remedial Works	Stormwater Levy	\$600,000
Loan Repayments	Stormwater Levy	\$205,964
Loan Repayments	General Fund Revenue	\$197,069
Stormwater Renewal Program	General Fund Revenue	\$250,000
Stormwater Remediation - Hart Street	Stormwater Reserve	\$40,000
North Haven Flood Mitigation Works - Investigation and concept design remedial works	Stormwater Reserve	\$55,000
Stormwater assets management condition rating via CCTV inspections	General Fund Revenue	\$50,000
Stormwater Remediation - Panorama Drive/Binbilla Drive, Bonny Hills	General Fund Revenue	\$420,000
Black Swan Terrace Stormwater Detention Facility	General Fund Revenue	\$158,186
Total		\$3,781,849

Waste Management Charges

The charges to apply to domestic waste management are determined in accordance with the Reasonable Cost guidelines issued by the NSW Office of Local Government. The charges determined using these guidelines will apply to domestic as well as non-domestic properties.

Domestic waste management services

All properties with a domestic waste management service available to their property will be charged an annual charge under Section 496 of the *Local Government Act* 1993.

The charge for multiple services is calculated by the "type of service" multiplied by the "number of services" provided.

The applicable charges for 2021-22 are outlined in the schedule below.

Description	Cost per Service	Services Provided	Estimated Income
DOMESTIC WASTE MANAGEMENT ANNUAL CHARGES			
Standard Service (240 Litre Fortnightly General Waste)	\$488.00	18,831	\$9,189,528.00
Minimiser Service (140 Litre Fortnightly General Waste)	\$428.00	10,357	\$4,432,796.00
Excess Service (240 Litre Weekly General Waste)	\$721.00	1,792	\$1,292,032.00
On Application Only Special Weekly Service (140 Litre Weekly General Waste)	\$603.00	7	\$4,221.00
Special Half Weekly Service (140 Litre Weekly General Waste) - (Shared) Multi-unit dwellings only	\$328.00	12	\$3,936.00
Half Minimiser Service (140 Litre Fortnightly General Waste) - (Shared) Multi-unit dwellings only	\$239.00	35	\$8,365.00
Half Excess Service (240 Litre Weekly General Waste) - (Shared) Multi-unit dwellings only	\$386.00	156	\$60,216.00
Half Standard Service (240 Litre Fortnightly General Waste) - (Shared) Multi-unit dwellings only	\$268.00	668	\$179,024.00
Availability Charge on Land Not Receiving Full Charge - Multi-unit dwellings only	\$143.00	2,269	\$324,467.00
Availability Charge on Vacant Land	\$79.00	1,300	\$102,700.00
Additional Recycling Service (240 Litre Fortnightly)	\$76.00	328	\$24,928.00
Additional Organics Service (240 Litre Weekly)	\$98.00	356	\$34,888.00
Additional Weekly Garbage Service (240 Litre General Waste)	\$373.00	107	\$39,911.00
Additional Weekly Garbage Service (140 Litre General Waste) - Multi-unit dwellings only	\$283.00	1	\$283.00
Minimum Waste Charge - Single dwelling	\$428.00	68	\$29,104.00
Less: Pensioner/Other Rebates			(\$510,000.00)
Net Income for Domestic Waste Management Service	s	36,287	\$15,216,399.00

Other waste management service

All properties with a non-domestic waste management service will be charged an annual charge under Section 501 of the *Local Government Act* 1993.

The charge for multiple services is calculated by the "type of service" multiplied by the "number of services" provided.

The applicable charges for 2021-22 are outlined in the schedule below.

Description	Cost per Service	Services Provided	Estimated Income
OTHER WASTE MANAGEMENT ANNUAL CHARGES			
Commercial Weekly Service (140 Litre Weekly General Waste)	\$594.00	468	\$277,992.00
Commercial Excess Service (240 Litre Weekly General Waste)	\$709.00	412	\$292,108.00
Commercial Half Weekly Service (140 Litre Weekly General Waste) - (Shared)	\$322.00	6	\$1,932.00
Commercial Half Excess Service (240 Litre Weekly General Waste) -(Shared)	\$380.00	10	\$3,800.00
Commercial Availability Charge on Land Not Receiving Full Charge	\$79.00	1,051	\$83,029.00
Commercial Availability Charge on Vacant Land	\$79.00	185	\$14,615.00
Commercial Additional Recycling Service (240 Litre Fortnightly)	\$76.00	245	\$18,620.00
Commercial Additional Organics Service (240 Litre Weekly)	\$98.00	32	\$3,136.00
Commercial Additional Weekly Garbage Service (240 Litre Weekly General Waste)	\$372.00	197	\$73,284.00
Commercial Additional Weekly Garbage Service (140 Litre Weekly General Waste)	\$283.00	4	\$1,132.00
Waste Management Access Charge – Rural Properties	\$31.00	1,680	\$52,080.00
Less: Pension/Other Rebates			(\$1,300.00)
Net Income for Other Waste Management Services		4,290	\$820,428.00

Interest charge under Section 566

Charged on all outstanding rates and charges - 6.0%.

Statement of Fees to be Charged and Pricing Policy of Goods and Services

Council fees for the 2021-22 financial year are listed in the Schedule of Fees and Charges.

In determining the appropriate fees to be charged for Council services and facilities in 2021-22, the basic principle applied is that charges should be considered fair and equitable to the general community. Individual members of the community should not be precluded from essential services because of economic circumstances. In accordance with Council's adopted practice Fees & Charges for the following price categories have generally increased by 1.5% as an approximation of CPI.

(d) User pays (e) Market Forces (f) Cost Plus

The range of services provided by Council to the community is diverse and requires different considerations when determining the associated fee or charge. The level of the fee or charge was determined having regard to the following categories:

Code	Description				
A	Economic Cost Total cost of providing services for private good.				
В	Community Service Services considered to have a level of benefit to the community. Generally benefits are not solely confined to users. Partially funded by rates.				
с	Regulated Charges Federal or State Government set charges.				
D	User Pays Principal Services under this category are such that individual costs can be determined and met by the user of the service.				
E	Market Forces Services that Council operates in a competitive market and needs to fix charges similar to other providers.				
F	Cost Plus Services provided on a commercial basis with an amount of risk profit included.				
G	Section 7.11 (Developer Contributions) Items are priced at the level determined by the adopted Council's Developer Contributions Plan. The plans provide for quarterly increases in these fees based on Consumer Price Index (CPI) increases.				

For information, one of the above codes has been included next to each fee or charge listed in Part C.

Statement of Borrowings

To provide for the future needs of our community, Council borrows funds to provide infrastructure and community assets which are not able to be funded out of normal revenue sources. The loans are based on periods that represent the economic life of the facility or asset or a reasonable fixed term, whichever is the lesser. Loans are raised by Council from banks or other recognised financial institutions and secured by a mortgage deed over the revenue of the Council. Proposed new borrowings for the next four years are:

Purposed New Borrowings	2021-22	2022-23	2023-24	2024-25
Nil	Nil	Nil	Nil	\$25m
Total	Nil	Nil	Nil	\$25m

At the 3 March 2021 Extraordinary Council Meeting, Council resolved, (in part), to fund \$30m for the Ocean Drive Duplication project by, in part, borrowing \$25m. Current assessment of cash flow requirements and associated borrowings, indicates that the borrowings component will be required to be drawn down in 2024-25.

For information on Council's debt cover ratio refer to the Annual Financial Statements available on Council's website.

Statement of Private Works

The Local Government Act 1993 enables Council to carry out the following works for residents and organisations on private land:

- Paving and road making (GST exempt);
- Kerbing and guttering (GST exempt);
- Demolition and excavation (GST applies);
- Water, sewerage and drainage connections (GST applies);
- Land clearing and tree felling (GST exempt);
- Tree planting and maintenance (GST exempt);
- Fencing and ditching (GST applies) and;
- Canal maintenance (GST exempt)

Council has adopted the following charging policy in relation to private works:

Actual cost plus adjustments to cover Council overheads.

Overhead Charges:

- To cover Workers' Compensation, leave entitlements, superannuation, etc, and
- To cover administration.

Overhead charges to cover administration, engineering, store running costs:

- Emergency works 25%;
- Prepaid works up to \$20,000 18%;
- Prepaid works \$20,000 to \$50,000 12%; and
- Prepaid works \$50,000 & above 6%.

Labour costs are increased to cover Workers' Compensation, leave entitlements, award provisions and superannuation.

Statement of Business and Commercial Activities

Council has conducted commercial activities for a number of years and currently undertakes the following:

- Port Macquarie Airport
- The Glasshouse Arts, Conference and Entertainment Centre

Whilst the majority of business undertakings have a primary requirement to make a profit on the funds invested, there is also a social and community component in their undertakings. Each of the commercial units contributes in some way to community wellbeing, as well as to the economic infrastructure of the Port Macquarie-Hastings area.

Council conducts regular reviews of the operations and of the commercial activities of each of these business units. The review ensures that their charges are commercially based and are not at variance with market requirements. Funds that are in excess of the operating and development requirements of the commercial activity are used as part of Council's revenue stream to enable Council to provide the Port Macquarie-Hastings community with a broad range of services and facilities.

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PORT MACQUARIE HASTINGS COUNCIL

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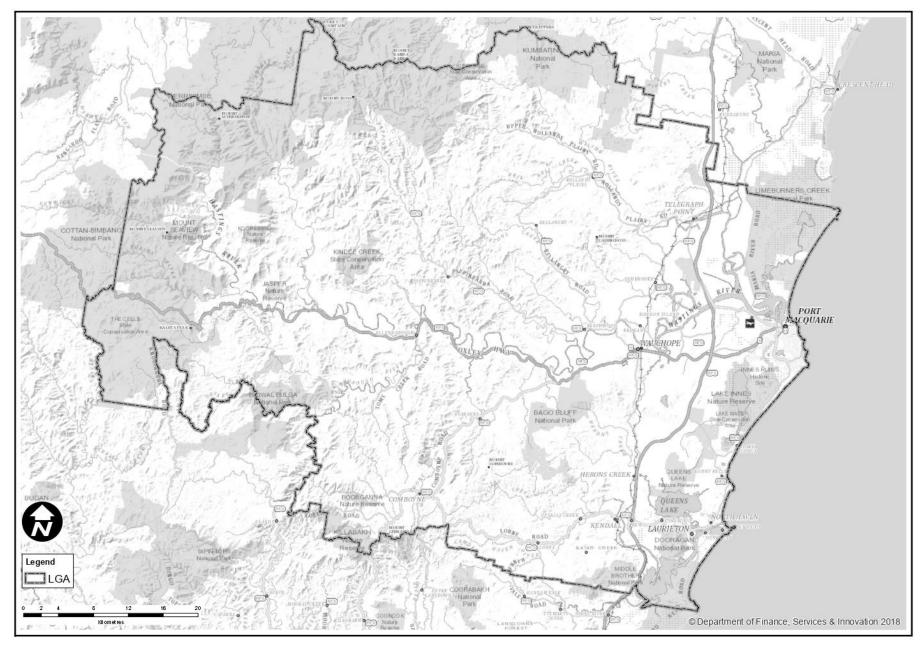


Attachment 3 RATING MAPS 2021-2022

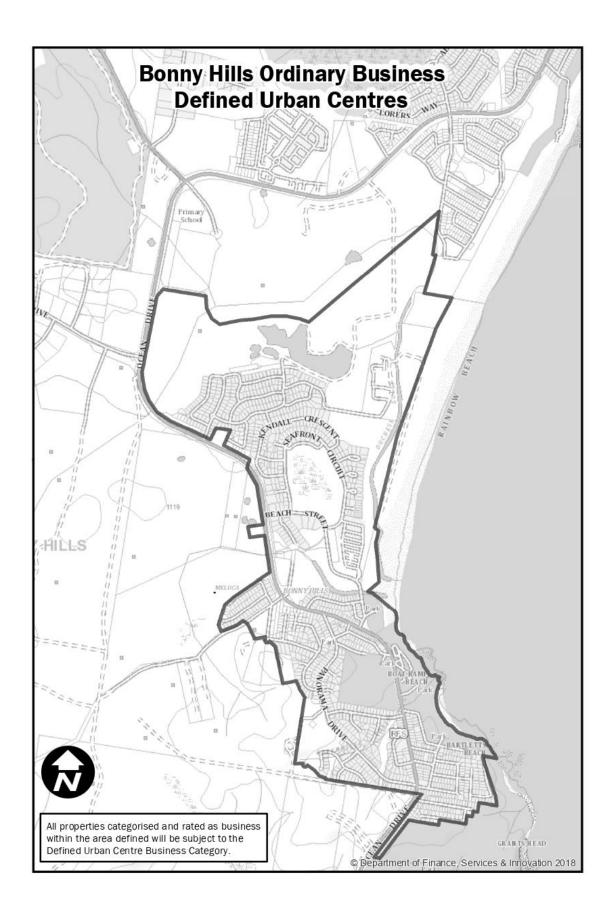


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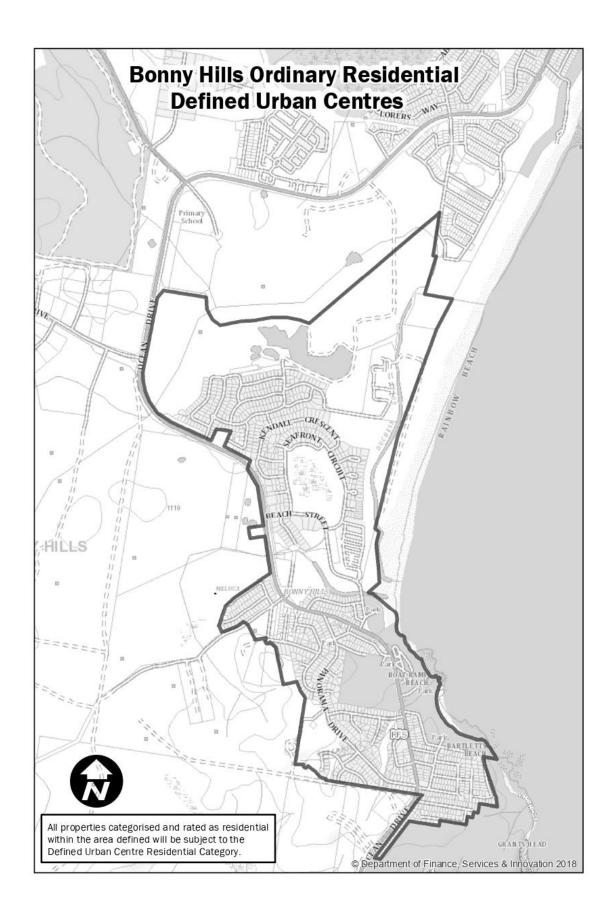




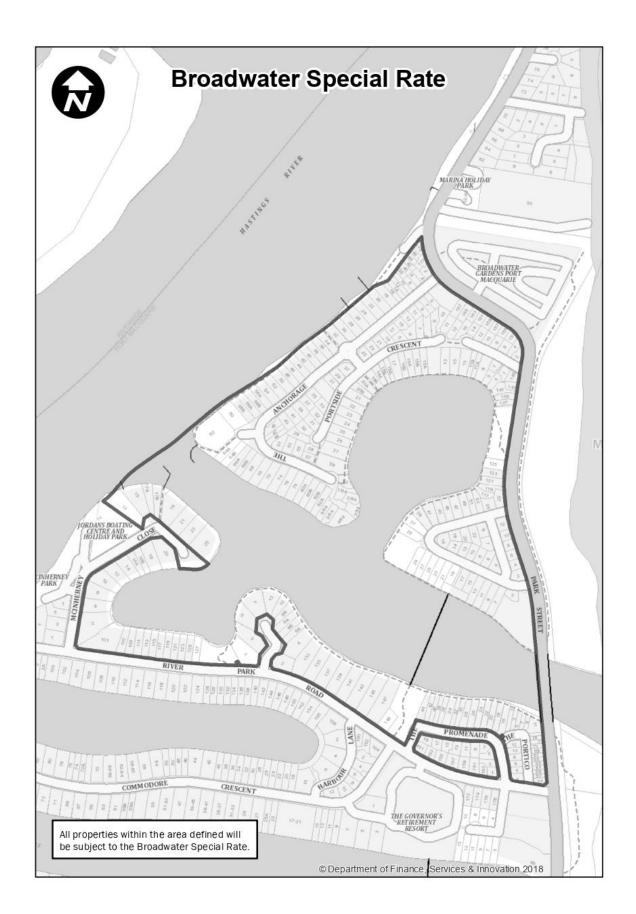
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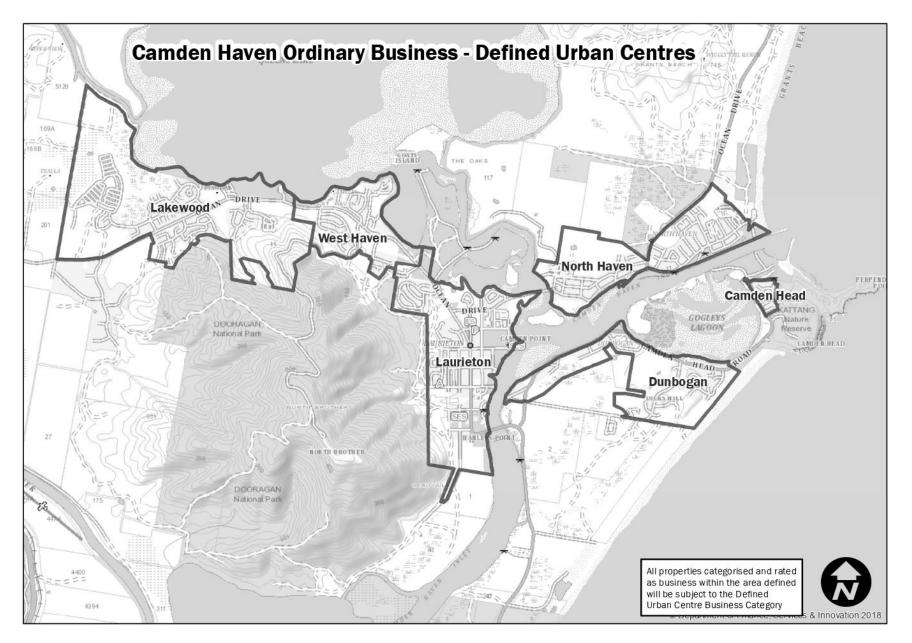


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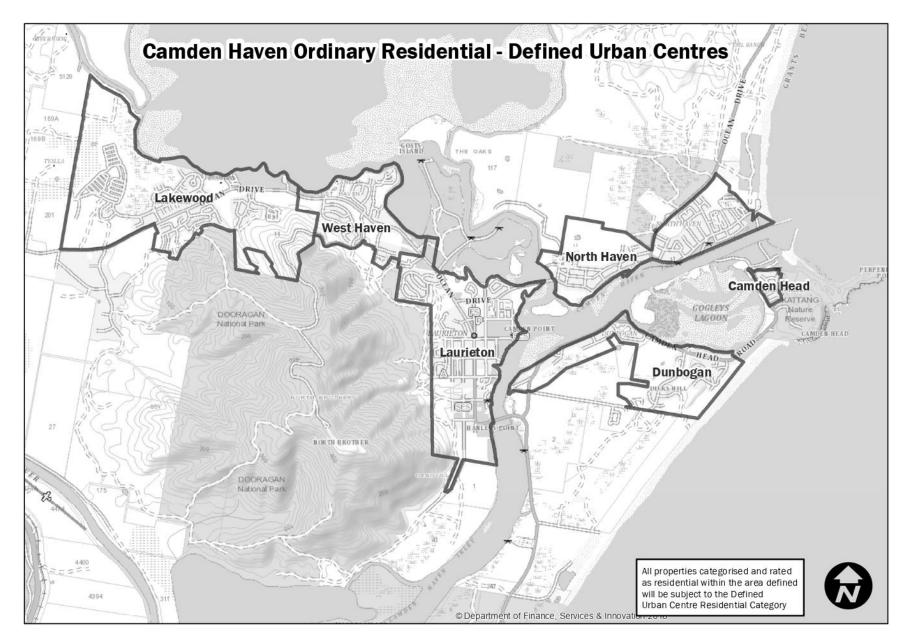


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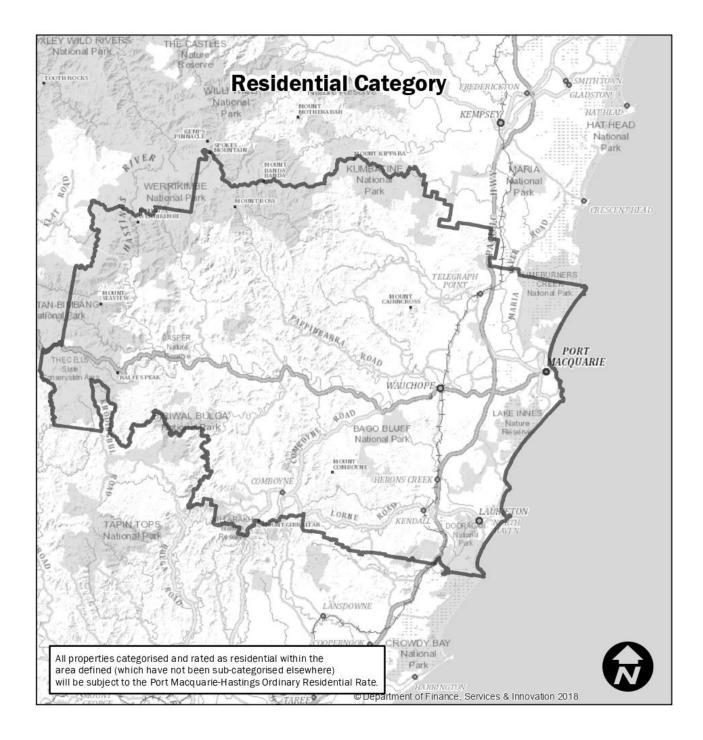


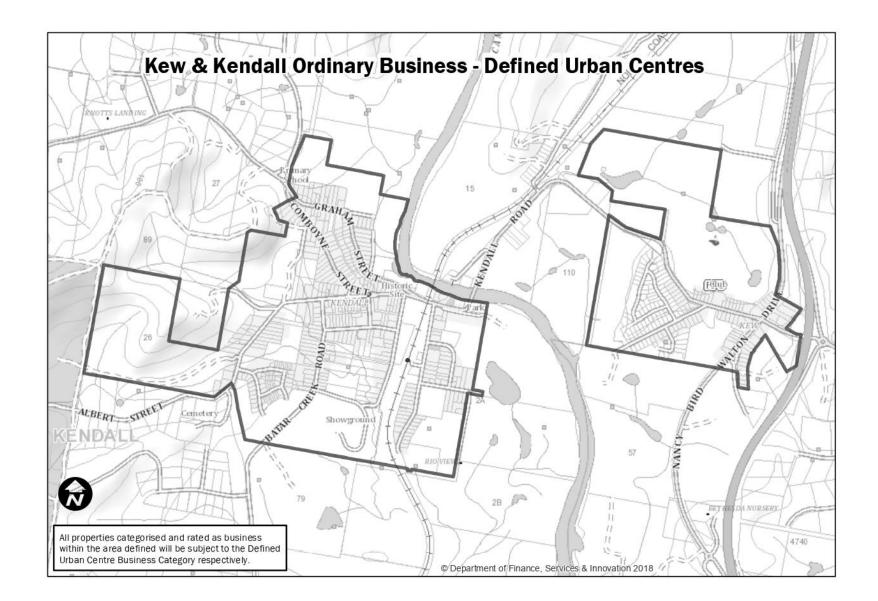
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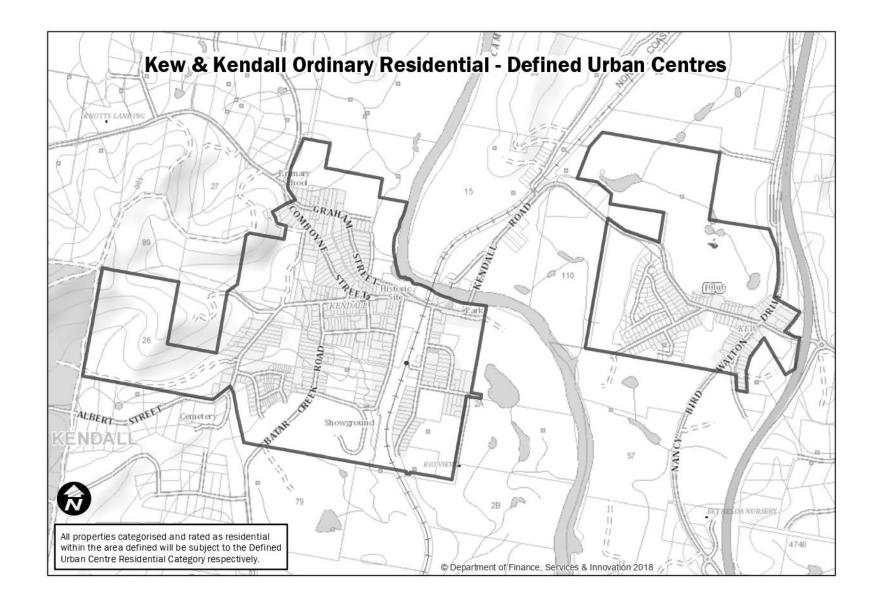
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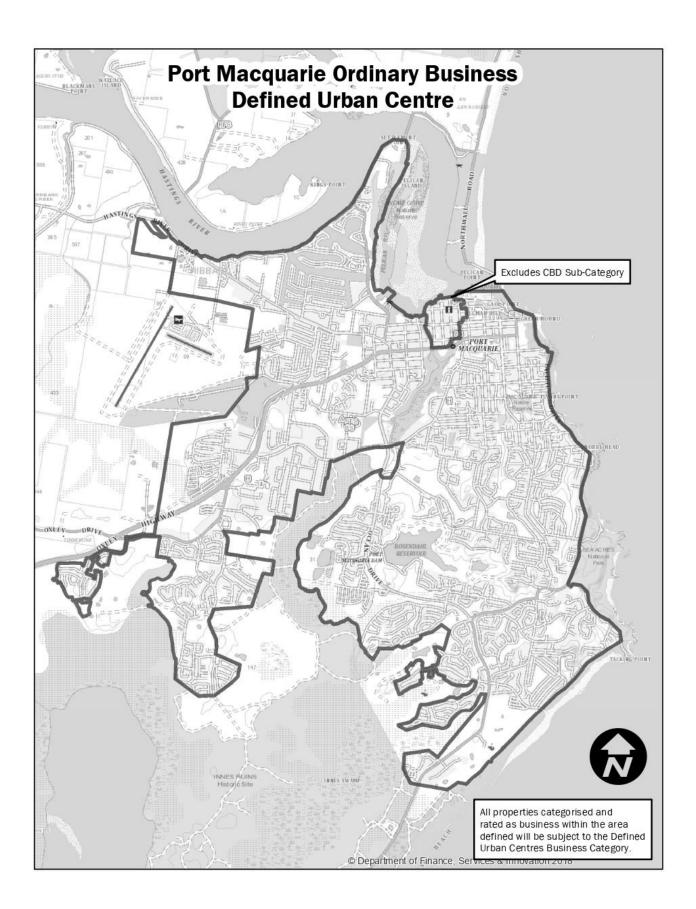
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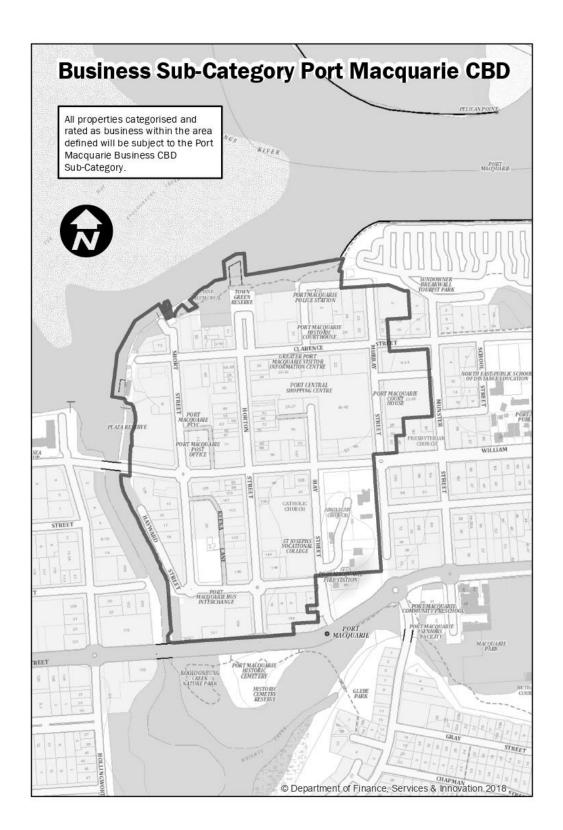


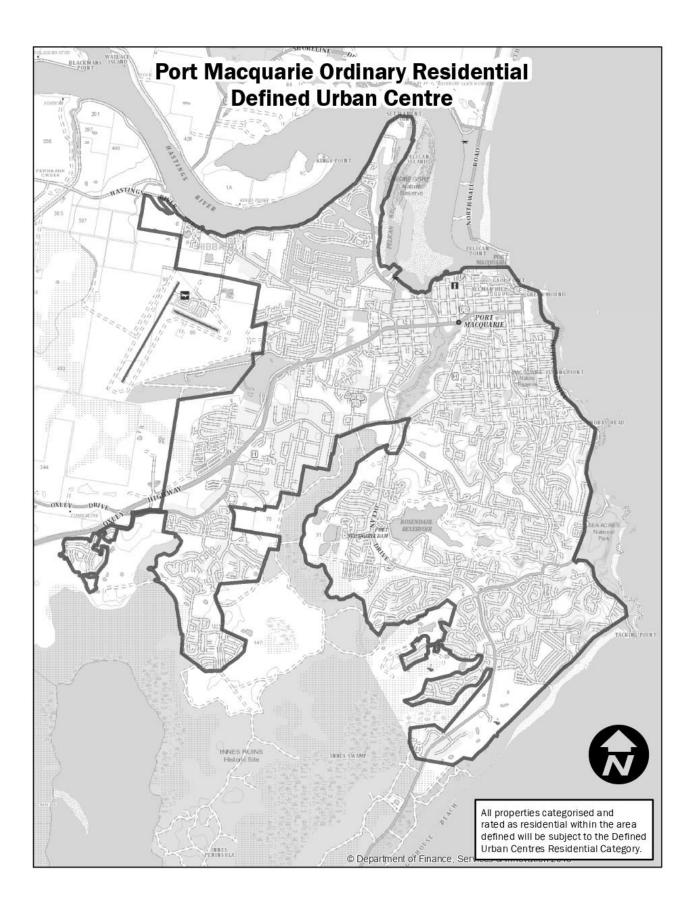
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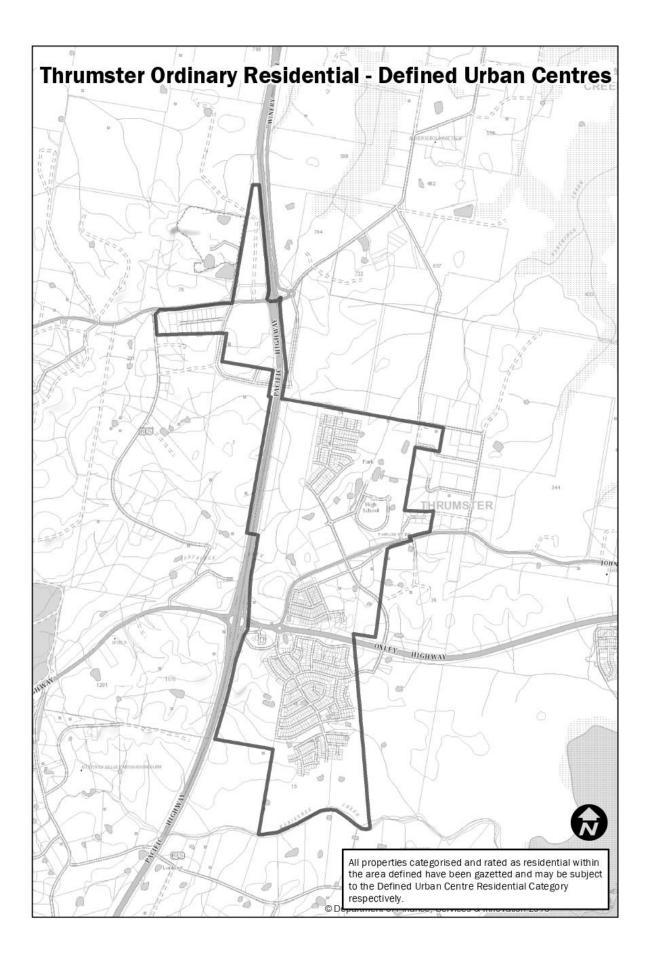


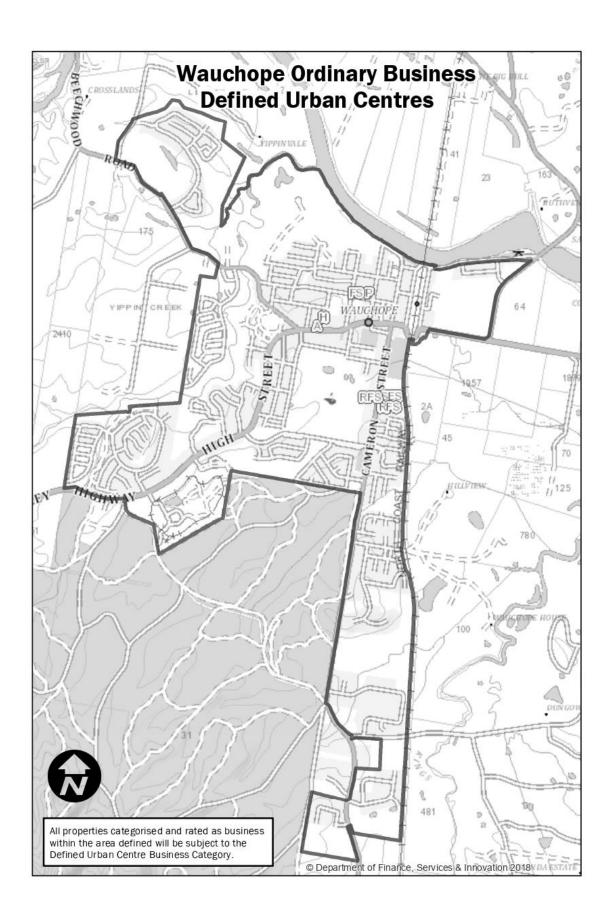


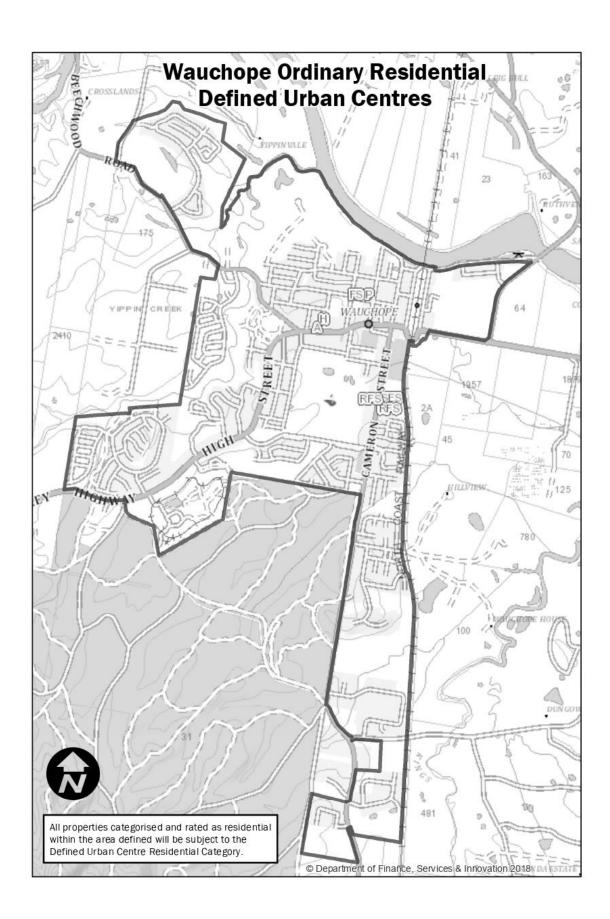












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Best-Practice Management of Water Supply and Sewerage Guidelines

H H	ų	Table Indicato	Table 1 – Required Outcomes for Best-Practice Criteria Indicators to Demonstrate Achievement of Outcome	Tools &	COAG/NCP/NWI/Statutory
(1) (2) (2)	Outcome (2)		(2)	saomosay	(4)
Strategic A current, sound Current SBP that includes: Business Strategic Business • Operating environment review Planning Plan (SBP) • Asset management plan (operating environment review Planning Rein (SBP) • Asset management plan (operating environment review Planning Plan (SBP) • Asset management plan (operating environment review Planning Function (SBP) • Asset management plan (operating environment review and financial plan. • Key performance indicators • Levels of service plan • Levels of service • Human resources plan • Address issues in Ref 1 and the C SBP and financial plan is one whithin the last 3 years.	· ·	 Current SBP that includes Operating environment Asset management pla Asset management plan is customer service plan Key performance indic Customer service plan Address issues in Ref 1 at SBP and financial plan is within the last 3 years. 	Current SBP that includes: Current SBP that includes: Operating environment review Asset management plan (operation, maintenance, capital works) Key performance indicators Customer service plan Levels of service Human resources plan Address issues in Ref 1 and the Check List* in Appendix A. A current SBP and financial plan is one which has been prepared or updated within the last 3 years.	Appendix A	Demonstrate long term financial sustainability of the business to comply with NCP and NWI. Page 12 of Ref 14.
Financial A robust financial Image: New State of the second and the check List* in / and the check List* in / address the issues in Ref 2 and the check List* in / address the issues covered by the relevant guide developments since publication of the guidelines.	• • A	 A robust minimum 20 year required stable typical resid Address the issues in Ref 2 Each check list is essent address the issues cover developments since public 	A robust minimum 20 year financial plan which identifies the lowest required stable typical residential bill (TRB). Address the issues in Ref 2 and the Check List* in Appendix A. * Each check list is essentially a road map to assist LWUs to quickly address the issues covered by the relevant guidelines as well as any developments since publication of the guidelines.	Appendix A	See above.
 Pricing ¹⁸ and (a) Full cost-recovery to reach of water Developer to reach of water supply and sewerage supply and sewerage businesses. Appropriate tariffs without significant cross-subsidies. Appropriate tariffs without significant cross-subsidies. Total annual income and projected TRB should be constructed and the substruct of the substructed and the subs	Full cost-recovery for each of water supply and sewerage businesses.	 Appropriate tariffs without s Total annual income and pr above financial plan. This g rate of return (ERRR). 	Appropriate tariffs without significant cross-subsidies. Total annual income and projected TRB should be consistent with above financial plan. This generally results in a positive economic real rate of return (ERRR).	Ref 4 Appendix B	Full cost-recovery with consumption based water supply pricing, trade waste charging and removal of cross-subsidies to comply with COAG Strategic Framework for Water Reform, NCP and NWI. Pages 12, 17, 18, 22 of Ref 14.

¹⁸ Agreement has been reached with the Local Government Association and the Shires Association of NSW to amendment of the Local Government Act 1993 in order to provide NSW LWUs with the option of using integrated water pricing for their water supply and sewerage services. An LWU implementing integrated water pricing in accordance with Attachment 2 on Page 35 will comply with elements 2 (a), 2 (b) and 2 (c) of Criterion 2 above.



	COAG/NCP/NWI/Statutory Requirements	(4)		
eria	Tools & Resources		Page 9 Ref 4 Page 10 Ref 4 Appendix B	Page 28 Ref 4 Page 29 Ref 4 Page 29 Ref 4
Table 1 – Required Outcomes for Best-Practice Criteria	Indicators to Demonstrate Achievement of Outcome	(3)	 Appropriate water usage charge/kL based on long-run marginal cost. Access charge relative to a customer's capacity requirements. No land value based charges (ie. rates) and no "free" or "pre-paid" water allowance. Any large increases in non-residential customer bills phased in over 5 years. To encourage water conservation, high water consuming residential customers should be subjected to a step price increase of at least 50% for incremental usage above a specified threshold. This threshold atould not exceed 450 kL/a per household, except for LWUs outside the DWE Coastal and Tablelands Zone with a high incidence of evaporative air coolers, where a threshold of up to 600kL/a per household may be used. LWUs with 4,000 or more connected properties to have at least 75% of residential revenue⁴ generated through usage charges by June 2008 (at least 50% required by June 2007). LWUs with under 4,000 connected properties to have at least 50% of residential water revenue⁴ generated through usage charges. LWUs with under 4,000 connected properties to have at least 50% of residential water revenue⁴ generated through usage charges. LWUs with under 4,000 connected properties to have at least 50% of residential water revenue⁴ generated through usage charges. LWUs with under 4,000 connected properties to have at least 50% of residential water revenue⁴ generated through usage charges. LWUs with under 4,000 connected properties to have at least 50% of residential water revenue⁴ generated through usage charges. LWUs may demonstrate compliance with this requirement on the basis of either (1) or (11) below. (1) their projected total residential levenue for the next financial year. (1) their projected total residential bill (on the basis of their average annual residential consumption per connected prosumers) for the next financial year. 	 Appropriate residential tariff. No land value based charges (ie. rates). Non-residential Two-part tariff.
	Required Outcome	(2)	Complying water supply tariff.	Complying sewerage tariff.
	Criterion	(1)	(q)	(c)

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Best-Practice Management of Water Supply and Sewerage Guidelines

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	COAG/NCP/NWI/Statutory Requirements	(4)			Sections 305 to 307 of Water Management Act 2000. Section 64 of Local Government Act 1993.	COAG, NCP and NWI – page 18 of Ref 14 Section 68 of Local Government Act 1993, Local Government (General) Regulation 2005.
eria	Tools & Resources		Page 31 Ref 4	Page 208 Ref 5 Page 209 Ref 5 Pages 209 to 212 Ref 5	Page iv Ref 6 Appendix B	Ref 5 Appendix A
Table 1 – Required Outcomes for Best-Practice Criteria	Indicators to Demonstrate Achievement of Outcome	(3)	 Appropriate sever usage charge/kL. Access charge that is reflective of the cost of providing these severage services. Any large increases in non-residential customer bills phased in over 5 years. 	 Annual trade waste fee for all liquid trade waste dischargers. Trade waste usage charge for dischargers with prescribed pre-treatment. Excess mass charges for large dischargers and industrial waste. 	 Development Servicing Plan[#] in accordance with Ref 6, with commercial developer charges. # LWUs with growth of under 5 lots/a exempted. 	 Liquid Trade Waste approvals issued in accordance with Ref 5. Liquid Trade Waste Policy adopted and implemented in accordance with Ref 5.
	Required Outcome	(2)		Complying liquid trade waste fees and charges for <u>all</u> liquid trade waste dischargers.	Commercial Developer Charges.	Liquid trade waste approval issued to each trade waste discharger.
	Criterion	(1)		(d)	Developer (e) Charges (DC)	Liquid Trade (f) Waste Approvals

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Criteria	Tools & COAG/NCP/NWI/Statutory Resources Requirements	(4)	Page 9 Ref 4 Appendix B	Appendix C COAG, NCP and NWI Page 52 of Ref 14 Water Management Act 2000.	Appendix D Water Management Act 2000. Ref 19 Local Government Act 1993.
Table 1 – Required Outcomes for Best-Practice Criteria	Indicators to Demonstrate Achievement of Outcome	(3)	 The potable water supply fariff in dual water supplies to comply with 2(b) above, except that step pricing is not a requirement. For the non-potable component of dual water supplies: LWUs are encouraged to install a non-potable water meter for each customer served where practical. Appropriate non-potable water usage charge/kL based on long-run marginal cost. Appropriate non-potable water allowance. No land value based charges (ie. rates) and no "free" or "pre-paid" non-potable water allowance. At least 50% of residential revenue⁺ generated through usage charges. Refer to the footnote to element 2 (b) above. 	 Sound water conservation and demand management implemented. Identification of most cost-effective demand management initiatives. Subsidisation and promotion of at least two of the identified demand management initiatives. Include demand monitoring, leakage measurement and reduction and community education. 	 Compile data on existing system, your LWU's drought management planning, including adoption of a schedule of trigger points for timely implementation of appropriate water restrictions. Sound drought management implemented in accordance with the LWU's adopted schedule.
	Required Outcome	(2)	Complying tariffs for dual water supplies.	Sound water conservation and demand management in place.	Sound drought management in place.
	Criterion	(1)	Dual Water (g) Supplies	3 Water Conservation	4 Drought Management

Best-Practice Management of Water Supply and Sewerage Guidelines



ORDINARY COUNCIL 16/06/2021

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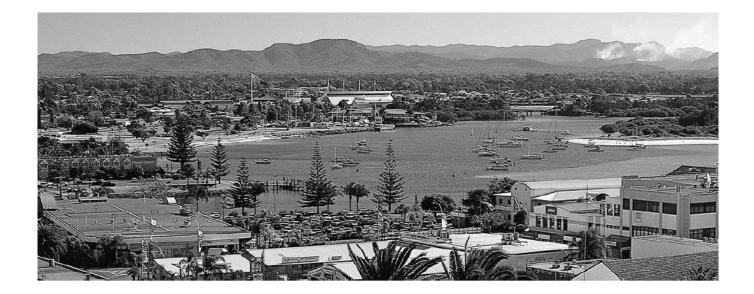
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Best-Practice Management of Water Supply and Sewerage Guidelines

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				Table 1 – Required Outcomes for Best-Practice Criteria	ria	
	Criterion	Required Outcome		Indicators to Demonstrate Achievement of Outcome	Tools & Resources	COAG/NCP/NWI/Statutory Requirements
	(1)	(2)		(3)		(4)
2	Performance Reporting	Completed performance reporting forms to DWE Review 2-page LWU Performance Report, prepare Action Plan.	• • • • •	Reporting forms provided to DWE by 15 September each year. Draft of Special Schedules 3 to 6 and Notes 2 and 3 of the LWU's Special Purpose Financial Reports provided to DWE by 15 September each year. LWUs with over 10,000 connected properties to arrange auditing of their core performance indicators in accordance with the auditing requirements of the <i>National Performance Framework</i> . Action Plan provided to Council following review of your LWU's 2-page Performance Report (water, sewerage). Statement of Compliance to Business Activities, a Compliance Audit Report and an unqualified independent Financial Audit Report).	Ref 3 Appendix E	COAG, NCP and NWI Page 31 of Ref 14, page 15 of Ref 15 and page 1 of Ref 16.
Q	Integrated Water Cycle Management (IWCM)	Sound IWCM implemented.	• • •	Completion of Integrated Water Cycle Management Evaluation by June 2007. Completion of Integrated Water Cycle Management Strategy by June 2008. Implementation of Integrated Water Cycle Management in accordance with the Strategy by June 2008.	Ref 8 Appendix F	COAG, NCP and NWI Page 43 of Ref 14. WMA 2000.





Port Macquarie-Hastings Council

Report on Audit of Best Practice Management of Water Supply and Sewerage Services in 2019/20

May 2021

A division of the Department of Regional NSW

Port Macquarie-Hastings Council

Report on Audit of Best Practice Management of Water Supply and Sewerage Services in 2019/20

May 2021

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Cover Image: PMH Council

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Public Works Advisory

PMHC – Best Practice Management Audit Report

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1. Report of Compliance

1.1 Introduction

Public Works Advisory were engaged by the Port Macquarie-Hastings Council (PMHC) to audit their water supply and sewerage services for compliance with the DPIE Water Best Practice Management Guidelines, May 2007 (BPM Guidelines) and the NSW Best Practice Management Framework for Water Supply and Sewerage, February 2019 (BPM Framework) for the reporting year 2019/20.

The audit involved off-site collection, collation and review of relevant documentary evidences, and this document is the audit report outlining the scope of audit and records the audit findings and conclusions regarding the compliance of Port Macquarie-Hastings Council (PMHC) with the BPM Guidelines and the BPM Framework for Water Supply and Sewerage services in the reporting year 2019/20.

1.2 Scope

We have performed the agreed procedures in accordance with our proposal for engagement by Port Macquarie-Hastings Council and described below with respect to the compliance of the PMHC's Water Supply and Sewerage services with the NSW Best Practice Management Guidelines, August 2007 as updated through the BPM Framework, February 2019 for the year ended 30 June 2020 based on relevant criteria as set forth in column (3) of Table 1 of the Guidelines. Our engagement was generally undertaken in accordance with Australian Auditing Standards applicable to agreed-upon procedures of engagements.

The responsibility for determining the adequacy or otherwise of the procedures agreed to be performed is that of Council and the DPIE Water. The procedures were performed solely to assist PMHC and DPIE Water in evaluating the validity of the compliance requirements and are summarised as follows:

- 1. We reviewed the current **Strategic Business Plan, September 2014** (SBP) to ensure that it included an:
 - Operating environment review
 - Total Asset management plan operation, maintenance, capital works
 - Key performance indicators

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- Customer service plan
- Levels of service
- Work Force plan
- We reviewed the Financial Plan as contained within the SBP to ensure that it covered a period of at least 20 years and it reports the lowest required stable typical residential bill (TRB)
- 3) We reviewed **full cost recovery** for both water supply and sewerage is consistent with the outcomes listed in column (3) of Table 1
- We reviewed water supply tariffs to confirm they complied with the outcomes listed in column (3) of Table 1
- 5) We reviewed the **sewerage tariffs** to confirm they complied with the outcomes listed in column (3) of Table 1
- 6) We reviewed the liquid trade waste fees and charges to confirm they complied with the outcomes listed in column (3) of Table 1
- We reviewed the commercial developer charges to confirm the existence of a Development Servicing Plan with commercial developer charges.
- 8) We reviewed the **liquid trade waste approvals** to confirm they complied with the outcomes listed in column (3) of Table 1
- 9) We reviewed the water conservation measures to confirm that it included the outcomes listed in column (3) of Table 1
- 10) We reviewed the **drought management** plan to confirm that it included the outcomes listed in column (3) of Table 1
- 11) We checked documentary evidence to ensure that **performance reporting** forms were completed and lodged to DPIE Water within the required time frame, and
- 12) We checked for completion and implementation of **IWCM Strategy** following substantial commencement of sound Integrated Water Cycle Management.

Public Works Advisory

PMHC – Best Practice Management Audit Report

1.3 Findings

We report as follows:

- a) With respect to (1) above, we found the Strategic Business Plan, September 2014 included the outcomes listed in column (3) of Table 1 of the Best-Practice Management Guidelines. Council has commenced the development of IWCM Strategy in accordance with the latest BPM Framework, hence review and update of the Strategic Business Plan was not due during the audit period being reported.
- b) With respect to (2) above, we found that PMHC have reported in the 2014 SBP the typical residential bills for water supply and sewerage arrived at using their "in-house" spreadsheet financial model. We also found that Council has undertaken annual update of the spreadsheet financial models for the water and sewer funds for 2019/20.
- c) With respect to (3) above, we found that the projected total annual water supply and sewerage services income in the Council's financial models provided full cost recovery, resulting in a positive economic real rate of return for 2019/20.
- d) With respect to (4) above, we found that water supply tariffs considered the outcomes listed in column (3) of Table 1 with 69.89% of residential revenue generated through residential usage charges for the year 2019/20. DPIE Water considers and reports Local Water Utilities that achieve 70-74% as *provisionally* complying with this best practice outcome and require them to improve their performance. To this end, Council reported that a phased restructure of water tariffs to achieve 75% of the residential revenue over a 3-year period starting from 2019/20 is being implemented; however, the strict water restriction regime implemented during 2019/20 due to the ongoing drought has resulted in reduced residential water usage, hence could not improve upon the 70.82% achieved in 2018/19.
- e) With respect to (5) above, we found that the residential sewerage tariffs considered the outcomes listed in column (3) of Table 1. We also found that not all categories of the non-residential customers are levied the sewerage usage charge as part of the adopted two-part tariff structure, and hence is **not fully compliant** with the BPM Guidelines.
- f) With respect to (6) above, we found that the liquid trade waste fees and charges considered the outcomes listed in column (3) of Table 1.
- g) With respect to (7) above, we found that the adopted Development Servicing Plans (DSPs) for Water Supply and Sewerage Services in September 2014 with the developer

charges calculated on the basis of the NSW Developer Charges Guidelines – Consultation Draft 2012. The Development Servicing Plans considered the outcomes listed in column (3) of Table 1 and noted that the Plans took into account the cross subsidy that will occur due to the adoption of uniform developer charges across all the DSP areas. Council reported that it plans to take-up the review and update the water and sewerage DSPs in accordance with the 2016 Developer Charges Guidelines in FY 2021/22 with a time-frame of 18 months to complete and adopte the same.

- h) With respect to (8) above, we found that the Liquid Trade Waste Policy amended and adopted in 29 August 2018 considered the outcomes listed in column (3) of Table 1 with regard to issue of liquid trade waste approvals.
- i) With respect to (9) above, we found that Council's Water Supply Policy, amended and adopted on 4 December 2018, considered implementation of a sound water conservation and demand management initiatives to achieve the outcomes listed in column (3) of Table 1. The initiatives are currently reviewed for update as part of IWCM Strategy development.
- j) With respect to (10) above, we found that Council's Water Supply Policy, amended and adopted on 4 December 2018, considered the outcomes listed in column (3) of Table 1 for sound drought management. The initiatives are currently reviewed for update as part of IWCM Strategy development.
- k) With respect to (11) above, we found that performance reporting forms for 2019/20 have been completed and lodged with DPIE Water within the required time frame.
- I) With respect to (12) above, we found that following a 'gap analysis' to review its IWCM Strategy completed in 2016, PMHC has engaged consultants to update the same to fully comply with IWCM checklist, February 2019. Council reported that the development and adoption of a compliant IWCM Strategy is planned to be completed by early 2022.

1.4 Conclusion

Based on the findings of our audit, we conclude that Port Macquarie-Hastings Council has demonstrated *'substantial compliance' with Best Practice Management of Water Supply and Sewerage* in line with the DPIE Water Guidelines as applicable for the year ended 30th June 2020.

Public Works Advisory

1.5 Definition

We have adopted the following definition for this engagement:

- "Substantial Compliance" means the level of compliance with the Guidelines such that any identified deficiencies do not detract from the general intent of the Guidelines to achieve Best Practice Management for Water Supply and/or Sewerage Services.
- What constitutes substantial compliance is also a function of at what point in time the issue is examined. Therefore, the best practice management adopted must take into account likely future scenarios and apply the current body of industry knowledge in regard to best practice.

1.6 Disclaimer

Our report is solely for the purpose set forth in the first paragraph of this report and for the information of Port Macquarie Hastings Council and the DPIE Water and is not to be used for any other purpose or distributed to any other party. This report relates only to the items specified above and does not extend to any financial report of the Council taken as a whole.

Signed:

Date signed: 17 May 2021

H. Arrof

Marimuthu Sundaravadivel Senior Engineer, Infrastructure Services Public Works Advisory 4 Parramatta Square Parramatta, NSW 2150



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INDEPENDENT AUDITOR'S REPORT

Report on the special purpose financial statements

Port Macquarie-Hastings Council

To the Councillors of Port Macquarie-Hastings Council

Opinion

I have audited the accompanying special purpose financial statements (the financial statements) of Port Macquarie-Hastings Council's (the Council) Declared Business Activities, which comprise the Statement by Councillors and Management, the Income Statement of each Declared Business Activity for the year ended 30 June 2020, the Statement of Financial Position of each Declared Business Activity as at 30 June 2020 and Note 1 Significant accounting policies for the Business Activities declared by Council.

The Declared Business Activities of the Council are:

- Water supply
- Sewerage
- Waste Management.

In my opinion, the financial statements present fairly, in all material respects, the financial position of the Council's declared Business Activities as at 30 June 2020, and their financial performance for the year then ended, in accordance with the Australian Accounting Standards described in Note 1 and the Local Government Code of Accounting Practice and Financial Reporting – update number 28 (LG Code).

My opinion should be read in conjunction with the rest of this report.

Basis for Opinion

I conducted my audit in accordance with Australian Auditing Standards. My responsibilities under the standards are described in the 'Auditor's Responsibilities for the Audit of the Financial Statements' section of my report.

I am independent of the Council in accordance with the requirements of the:

- Australian Auditing Standards
- Accounting Professional and Ethical Standards Board's APES 110 'Code of Ethics for Professional Accountants (including Independence Standards)' (APES 110).

I have fulfilled my other ethical responsibilities in accordance with APES 110.

Parliament promotes independence by ensuring the Auditor-General and the Audit Office of New South Wales are not compromised in their roles by:

- providing that only Parliament, and not the executive government, can remove an Auditor-General
- mandating the Auditor-General as the auditor of councils
- precluding the Auditor-General from providing non-audit services.

I believe the audit evidence I have obtained is sufficient and appropriate to provide a basis for my audit opinion.

Emphasis of Matter - Basis of Accounting

Without modifying my opinion, I draw attention to Note 1 to the financial statements which describes the basis of accounting. The financial statements have been prepared for the purpose of fulfilling the Council's financial reporting responsibilities under the LG Code. As a result, the financial statements may not be suitable for another purpose.

Other Information

The Council's annual report for the year ended 30 June 2020 includes other information in addition to the financial statements and my Independent Auditor's Report thereon. The Councillors are responsible for the other information. At the date of this Independent Auditor's Report, the other information I have received comprise the general purpose financial statements and Special Schedules (the Schedules).

My opinion on the financial statements does not cover the other information. Accordingly, I do not express any form of assurance conclusion on the other information. However, as required by the *Local Government Act 1993*, I have separately expressed an opinion on the general purpose financial statements and Special Schedule 'Permissible income for general rates'.

In connection with my audit of the financial statements, my responsibility is to read the other information and, in doing so, consider whether the other information is materially inconsistent with the financial statements or my knowledge obtained in the audit, or otherwise appears to be materially misstated.

If, based on the work I have performed, I conclude there is a material misstatement of the other information, I must report that fact.

I have nothing to report in this regard.

The Councillors' Responsibilities for the Financial Statements

The Councillors are responsible for the preparation and fair presentation of the financial statements and for determining that the accounting policies, described in Note 1 to the financial statements, are appropriate to meet the requirements in the LG Code. The Councillors' responsibility also includes such internal control as the Councillors determine is necessary to enable the preparation and fair presentation of the financial statements that are free from material misstatement, whether due to fraud or error.

In preparing the financial statements, the Councillors are responsible for assessing the Council's ability to continue as a going concern, disclosing, as applicable, matters related to going concern and using the going concern basis of accounting.

Auditor's Responsibilities for the Audit of the Financial Statements

My objectives are to:

- obtain reasonable assurance about whether the financial statements as a whole are free from material misstatement, whether due to fraud or error
- issue an Independent Auditor's Report including my opinion.

Reasonable assurance is a high level of assurance, but does not guarantee an audit conducted in accordance with Australian Auditing Standards will always detect material misstatements. Misstatements can arise from fraud or error. Misstatements are considered material if, individually or in aggregate, they could reasonably be expected to influence the economic decisions users take based on the financial statements.

A description of my responsibilities for the audit of the financial statements is located at the Auditing and Assurance Standards Board website at: <u>www.auasb.gov.au/auditors_responsibilities/ar4.pdf</u>. The description forms part of my auditor's report.

The scope of my audit does not include, nor provide assurance:

- that the Council carried out its activities effectively, efficiently and economically
- about the security and controls over the electronic publication of the audited financial statements on any website where they may be presented
- about any other information which may have been hyperlinked to/from the financial statements.

Goard Fingerald

Gearoid Fitzgerald Delegate of the Auditor-General for New South Wales

29 October 2020 SYDNEY



Independent Auditor's Review Report To the Councillors' of Port Macquarie-Hastings Council

Report on the 2020 Overhead Reallocation Charges to Water and Sewer Business Activities

We have reviewed the methodology and calculations supporting the reallocation of overheads from general fund to water and sewer funds as disclosed in the Port Macquarie-Hastings Council (Council) special purpose financial statements for the year ended 30 June 2020.

Management's Responsibility for the Income Statement

Management of Council is responsible for determining the methodology supporting the calculation of overheads and their reallocation to the water and sewer business activities so they reflect a fair and reasonable assignment of costs to each business activity. This responsibility includes establishing and maintaining internal control relevant to the calculation and reallocation of overheads to each business activity so they reasonably reflect the costs incurred by each business activity and they are free from material misstatement, whether due to fraud or error.

Auditor's Responsibility

Our responsibility is to express a conclusion on the allocation of overheads to each business activity based on our review. We conducted our review in accordance with Auditing Standard on Review Engagements ASRE 2405 *Review of Historical Information Other Than a Financial Report* in order to state whether, on the basis of the procedures described, we have become aware of any matter that makes us believe that the overhead reallocations to the water and sewer business activities in the 2020 special purpose financial statements is not fair and reasonable. No opinion is expressed as to whether the overhead calculation methodology used by Council is appropriate to the needs of the Council.

A review consists of making enquiries, primarily of persons responsible for financial and accounting matters, and applying analytical and other review procedures. A review is substantially less in scope than an audit conducted in accordance with Australian Auditing Standards and consequently does not enable us to obtain assurance that we would become aware of all significant matters that might be identified in an audit. Accordingly, we do not express an audit opinion.

Independence

In conducting our review, we have complied with the independence requirements of the Accounting Professional and Ethical Standards Board.

Conclusion

Based on our review, which is not an audit, nothing has come to our attention that causes us to believe that the reallocation of overheads to Council's water and sewer business activities disclosed in the special purpose financial statements for the year ended 30 June 2020 is not fair and reasonable.

THOMAS NOBLE & RUSSELL CHARTERED ACCOUNTANTS

Per: **K R FRANEY** (Partner)

Dated at Lismore this 25th day of May 2021

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BUDGET VARIATIONS - May 2021											
Section	Project	Project Description	Capital/ Operating	Division	Full Year Original Budget	Full Year Current Budget	Actuals to May 2021	New Yearly Proposed Budget - May 2021	Movement	Funding Source	EFFEC OI FUNDING POSITIO
Grants & Other Funding											
To recognise additional Grant Fund	ling received fro	om Transport for NSW for Hill Street C	Comboyne Pede	strian Facilities.							
Transport & Traffic	41464	Footpath, Cycleway & Pedestrian Management	Capital	_ Infrastructure	500,000	600,000	521,766	683,750	-83,750	Grant	
Transport & Traffic	19342	Grants Funding - Capital	Capital		-6,119,089	-6,076,563	-17,361,928	-6,160,313	83,750	Grant	(
Grant received from Department of	Planning, Indus	stry & Environment for Regional NSW	Online DA Plan	ning Portal.							
Digital Technology	42178	Transition to Online DA Planning	Capital	Development &	0	0	59,095	50,000	-50,000	Grant	(
Digital Technology	19092	Grant Funding - Capital	Capital	Environment	0	0	-50,000	-50,000	50,000	Grant	(
Total Grants & Other Funding									133,750		(
Reserve Movements											
Reserve funding required for the Po	olice Precinct C	oncept project.									
PM Town Centre Masterplan	42183	Police Precinct Concept	Operating	Development &	0	0	0	26,000	-26,000	Reserve	(
PM Town Centre Masterplan	19359	Transfer From Reserve	Operating	Environment	0	-196,075	0	-222,075	26,000	Reserve	(
Total Reserve Movements									26,000		(
Movement between Projects											
Transfer from Disabled Access Bud	lget to Fund Ac	cessible Chairlift at Port Macquarie Po	ool & Accessibl	e Car Parks at Li	ghthouse Beac	:h.					
Facilities	40415	Building Rectification Works	Capital	Development &	317,800	350,767	261,740	377,767	-27,000	Revenue	-27,000
Transport & Traffic	660	Sealed Road Maintenance	Operating	Environment/ Infrastructure/ Strategy &	2,155,956	2,137,740	1,608,390	2,147,740	-10,000	Revenue	-10,00
Community Inclusion	40212	Disabled Access	Capital	Growth	109,000	81,000	0	44,000	37,000	Revenue	37,000

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BUDGET VARIATIONS - May 2021											
Section	Project	Project Description	Capital/ Operating	Division	Full Year Original Budget	Full Year Current Budget	Actuals to May 2021	New Yearly Proposed Budget - May 2021	Movement	Funding Source	EFFECT ON FUNDING POSITION
Accounting entry only - transfer of sa	laries budgets	s within Natural Resources.									
Natural Resources	370	Natural Resources	Operating	Development &	729,712	755,559	800,285	898,480	-142,921	Reserve	0
Natural Resources	392	Bush Regeneration Team	Operating	Environment	496,084	491,646	326,419	348,725	142,921	Reserve	0
Transfer of Funds from Consultancies	s Budget to Pi	rojects (those projects bid for as part of	the Consulta	ncies Bidding 20	/21 process).						
Development Assessment	255	Development Assessment Planning	Operating	Development &	1,167,816	1,246,468	1,111,056	1,252,738	-6,270	Revenue	-6,270
Procurement & Stores	328	Procurement	Operating	Environment/ Corporate	474,236	474,236	369,364	486,236	-12,000	Revenue	-12,000
Corporate Performance Management	323	Consultancies - Council Wide	Operating	- Performance	99,996	60,000	0	41,730	18,270	Revenue	18,270
Transfer of portion of Community Pla	n funding to a	Footpaths Project, to construct a missi	ng link footpa	ath in Bonny Hill	s.			·			
Transport & Traffic	41464	Footpath, Cycleway & Pedestrian Management	Capital	Infrastructure/ Strategy &	500,000	683,750	521,766	713,750	-30,000	Reserve	0
Community Engagement	41961	Community Plans	Operating	Growth	100,000	100,000	5,885	70,000	30,000	Reserve	0
Budget adjustment to support the cor	npletion of th	e Aus Spec review.									
Transport & Traffic	41481	Additional Design Works Funding	Capital	- Infrastructure	50,000	50,000	14,140	70,000	-20,000	Reserve	0
Drainage	40619	Stormwater Remediation Designs	Capital	- innastructure	0	29,466	3,544	9,466	20,000	Reserve	0
Accounting entry only - transfer of bu	dgets within I	Parks & Recreation.									
Parks & Recreation	42047	Port Macquarie Regional Sport Stadium Upgrade	Capital	Development &	150,000	150,000	0	326,550	-176,550	Reserve	0
Parks & Recreation	42061	Sporting Infrastructure Renewals	Capital	Environment	240,000	176,550	0	0	176,550	Reserve	0
Total Movements between Projects									424,741		0

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BUDGET VARIATIONS - May 2021	-										
Section	Project	Project Description	Capital/ Operating	Division	Full Year Original Budget	Full Year Current Budget	Actuals to May 2021	New Yearly Proposed Budget - May 2021	Movement	Funding Source	EFFECT ON FUNDING POSITION
Budget Variation Requests - Approv	ed by Executiv	/e									
Budget Variance Request for the Ca	r Parking Polic	cy and Action Plan Project.									
Transport & Traffic	41934	Carparking Strategy Review	Operating		0	64,963	11,900	154,963	-90,000	Revenue	-90,000
Transport & Traffic	41464	Footpath, Cycleway & Pedestrian Management	Capital	Infrastructure	500,000	713,750	521,766	663,750	50,000	Revenue	50,000
Transport & Traffic	40141	Bridge and Pavement Tests	Operating		50,000	50,000	0	10,000	40,000	Revenue	40,000
Budget Variance Request for Water	Network maint	enance, increased activities such as Ice	Pigging and	Reservoir cleani	ng.						
Water Supply	2700	Planned Maintenance - Water Network	Operating		759,984	759,984	1,633,090	1,900,000	-1,140,016	Reserve	0
Water Supply	20223	Site Security Upgrades	Capital	Capital Capital		250,000	0	20,000	230,000	Reserve	0
Water Supply	20219	Installation of Baffles to Ensure Chlorine Contact	Capital			300,000	0	0	300,000	Reserve	0
Water Supply	20222	Install Solar Panels at Wauchope WTP	Capital		70,000	70,000	0	0	70,000	Reserve	0
Water Supply	20217	Widderson St Reservoir Preconstruction of Roof Replacement	Capital	Infrastructure	50,000	50,000	0	0	50,000	Reserve	0
Water Supply	20226	Replacement of 3 Villages Inlet Platform Structure	Capital		90,000	90,000	0	0	90,000	Reserve	0
Water Supply	20220	Annual Energy Efficiency Initiatives - VSD Pumps	Capital		150,000	150,000	95,333	100,000	50,000	Reserve	0
Water Supply	29251	Renewals and Minor Works	Capital		900,000	845,486	501,525	545,470	300,016	Reserve	0
Water Supply	20152	Beechwood Rosewood Reservoir - Beechwood Connection	Capital		0	69,479	5,911	19,479	50,000	Reserve	0
Budget Variance Request for Gordo	n Street Rehab	ilitation - Accounting entry only to reallo	cate funding.								
Transport & Traffic	19342	Grant Funding - Capital	Capital		-6,119,089	-6,160,313	-17,528,047	-4,929,298	-1,231,015	Grant	0
Transport & Traffic	41465	Road Resealing Works	Capital	Infrastructure	2,080,077	2,080,077	1,007,006	1,262,018	818,059	Grant	C
Transport & Traffic	669	Capitalised Works - Maintenance Funding	Operating		425,502	425,502	12,546	12,546	412,956	Grant	0

BUDGET VARIATIONS - May 2021											
Section	Project	Project Description	Capital/ Operating	Division	Full Year Original Budget	Full Year Current Budget	Actuals to May 2021	New Yearly Proposed Budget - May 2021	Movement	Funding Source	EFFECT ON FUNDING POSITION
Budget Variance Request for Jindalee	e Road Aspha	lting.									
Transport & Traffic	42075	Jindalee Road - Resealing/ Resurfacing	Capital		0	0	1,238,895	1,238,894	-1,238,894	Revenue/ Reserve	-782,481
Transport & Traffic	41259	High Traffic Road Resurfacing	Capital	Infrastructure	957,692	988,504	50,670	206,023	782,481	Revenue	782,481
Transport & Traffic	19346	Section 7.11 Funding	Operating		-4,249,607	-3,974,193	0	-4,430,606	456,413	Reserve	0
Budget Variance Request for Comboyne Road Stabilisation.											
Transport & Traffic	668	Regional Roads Block Grant Funding	Operating		1,643,994	1,643,994	1,974,281	1,974,581	-330,587	Revenue/ Reserve	-41,176
Transport & Traffic	662	Sealed Roads Repairs - Resealing	Operating	Infrastructure	747,735	747,735	32,786	706,559	41,176	Revenue	41,176
Transport & Traffic	19346	Section 7.11 Funding	Operating		-4,249,607	-4,430,606	0	-4,720,017	289,411	Reserve	0
Total Budget Variations approved by	Executive		-						4,030,512		0
Works Program Amounts Re-allocate	d from 20/21	to 21/22									
Project movements between financial	years										
Transport & Traffic	42069	Timber Bridge Program	Capital		1,200,000	700,000	0	350,000	350,000	Loans	C
Transport & Traffic	19348	Loan Funding	Capital	 Infrastructure 	-1,200,000	-700,000	0	-350,000	-350,000	Loans	C
	1		1						350,000		0

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BUDGET VARIATIONS - May 2021											
Section	Project	Project Description	Capital/ Operating	Division	Full Year Original Budget	Full Year Current Budget	Actuals to May 2021	New Yearly Proposed Budget - May 2021	Movement	t Funding Source	EFFECT ON FUNDING POSITION
ORGANISATI	ONAL TOTAL	- THIS REVIEW	1					-	4,965,003	3	0
FORECAST FOR FIN	ANCIAL YEAR	RENDED 30 JUNE 2021									
FOREC	AST FOR 30 J	Original Budget as at 1 July 2020 <u>Plus: Adjustments</u> July Review August Review September Review October Review November Review January Review February Review March Review April Review May Review UNE 2021			Shortfall Surplus Balanced Balanced Surplus Balanced Surplus Balanced Surplus Balanced Surplus	-676,730 0 151,102 63,165 0 0 117,330 0 220,000 161,799 0 36,666					
Notes:	1	The result shown above is the general fur	nd result. All s	urpluses/deficits	in the water, sewe	erage and was	te funds are tr	ansferred to/fron	n reserves.		
	2 3	Reserve are internal restrictions that hold credited/debited to that reserve. Council projects are funded from a variety Revenue - All funds that are generated th	, of funding so	ources. Below is	a definition of the	various types (of funding tha	t are used to fund	d projects.		
		considers appropriate. Grants - Government grants can either be of a road. Untied grants may be applied t				d. Tied grants	are required	to be used for a	specific purpo	ose such as the	e construction
		Contributions - Contributions are non-reci contributor. Examples are contributions g					ired to give va	alue in exchange	for the contri	ibutions directly	∕ to the
		Reserves - Reserves are internal restriction credited/debited to that reserve.	ons held for a	specific purpose	, e.g. The airport i	has its own res	erve and all i	ncome and expe	nditure relatir	ng to the airpor	t is
		S7.11 and S64 Contributions - Section 7. government with a formal legal framework	11 of the NSW (for levying de	/ Environmental evelopers for the	and Planning Act (provision of infras	1979) and sec tructure, servio	tion 64 of the ces and amen	Local Governme ities - known as	nt Act (1993) developer co	provides NSW ntributions.	/ local
	4 Some projects are funded by multiple funding sources, e.g. a capital project may be funded by s7.11 funds, grants and revenue. The effect on capital column will only show the revenue funding adjustment as the other types of funding will have an income line budget adjustment shown in the report.							only show the			



Monthly Investment Report

May 2021



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Executive Summary

Compliance

Compliance Measure	Within Policy Limits (Y/N)	Reason if Not Compliant
Term to Maturity	Yes – Compliant	n/a
Counterparty	Yes – Compliant	n/a
Credit Quality	Yes – Compliant	n/a

<u>Performance</u>

As at 31/05/2021	1m (actual)	1m (% p.a.)	FYTD (actual)	FYTD (% p.a.)
AusBond Bank Bill Index	0.00%	0.05%	0.06%	0.06%
Council's Portfolio^	0.16%	1.87%	1.90%	2.08%
Outperformance	0.16%	1.82%	1.84%	2.02%

^Total portfolio performance excludes Council's cash account holdings. Overall returns would be lower if cash was included.

Impact of COVID-19 to Council's Portfolio

COVID-19 has adversely impacted financial markets, which in turn, has also affected Council's investment portfolio. We provide a quick summary in this section.

With regards to financial markets, shares (equities) experienced a significant correction in March 2020 but have since, recovered substantially due to the unprecedented fiscal and monetary policy support from global central banks and governments. **The RBA cut rates to record lows on 3rd November 2020 to 0.10%.** Equities have continued their rally over the course of 2021 with the accelerated rollout of multiple vaccines and ongoing fiscal stimulus. Longer-term bond yields have also risen over the past few months on the prospects of higher inflation over coming years.

With regards to the medium-longer term outlook for financial markets, of importance is the RBA's outlook and stance on the current situation:

- 1. The RBA's official cash rate will remain unchanged at its emergency level of 0.10% until its objectives of full employment and inflation are reached.
- 2. They suggested the NAIRU could be in the low 4s, or even the high 3s, well below the current unemployment rate of ~5½%;
- 3. <u>The Board does not expect the conditions for a rate hike "to be met until 2024 at the</u> <u>earliest</u>".

The biggest impact to Council's investment portfolio is with regards to its largest exposure being assets held in bank term deposits, which accounts for around ~91% of Council's total investment, and cash (~8% of the total investment portfolio). The biggest risk that PMHC faces over the medium-longer term in this environment is not the potential loss of capital (given all the banks are well capitalised and regulated by APRA), but the rapid loss of interest income as interest rates have plummeted.

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Council's term deposit portfolio was yielding 1.86% p.a. at month-end, with a weighted average duration of around 653 days or 1.79 years. This average duration will provide some income protection against the low interest rate environment over the next 18 months. As existing deposits mature however, they will inevitably be reinvested at much lower prevailing rates.

We note the current interest rates in the term deposit market:

- The highest deposit rate from any rated ADI in the market is now ~1.40% p.a. for 5 years;
- The highest deposit rates amongst the "AA-" rated ADIs (major banks) is now yielding between 0.40%-1.30% p.a. (depending on term);
- The highest deposit rates amongst the "A" rated ADIs was yielding between 0.60%-1.40% p.a. (depending on term);
- The highest deposit rates amongst the "BBB" rated ADIs was yielding between 0.40%-1.10% p.a. (depending on term).

Given official rates have fallen to record lows, Council is likely to see a rapid decline in interest income over future financial years. Its budgeted income over the <u>medium-longer term</u> needs to be revised to reflect the low interest rate environment. Returns between 0.40%-0.80% p.a. may potentially be the "norm" over the next few financial years, especially if rolling the majority of surplus funds for terms less than 2-3 years. Yields may in fact be lower if electing to invest for terms under 12 months.

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Council's Portfolio

Asset Allocation

The portfolio is predominately directed to fixed term deposits (91.40%). The remainder of the portfolio is directed to the overnight cash account with Westpac (7.70%) and the single FRN with Bendigo-Adelaide (0.90%).

With the RBA cutting interest rates in November 2020 to 0.10%, the priority should be to lock in any remaining attractive medium-longer dated fixed deposits or senior fixed bonds that may still be available to address reinvestment risk as margins continue to compress.

	TD - 91.40%
FRN - 0.90%	
CASH - 7.70%	

Term to Maturity

All maturity limits (minimum and maximum) comply with the Investment Policy. Medium-Term (3-5½ years) assets account for around 25% of the total investment portfolio, with capacity of around \$51m at month-end.

Limits	Compliant Non-compliant			
100%				
75%				
50%				
25%				
0%	0 - 365 deys	1-3) years	3 · 5.5 years

Where there is (counterparty) capacity to invest in attractive 3-5½ year investments, we recommend this be allocated to new any remaining attractive fixed term deposits or fixed bonds (refer to respective sections below).

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Compliant	Horizon	Invested (\$)	Invested (%)	Min. Limit (%)	Max. Limit (%)	Available (\$)
~	0 - 365 days	\$155,105,610	45.80%	0%	100%	\$183,553,937
\checkmark	1 - 3 years	\$99,500,000	29.38%	0%	70%	\$137,561,683
\checkmark	3 - 5.5 years	\$84,053,937	24.82%	0%	40%	\$51,409,882
\checkmark	5.5 - 10 years	\$0	0.00%	0%	10%	\$33,865,955
		\$338,659,547	100.00%			

Counterparty

As at the end of May, Council did not have an overweight position to any single ADI. Overall, the portfolio is diversified across the investment grade credit spectrum (rated BBB- or higher), with no exposure to unrated ADIs.

Compliant	lssuer	Rating	Invested (\$)	Invested (%)	Max. Limit (%)	Available (\$)
~	NAB	AA-	\$92,500,000	27.31%	30.00%	\$9,097,864
\checkmark	WBC (St George)	AA-	\$71,605,610	21.14%	30.00%	\$29,992,254
~	Rabobank	A+	\$13,000,000	3.84%	20.00%	\$54,731,909
\checkmark	ICBC Sydney	А	\$59,500,000	17.57%	20.00%	\$8,231,909
\checkmark	ING Bank Aus.	А	\$15,500,000	4.58%	20.00%	\$52,231,909
\checkmark	Aus Military Bank	BBB+	\$7,500,000	2.21%	10.00%	\$26,365,955
~	BOQ	BBB+	\$22,000,000	6.50%	10.00%	\$11,865,955
\checkmark	Bendigo	BBB+	\$3,053,937	0.90%	10.00%	\$30,812,018
\checkmark	Auswide	BBB	\$24,000,000	7.09%	10.00%	\$9,865,955
\checkmark	MyState	BBB	\$13,000,000	3.84%	10.00%	\$20,865,955
~	Newcastle PBS	BBB	\$17,000,000	5.02%	10.00%	\$16,865,955
			\$338,659,547	100.00%		

We remain supportive of the regional and unrated ADI sector (and have been even throughout the GFC period). They continue to remain solid, incorporate strong balance sheets, while exhibiting high levels of capital – typically, much higher compared to the higher rated ADIs. Some unrated ADIs have up to 25-40% more capital than the domestic major banks, and well above the Basel III requirements.

RBA Governor Lowe has commented that they have not seen any signs of stress in the financial system and that unlike during the GFC, the banks (all ADIs) now have cash, are well capitalised and are acting as "shock absorbers" in the current crisis.

Overall, the lower rated ADIs (BBB and unrated) are generally now in a better financial position then they have been historically (see the Capital Ratio figure below). We believe that deposit investments with the lower rated ADIs should be continued going forward, particularly when they offer 'above market' specials. Not only would it diversify the investment portfolio and reduce credit risk, it would also improve the portfolio's overall returns.

In the current environment of high regulation and scrutiny, all domestic (and international) ADIs continue to carry high levels of capital. There is minimal (if any) probability of any ADI defaulting on

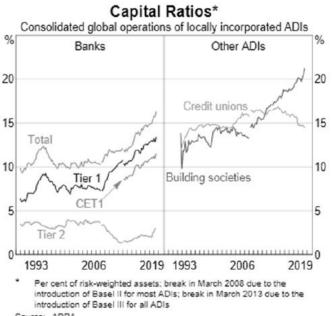
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their deposits going forward – this was stress tested during the GFC. APRA's mandate is to "protect depositors" and provide "financial stability".

The biggest single risk that depositors face in the current low interest rate environment is not capital or credit risk, but reinvestment risk. Interest rates are now at their effective lower bound of 0.10%.



Source: APRA

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Fossil Fuel Investments

What is Council's current exposure to institutions that fund fossil fuels?

Using the following link <u>http://www.marketforces.org.au/banks/compare</u>, based on the Council's investment portfolio balance as at 31/05/2021 (\$338.67m), we can roughly estimate that ~60% of the investments have some form of exposure.

How would Council modify its Investment Policy to cater for divestment of fossil fuels?

If the major banks were withdrawn from investments, some members of the community may look at that remaining list of ADIs (banks) and say "Why the do we have all our money with those no-name institutions? I've never even heard of them. We don't want to take risks with our money after councils lost \$100's of millions speculating in the GFC". It will be difficult to please everyone. We suggest starting the discussion with all Councillors asking "are you comfortable investing all the money with the lower rated regionals and credit unions?" If not, then a full divestment campaign will be complicated.

Some ways to potentially 'make changes' to the policy, or at least have a discussion, includes:

- "Where possible within policy and without compromising the risk and return profile, we favour..."
- "We have not yet made a decision to divest because it will have implications on credit quality, ratings and income, but we are actively discussing..."

What would be implications on our portfolio credit rating?

By adopting a free fossil fuel policy or an active divestment strategy, this would eliminate the major banks rated "AA-" as well as some other "A" rated banks (AMP, BoQ and ING). Council would be left with a smaller sub-sector of banks to choose to invest with.

What would be risks and implications on Council's portfolio performance?

Some implications include:

- High concentration risk limiting Council to a selected number of banks;
- Increased credit/counterparty risk;
- May lead to a reduction in performance (most of the senior FRN issues are with the higher rated ADIs) which could result in a significant loss of income generated – could be in excess of hundreds of thousands of dollars per annum;
- Underperformance compared to other Councils.

It may be contrary to Council's primary objective to preserve capital as the investment portfolio's risk would increase (all things being equal). Council may not be maximising its returns – this is one of the primary objectives written in the Investment Policy.





Counterparty	Credit Rating	Funding Fossil Fuel
NAB	AA-	Yes
WBC (St George)	AA-	Yes
Rabobank	A+	No
ICBC, Sydney	А	No
ING Bank Australia	A	Yes
Australian Military	BBB+	No
BoQ	BBB+	Yes
Bendigo-Adelaide	BBB+	No
Auswide	BBB	No
MyState Bank	BBB	No
Newcastle Permanent	BBB	No

Council's exposure is summarised as follows:

^^The underlying exposure in these managed funds includes the domestic major banks. Source: https://www.marketforces.org.au/info/compare-bank-table/

Summary	Amount	Invested %
Yes	\$201,605,610	60%
No	\$137,053,937	40%
	\$338,659,547	100%

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Credit Quality

The portfolio remains lightly diversified from a credit ratings perspective. The portfolio is entirely directed to the investment grade ADIs (BBB- or higher), with zero allocation to unrated ADIs. There is high capacity to invest in the higher rated ADIs (A or higher), particularly after the downgrades of BoQ and Bendigo-Adelaide Bank in May 2017.

Following the adoption of a new policy, there is now more capacity to invest with the "BBB" rated ADIs (~\$32m remaining at month-end), although most ADIs in this sector are currently full on liquidity due to the combination of low credit growth and the term funding facility (TFF) provided by the RBA (access to cheap funding).

If there are any attractive deposits being offered in the "BBB" rated sector (outside of BoQ, which Council is close to maximum limits), we will inform Council to take advantage and invest accordingly.

Compliant	Credit Rating	Invested (\$)	Invested (%)	Max. Limit (%)	Available (\$)
√	AA Category	\$164,105,610	48.46%	100%	\$174,553,937
1	A Category	\$88,000,000	25.98%	60%	\$115,195,728
\checkmark	BBB Category	\$86,553,937	25.56%	35%	\$31,976,904
~	Unrated ADIs	\$0	0.00%	5%	\$16,932,977
		\$338,659,547	100.00%		

All ratings categories are within the current Policy limits:

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Performance

Council's performance for the month ending 31 May 2021 is summarised as follows:

Performance (Actual)	1 month	3 months	6 months	FYTD	1 year
Official Cash Rate	0.01%	0.03%	0.05%	0.14%	0.16%
AusBond Bank Bill Index	0.00%	0.01%	0.01%	0.06%	0.06%
Council's T/D Portfolio	0.16%	0.47%	0.96%	1.91%	2.12%
Council's FRN Portfolio	0.09%	0.25%	0.50%	0.94%	1.04%
Council's Portfolio^	0.16%	0.47%	0.96%	1.90%	2.11%
Outperformance	0.15%	0.46%	0.95%	1.85%	2.05%

^Total portfolio performance excludes Council's cash account holdings. Overall returns would be lower if cash was included.

Performance (Annualised)	1 month	3 months	6 months	FYTD	1 year
Official Cash Rate	0.10%	0.10%	0.10%	0.16%	0.16%
AusBond Bank Bill Index	0.05%	0.04%	0.02%	0.06%	0.06%
Council's T/D Portfolio	1.88%	1.89%	1.94%	2.09%	2.12%
Council's FRN Portfolio	1.01%	1.01%	1.00%	1.03%	1.04%
Council's Portfolio^	1.87%	1.88%	1.93%	2.08%	2.11%
Outperformance	1.82%	1.85%	1.91%	2.01%	2.05%

^ATotal portfolio performance excludes Council's cash account holdings. Overall returns would be lower if cash was included.

For the month of May, the total portfolio (excluding cash) provided a solid return of +0.16% (actual) or +1.87% p.a. (annualised), outperforming the benchmark AusBond Bank Bill Index return of +0.00% (actual) or +0.05% p.a. (annualised). The strong performance continues to be driven by the handful of deposits still yielding above 3% p.a. However, some of these individual deposits are approaching maturity and will be reinvested at much lower prevailing rates.

Over the past 12 months, the portfolio returned +2.11% p.a., outperforming bank bills by 2.05% p.a. and over twelve times the official cash rate. This has been very strong given deposit rates reached their all-time lows and margins have generally contracted over the past 3 years.

We are pleased that PMHC remains amongst the best performing Councils in the state of NSW where deposits are concerned, earning on average, around \$2,400,000 in additional interest income compared to its peers (refer to our January 2021 rankings). We have been pro-active in our advice about protecting interest income and addressing reinvestment risk for many years and encouraged to maintain a long duration position. This is now reflected by the high performance of the investment portfolio. Of the 77 individual deposits PMHC held, 18 are still yielding higher than 3.00% p.a. That is, around 23% of outstanding deposits held is earning an interest rate that is thirty times the prevailing cash rate of 0.10%.

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Council's Term Deposit Portfolio & Recommendation

As at the end of May 2021, Council's deposit portfolio was yielding 1.86% p.a. (down 3bp from the previous month), with an average duration of ~1.8 years. Where possible, we recommend Council extends or at least maintains this average duration. In the low interest rate environment, the biggest collective risk that the local government sector has faced over the post-GFC era has been the dramatic fall in interest rates - from 7½% to the historical low levels of 0.10%.

As the past decade has highlighted (post-GFC era), we have seen too many portfolios' roll a high proportion of their deposits between 3-6 months, resulting in their deposits being reinvested at lower prevailing rates. That is, depositors have overpaid for liquidity and generally not insured themselves against the low interest rate environment by diversify their funding across various tenors (out to 5 years) but rather placed all their 'eggs in one basket' and kept all their deposits short. **Reinvestment risk has collectively been and continues to be the biggest detriment to depositors' interest income over the post-GFC period**.

ADI	LT Credit Rating	Term	T/D Rate
ICBC, Sydney	А	5 years	1.40% p.a.
NAB	AA-	5 years	1.30% p.a.
ICBC, Sydney	А	4 years	1.12% p.a.
NAB	AA-	4 years	1.00% p.a.
ICBC, Sydney	А	3 years	0.85% p.a.
NAB	AA-	3 years	0.80% p.a.
ICBC, Sydney	А	2 years	0.60% p.a.
NAB	AA-	2 years	0.60% p.a.

At the time of writing, we see value in:

The above deposits are suitable for investors looking to provide some income protection and mitigate reinvestment/rollover risk in the low interest rate environment.

For terms under 12 months, we believe the strongest value is currently being offered by the following ADIs (dependent on daily funding requirements):

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ADI	LT Credit Rating	Term	T/D Rate
Judo Bank	Unrated ADI	9-12 months	~0.70% p.a.
BNK Bank	Unrated ADI	6 months	~0.60% p.a.
ME Bank	BBB	12 months	~0.50% p.a.
BoQ	BBB+	6 months	0.45% p.a.
BoQ	BBB+	9-12 months	0.40% p.a.
СВА	AA-	12 months	~0.38% p.a.
AMP Bank	BBB	9-12 months	0.35% p.a.^
NAB	AA-	12 months	0.35% p.a.
Westpac	AA-	12 months	~0.35% p.a.
Bendigo	BBB+	9-12 months	0.35% p.a.

^ AMP T/Ds – contact us to receive an additional 0.20% p.a. rebated commission on top of the rate shown

Amongst the investment grade sector, short-dated term deposits (maturing less than 12 months) are yielding under 0.50% p.a. (most are under 0.40% p.a.). We believe there is not much value being offered in short-dated deposits.

In contrast, there is an upward pick-up in yield for investors that can take advantage of 2-5 year fixed T/Ds whilst official rates are stuck at depressed levels for the foreseeable future. If Council does not require high levels of liquidity and can stagger its investments longer-term, it will be rewarded over coming years if it can roll for a minimum term of 2 years (rolling for 3-5 years is even better, where possible), potentially yielding, on average, more than double or triple the return compared to those investors that purely invest in short-dated deposits.

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Senior FRNs & Recommendations

Over May, amongst the senior major bank FRNs, physical credit securities widened by around 5bp at the long-end of the curve. Secondary market securities remain relatively expensive on the back of the RBA announcing its \$200bn quantitative easing (QE) package (doubled in Feb 2021).

A new 5 year senior major bank FRN would now be issued around the +50bp, which remains tight on a historical basis. We may finally see some primary issuances in Q3-Q4 this year from the major banks as the RBA's term funding facility (TFF) ends as of 30 June 2021. The lack of supply from new (primary) issuances has played a major role with the rally in credit markets over the past year.

During the month, HSBC Sydney Branch (AA-) issued a new 3 year senior FRN at +42bp, printing \$500m, tightening from initial guidance of +50bp, after receiving orders in excess of \$1.4bn. While it tightened 8bp from initial guidance, relative to where the domestic major banks were being marked, we thought this was issued at a relatively fair level. Separately, China Everbright Bank, Sydney Branch (BBB+) issued a 3 year senior FRN deal at +68bp, tightening from +73bp and printed \$300m.

Amongst the "A" and "BBB" rated sector, the securities were also marked around 2-5bp wider at the long-end of the curve. While turnover in the secondary market is still predominately dominated by commonwealth, semi-government and major bank senior paper, given the lack of supply, we have started to observe that even a handful of regional bank senior paper has sometimes been trading inside "mid" levels over recent months.

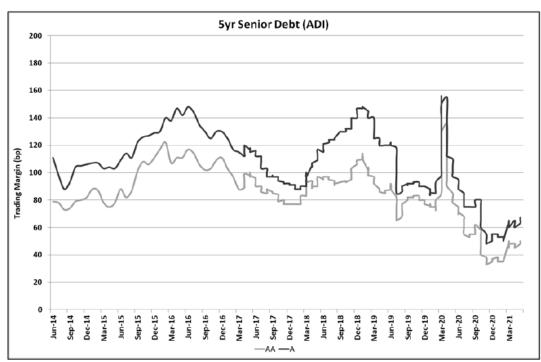
Credit securities remain tight on a historical level but are looking slightly more attractive following the widening experienced over the past few months. FRNs will continue to play a role in investor's portfolios mainly on the basis of their liquidity and the ability to roll down the curve and gross up returns over ensuing years (in a relatively stable credit environment).

Senior FRNs (ADIs)	31/05/2021	30/04/2021
"AA" rated – 5yrs	+50bp	+45bp
"AA" rated – 3yrs	+27bp	+27bp
"A" rated – 5yrs	+67bp	+60bp
"A" rated – 3yrs	+45bp	+42bp
"BBB" rated – 3yrs	+55bp	+52bp

Source: IBS Capital







Source: IBS Capital

We now generally **recommend switches** ('benchmark' issues only) into new attractive primary issues (or longer-dated alternatives), out of the following senior FRNs that are maturing:

On or before mid-2024 for the "AA" rated ADIs (domestic major banks);

- On or before mid-2022 for the "A" rated ADIs; and
- > Within 12 months for the "BBB" rated ADIs (consider case by case).

Investors holding onto the above senior FRNs ('benchmark' issues only) in their last 1-2 years are now generally holding sub-optimal investments and are not maximising returns by foregoing realised capital gains. In the current low interest rate environment, any boost in overall returns should be locked in when it is advantageous to do so.

In late August 2019, Council placed a bid of \$3m into the new Bendigo (BBB+) 5 year FRN at +97bp maturing 06/09/2024 (ISIN: AU3FN0050019). This FRN should be viewed as a 3½-4 year holding period, with the ability to 'roll down the curve', realise capital gains which would boost the overall return of the investment portfolio. The security was marked around +41bp (from +42bp the previous month) or a capital price ~\$101.80 at month-end. We recommend Council holds this FRN.

Monthly Investment Report: May 2021



Economic Commentary

International Market

Despite ongoing concerns regarding inflation and higher commodity prices, equity markets continued their positive momentum in May, providing positive returns across most major markets.

In the US, the S&P 500 Index gained +0.55%, while the tech-heavy NASDAQ Index fell -1.53%. Equities finished positively across Europe's main indices, led by France's CAC (+2.83%), Germany's DAX (+1.88%) and UK's FTSE (+0.76%).

The US payroll numbers in April disappointed, coming in at 266k against expectation of 1 million. The unemployment rate unexpectedly ticked up from 6.0% to 6.1% against expectations of a fall to 5.8%.

The US April core PCE deflator rose to +3.1% from +1.9%, its highest annual rate since 1992 and a little above the 2.9% expected.

President Biden outlined his Budget plan for FY22, proposing US\$6 trillion of spending that would significantly boost discretionary spending and sharply raise taxes on corporates and high-income households.

European GDP data confirmed their recession with Q1 GDP at -0.6% q/q. With the vaccination programme back on track in the region and restrictions likely to ease, Q2 is expected to be much better.

The UK economy contracted by -1.5% q/q in Q1, but the monthly track showed a decent pick-in in growth in March, confirming that a strong rebound is underway as lockdown restrictions ease. The Bank of England kept all its policy settings unchanged, including its £875bn government bond buying target.

China's monthly activity data did not provide any support to risk appetite, with retail sales coming in weaker than expected (17.7% y/y vs 25% expected). The unemployment rate fell to 5.1% (the lowest since November 2019), while new home prices were up +0.5% m/m in April.

The RBNZ surprised the market by re-introducing its forecasts for their Official Cash Rate (held steady at 0.25%) into their Monetary Policy statement. OCR forecasts point to a sequence of hikes from the Q3 of 2022.

Index	1m	3m	1yr	3yr	5yr	10yr
S&P 500 Index	+0.55%	+10.31%	+38.10%	+15.83%	+14.93%	+12.07%
MSCI World ex-AUS	+1.22%	+9.15%	+38.37%	+12.59%	+12.31%	+8.42%
S&P ASX 200 Accum. Index	+2.34%	+8.48%	+28.23%	+9.95%	+10.11%	+8.82%

The MSCI World ex-Aus Index rose +1.22% for the month of May:

Source: S&P, MSCI

Monthly Investment Report: May 2021

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Domestic Market

The RBA kept its rates and guidance unchanged at its meeting in May, however it upgraded its baseline forecasts for economic growth and substantially reduced its unemployment forecasts. The RBA has lowered its unemployment rate forecasts to 4.5% by the end of 2022 (previously 5.5%), which is at the bottom end of model-based NAIRU estimates (NAIRU is pegged around 4.5-5.0%).

The RBA indicated it will not extend their 3yr yield target from the April 2024 bond to the November 2024 bond, which could signal their intention to announce a tapered QE3.

The underlying cash deficit for 2020-21 was revised almost \$40bn lower to \$161bn (7.8% of GDP) from \$213.7bn (11%) at the previous budget in October 2020, on the back of the better-than-expected labour market recovery and elevated iron ore price. Deficits are forecast all the way to 2031-32. Gross debt is expected to be \$829bn (40.2% of GDP) in 2020-21 before stabilising at around 51% of GDP in the medium term.

In terms of spending, the largest item was the Government's aged care package – at around \$17.7bn over the forward estimates. But there were other big spends in the areas of Infrastructure (\$15bn) and NDIS (\$13.2bn). The Low and Middle-Income tax offset was extended a year (\$7.8bn) and the Investment Asset Write Off was also extended.

Headline employment fell -30.6k in April, well below expectations of +20k. The unemployment rate fell by 0.2% to 5.5% from an upwardly revised 5.7%, partially driven by falling participation from 66.3% to 66.0%. Interestingly, youth unemployment fell 1.1% to 10.6%, its lowest since 2009. Underemployment also fell by 0.2% to 7.8%, its lowest level since May 2014.

The Wage Price Index (WPI) rose +0.6% q/q and +1.5% y/y in March, one-tenth higher than the consensus.

The trade surplus fell by \$2.0bn to \$5.6bn in March, driven mostly by higher imports (+4% m/m or \$1.3bn).

Australia has now administered vaccine doses equivalent to 14.9% of the population, while only 2% of the population is now fully vaccinated.

The Australian dollar fell -0.66%, finishing the month at US77.25 cents (from US77.76 cents the previous month).

Credit Market

The main credit indices remained flat over May. The indices now trade back to their levels experienced in late 2020:

Index	May 2021	April 2021
CDX North American 5yr CDS	51bp	50bp
iTraxx Europe 5yr CDS	50b p	50bp
iTraxx Australia 5yr CDS	59b p	61bp
Source: Markit		

Monthly Investment Report: May 2021

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Fixed Interest Review

Benchmark Index Returns

Index	May 2021	April 2021
Bloomberg AusBond Bank Bill Index (0+YR)	+0.00%	+0.00%
Bloomberg AusBond Composite Bond Index (0+YR)	+0.26%	+0.56%
Bloomberg AusBond Credit FRN Index (0+YR)	+0.07%	+0.07%
Bloomberg AusBond Credit Index (0+YR)	+0.22%	+0.54%
Bloomberg AusBond Treasury Index (0+YR)	+0.30%	+0.57%
Bloomberg AusBond Inflation Gov't Index (0+YR)	+0.90%	+1.61%

Source: Bloomberg

Other Key Rates

Index	May 2021	April 2021
RBA Official Cash Rate	0.10%	0.10%
90 Day (3 month) BBSW Rate	0.04%	0.04%
3yr Australian Government Bonds	0.10%	0.10%
10yr Australian Government Bonds	1.61%	1.65%
US Fed Funds Rate	0.00%-0.25%	0.00%-0.25%
10yr US Treasury Bonds	1.58%	1.65%

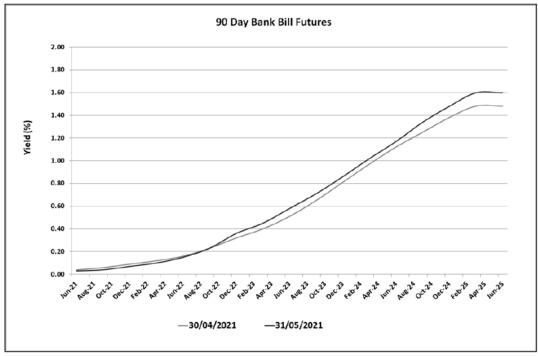
Source: RBA, AFMA, US Department of Treasury

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90 Day Bill Futures

Over May, bill futures marginally rose across the board on anticipation of a tapering in QE programs and rising inflationary expectations. Overall, bill futures continue to depict a low rate environment over the long-run, despite the steeping curve in recent months:



Source: ASX

Monthly Investment Report: May 2021

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Fixed Interest Outlook

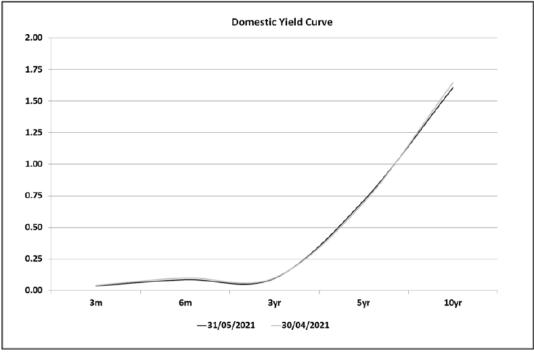
The unprecedented fiscal support for the global economy and the accelerated vaccine rollout in countries such as the US and the UK has aided financial markets. Further significant stimulus packages are also being proposed in the US, with President Biden announcing a \$US2.25 trillion infrastructure plan – about half of it for physical infrastructure and the other half for social infrastructure.

US Fed Chair Powell appeared to be more optimistic on the outlook, saying "we feel like we're at a place where the economy's about to start growing much more quickly and job creation coming much more quickly". Powell remains cautious and has made it clear that it was "not time yet" to have a conversation about tapering its US\$120bn monthly QE bond buying programme. He emphasised that the US was "not close to" the substantial progress toward its employment and price stability goals that has been set as the condition for contemplating its first steps of tapering.

Global central banks (including the RBA) have stressed that they will look through temporary increases in inflation from base effects and supply chain disruptions.

Domestically, the RBA is of the view that "*a materially lower unemployment rate*" (NAIRU of low 4s or even 3s) is needed to generate wages growth in excess of 3%, which is the level the RBA thinks is needed to deliver inflation sustainably within the 2-3% inflation target. The Bank does not see this occurring "*until 2024 at the earliest*", which underpins their rates guidance.

The domestic bond market continues to suggest a prolonged low period of interest rates. Over the month, yields fell around 4bp at the long-end of the curve:



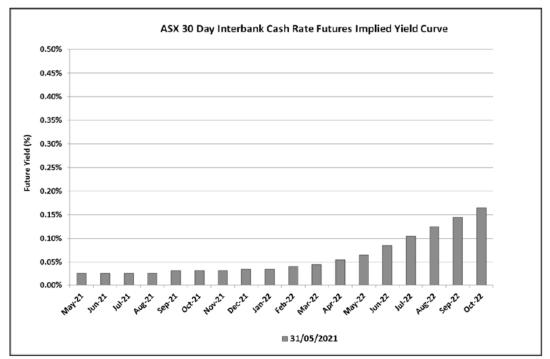
Source: AFMA, ASX, RBA

Monthly Investment Report: May 2021

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RBA Governor Lowe has pushed back on market pricing of rate hikes as early as late 2022. Dr Lowe reiterated his 'best guidance' was that it is "unlikely to see wages growth consistent with the inflation target before 2024. This is the basis for our assessment that the cash rate is very likely to remain at its current level until at least 2024":



Source: ASX

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Investment Report

01/05/2021 to 31/05/2021

Item 09.08 Attachment 2

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PORT MACQUARIE-HASTINGS

*

Portfolio Valuation as at 31/05/2021

Issuer	Rating	Туре	Alloc	Interest	Purchase	Maturity	Rate	Value	Accrued	Accrued MTD
Rabobank Australia Branch	A+	TD	GENERAL	Annual	08/06/2017	07/06/2021	3.0200	5,000,000.00	147,690.41	12,824.66
Westpac	AA-	TD	GENERAL	Quarterly	06/06/2018	15/06/2021	3.1000	3,000,000.00	21,657.53	7,898.63
NAB	AA-	TD	GENERAL	At Maturity	19/06/2020	21/06/2021	1.0000	4,000,000.00	38,027.40	3,397.26
NAB	AA-	TD	GENERAL	Annual	03/07/2018	22/06/2021	3.0000	4,000,000.00	109,479.45	10,191.78
NAB	AA-	TD	GENERAL	Annual	03/07/2018	06/07/2021	3.0000	3,000,000.00	82,109.59	7,643.84
Westpac	AA-	TD	GENERAL	Quarterly	17/07/2018	13/07/2021	3.0400	5,000,000.00	17,906.85	12,909.59
NAB	AA-	TD	GENERAL	Annual	26/07/2018	20/07/2021	3.0400	4,000,000.00	102,943.56	10,327.67
Westpac	AA-	TD	LOCAL BRANCH	Annual	24/07/2019	23/07/2021	1.7500	4,000,000.00	59,835.62	5,945.21
NAB	AA-	TD	GENERAL	Annual	02/08/2018	03/08/2021	3.0700	5,000,000.00	126,584.93	13,036.99
Westpac	AA-	TD	GENERAL	At Maturity	21/01/2021	23/08/2021	0.2000	20,000.00	14.36	3.40
ICBC Sydney Branch	А	TD	GENERAL	Annual	30/08/2019	30/08/2021	1.6200	5,000,000.00	60,805.48	6,879.45
NAB	AA-	TD	GENERAL	At Maturity	31/08/2020	31/08/2021	0.8000	5,000,000.00	30,027.40	3,397.26
NAB	AA-	TD	GENERAL	Annual	31/08/2020	31/08/2021	0.8000	5,000,000.00	30,027.40	3,397.26
Westpac	AA-	TD	GENERAL	Quarterly	13/09/2018	14/09/2021	2.8800	5,000,000.00	30,772.60	12,230.14
NAB	AA-	TD	GENERAL	Annual	27/09/2018	28/09/2021	3.0500	5,000,000.00	102,780.82	12,952.05
Auswide Bank	BBB	TD	GENERAL	Annual	30/09/2019	30/09/2021	1.7500	4,000,000.00	46,794.52	5,945.21
Westpac	AA-	TD	GENERAL	Quarterly	13/09/2018	12/10/2021	2.8900	5,000,000.00	30,879.45	12,272.60
Auswide Bank	BBB	TD	GENERAL	Annual	23/10/2019	25/10/2021	1.6500	5,000,000.00	49,952.05	7,006.85

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ATTACHMENT

ORDINARY COUNCIL 16/06/2021

								Ŵ.		QUARIE-HASTINGS
Issuer	Rating	Туре	Alloc	Interest	Purchase	Maturity	Rate	Value	Accrued	Accrued MTD
Auswide Bank	BBB	TD	GENERAL	At Maturity	25/11/2019	25/11/2021	1.7000	5,000,000.00	129,013.70	7,219.18
ICBC Sydney Branch	А	TD	GENERAL	Annual	05/12/2018	07/12/2021	3.0100	4,000,000.00	58,055.89	10,225.75
ICBC Sydney Branch	А	TD	GENERAL	Annual	16/12/2019	16/12/2021	1.5700	4,000,000.00	28,733.15	5,333.70
ING Direct	А	TD	GENERAL	Annual	30/01/2020	31/01/2022	1.6500	5,000,000.00	27,801.37	7,006.85
Westpac	AA-	TD	GENERAL	At Maturity	30/01/2021	31/01/2022	0.2500	10,000.00	8.36	2.12
Newcastle Permanent	BBB	TD	GENERAL	Quarterly	07/02/2019	08/02/2022	3.0500	4,000,000.00	8,356.16	8,356.16
Westpac	AA-	TD	GENERAL	Annual	21/02/2017	21/02/2022	3.6100	2,000,000.00	19,583.01	6,132.05
NAB	AA-	TD	GENERAL	Annual	21/02/2017	21/02/2022	3.4600	5,000,000.00	46,923.29	14,693.15
ING Direct	А	TD	GENERAL	Annual	02/03/2020	02/03/2022	1.5000	5,000,000.00	18,698.63	6,369.86
BOQ	BBB+	TD	GENERAL	Annual	15/03/2017	15/03/2022	3.8000	2,000,000.00	16,241.10	6,454.79
Newcastle Permanent	BBB	TD	GENERAL	Quarterly	12/03/2019	22/03/2022	2.9000	4,000,000.00	25,742.47	9,852.05
Newcastle Permanent	BBB	TD	GENERAL	Annual	27/03/2019	29/03/2022	2.8000	5,000,000.00	24,547.95	11,890.41
Newcastle Permanent	BBB	TD	GENERAL	Quarterly	18/04/2019	19/04/2022	2.7000	4,000,000.00	12,723.29	9,172.60
BOQ	BBB+	TD	GENERAL	Annual	28/05/2019	30/05/2022	2.4000	4,000,000.00	1,052.05	1,052.05
Rabobank Australia Branch	A+	TD	GENERAL	Annual	08/06/2017	07/06/2022	3.2200	5,000,000.00	157,471.23	13,673.97
Australian Military Bank	BBB+	TD	GENERAL	Annual	19/06/2020	20/06/2022	1.1500	2,000,000.00	21,865.75	1,953.42
Australian Military Bank	BBB+	TD	GENERAL	Annual	19/06/2020	20/06/2022	1.1500	2,000,000.00	21,865.75	1,953.42
Westpac	AA-	TD	GENERAL	Quarterly	23/06/2020	23/06/2022	1.0100	3,500,000.00	6,779.45	3,002.33
Australian Military Bank	BBB+	TD	GENERAL	Annual	23/06/2020	23/06/2022	1.0500	3,500,000.00	34,534.93	3,121.23
Westpac	AA-	TD	GENERAL	Quarterly	24/06/2020	24/06/2022	1.0200	5,000,000.00	9,641.10	4,331.51

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										QUARIE-HASTINGS
Issuer	Rating	Туре	Alloc	Interest	Purchase	Maturity	Rate	Value	Accrued	Accrued MTD
NAB	AA-	TD	GENERAL	Annual	02/08/2018	02/08/2022	3.2200	4,000,000.00	106,215.89	10,939.18
NAB	AA-	TD	GENERAL	Annual	16/08/2018	16/08/2022	3.0500	4,000,000.00	96,263.01	10,361.64
ICBC Sydney Branch	А	TD	GENERAL	Annual	28/08/2019	29/08/2022	1.6400	5,000,000.00	62,230.14	6,964.38
Auswide Bank	BBB	TD	GENERAL	At Maturity	06/09/2019	06/09/2022	1.8000	3,000,000.00	93,797.26	4,586.30
Westpac	AA-	TD	LOCAL BRANCH	Annual	13/09/2017	13/09/2022	3.4100	3,000,000.00	72,871.23	8,688.49
Rabobank Australia Branch	A+	TD	GENERAL	Annual	13/09/2017	13/09/2022	3.3800	3,000,000.00	72,230.14	8,612.05
MyState Bank	BBB	TD	GENERAL	Annual	29/09/2020	29/09/2022	0.8500	5,000,000.00	28,527.40	3,609.59
Westpac	AA-	TD	GENERAL	Quarterly	29/09/2020	29/09/2022	0.7200	5,000,000.00	6,312.33	3,057.53
Auswide Bank	BBB	TD	GENERAL	Annual	30/09/2019	30/09/2022	1.7500	4,000,000.00	46,794.52	5,945.21
ICBC Sydney Branch	А	TD	GENERAL	Annual	23/10/2019	24/10/2022	1.7000	3,000,000.00	30,879.45	4,331.51
ICBC Sydney Branch	А	TD	GENERAL	Annual	31/10/2019	31/10/2022	1.7300	5,000,000.00	50,715.07	7,346.58
ING Direct	А	TD	GENERAL	Annual	27/02/2020	27/02/2023	1.6000	2,500,000.00	10,410.96	3,397.26
BOQ	BBB+	TD	GENERAL	Annual	28/05/2019	29/05/2023	2.5500	4,000,000.00	1,117.81	1,117.81
BOQ	BBB+	TD	GENERAL	Annual	19/06/2020	19/06/2023	1.3500	2,000,000.00	25,668.49	2,293.15
BOQ	BBB+	TD	GENERAL	Annual	27/06/2019	27/06/2023	2.2000	5,000,000.00	101,561.64	9,342.47
MyState Bank	BBB	TD	GENERAL	Annual	14/09/2020	14/09/2023	0.9000	4,000,000.00	25,643.84	3,057.53
MyState Bank	BBB	TD	GENERAL	Annual	14/09/2020	14/09/2023	0.9000	4,000,000.00	25,643.84	3,057.53
ICBC Sydney Branch	А	TD	GENERAL	Annual	27/11/2019	27/11/2023	1.7600	6,000,000.00	53,812.60	8,968.77
ICBC Sydney Branch	А	TD	GENERAL	Annual	16/12/2019	18/12/2023	1.8100	4,000,000.00	33,125.48	6,149.04
Auswide Bank	BBB	TD	GENERAL	At Maturity	02/03/2020	04/03/2024	1.7500	3,000,000.00	65,589.04	4,458.90

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								×	PORT MAC	QUARIE-HASTINGS
Issuer	Rating	Туре	Alloc	Interest	Purchase	Maturity	Rate	Value	Accrued	Accrued MTD
BOQ	BBB+	TD	GENERAL	Annual	18/06/2020	18/06/2024	1.4500	3,000,000.00	41,473.97	3,694.52
ING Direct	A	TD	GENERAL	Annual	23/06/2020	24/06/2024	1.2500	3,000,000.00	35,239.73	3,184.93
Bendigo and Adelaide	BBB+	FRN	GENERAL	Quarterly	06/09/2019	06/09/2024	1.0066	3,053,937.00	7,032.41	2,564.76
BOQ	BBB+	TD	GENERAL	Annual	27/09/2019	27/09/2024	2.0000	2,000,000.00	26,958.90	3,397.26
NAB	AA-	TD	GENERAL	Annual	16/10/2020	16/10/2024	0.8000	5,000,000.00	24,986.30	3,397.26
NAB	AA-	TD	GENERAL	Annual	18/11/2020	18/11/2024	0.7000	5,000,000.00	18,698.63	2,972.60
NAB	AA-	TD	GENERAL	Annual	29/01/2021	29/01/2025	0.7200	4,000,000.00	9,705.21	2,446.03
NAB	AA-	TD	GENERAL	Annual	02/03/2021	03/03/2025	0.9500	4,000,000.00	9,473.97	3,227.40
ICBC Sydney Branch	А	TD	GENERAL	Annual	18/06/2020	18/06/2025	1.7500	2,000,000.00	33,369.86	2,972.60
ICBC Sydney Branch	A	TD	GENERAL	Annual	16/10/2020	16/10/2025	1.2000	5,000,000.00	37,479.45	5,095.89
ICBC Sydney Branch	А	TD	GENERAL	Annual	17/11/2020	17/11/2025	1.3000	6,500,000.00	45,375.34	7,176.71
NAB	AA-	TD	GENERAL	Annual	17/11/2020	17/11/2025	0.8500	3,500,000.00	15,975.34	2,526.71
ICBC Sydney Branch	А	TD	GENERAL	Annual	03/12/2020	03/12/2025	1.2000	5,000,000.00	29,589.04	5,095.89
ICBC Sydney Branch	А	TD	GENERAL	Annual	07/12/2020	08/12/2025	1.2000	5,000,000.00	28,931.51	5,095.89
NAB	AA-	TD	GENERAL	Annual	07/12/2020	08/12/2025	0.9000	5,000,000.00	21,698.63	3,821.92
NAB	AA-	TD	GENERAL	Annual	29/01/2021	29/01/2026	0.9100	4,000,000.00	12,266.30	3,091.51
NAB	AA-	TD	GENERAL	Annual	02/03/2021	02/03/2026	1.2100	4,000,000.00	12,066.85	4,110.68
Westpac	AA-	TD	GENERAL	Quarterly	09/03/2021	09/03/2026	1.2500	5,000,000.00	14,383.56	5,308.22
NAB	AA-	TD	GENERAL	Annual	16/03/2021	16/03/2026	1.2500	5,000,000.00	13,184.93	5,308.22
NAB	AA-	TD	GENERAL	Annual	26/05/2021	26/05/2026	1.2500	5,000,000.00	1,027.40	1,027.40

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Issuer

Rating

Туре

Alloc

	~		QUARIE-HASTINGS
Rate	Value	Accrued	Accrued MTD

Westpac	AA-	CASH	GENERAL	Monthly	31/05/2021	31/05/2021	0.3500	26,075,609.95	4,709.44	4,709.44
TOTALS								338,659,546.95	3,210,964.98	476,567.26

Purchase

Maturity

Interest

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Operational Plan

2021-2022



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Acknowledgement of Country

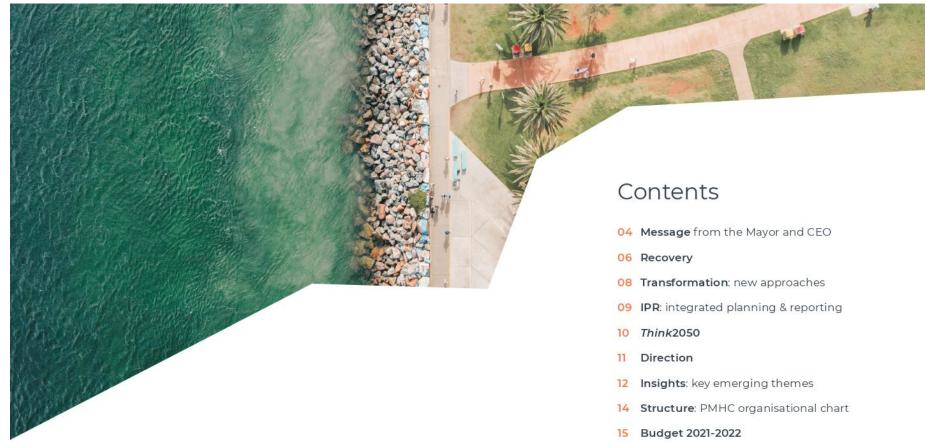
We acknowledge the Birpai people, the traditional owners of the land in which we work and live, and pay our respects to Elders past, present and emerging. We extend our respect to all Aboriginal and Torres Strait Islander people who choose to call Port Macquarie-Hastings home.



Airport welcome mural Artist: Angela Marr-Grogan

ATTACHMENT

ORDINARY COUNCIL 16/06/2021



- 19 Community Plan: activities 2021-2022
- 24 Capital Works
- 27 Scope

03 PMHC 2021-2022 Operational Plan

Message

From our Mayor and CEO

Our community has gone through so much. In the past two years we have gone through drought, fires, the pandemic and now floods.

The draft of this plan was presented to the community just two days before we were all caught by surprise with the biggest natural disaster experienced in our community. The impacts of this disaster are still being assessed and our teams now must refocus their efforts not just on our planned deliverables, but also on fixing, repairing and replacing roads, bridges, boardwalks, jetties and other recreation areas. This means that this year's achievements will be a little different to what we first initially envisaged. At the time of writing, we are still taking stock of the extent of the damage, the cost to repair and ensuring that we have a sufficient workforce, contractors and materials to deploy to areas of highest community priority.

This plan highlights many significant capital works projects that are already underway and must continue to ensure that we meet our contractual obligations with other levels of government. But equally as important is a renewed focus on taking stock of the backlog of maintenance and repair across our stormwater, sewer & water services technology and administrative systems.



"Circumstances do not make you, they reveal you"

James Allen, Philosopher

04 PMHC 2021-2022 Operational Plan

We are turning our efforts, also, to this remedial work, to ensure we are well placed to deliver into the future. This means that we are foreshowing some changes are likely to the proposed budget allocation. But we will keep talking and listening to our community and coming back to you as things change.

With the NSW local government election in the first quarter of this year, we are also focussing this year on ensuring that we have a systematic and structured approach to improve our performance, our strategy and our governance.

In March 2021, Council resolved to introduce the Australian Business Excellence Framework. We trust that this integrated approach to leadership and will aid our efforts in achieve outstanding success and long-term sustainability. The focus on the ABEF does not take away from the myriad of work happening to improve how we deliver service to our community. We will continue to work closely with our community to seek their feedback on how we can do things better and make sure that not only are our customers and community at the centre of everything we do, but also working with us to co-design solutions to meet our shared aspirations and goals.

This document also provides a synopsis of the work undertaken to understand our community's vision for 2050. The new Council will adopt the new Community Plan which will in turn then inform the new Delivery program and subsequent operational plans for the new Council.

It's an honour to serve this community and to be in a position to work with the many skills and talents in our organisation and our community to ensure that we are well set up for our shared futures.



"We will keep talking and listening to our community"

05 PMHC 2021-2022 Operational Plan

Recovery

2021 Floods

Ongoing work

Disaster recovery and repair features in this year's program of works. The extent of what work can be completed is still underway. The rebuild will span the next few years as the scale of events and their impacts are unprecedented.

With the region declared a natural disaster zone, following the floods of March 2021, repair work to our community infrastructure has commenced. Assessment of the impacts is ongoing and a number of assistance measures have been announced by State and Federal Governments. However, given the magnitude of damages, it will be some time before we can fully quantify the impacts or determine exact timeframes for when damaged assets can be restored.

Flood repair & build costs

\$70m+ repair work

\$19.5m Immediate emergency reconstruction

\$31.8m restoration and replacement (roads, bridges, footpaths)
\$2.6m landslips & coastal erosion
\$4m boardwalk replacement
\$2m marine assets
\$2.1m picnic, playground and recreation
\$10m Rocks Ferry



"We're counting the cost. The financial cost at this point in time is \$70 million over the next two years. The emotional cost will go on for years to come."

Dr Clare Allen, PMHC CEO

06 PMHC 2021-2022 Operational Plan

Community priorities

Many members of our community were significantly impacted by the floods. Volunteers and community organisations have been working to assist people return to their homes or make their homes safe and accessible..

Significant work is required to support impacted people to access safe and affordable housing. Responsibility for responding to the many needs and priorities of people who have been impacted rests with State and Federal Government agencies and community based organisations. Whilst much of what community members need for wellbeing is outside the scope for Council, we play a significant role in bringing community people together, advocating and lobbying. This year, we will continue to operate flood recovery centres in Port Macquarie, Laurieton and Wauchope and provide ongoing outreach to other communities.

Additionally, our community team will continue to work closely with our CCATs (Community-Council Action Teams), community organisations, emergency services, and local people to build on our understanding of this event to develop local emergency response plans.

Outreach services

3986 visits to flood recovery centres
473 visits at 29 flood recovery outreach
294 attended 12 flood recovery
community meetings
13 Community Emergency Response
Plans will be developed this year



07 PMHC 2021-2022 Operational Plan

Transformation

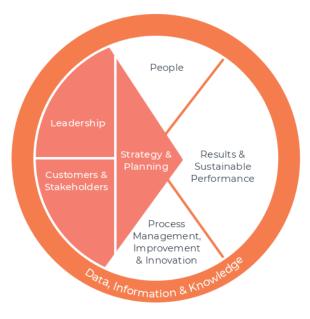
Key frameworks and approaches

Australian Business Excellence Framework (ABEF)

The ABEF was adopted by Council in March 2021 and over the life of this plan, the ABEF will become one of the key tools that we use to inform our development and continuous improvement.

In future years, we will utilise the ABEF to assesses how we function as a whole, including leadership and staff practices, customer focus, strategies, processes and knowledge management. Following the previous year's drought, fires and impacts of COVID19 we will be utilising this framework to ensure our organisation is both sustainable and adaptable to meet changing needs of our key stakeholders (State and Federal Governments, businesses, residents) and community needs.

Next year's Delivery Program and future operational plans will continue to ensure that the improvements we identify this year are actioned and are reflected as inputs to future planning cycles.



This year, our key focus will be leadership, customers & stakeholders, strategy & planning

Refer to the left hand side of the circle

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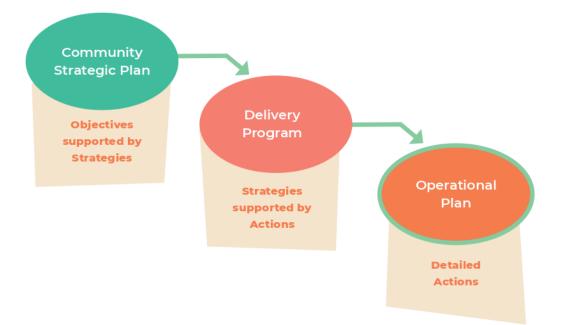
IPR

Integrated Planning and Reporting

This Operational Plan is a one year plan that spells out the detail of the Delivery Program.This document lists the high level focus and budget for the year ahead.

Part B* of this plan lists the specific projects and activities that will be undertaken this financial year to achieve the commitments made in the Delivery Program.

This year's plan is a little different than previous years. With a new Council election in September, the impact of recovery projects and an increased focus on remedial maintenance and system repair, it is foreshadowed that there will be changes to our prioritisation and spend as the year progresses.



We will continue to return to Council and to the community with these changes and our role out of the Australian Business Excellence Framework will assist us.

The new Council will adopt the new Community Plan in the first half of this year and we will commence work on the new Delivery Program. The community and have been clear with us over the past 18 months in identifying what is important to them.

This year is an exciting year of transition as we move towards responding to these emerging themes and insights whilst we complete our commitments under the current Community Plan and Delivery Program.

* Detailed strategies for the Delivery Program and details actions for the Operational Plan are listed in Part B supplementary tables.

09 PMHC 2021-2022 Operational Plan

Think2050

Creating our future together

Informed by community

Over the past 18 months we have been meeting with, talking to and listening to our residents and businesses on many topics. The *Think*2050 website had 1100 visits and we gratefully thank our 15 local Ambassadors for all their efforts.

It has been an immersive and energising experience hearing the range of ideas, listening to and discovering the opinions and aspirations many people have so generously shared. Our new Community Plan, *Think*2050 is the culmination of these ideas and suggestions. In July 2021, we will return to our community to check the Community Strategic Plan informed by *Think*2050.



10 PMHC 2021-2022 Operational Plan

2019 December Launch



2020

January > July Postcard campaign > 1500 feedback postcards

July > August Focus groups 1

July > November Minecraft competition

September

Youth summits > 120 school students attended schools virtual summit or participated in Minecraft competition

September > October **Future***Think* symposium > 270 attendees, 4000 views

October > November Round tables > 270 participants

December Community survey > 675 surveys

2021

March Focus groups 2 > 232 participants (overall)

June Consultation on draft Community Strategic Plan

Have we got it right?

September Newly elected Council

December New Community Plan

ORDINARY COUNCIL 16/06/2021

Direction

Think2050

3

- Less traffic congestion: with a clear road network approach to address present and future needs
- Stable infrastructure: a solid infrastructure approach that can ensure sewer, water, stormwater capability now and in the future
- Improved pathways: footpaths, shared paths, cycleways and access to public transport across our region
- Environmental focus: to protect our natural environment and supporting climate friendly initiatives
- Affordable living: for now and future generations (eg housing)





8

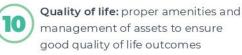
9

Inclusive community: who celebrate our history and

First Nations people

Collaboration: a greater level of trust between Council and the community through engagement in decision making







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ORDINARY COUNCIL 16/06/2021

Insights

Key emerging themes

From Think2050

The themes that have emerged as we have listened to our community are:

Our community is clear about the future and they want a community that is well-planned, built and run, and supports all of our residents to live well and make the most of the opportunities open to them, whist managing our asset and finite resources.

We are stewards of our natural and built environment, with vibrant neighbourhoods and places that are wellconnected and accessible to everyone.

The health of our unique natural ecosystem of plants and animals is protected and enhanced and we foster a healthy and thriving community and environment that is resilient in times of stress. We embrace technology, creativity and innovation to solve complex problems and improve our environment and the way we live, work and play.

Our existing and future residents live in harmony with the natural environment while enjoying a high quality of life.

From our CEO

Our community is looking for us to step up and make sure that our infrastructure, and amenities meets current and future needs, through a transparent and inclusive process.

Our community is looking for us to lead and be socially responsible and financially prudent and this is exactly what we will do, over this year and beyond. We will ensure that we have sound governance, and strategy and operations that meet the expectations of our community. It will not happen overnight, but it will happen.

"Our community is clear about the future and they want a community that is well-planned, built and run, and supports all of our residents"

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"Progress is impossible without change, and those who cannot change their minds cannot change anything"

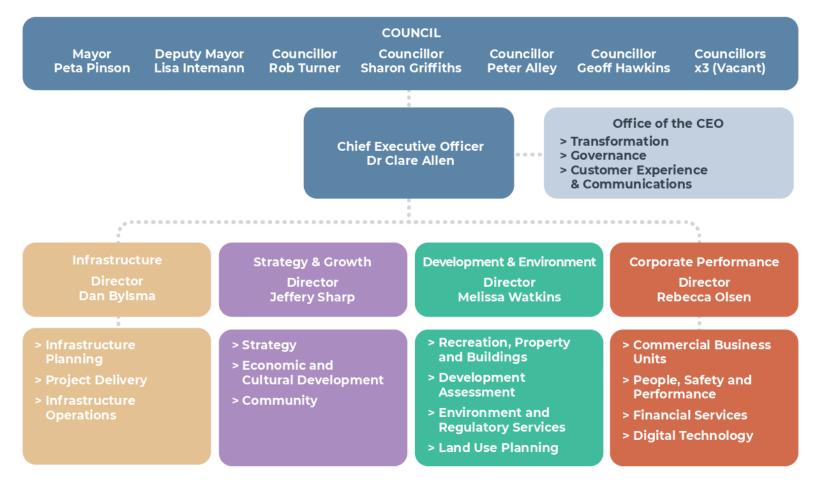
George Bernard Shaw

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Structure

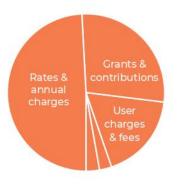


*Local Government Elections will be held on 4 September 2021. Council is responsible for setting the strategic direction which the organisation then implements.

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Budget

Operational Plan 2021-2022



Income \$219.7m (approx)

Projected income received from:

Rates and annual charges: \$108.4m

Grants and contributions: \$60.8m from state and federal government grants and developer contributions

User charges and fees: \$39.6m for services such as water, waste management, building and planning, animal registration, airport and library

Interest: \$5.4m

Other revenue: \$5.5m such as waste management income, fines and rental of Council facilities

15 PMHC 2021-2022 Operational Plan



Expenditure \$203.1m

Projected expenditure includes:

Operational costs: \$115.8m excluding depreciation and loss on disposal of assets

Capital works projects: \$67.6m

Financing: \$13.2m cost of servicing debts including loan repayments and interest

Community assets: \$6.5m on developer provided assets

ATTACHMENT

ORDINARY COUNCIL 16/06/2021

Budget

Statement for 2021-2022

Income

Rates and Annual Charges: Local taxes levied by Local Government based on the value of property. User Charges and Fees: A charge or fee made in relation to a specified service provided by Council. Interest Received: Interest received from financial institutions on Council investments. Grants: Grants received from other levels of government to assist in the provision of Council services. Contributions: Contributions received from developers etc. towards the provision of Council services. Other Operating Receipts: Other income not included above including Glasshouse receipts and Library charges etc.

Transfer from Restricted Assets: Council puts funds into a restricted asset for future use. This transfer from restricted asset is the spending of those funds. Transfer to Restricted Assets: This is the transfer of funds into the restricted asset for future use.

Proceeds from Borrowings and Advances:

This is the amount that Council intends to borrow from financial institutions. **Repayment of Borrowings and Advances**: This is the repayment of principal on Council Ioans

16 PMHC 2021-2022 Operational Plan

Operating Activities movements	
Income from continuing operations	
Rates & Annual Charges	108,389,830
User Charges & Fees	39,587,808
Interest Received	5,454,710
Grants & Contributions	60,845,445
Other Operating Receipts	5,457,657
Expenses from continuing operations	
Employee Costs	(55,268,240)
Materials & Contracts	(45,200,921)
Depreciation	(50,236,200)
Interest Paid	(2,157,754)
Other Operating Payments	(15,351,600)
Loss on Disposal of Assets	(3,000,000)
Operating result from continuing operations	48,520,735
Restricted Asset movements	
Restricted asset movements receipts	
Transfer from Restricted Assets	38,796,355
Restricted asset movements payments	
Restricted asset movements payments Transfer to Restricted Assets	(55,465,759)
	(55,465,759) (16,669,404)
Transfer to Restricted Assets	
Transfer to Restricted Assets Net Revenue: Restricted Assets	
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements	
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments	(16,669,404)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets	(16,669,404) (67,573,068)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets	(16,669,404) (67,573,068) (6,500,000)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment	(16,669,404) (67,573,068) (6,500,000)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment Financing Activities movements	(16,669,404) (67,573,068) (6,500,000)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment Financing Activities movements Financing activities receipts	(16,669,404) (67,573,068) (6,500,000)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment Financing Activities movements Financing activities receipts Proceeds from Borrowings & Advances	(16,669,404) (67,573,068) (6,500,000)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment Financing Activities movements Financing activities receipts Proceeds from Borrowings & Advances Financing activities payments	(16,669,404) (67,573,068) (6,500,000) (74,073,068)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment Financing Activities movements Financing activities receipts Proceeds from Borrowings & Advances Financing activities payments Repayments of Borrowings & Advances	(16,669,404) (67,573,068) (6,500,000) (74,073,068) (11,014,463
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment Financing Activities movements Financing activities receipts Proceeds from Borrowings & Advances Financing activities payments Repayments of Borrowings & Advances Net Revenue: Financing Activities	(16,669,404) (67,573,068) (6,500,000) (74,073,068) (11,014,463 (11,014,463)
Transfer to Restricted Assets Net Revenue: Restricted Assets Property, Plant and Equipment movements Property, plant & equipment movements payments Purchase of Assets Developer Provided Assets Net Revenue: Property, Plant & Equipment Financing Activities movements Financing activities receipts Proceeds from Borrowings & Advances Financing activities payments Repayments of Borrowings & Advances Net Revenue: Financing Activities Budget Position (Inc Depreciation)	(16,669,404) (67,573,068) (6,500,000) (74,073,068) (11,014,463) (11,014,463) (53,236,200)

Expenses

Employee Costs: All costs related to employees including wages and salaries, workers compensation, employee leave entitlements, superannuation, fringe benefits taxation, payroll taxation and travelling etc. but not including employee costs associated with asset construction. Materials and Contracts: Includes all materials and contracts used in delivering operational activities Depreciation: This is the estimated amount by which Council's assets will deteriorate by in the 2021-2022 financial year. Interest Paid: The amount that is paid to financial institutions for interest on loans made to Council. Other Operating Payments: Includes all payments not included in the above categories including telephone costs, bank charges, electricity, insurance, street lighting and Mayoral and Councillor fees etc.

Purchase of Assets: This item includes the purchase of assets (such as heavy machinery) along with the construction by Council of assets (such as roads and footpaths).

Budget Position

Budget Surplus/(Shortfall): The balance of income vs expenditure. When this figure is nil, Council has a balanced budget which means that its sources of income for the year equals its expenditure for the year.

Service cost

This table outlines the cost of running the Council and its support services, outward facing services and capital works^

Operation	Direct Expenditure*	Income	Net Cost	Capital Works Program	et Cost after ks Programs
Business & Industry	1,462,582	-329,370	1,133,212	160,000	1,293,212
Business Units	1,748,408	-3,214,487	-1,466,078	-	-1,466,078
Cemeteries	180,246	-216,422	-36,176	20,000	-16,176
Communications	895,383	-	895,383	-	895,383
Community Engagement	2,016,618	-62,500	1,954,118	50,000	2,004,118
Corporate	3,851,876	-61,608,598	-57,756,722	-	-57,756,722
Customer Services Centre	1,550,269	-	1,550,269	-	1,550,269
Destination & Cultural Development	1,405,953	-632,800	773,153	5,000	778,153
Development Assessment	3,797,844	-3,951,142	-153,298		-153,298
Development Contributions	925,350	-8,778,523	-7,853,173	-	-7,853,173
Digital Technology	4,010,244	1,309	4,011,553	2,020,000	6,031,553
Drainage	792,994	-847,310	-54,316	923,186	868,870
Emergency Services	1,877,956	-320,962	1,556,994	148,968	1,705,962
Environment & Compliance	1,559,153	-569,118	990,035	-	990,035
Events	1,072,567	-31,787	1,040,780	-	1,040,780
Ferries	1,235,080	-698,476	536,604	-	536,604
Financial Services	3,720,656	-416,158	3,304,498	-	3,304,498
Glasshouse	4,122,548	-2,123,090	1,999,458	66,000	2,065,458
Governance	2,954,681	-34,762	2,919,919	-	2,919,919
Infrastructure - Other	4,985,679	-410,340	4,575,338	2,500,000	7,075,338
Library	2,190,457	-279,278	1,911,179	465,000	2,376,179
Natural Resources	2,965,183	-130,825	2,834,358	867,124	3,701,482
Organisational Performance	2,110,649	-	2,110,649	-	2,110,649
Recreation, Property & Buildings	10,540,286	-1,371,962	9,168,324	12,974,996	22,143,320
Regulatory Services	1,494,964	-612,339	882,625	-	882,625
Sewerage Services	12,271,834	-34,844,774	-22,572,940	5,916,153	-16,656,787
Strategic Planning	1,995,741	-36,298	1,959,443	100,000	2,059,443
Transport & Traffic	13,047,117	-4,474,667	8,572,449	33,187,343	41,759,792
Waste Management	18,655,919	-25,432,347	-6,776,428	4,310,000	-2,466,428
Water Supply	9,274,245	-38,449,340	-29,175,095	10,359,297	-18,815,798
Totals	118,712,480	-189,876,367	-71,163,887	74,073,067	2,909,180

Includes developer
 provided assets of \$6.5m
 *Does not include
 overheads or loans

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"Our community is looking for us to step up and make sure that our infrastructure, and amenities meets current and future needs, through a transparent and inclusive process."

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Community Plan

Key themes

Leadership & Governance

A collaborative community that works together and uses opportunities for community participation in decision-making that is defined as ethically, socially and environmentally responsible.

Community Life

A healthy, inclusive and vibrant community.

Business and Industry

The Port Macquarie Hastings region is a successful place that has vibrant, diversified and resilient regional economy for people to live, learn, work, play and invest.

Natural & Built Environment

A connected, sustainable, accessible community and environment that is protected now and into the future.



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Leadership & Governance

2021-2022 activities

Recovery

- Work with communities to develop local Community Emergency Response Plans
- Update PMHC disaster management & business continuity plans
- Advocate for improved telecommunications service to remote areas

Focus

- Implement Australian Business
 Excellence Framework
- Conduct election and poll
- On-board new Councillors
- Talk to our community and listen to their ideas, feedback and impacts on capital projects, plans and policies
- Communicate regularly and effectively through multiple methods and channels
- Implement road map for Service PMH and Digital Technology
- Improve our customer
 experience
- Implement smart
 community roadmap
- Draft sustainability strategy



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Community Life

2021-2022 activities

Recovery: Flood

Note: whilst risk rating criteria used to assess and prioritise, many repairs will take in excess of 2 years to complete and some many more years.

- Repair landslips and address coastal
 erosion in key locations
- Repair & replace playgrounds, parks, picnic tables, public art & recreation areas

Focus

- Distribute community grants to deliver projects
- Finalise & implement new cultural plan

Complete

- Infrastructure plan
- Vince Imon sporting fields
- Thrumster Sports Fields design and approvals
- Port Macquarie Town Centre Master Plan improvements

Design

- Port Macquarie Aquatic facility
- CSU-Googik connection
- Bain Park
- Westport Park boat ramp parking



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Business & Industry

2021-2022 activities

Recovery

 Advocate for, support and deliver bushfire, COVID19 and flood recovery programs and initiatives for local business and industry

Focus

- Port Macquarie CBD Cultural
 Precinct Plan
- Artwalk to support local arts, culture and creative industries
- Innovation Hub partnership with Charles Sturt University
- Finalise and roll out new
 Destination Brands
- Work to become an Eco-certified destination with EcoTourism Australia
- Implement smart community
 roadmap

- Develop a new 'Eat Sea Do' Official Visitor Guide
- Install new public art
- Host & sponsor events that encourage visitor economy
- Plan & investigate general aviation precinct
- Airport Stage 1 taxiway
 & pavement renewal
- Complete development contribution plan

Natural & Built Environment

2021-2022 activities

Recovery: Flood

Note: over 300 roads and bridges with major damage and hundreds more requiring minor repair.

- Repair, replace or rebuild flood damaged roads, footpaths, bridges, boardwalks, jetties, pontoons, boat ramps, wharves
- Commence planning for Rocks Ferry Reserve

Focus

Refer to supplementary tables for full capital works program.

- Undertake flood mitigation works
- Plan, investigate and construct water, sewerage & stormwater for future growth
- Annual maintenance & preventative works program for water, sewer & stormwater
- Finalise construction of new landfill at Cairncross & Kingfisher Waste Transfer station road rehabilitation
- Complete flood study for North Brother local catchment
- Develop Coastal Management program specific to Lake Cathie & Kenwood Drive bridge
- Continue with footpath, cycleway & pedestrian works
- Finalise corridor planning for Ocean Drive



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Capital Works

Finishing, fixing and the future

A balanced approach

In every conversation we have, we hear loudly and clearly that we need to finishing what needs finishing, fix what needs fixing, whilst also addressing growth and plan for the future in a responsible way. We need to balance how this is achieved.

Listed are our highest spend capital works projects being undertaken this year. Total capital spend is **\$67.6m***

All projects are listed in Part B supplementary tables.

Finishing

Project	Operational Plan 2021-2022		Funding source 2021-2022
Pembrooke Road	2,980,000	2,980,000	Grant
Foreshore Walkway	3,100,000	3,100,000	Grant/Reserve
Gordon Street ¹	6,380,152	8,600,000	Grant/Reserve

¹ Gordon Street: Commenced in 2020, this project involves reconstruction of the existing road pavement, upgrade to stormwater and replacement of existing water and sewer mains



*Excluding developer assets of \$6.5m

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Fixing

Project	Operational Plan 2021-2022	Total Project	Funding source 2021-11
Bottlebrush No 1 Bridge. Bril Bril Road	439,500	3,076,000	Grant
Bottlebrush No 2 Bridge. Bril Bril Road	256,500	1,710,000	Grant
Timber Bridges: Logans Crossing	100,000	2,770,000	Grant/Reserve
Papinbarra Bridge ²	2,801,140	4,800,000	Grant
King Creek Bridge ³	543,000	3,620,000	Grant
Little Mortons Creek Bridge	390,000	2,600,000	Grant
Illaroo Road Stormwater	867,124	870,000	Grant/Reserve

Future proofing

Cowarra WTP ⁴	100,000	80,000,000	Reserve
Thrumster Reclaimed: Interim Supply ⁵	3,100,000	10,500,000	s64 Reserve
Sewerage Pump Stations: Upgrades	312,010	7,000,000	Reserve
Bonny Hills to Kew Water Trunk Main ⁶	250,000	8,500,000	Reserve
Maria River Rd Upgrade ⁷	300,000	17,011,000	Grant
Ocean Drive Duplication	200,000	95,000,000	Grant/Reserve
Kew Main Street	1,211,316	1,800,000	Grant/Reserve
Vince Imon Sporting Fields	1,178,783	1,180,000	Grant/s7.11 Reserve
Carincross WMF Expansion	3,165,000	8,000,000	Reserve
Chemical Dosing - Wauchope WTP	1,700,000	1,700,000	Reserve
Bonny Hills Recycled Water	777,144	2,600,000	Reserve
Port Macquarie Aquatic Centre ⁸	748,215	50,000,000	Reserve
Kew WWTP	500,000	18,000,000	Reserve
Parallel Taxiway	100,000	7,457,000	Reserve
Thrumster WWTP	50,000	60,000,000	Reserve
Rainbow Beach Sporting Facilities	3,336,428	6,500,000	s7.11 Reserve

² Papinbarra Bridge: Design and construction of a new road bridge on Pappinbarra Right Arm Road required to replace the existing timber bridge (B085) destroyed by fire in November 2019

³ King Creek Road and Bridge: Design and construction works for shoulder widening and safety barrier installation east of Berowara Place

⁴ Cowarra WTP: Design and construction of a bulk water treatment plant at the Cowarra Dam which supplies the Port Macquarie and Camden Haven regions

⁵ Thrumster Reclaimed water: Completion of 8625m of recycled water pipeline (various sizes) to connect the existing Thrumster recycled pipelines to the Port Macquarie recycled water scheme and 2025m of water main along John Oxley Drive from west of the Oxley Hwy Overpass to Best Glen Place.

⁶ Bonny Hills recycled water system upgrade: Improved treatment deliver recycled water to the dual reticulation customers within this area whilst also. This project will also address the NSW Environmental Protection Authorities Pollution Reduction Program (PRP) for this location.

⁷ Maria River Road: Sealing 24km of Maria River Road spans both the PMHC and Kempsey and is being delivered by PMHC through a Memorandum of Understanding with KSC.

⁸ Port Macquarie Aquatic Centre: We will continue to refine the design for the new facility in consultation with our residents and users to ensure the project will be financially & environmentally sustainable, accessible and inclusive and the needs for future generations.

WMF Waste Management Facility WTP Water Treatment Plant

25 PMHC 2021-2022 Operational Plan

Timber bridges

Ongoing maintenance

Maintenance of deteriorating timber bridges is one of the largest ongoing costs to councils across regional NSW.

The NSW Government's Fixing Country Bridges Program will assist us to repair 17 timber bridges which expands our timber bridge program substantially.



Bridge	Total cost (estimated)	Spend 2021-2022
Black Creek Bridge	\$350,000	
Bridge on Bril Bril Road (Bottlebrush No. 1)	\$2,930,000	\$439,500
Bridge on Deep Creek Road*	\$20,000	\$3,000
Bridge on Farrawells Road	\$370,000	\$55,500
Bridge on Mundays Lane	\$440,000	\$66,000
Bridge on Mundays Lane	\$440,000	\$66,000
Bridge on Mundays Lane	\$510,000	\$76,500
Bridge on Thone River Road	\$340,000	\$51,000
Bril Bril Bridge	\$860,000	\$500,000
Cowal Creek Bridge	\$340,000	\$51,000
Crowe Roods Bridge	\$680,000	\$102,000
Cutty Bridge	\$770,000	\$115,500
Donkins Flat Bridge (\$315k FCB/\$225k BRP)	\$540,000	\$540,000
Fernbank Creek Bridge*	\$60,000	\$9,000
Joes Bridge	\$770,000	\$115,500
Kindee Bridge Stage 2	\$600,000	
King Creek Bridge	\$3,620,000	\$543,000
Langdons (Bottlebrush No. 2)	\$1,710,000	\$256,500
Little Mortons Creek Bridge	\$2,600,000	\$390,000
Logans Crossing Bridge (\$500k TBP cont.)	\$2,770,000	\$500,000
Old School Road Bridge (\$410k FCB/\$160k BRP)	\$570,000	\$570,000
Quarry Bridge*	\$73,000	\$10,950
Steels Bridge*	\$108,000	\$16,200
Tower Road Bridge	\$940,000	\$141,000

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* Funded from alternate source to Fixing County Bridges program

Scope

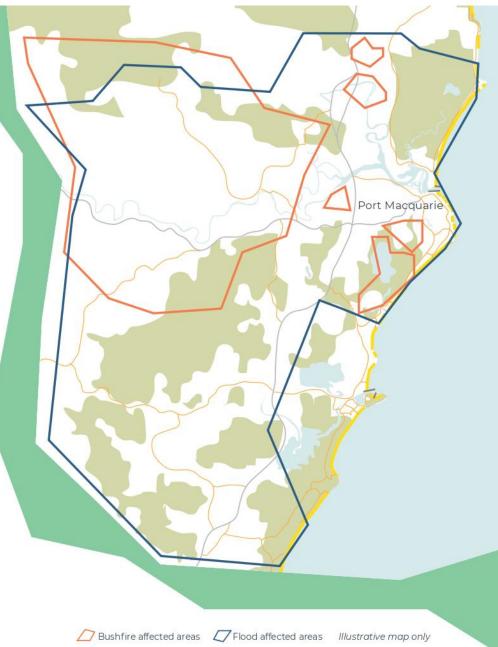
We are changing

Future plans

Our region has been hit hard over the last couple of years with the 2019 bushfires and the floods of March 2021.

The draft version of this Operational Plan went on display only 2 days prior to the floods. This means there are some differences between the draft and this version.

Supplementary tables that detail activities are published separately to this document.



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ATTACHMENT

ORDINARY COUNCIL 16/06/2021

Contact

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Item 09.10 Attachment 2

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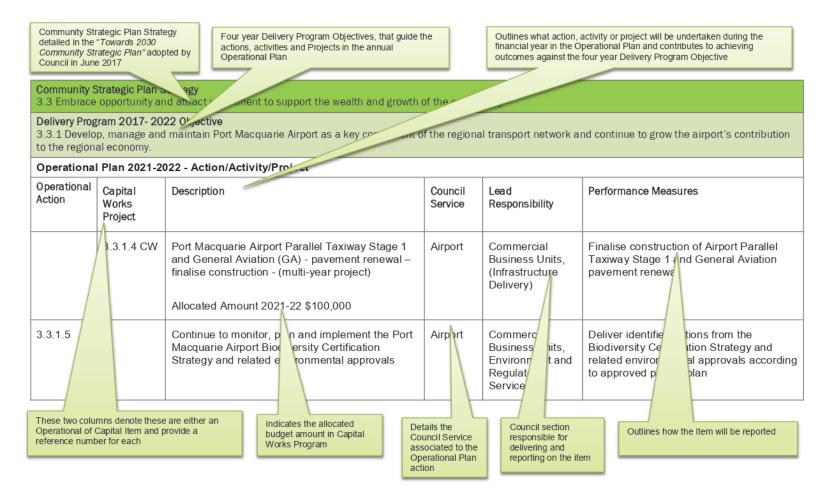


	<u>*</u>	PORT MACQUARIE HASTINGS COUNCIL
Understanding the tables		
Community Theme 1 – Leadership and Governance		4
Community Theme 2 – Your Community Life		15
Community Theme 3 – Your Business and Industry		
Community Theme 4 – Your Natural and Built Environment		

Sewer and Water team members Derek Aryiss and Chaz Wiegold at Munster Street Sewer Pump Station, Port Macquarie

2 PORT MACQUARIE-HASTINGS COUNCIL

Understanding the tables



ONE YEAR OPERATIONAL PLAN 2021-202 | PART B 3



Community Theme 1

Leadership and Governance

COMMUNITY STRATEGIC PLAN

What we are trying to achieve

A collaborative community that works together and uses opportunities for community participation in decision-making that is defined as ethically, socially and environmentally responsible.

How we will get there

- 1.1 Inform and engage with the community about what Council does using varied communication channels
- 1.2 Maintain strong partnerships between all stakeholders
 - local, state and federal so that they are affective advocates for the community
- 1.3 Demonstrate leadership
- 1.4 Use innovative, efficient and sustainable practices
- 1.5 Ensure strong corporate and financial management that is transparent and accountable

4 PORT MACQUARIE-HASTINGS COUNCIL

, ,	am 2017-2022 O ariety of tools to en	pjective gage with the community in a manner that is tran	sparent, effective, rele	evant and inclusive.	
Operational P	lan 2020-2021 - A	ction/Activity/Project			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.1.1.1		Engage with the community using a range of methods to facilitate community involvement in decision making	Community	Community	 Capture the number of engagement activities undertaken and number of participants including online Increased community participation on Council's Have your Say online portal Improve overall engagement approach and levels of engagement and report through annual survey
1.1.1.5		Implement the Education Framework to educate and inform the community about Council's services and priorities.	Community Education	Community	 Education Framework for Council finalised and implementation commenced Deliver regular and engaging education through a variety of channels.
1.1.1.8		Use targeted communication channels to ensure the community are informed of our activities and understand our priorities.	Communications	Customer Experience and Communications	Increased engagement across the digital channels we use to communicate with, year on year.
, ,	am 2017- 2022 0	·			
1.1.2 Support	community involve	ement in decision making through education arou	nd Council matters an		
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.1.2.1		Engage the community on developing the 2021 Community Strategic Plan to drive council projects and programs into the future	Community	Community	 Engagement continues for the Community Strategic Plan until June 2022 Community Strategic plan developed and adopted by June 2021
1.1.2.3		Undertake the Community Planning Program	Community	Community	 Community plans developed by 1 December 2021 Implement Council actions identified in Community Plans and embed in future operational plans
1.1.2.4		Local Strategic Planning Statement	Strategic Land Use Planning	Strategy	Embed the LSPS in the Delivery Program, Operational Plans and actions across Council, ensuring alignment with the CSP

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Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	Develop an annual Community Engagement program to enable community involvement in decision making in line with the Working Together Framework	Community	Community	The Community Engagement program is developed and implementation of actions commenced and completed to approved plan by 30 June 2022
Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	Produce and submit the annual report in accordance with Local Government Act requirements	Integrated Planning and Reporting	People, Safety and Performance	 Annual report adopted by Council and submitted to the Office of Local Government (OLG) by 30 November 2021 The community report card (Part A); statutory report (Part B); and financial statements (Part C) is published for community viewing
	Provide progress reports on implementation of the Delivery Program in accordance with Local Government Act requirements	Integrated Planning and Reporting	People, Safety and Performance	Report Delivery Program progress to Council on a six monthly basis
	Develop the one year Operational Plan in accordance with Local Government Act requirements	Integrated Planning and Reporting	People, Safety and Performance	Operational Plan adopted by 30 June 2022
1.1.4.4 CW	Implement new Corporate Reporting Tool.	Integrated Planning and Reporting	People, Safety and Performance	Implementation of the new corporate reporting tool completed by June 2022
	Review the Resourcing Strategy in accordance with Local Government Act requirements	Integrated Planning and Reporting	People, Safety and Performance	Resourcing strategy review completed by 30 June 2022
		Strategy.	·	
Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	Develop human centred brand and communications strategy.	Communications	Customer Experience and Communications	Develop human centred brand and communications strategy.
	A capital Works Project am 2017- 2022Obj easy to understand Capital Works Project 1.1.4.4 CW am 2017- 2022 Ob an effective and co Capital Works	ProjectDevelop an annual Community Engagement program to enable community involvement in decision making in line with the Working Together Frameworkam 2017- 2022Objective easy to understand and accessible community reporting.DescriptionCapital Works ProjectDescriptionProduce and submit the annual report in accordance with Local Government Act requirementsProvide progress reports on implementation of the Delivery Program in accordance with Local Government Act requirementsDevelop the one year Operational Plan in accordance with Local Government Act requirements1.1.4.4 CWImplement new Corporate Reporting Tool.am 2017- 2022 Objective an effective and coordinated community focused CommunicationsCapital Works ProjectDescription	With the community on impacts and changes to services. Council Service Capital Works Project Description Council Service Develop an annual Community Engagement program to enable community involvement in decision making in line with the Working Together Framework Community am 2017- 2022Objective pasy to understand accessible community reporting. Council Service Capital Works Project Description Council Service Provide and submit the annual report in accordance with Local Government Act requirements Integrated Planning and Reporting Provide progress reports on implementation of the Delivery Program in accordance with Local Government Act requirements Integrated Planning and Reporting 1.1.4.4 CW Implement new Corporate Reporting Tool. Integrated Planning and Reporting 1.1.4.4 CW Review the Resourcing Strategy in accordance with Local Government Act requirements Integrated Planning and Reporting am 2017- 2022 Objective an effective and coordinated community focused Communications Strategy. Capital Works Project Description Council Service	With the community on impacts and changes to services. Council Service Lead Responsibility Capital Works Project Description Community Community Community Develop an annual Community involvement in decision making in line with the Working Together Framework Community Community Community am 2017- 2022Objective aasy to understand and accessible community reporting. Council Service Lead Responsibility Capital Works Project Description Council Service Lead Responsibility Project Produce and submit the annual report in accordance with Local Government Act requirements Integrated Planning and Reporting People, Safety and Performance Develop the one year Operational Plan in accordance with Local Government Act requirements Integrated Planning and Reporting People, Safety and Performance 1.1.4.4 CW Implement new Corporate Reporting Tool. Integrated Planning and Reporting People, Safety and Performance am 2017- 2022 Ubjective and 2017- 2022 Ubjective and 2017- 2022 Ubjective and 2017- 2022 Ubjective and Elective and coordinated community focused Communications Integrated Planning and Reporting People, Safety and Performance am 2017- 2022 Ubjective and Elective and coordinated community focused Communications Strategy. Lead Responsibility

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, ,	am 2017- 2022 Ob	*			
1.1.6 Continu	e to promote acces	s by the community to Councillors.			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.1.6.1		Manage Councillor development program	Governance and Executive Services	Governance	Build training program for Councillors
1.1.6.2		Deliver the Take the Council to the Community program	Governance and Executive Services	Governance	Hold two off-site Council meetings during the year (Wauchope and Laurieton)
Community St	rategic Plan Strate				
1.2 Maintain s	strong partnerships	between all stakeholders - local, state and fed	eral — so that they are a	affective advocates for	the community
Delivery Progr	am 2017- 2022 Ob				
Delivery Progr 1.2.1 Promote	am 2017- 2022 Ob	jective			
Delivery Progr 1.2.1 Promote the region. Operational	am 2017- 2022 Ol e Council participat	jective on and build linkages in local, state and federal	initiatives, forums and	opportunities to support	Council's continued planning for the growth of
Delivery Progr 1.2.1 Promote the region. Operational Action	am 2017- 2022 Ol e Council participat	jective on and build linkages in local, state and federal Description Convene meetings with local business	initiatives, forums and Council Service	opportunities to support	Council's continued planning for the growth of Performance Measures

-	trategic Plan Strateg ate leadership				
, ,	am 2017- 2022 Ob effective leadership	-			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.3.1.1		Participate in the Mid North Coast Joint Organisation (MNCJO)	Governance and Executive Services	CEO Office	90% participation in the Regional Joint Organisation meetings by the Mayor and Chief Executive Officer
1.3.1.2		Participate in the Regional Cities NSW quarterly meetings	Governance and Executive Services	CEO Office	Attendance at the meetings by the Mayor and Chie Executive Officer
1.3.1.3		Ongoing community disaster preparedness and support	Governance and Executive Services	CEO Office	Preparation of Business Continuity Plans
	am 2017- 2022 Ob Ist and improve Co	jective uncil's reputation through transparency, good de	cision making and livin	g Council's Values	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.3.2.1		Manage the Legislative Compliance Register	Governance and Executive Services	Governance	Undertake annual review of the Legislative Compliance Register and report on it and consider improvements to the Legislative Compliance process.
1.3.2.8		Meet statutory reporting obligations	Governance and Executive Services	Governance	 Satisfactory reporting of: Code of Conduct Complaints Report presented to Council no later than 31 December each year Government Information Public Access (GIPA) Report to Information and Privacy Commission no later than 31 October each year Report on Public Interest Disclosure (PID) biannually in July and February Submit PID annual report to NSW Ombudsman by 31 October each year
1.3.2.9		Coordinate lodgement of annual Disclosures of Interest Returns	Governance and Executive Services	Governance	Annually update Disclosure Register by 30 September
1.3.2.7		Submit annual performance reporting for the water and sewer business to NSW Office of Water for benchmarking comparisons	Water Supply	Infrastructure Planning	Collate and forward performance reporting data to NSW Office of Water annually

8 PORT MACQUARIE-HASTINGS COUNCIL

Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.3.3.2		Manage the Risk Management Action Plan, as part of the StateWide continuous improvement program	Governance and Executive Services	Governance	Review and implement the Risk Management Action Plan on a quarterly basis and report to Executive
1.3.3.5		Improve the Risk Management Framework	Governance and Executive Services	Governance	Improve the Risk Management Framework in line with project milestones
1.3.3.7		Manage the Delegation Framework	Governance and Executive Services	Governance	Review and approve before October end of year
1.3.3.8		Respond to formal information requests within required timeframes	Governance and Executive Services	CEO Office	Respond to formal GIPA requests within the 20 days varying for applied exception
1.3.3.9		Coordination of the Local Government Election with the inclusion of the Fluoridisation Poll	Governance and Executive Services	Governance	Coordinate all requirements for the 2021 Local Government Election and Implement the Councillor Induction Program
1.3.3.10		Implement all requirements as specified in the Office of Local Government Risk Management and Internal Audit Framework	Governance and Executive Services	Governance	Implement all requirements as specified in the new Office of Local Government Risk Management and Internal Audit Framework
	ram 2017- 2022 Ol e our workforce to d	ojective Jeliver community outcomes.			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.3.4.1		Implement workplace strategies and processes to continue to improve Human Resources	People and Culture	People, Safety and Performance	Actions implemented by 30 June 2022 as per the Workforce Management Strategy.
1.3.4.2		Implement workplace strategies and processes to continue to improve Work, Health and Safety	Corporate Work Health and Safety	People, Safety and Performance	Actions implemented by 30 June 2022 as per the Work, Health Safety Strategy.
1.3.4.3		Implement workplace strategies and processes to continue to improve Learning and Development	People and Culture	People, Safety and Performance	To develop a Learning and Strategy by June 2022
	1.3.4.4 CW	Port Macquarie Depot - Rationalisation of Port Macquarie and Wauchope Council Depot operations Allocated Amount 2021-22 \$100,000	Property Investment Portfolio	Strategy	Holistic review of depot holdings and plans by 30 June 202 -

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, ,	am 2017- 2022 Ob	-			
	engaged workforce	1			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.3.5.1		Implement Organisational Development initiatives and strategies to develop a highly engaged workforce who are solutions focused.	People and Culture	People, Safety and Performance	Actions implemented by 30 June 2022 as per the Organisational Development Strategy
	1.3.5.3 CW	Implement Phase 2 of Human Resource Information System (HRIS) - Including Learning Management System (LMS).	People and Culture	People, Safety and Performance	HRIS system Implemented. Online Leave and Timesheet Implemented
Community St	rategic Plan Strateg	(y			
1.4 Use innova	ative, efficient and	sustainable practices			
Delivery Progra	am 2017- 2022 Ob	jective			
1.4.1 Provide	efficient technology	and inclusive digital systems that are easy to	use and access		
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	1.4.1.1 CW	Undertake delivery of Implement phase 2 internal projects (DT - Roadmap Program	Digital Technology	Digital Technology	Delivery of projects according to approved project roadmap, including:
		- multi-year projects) Information and Communications –			Human Resources Information System - complete - June 2022
		Projects Allocated Amount 2021-22 \$1,200,000			Commencement of the multi-year Core System Improvement Program
					Office 365 Rollout - June 2022
					 Continue of the multi-year Service PMH project
					Digital Initiatives to support the Corporate transformation program
	1.4.1.3 CW	Undertake delivery of Digital Technology projects (servers, storage, switches etc)	Digital Technology	Digital Technology	Delivery of projects according to approved proje roadmap, including:
		Allocated Amount 2021-22 \$820,000			Partial Infrastructure Hardware Replacement complete by June 2022
					Digital Initiatives to support the Corporate transformation program

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1.4.1.5		Develop asset design and as-constructed templates as part of the Asset Data Standards Review	Asset Management	Infrastructure Planning	Develop an Auto CAD file template for asset design and as-constructed plans that meets Asset Design As Constructed (ADAC) standards by 30 June 2022
, ,	am 2017- 2022 Ob				
	1	he agreed service level at best value			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.4.2.1		Work across the organisation to facilitate the implementation of the Business Improvement Strategy	Business improvement	People, Safety and Performance	 Develop Transformation Strategy, Roadmap and Scope 1. Business case and program plan is endorsed 2. Transformation program implemented Nov/Dec 2021
,	am 2017- 2022 Ob a customer focused	jective service that provides the community a consiste	ent experience of Coun	cil	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.4.3.3		Develop a human centred customer experience strategy that delivers a roadmap for improving customer journey.	Customer Experience	Customer Experience and Communications	Customer experience strategy adopted Q2. Customer journey mapped and improved for 10 key interactions by Q3.
1.4.3.4		Introduce online payments for customers to purchase all products and services.	Customer Experience	Customer Experience and Communications	Online payments introduced end Q2.
-	rategic Plan Strateg ong corporate and	§y financial management that is transparent and	accountable		
ý U	am 2017- 2022 Ob Council's financial	jective assets and provide accurate, timely and reliable	e information		
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.5.1.1		Monitor and accurately report on Council's financial position in accordance with Local Government Act requirements	Financial Management	Financial Services	 Submit three quarterly budget review statements and an annual report to Council Lodge audited financial statements with Office of Local Government by 31 October

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1.5.1.2		Manage Council's investment portfolio to optimise investment returns within the constraints of the policy, the Local	Financial Management	Financial Services	 Exceed benchmark for investment return Table report to Council monthly
		Government Act and Regulations			
1.5.1.3		Develop the annual Operational Plan budget and review the Long Term Financial Plan	Financial Management	Financial Services	 Develop the draft annual budget in line with Integrated Planning and Reporting timeframes Review the Long Term Financial Plan in line with the budget cycle Final budget adopted by Council by 30 June
4544				<u> </u>	
1.5.1.4		Prepare monthly financial reports for Council	Financial Management	Financial Services	Submit financial reports to Council monthly
1.5.1.6		Progress Partridge Creek residential development planning	Property Investment Portfolio	Strategy	Provide a report to Council by June 2022 outlining Partridge Creek residential development options upon receipt of updated land valuation
1.5.1.9		Progress Emily Avenue residential development planning	Property Investment Portfolio	Strategy	Receive Development Consent and Commence Design by 30 June 2022
1.5.1.10		Implement Asset Designed As Constructed (ADAC) computerised automated process system	Asset Management	Infrastructure Planning	Complete implementation of ADAC within Council operations by 30 June 2022
1.5.1.14		Property Portfolio Investigations. Contamination reports/EOI	Property Investment Portfolio	Strategy	Evolve the Operational, Financial, Physical and Environmental profile of existing and potential land and property assets through contemporary reports and studies.
	am 2017-2022 Ob	jective g. purchasing and contract management approa	chos that are transpare	nt and equitable	
Operational	Capital Works	Description	Council Service	Lead Responsibility	Performance Measures
Action	Project				
1.5.2.1		Implement the recommendations of the external review of Council's Procurement function	Procurement and Stores	Financial Services	Action plan for delivery of the recommendations devised with agreed timeframes and responsibilities. Complete implementation of actions with a 2022 timeframe.

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	1.5.2.2 CW	Ensure plant purchases are in line with the plant replacement program (Plant Purchases and Disposals - multi-year project) Allocated Amount 2021-22 \$2,500,000	Fleet and Depot Management	Infrastructure Operations	Plant replacement program delivered according to approved schedule Purchase up to 150 items of plant completed by June 2022
1.5.2.3		Manage and maintain Council's Plant and Fleet to support the operational activities of Council	Fleet and Depot Management	Infrastructure Operations	Plant and Fleet managed and maintained in accordance with adopted program and budget.
1.5.2.5		Undertake procurement activities in accordance with legislative requirements, that are transparent	Procurement and Stores	Financial Services	Capture contracts awarded with a value of \$150,000 (excluding GST) or more.
Delivery Progr	ram 2017-2022 Ob	ojective			
1.5.3 Develop	o, manage and mair	ntain Council Business Units through effective co	mmercial management	t	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
1.5.3.1		Increase operating revenue at the Airport, the Glasshouse, and the Environmental Laboratory	Airport, Glasshouse, Environmental Testing	Commercial Business Units	Increase operating revenue by 3% per annum per business unit - including: Airport, Glasshouse, and Environmental Laboratory
1.5.3.2		Implement the Glasshouse Plan 2020- 2022 in consideration of Council's Cultural Plan	Glasshouse	Commercial Business Units	 Present biannual reports to Council Deliver actions according to adopted Plan
1.5.3.6		Manage Council's property management, leasing and licensing, and statutory property functions for the most appropriate return to Council and the community	Property and Leasing	Recreation, Property and Buildings	 Manage Council's lease and license portfolio to ensure the most appropriate return to Council, maximisation of the use Council properties and equity amongst community groups Deliver statutory property projects according to approved project plan Undertake statutory property transactions and projects as required
	1.5.3.8 CW	Land and Easement Acquisitions for Council Purposes - Survey, Valuation and Land Transactions Allocated Amount 2021-22 \$45,000	Property and Leasing	Recreation, Property and Buildings	Undertake statutory property transactions as required

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Delivery Program 2017- 2022 Objective 1.5.4 Identify new commercially viable revenue sources						
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures	
1.5.4.1		Review current revenue sources and investigate the commercial viability of identified new revenue sources	Airport, Environmental Testing, Glasshouse, and Waste Management	Commercial Business Units	Identify new revenue sources	



Community Theme 2



COMMUNITY STRATEGIC PLAN

What we are trying to achieve

A healthy, inclusive and vibrant community.

How we will get there

2.1 Create a community that feels safe

2.2 Advocate for social inclusion and fairness

- 2.3 Provide quality programs, community facilities and public spaces, for example, community halls, parks and vibrant town centres
- 2.4 Empower the community through active involvement in projects, volunteering and events
- 2.5 Promote a creative and culturally rich community

Community St	rategic Plan Strateg	У			
2.1 Create a c	ommunity that feels	safe			
, .	am 2017-2022 Obj Community Safety				
Operational Pl	an 2021-2022 - Ac	tion/Activity/Project			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
2.1.1.1		Manage the contract for approved lifeguard services	Sports and Recreation	Recreation, Property and Buildings	 Deliver approved lifeguard services Deliver lifeguard education programs to schools and community groups (target 2,500 students)
2.1.1.2		Work with our communities to ensure safe access to Council facilities, events, programs and in time of emergency.	Community Place	Community	 Working with communities to develop preparedness emergency recovery plar to ensure their safety in times of emergency Develop a community checklist to improve the community's perception of safety at our facilities and events.
2.1.1.3		Working with our communities and CCATs to finalise Emergency Preparedness Plans for our communities	Community Place	Community	Communities engaged and plans developed.
,	am 2017- 2022 Ob				
2.1.2 Advocat	e for, support and c	oordinate emergency services.			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	2.1.2.2 CW	Council supports Emergency Management Operations and Agencies Bushfire Tank/Vehicle purchases	Emergency Management	Infrastructure (Director)	Provide coordination and support to emergency service units as required
	2.1.2.3 CW	Upgrade to Port Macquarie Emergency Operations Centre Allocated Amount 2021-22 up to \$148,968	Emergency Management	Infrastructure (Director)	Backbone EOC technology upgrades implemented in line with funding deed

Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
Action 2.1.3.1	Project	Monitor and take action as appropriate to ensure compliance with development approvals and building, environmental, public health and on-site sewage standards	Health and Building Regulation	Environment and Regulatory Services	 Risk Management Plans and annual audits monitored for all regulated cooling and heating systems and inspected as required. On-site sewage system inspections completed in accordance with approve program Food Premises inspection program completed by 30 June Pollution incidents responded to within the service standard for type of incider Public Health inspections completed for all public pools and spas in accordance with approved program Inspections of personal appearance premises undertaken in accordance with approved program Underground Petroleum Storage Systems (UPSS) inspected in accordant with approved program Building certificate applications assessed within service standard Mandatory pool compliance inspection completed in accordance with approve program Notices of Completion applications for caravan park and manufactured homes assessed within service standard

2.1.3.3	Provide ranger and local law enforcement services to ensure compliance with legislation, regulations and Council policies relating to parking, regular beach patrols, illegal signage, sale of goods on roads, building site sediment control and companion animal management	Compliance	Environment and Regulatory Services	 Respond to 80% of customer requests regarding compliance with parking, beach patrols, illegal signage, sale of goods on roads, building site sediment control and companion animal management within set customer service standard Monitor the number of offences detected during proactive patrols Monitor the number of companion animal incidents Maintain Animal Shelter return and rehoming rates Respond to Development Compliance and Stormwater complaints within service standards
2.1.3.4	Manage Council's Environmental Laboratory and provide sampling, analysis, reporting and professional advice of water quality analysis and environmental programs in accordance with operational and regulatory requirements to internal and external customers on an approved fee for service basis	Environmental Testing	Commercial Business Units	 Complete all sampling, analysis and reporting of operational and regulatory requests in accordance with approved budgets Complete all sampling, analysis and reporting of operational and regulatory requests in accordance with agreed service standards Maintain National Association of Testing Authorities (NATA) corporate accreditation
2.1.3.5	Provide a safe water supply in accordance with Australian Drinking Water Quality Guidelines	Water Supply	Infrastructure Planning	Have nil reportable incidents in accordance with NSW Health agreed protocols

Oomana unity Stre	taria Dian Stratar						
-	ategic Plan Strateg r social inclusion a	-					
	m 2017- 2022 Obj						
2.2.1 Support and advocate for all community sectors.							
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures		
2.2.1.1		Implement the Community Inclusion Plan	Community Inclusion	Community	Imbed the Community Inclusion Committee and develop reconciliation action plan		
2.2.1.3		Through the Community Inclusion Plan coordinate the Community grants program to assist the community to deliver projects that contribute to a sense of place	Community Inclusion	Community	Deliver Community Grants Program through two rounds per year		
2	ategic Plan Strateg	*					
2.3 Provide qua	llity programs, com	nmunity facilities and public spaces, e.g. for example, Co	ommunity halls, parks an	d vibrant town centres			
Delivery Program	m 2017- 2022 Obj	jective					
2.3.1 Ensure ac	cess to communit	y facilities and activities: including access to natural en	vironment				
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures		
2.3.1.1		Disabled Access – Buildings/Recreation Facilities	Community Inclusion	Community	Review and implement the actions as per the Disability Inclusion Action Plan		
2.3.1.2		Through the Community Inclusion plan continue to manage Community Halls	Community Inclusion	Community	Review current Community Halls management structure. Complete EOI to convert Hall Management to community.		
Delivery Program	m 2017 - 2022 Ob	jective					
2.3.2 Provide a	range of inclusive	sporting and recreational opportunities and facilities to	encourage a healthy and	lactive lifestyle			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures		
	2.3.2.2 CW	Deliver sporting facility renewal and upgrades program across the LGA	Sports and Recreation	Recreation, Property and	Deliver project to approved project plan		
		Vince Imon Sporting Fields – Sports Facility Upgrade		Buildings			
		Allocated Amount 2021-22 \$1,178,783					
		Sporting Infrastructure Renewals Allocated Amount 2021-22 \$475,000					

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2.3.2.3		Manage the Mayor's Sporting Fund	Cultural Development	Economic and Cultural Development	Distribute funds as required and assist with fundraising events
2.3.2.4		Undertake regional master planning for recreational facilities	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
	2.3.2.12 CW	McInherney Park - Contribution to public amenities in new boat storage facility Allocated Amount 2021-22 \$120,000	Sports and Recreation	Recreation, Property and Buildings	Contribution in accordance with agreed project milestones
	2.3.2.13 CW	Westport Park - boat ramp parking upgrade Allocated Amount 2021-22 \$937,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
Delivery Progra	am 2017- 2022 Ob	jective		·	
2.3.3 Develop	and implement ma	magement of operational and maintenance programs for	open space, recreation	al and community facili	ities
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
2.3.3.1		Undertake the maintenance program for parks, reserves, sporting fields and beaches including parks signage	Sports and Recreation	Recreation, Property and Buildings	Deliver program, including beach cleaning and playground inspections according to approved maintenance schedules
	2.3.3.2 CW	Undertake scheduled and reactive maintenance programs of all Council-owned buildings including office furniture replacement Allocated Amount 2021-22 \$47,700	Building Maintenance	Recreation, Property and Buildings	Provide office furniture replacement within 2 weeks of the request
	2.3.3.3 CW	Undertake building rectification works in line with Council Asset Management - (multi-year project) Allocated Amount 2021-22 \$319,600	Building Maintenance	Recreation, Property and Buildings	Deliver project according to approved project plan
2.3.3.4		Provide, maintain and manage public aquatic facilities	Aquatic Facilities	Recreation, Property and Buildings	 Undertake annual off-season facility maintenance in accordance with approved program to ensure facility is fit for purpose Undertake quarterly meeting to oversee the management of Council's public aquatic facilities in accordance with lease obligation checklist

	2.3.3.5 CW	Deliver park furniture renewals across the local government area, including donated seats program Allocated Amount 2021-22 \$78,000	Sports and Recreation	Recreation, Property and Buildings	Deliver replacement program according to approved schedule
2.3.3.9		Implement maintenance programs for boat ramps, wharves and jetties	Sports and Recreation	Recreation, Property and Buildings, (Infrastructure Operations)	Deliver programs according to approved maintenance and schedule (for boat ramps, wharves and jetties)
2.3.3.10		Undertake the maintenance program for beaches	Sports and Recreation	Recreation, Property and Buildings	 Undertake beach and beach access maintenance as per program Undertake inspections of beach access signs
	2.3.3.12 CW	Thrumster Sports Fields- Detailed design and approvals Allocated Amount 2021-22 \$406,441	Sports and Recreation	Recreation, Property and Buildings, (Infrastructure Delivery)	Deliver project according to approved project plan
	2.3.3.17 CW	Carry out improvement and expansion works at Wauchope, Laurieton and other outlying cemeteries Allocated Amount 2021-22 \$20,000	Cemeteries	Customer Experience & Communications	Deliver projects according to approved project plan
2.3.3.18		Administration of public roads, public spaces, events and customer engagement: - Section 138 Road Applications - Road Encroachments - Customer Enquiries and Responses - Other Statutory Road Functions - Road Policies	Roads, Bridges and Transport	Infrastructure Planning	Responding to enquiries and issuing relevant permits within required service standards (these are not fully defined)
2.3.3.19		Review and update the policy that governs how we operate our public cemeteries	Cemeteries	Customer Experience & Communications	Policy completed end of Q3

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Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	2.3.4.12 CW	Port Macquarie Town Centre Master Plan (TCMP) improvements Allocated Amount 2021-22 \$3,100,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
	2.3.4.14 CW	Port Macquarie Pool - Continuation of Strategic Concept and Business Case development Allocated Amount 2021-22 \$748,215	Aquatic Facilities	Recreation, Property and Buildings (Project Delivery)	Deliver project according to approved project plan
	2.3.4.18 CW	Rainbow Beach Sports Fields - Commence construction of District Facilities Allocated Amount 2021-22 \$3,336,428	Sports and Recreation	Recreation, Property and Buildings (Project Delivery)	Deliver project according to approved project plan
	2.3.4.22 CW	Parks and Gardens Future Designs - Preparation of engineering designs in advance of construction programs for Parks and Gardens Allocated Amount 2021-22 \$32,200	Sports and Recreation	Recreation, Property and Buildings	Parks and Gardens Future Designs preparation undertaken as required
	2.3.4.25 CW	Town Green Central & West/Playground Allocated Amount 2021-22 \$400,989	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
	2.3.4.28 CW	Town and Village Signage - Finalise the Installation of Signage in the Local Government area Allocated Amount 2021-22 \$50,000	Community Place	Community	Deliver town and village signage to the northern region of LGA to complete the signage project by 30 June 2022
	2.3.4.33 CW	CSU-Googik Connection Consultancies COVID RELIEF RESERVE FUNDING Allocated Amount 2021-22 \$90,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of detailed design for the Googik - CSU connection

2.3.4.35 CW	Aquatic facilities - renewal of assets at end of useful life Allocated Amount 2021-22 \$150,000	Aquatic Facilities	Recreation, Property and Buildings	Carry out reactive maintenance for pool components at end of useful life
2.3.4.36 CW	Undertake building rectification for 1 Commerce St Building Allocated Amount 2021-22 \$100,000	Building Maintenance	Recreation, Property and Buildings	Rectification works as per building inspection on purchase by June 2022
2.3.4.37 CW	Camden Haven Surf Life Saving Clubhouse - connect to sewer Allocated Amount 2021-22 \$100,000	Building Maintenance	Recreation, Property and Buildings	To be completed as per completion of Development Application
2.3.4.26 CW	Park and Reserve Upgrades - Pioneer Park, Log Wharf & Long Flat Recreation Grounds Allocated Amount 2021-22 - \$250,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
2.3.4.27 CW	Detailed design - Bain Park revitalisation, Westport Park, Town Beach Amphitheatre Allocated Amount 2021-22 - \$450,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
2.3.4.28 CW	Kendall Tennis Club Upgrade Allocated Amount 2021-22 - \$150,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
2.3.4.29 CW	Lake Cathie Community Reserve Basketball Court Allocated Amount 2021-22 - \$70,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
2.3.4.30 CW	Town Beach Reserve Basketball Courts Allocated Amount 2021-22 - \$70,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
2.3.4.31 CW	Wayne Richards Park Bike Track Upgrade Allocated Amount 2021-22- \$350,000	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan
2.3.4.32 CW	Bonny Hills Reserve Master Plan - Landscaping Improvements Allocated Amount 2021-22- \$59,640	Sports and Recreation	Recreation, Property and Buildings	Deliver project according to approved project plan

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Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	2.3.5.1 CW	Provide a range of library programs and lending services across the local government area.	Library	Community	1. Annual library program delivered 2. Increase Library membership by 5%
		That include the maintenance of the collection, management of the facility and delivery of services, programs and events.			annually
		Furniture and Fixings.			
		Allocated Amount 2021-22 \$40,000			
	2.3.5.2 CW	Provide a range of library programs and lending services across the local government area.	Library	Community	Complete book purchases by 30 June 2022
		That include the maintenance of the collection, management of the facility and delivery of services, programs and events.			
		Purchase of Library Books - (multi-year project)			
		To provide book stock at NSW State average per capita as a minimum level			
		Furniture and Fixings			
		Allocated Amount 2021-22 \$277,000			
2.3.5.4		Implement the Library Strategic Plan	Library	Community	Implement actions as per the Library Strategic Plan, e.g. determine future library needs for Rainbow Beach and Sovereign Hills
	2.3.5.7 CW	Deliver Special Library Projects to enhance library delivery program Allocated Amount 2021-22 \$20,000	Library	Community	Deliver Special Library Projects to enhanc library delivery program through re-design and inclusion of new elements in the library spaces
	2.3.5.8 CW	Creation of anytime library to provide resources to our other communities in Sovereign Hills and Lake Cathie Allocated Amount 2021-22 \$128,000	Library	Community	That the creative library solutions are implemented at the defined location in Lake Cathie and Sovereign Hills in accordance with grant timelines

Delivery Progra	am 2017-2022 Obj	ective			
2.3.6 Support a	a range of inclusive	community activities and programs			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
2.3.6.11		Port Macquarie Hastings (PMH) Cares COVID RELIEF RESERVE FUNDING	Community	Community	Develop Volunteer Program to support community members impacted by disaste or social issues
-	ategic Plan Strateg he community thro	y ugh encouraging active involvement in projects, volunte	ering and events		
, ,	am 2017-2022 Obj h the community to	ective identify and address community needs, to inform Coun	cil processes, services	and projects	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
2.4.1.1		Support and connect with our community through delivering the actions outlined in the Community Inclusion Plan	Community Inclusion	Community	 Implement Events and Volunteering programs to encourage residents to actively contribute to their Community Increase volunteers by 10% The community events delivered annually
2.4.1.8		Design the Strategic Education Program	Community Education	Community	Implement the Annual Education program that influences community behaviours, including the Schools Program
-	ategic Plan Strateg creative and cultur	y ally rich community			
	am 2017-2022 Obj cultural activities w	ective ithin the community			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
2.5.1.1		Public Art Maintenance and Implementation	Cultural Development	Economic and Cultural Development	 Deliver the 'lost at sea' art piece Deliver maintenance as per asset management plan priorities and established budget

	2.5.1.2 CW	Undertake Glasshouse back of house maintenance Plant and Equipment Purchases Allocated Amount 2021-22 \$10,000 Technical Equipment Purchases Allocated Amount 2021-22 \$56,000	Glasshouse	Commercial Business Units	Undertake maintenance as required
2.5.1.3		Manage the delivery of a range of high quality performing and visual arts events at the Glasshouse in consideration of Council's Glasshouse Plan and Cultural Plan	Glasshouse	Commercial Business Units	 Deliver the performing arts program as per the annual season launch – 15 events Deliver the exhibition program as per the annual gallery program - 20 exhibitions
2.5.1.4		Finalise and implement new Cultural Plan	Cultural Development	Economic and Cultural Development	 Finalise new Cultural Plan Implement plan working with the Cultural Steering Group and key stakeholders and report annually to Council
2.5.1.8		Art Walk	Cultural Development	Economic and Cultural Development	Artwalk event delivered successfully over October 2021 long weekend

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Community Theme 3

Your Business and Industry

COMMUNITY STRATEGIC PLAN

What we are trying to achieve

The Port Macquarie-Hastings region is a successful place that has vibrant, diversified and resilient regional economy for people to live, learn, work, play and invest.

How we will get there

3.1 Embrace business and a stronger economy

- 3.2 Create vibrant and desirable places
- 3.3 Embrace opportunity and attract investment to support the wealth and growth of the community
- 3.4 Partner for success with key stakeholders in business, industry, government, education and the community

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-	ategic Plan Strateg usiness and a stro				
, ,	m 2017- 2022 Ob e growth of local bi	jective Isiness and industry, ensuring this is a central o	consideration of Cour	cil activities	
Operational Pla	an 2021-2022 - Ac	tion/Activity/Project			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
3.1.1.1		Implement actions from the 2017-2021 Economic Development Strategy to lead, create and proactively support an environment that stimulates sustainable industry, business and investment growth.	Economic Development	Economic and Cultural Development	Implement actions from the Economic Development Strategy and report to Council on implementation twice per year (November and May)
3.1.1.2		Finalise development of new Economic Development Strategy	Economic Development	Economic and Cultural Development	Finalise and commence implementation of the new Economic Development Strategy by June 2022
	3.1.1.14 CW	Christmas Tree Decoration Renewal Allocated Amount 2021-22 \$5,000	Cultural Development	Economic and Cultural Development	Purchase complete
Delivery Progra	m 2017-2021 Obj	ective			
3.1.2 Optimise	the use of approp	riately zoned land for business uses			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
3.1.2.2		Review and prepare planning proposals for specific sites within the Port Macquarie- Hastings Local Government Area based on priorities as determined by Council	Strategic Land Use Planning	Development Assessment	Report to Council bi-annually on progress of the progress and status of site specific amendments
3.1.2.3		Greenmeadows Drive - Development Planning and Feasibilities	Property Investment Portfolio	Strategy	Consider Greenmeadows Drive appropriateness for strategy on community use assets by 30 June 2022

Delivery Progra	am 2017- 2022 Obj	jective			
3.1.3 Impleme	ent Major Events Sti	rategy.			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
3.1.3.2		Implement the Port Macquarie- Hastings Events Plan	Economic Development	Economic and Cultural Development	 Successful distribution of Events Grants and Sponsorships Implementation of Events Plan actions relating to 'Major Events'
Delivery Progra	am 2017- 2022 Ob	jective			
3.1.4 Impleme	ent the Destination	Management Plan			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
3.1.4.1		Work with stakeholders to implement actions from the Destination Management Plan in accordance with the identified strategic outcomes	Economic Development	Economic and Cultural Development	 Implement actions within the Destination Management Plan Increase year on year website visits by 15%
3.1.4.2		Finalise Greater Port Macquarie Brand Review and Marketing Strategy	Economic Development	Economic and Cultural Development	 New brand developed Marketing Strategy finalised
3.1.4.4		Finalise and implement the public spaces interpretation strategy and guidelines COVID RELIEF RESERVE FUNDING	Economic Development	Economic and Cultural Development	 Strategy and guidelines developed Commence installation of new interpretive signs/assets in accordance with established budget
Community Str	rategic Plan Strateg	У			
3.2 Create vib	rant and desirable (places			
Delivery Progra	am 2017- 2022 Obj	jective			
3.2.1 Support	vibrant commercial	, tourism, recreational and or community hubs	across the region		
		Program Objective are included in 3.1.1.1 - Imp nt that stimulates sustainable industry, busines			omic Development Strategy to lead, create and

Community Strategic Plan Strategy

3.3 Embrace opportunity and attract investment to support the wealth and growth of the community

Delivery Program 2017- 2022 Objective

3.3.1 Develop, manage and maintain Port Macquarie Airport as a key component of the regional transport network and continue to grow the airport's contribution to the regional economy.

Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
3.3.1.1		Continue to monitor, plan and implement the Port Macquarie Airport Master Plan to meet demand and regulatory requirements - (multi- year project)		Commercial Business Units, (Project Delivery)	 Undertake further planning and investigate the business case for an expansion of the General Aviation (GA) precinct to provide for new hangar development sites Finalise relocation of the existing Bureau of Meteorology weather station (AWIS)
3.3.1.2		Support, facilitate and advocate for regular public transport (RPT) airline services at Port Macquarie Airport	Airport	Commercial Business Units	Annual passenger numbers compared to previous year
	3.3.1.4 CW	Port Macquarie Airport Parallel Taxiway Stage 1 and General Aviation (GA) - pavement renewal – finalise pre-construction - (multi- year project) Allocated Amount 2021-22 \$100,000	Airport	Commercial Business Units, (Project- Delivery)	Finalise pre-construction planning and design of Stage 1 parallel taxiway and General Aviation pavement renewal by 30 June 2022
3.3.1.5		Continue to monitor, plan and implement the Port Macquarie Airport Biodiversity Certification Strategy and related environmental approvals	Airport	Commercial Business Units, Environment and Regulatory Services	 Prepare Vegetation Management Plan in accordance with NSW Biodiversity & Conservation Division (BCD) requirements Prepare a Threatened Allocasuarina Management Strategy in accordance with Australian Government Department of Environment requirements
3.3.1.6		Operate and maintain Port Macquarie Airport in accordance with regulatory, safety and security requirements Maintenance and refurbishment of Council-owned Hangars	Airport	Commercial Business Units	 Carry out maintenance / refurbishment of Council-owned hangars as required Ensure airport operational manuals are reviewed by 30 June Carry out maintenance / refurbishment of Council-owned hangars as required

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	3.3.1.7 CW	Continue development planning for the Port Macquarie Airport Business and Technology Park - (multi-year project) Allocated Amount 2021-22 \$60,000	Strategy	Strategy, (Commercial Business Units)	Brief Councillors on the proposed Airport Business & Technology Park by 30 June 2022
Delivery Progra	m 2017-20220bje	ctive			
3.3.2 Promote	nvestment, educat	tion and lifestyle opportunities			
		Program Objective are included in 3.1.1.1 - Imp nt that stimulates sustainable industry, busines			ic Development Strategy to lead, create and
Community Stra	ategic Plan Strateg	у			
3.4 Partner for	success with key s	takeholders in business, industry, government,	education and the c	ommunity	
Delivery Progra	m 2017- 2022 Obj	ective			
3.4.1 Foster pa	rtnerships with hig	her education institutions through research and	l development, inno	vation and local skills de	velopment
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
3.4.1.2		Partner with Charles Sturt University in delivering the Port Macquarie Innovation Hub.	Economic Development	Economic and Cultural Development	 Maintain co-working use Support and grow innovation events and opportunities
		Program Objective are included in 3.1.1.1 - Imp nt that stimulates sustainable industry, busines			ic Development Strategy to lead, create and
Delivery Progra	m 2017- 20220bje	ective			
3.4.2 Support l	ocal business netw	rorks			
		Program Objective are included in 3.1.1.1 - Imp nt that stimulates sustainable industry, busines			ic Development Strategy to lead, create and
Delivery Progra	m 2017- 2022 Obj	ective			
3.4.3 Encourag	ge innovation that v	will support our growth as a regional city includi	ng smart community	technology	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
3.4.3.3		Implement Council's Smart Community Roadmap	Economic Development	Economic and Cultural Development	Smart Communities Initiatives developed and implemented in accordance with priorities identified in Roadmap



Community Theme 4

Your Natural and Built Environment

COMMUNITY STRATEGIC PLAN

What we are trying to achieve

A connected, sustainable, accessible community and environment that is protected now and into the future.

How we will get there

- 4.1 Provide (appropriate) infrastructure and services including water cycle management, waste management, and sewer management
- 4.2 Aim to minimise the impact of natural events and climate, e.g. floods, bushfires and coastal erosion
- 4.3 Facilitate development that is compatible with the natural and built environment
- 4.4 Plan for integrated transport systems that helps people get around and link our communities
- 4.5 Plan for integrated and connected communities across the Port Macquarie-Hastings area
- 4.6 Restore and protect natural areas
- 4.7 Provide leadership in the development of renewable energy opportunities
- 4.8 Increase awareness of issues affecting our environment, including the preservation of flora and fauna

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Item 09.10 Attachment 2

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Community Strategic Plan Strategy

4.1 Provide (appropriate) infrastructure and services including water cycle management, waste management, and sewer management

Delivery Program 2017- 2022 Objective

4.1.1 Plan, investigate, design and construct water supply assets ensuring health, safety, environmental protection and security of supply for the future growth of the region

Operational Pla	an 2021-2022 - Actio	n/Activity/Project			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	4.1.1.1 CW	Construction planning for the Sancrox Reservoir to Area 13 (Thrumster) trunk main (DN750) Allocated Amount 2021-22 \$34.297	Water Supply	Infrastructure Planning (Project Delivery)	Resolution of land matters and approval of detailed project plan
	4.1.1.4 CW	Allocated Amount 2021-22 \$34,297 Installation of new water supply services to residential and business premises - to cater for new development Allocated Amount 2021-22 \$100,000	Water Supply	Infrastructure Planning (Infrastructure Operations)	Delivery of works as required to support development.
	4.1.1.16 CW	Thrumster Recycled Water Interim Supply - Rising Main to Thrumster Reservoir - Booster Pumping Station and Interim Reservoir - undertake construction Allocated Amount 2021-22 \$3,100,000	Water Supply	Infrastructure Planning (Project Delivery)	Completion of construction
	4.1.1.26 CW	Continue construction of the Southern Arm Water Trunk Main (D750) - Pacific Hwy Bonny Hills Allocated Amount 2021-22 \$50,000	Water Supply	Infrastructure Planning (Project Delivery)	Completion of detailed designs and approva of detailed project plan for construction
	4.1.1.27 CW	Commencement of design for the Water treatment/Filtration Plant at Cowarra dam Allocated Amount 2021-22 \$100,000	Water Supply	Infrastructure Planning	Approval of detailed project plan and completion of concept designs
	4.1.1.28 CW	Preconstruction Works - Trunk Main from Bonny Hills to Kew (Area 15) Reservoir - Stage 1 Allocated Amount 2021-22 \$250,000	Water Supply	Infrastructure Planning (Project Delivery)	Completion of detailed designs for Stage 1

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4.1.1	1.31 CW	Water Supervisory Control and Data Acquisition (SCADA) System - Replacement Allocated Amount 2021-22 \$600,000	Water Supply	Infrastructure Planning (Infrastructure Operations)	Completion of system replacement
4.1.1	1.35 CW	Preconstruction/construction of Widderson St Reservoir Roof Replacement Allocated Amount 2021-22 \$50,000	Water Supply	Infrastructure Planning (Infrastructure Operations)	Completion of detailed design
4.1.1	1.36 CW	Water critical infrastructure - Site Security Upgrades Allocated Amount 2021-22 \$150,000	Water Supply	Infrastructure Planning (Infrastructure Operations)	Completion of programmed works
4.1.1	1.42 CW	Installation of Baffles within Reservoirs to ensure chlorination contact time for disinfection Allocated Amount 2021-22 \$80,000	Water Supply	Infrastructure Operations	Completion of programmed works
4.4.1	1.43 CW	Annual Energy Efficiency Initiatives Allocation - Variable Speed Drive Pumps - water sites Allocated Amount 2021-22 \$150,000	Water Supply	Infrastructure Operations	Deliver project according to approved project plan
4.1.1	1.46 CW	Bonny Hills Recycled Water System Upgrade Allocated Amount 2021-22 \$777,144	Sewerage	Infrastructure Planning (Project Delivery)	As per water list
4.1.1	1.47 CW	Bulk water system chlorination upgrade Allocated Amount 2021-22 \$1,700,000	Water Supply	Infrastructure Planning, (Infrastructure Operations)	Completion of construction works
4.1.1	1.48 CW	Commence preconstruction for an elevated reservoir at Bonny Hills Allocated Amount 2021-22 \$50,000	Water Supply	Infrastructure Planning, (Project Delivery)	Approval of detailed project plan and commencement of concept designs
4.1.1	1.49 CW	Commence preconstruction works for the Kew (Area 15) Reservoir Allocated Amount 2021-22 \$50,000	Water Supply	Infrastructure Planning, (Project Delivery)	Approval of detailed project plan and commencement of concept designs

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	4.1.1.50 CW 4.1.1.52 CW	Construction of Rock Ramp to secure water level at Koree Island Pumping Stations Intake Pool Allocated Amount 2021-22 \$150,000 Microwave link Cowarra to Rosewood Allocated Amount 2021-22 \$60,000	Water Supply Water Supply	Infrastructure Planning, (Project Delivery) Infrastructure Planning, (Infrastructure	Completion of survey works - dependent on river water levels Completion of programmed works
	4.1.1.53 CW	Pre Construction of Cowarra to Wauchope Backfeed PS Allocated Amount 2021-22 \$100,000	Water Supply	Operations) Infrastructure Planning, (Infrastructure Operations)	Completion of detailed designs
4.1.1.54		Undertake asset revaluations for water supply network	Water Supply	Infrastructure Planning	Completion of revaluation to the required standard for external audit
ý U	m 2017-2022 Object and implement annu	ctive al maintenance and preventative works program	n for water supply assets	6	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	4.1.2.1 CW	Conduct water asset replacement and renewal programs for live water mains, water meters, renewals and minor works,	Water Supply	Infrastructure Planning (Infrastructure Operations)	Completion of programmed works

	4.1.3.23 CW	Annual Energy Efficiency Initiatives Allocation - Variable Speed Drive Pumps	Sewerage	Infrastructure Planning	Completion of programmed works
	4.1.3.22 CW	Investigation, Design and Construction of Kew Sewer Treatment Plant (STP) Upgrade (Multi-Year project)	Sewerage	Infrastructure Planning (Project Delivery)	Award of construction contract
	4.1.3.18 CW	Continuation of Preconstruction of Thrumster Sewerage Treatment Plant (Area 13) - Phase 1 Allocated Amount 2021-22 \$50,000	Sewerage	Infrastructure Planning (Project Delivery)	Approval of detailed project plan and commencement of treatment process scoping
	4.1.3.3 CW	Small Towns Sewerage Scheme Construction - Long Flat, Comboyne, Telegraph Point (multi-year project)	Sewerage	Infrastructure Planning (Project Delivery)	Completion of project reporting and finalisation
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
, ,	n 2017- 2022 Object stigate, design and c	tive onstruct sewerage assets ensuring health, safe	ety, environmental protect	ion and the future growth	of the region
		Allocated Amount 2021-22 \$100,000			
	4.1.2.4 CW	Property Acquisition associated with the Sancrox Reservoir to Wauchope Trunk Main Augmentation and mechanical assets	Water Supply	Infrastructure Operations	Completion of programmed works
	4.1.2.3 CW	Marbuk Motorised Valve Relocation Allocated Amount 2021-22 - \$600,000	Water Supply	Infrastructure Operations	Completion of programmed works
		Water Treatment Plant (WTP) electrical and mechanical assets Allocated Amount 2021-22 - \$100,000		Operations	
	4.1.2.2 CW	Carry out programmed replacement of	Water Supply	Infrastructure	Completion of programmed works

4.1.3.26 CW	Preconstruction works for upgrade of Lake Innes Sewerage Pump Station	Sewerage	Infrastructure Planning (Project Delivery)	Completion of concept designs and construction contract award
	#71 (PMSPS71) Allocated Amount 2021-22 \$312.010			
	Preconstruction works for Camden Haven Sewer Pump Station 2 Refurbishment			
	Preconstruction works for upgrade of Port Macquarie Sewerage Pump Station #18			
4.1.3.25 CW	Port Macquarie Waste Water Treatment Plant Odour control mitigation works Allocated Amount 2021-22 \$107,000	Sewerage	Infrastructure Planning (Project Delivery)	Completion of mitigation works in accordance with EPA requirements
4.1.3.28 CW	Camden Haven Waste Water Treatment Membrane Replacements Allocated Amount 2021-22 \$400,000	Sewerage	Infrastructure Planning (Project Delivery)	Commencement of works
4.1.3.29 CW	Preconstruction of Wauchope Waste Water Treatment Plant Inlet Rationalisation	Sewerage	Infrastructure Planning (Project Delivery)	Approval of detailed project plan and commencement of concept design
4.1.3.39 CW	Sewer Critical infrastructure Site Security Upgrades	Sewerage	Infrastructure Operations	Deliver project to approved project plan
4.1.3.42 CW	Centrifuge upgrade and Process Optimisation at PM WwTP Allocated Amount 2021-22 \$100,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Approval of detailed project plan and commencement of investigations
4.1.3.44 CW	Installation of new Microwave Telemetry Links - Jolly Nose to BH WwTP Allocated Amount 2021-22 \$60,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Deliver project to approved project plan
4.1.3.45 CW	Installation of new Microwave Telemetry Links - Jolly Nose to CH WwTP Allocated Amount 2021-22 \$60,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Completion of programmed works

	4.1.3.46 CW	Installation of new Microwave Telemetry Links - Rosewood/Bago to Wauchope WwTP Allocated Amount 2021-22 \$60,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Completion of programmed works
4.1.3.47		Integrated Water Cycle Management Strategy Delivery (Multi-Year Project) Allocated Amount 2021-22 \$225,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Council adoption of the Integrated Water Cycle Management Strategy
	4.1.3.48 CW	Kew Sewerage Treatment Plant (STP) Upgrade, Complete project planning and commence construction (Multi- Year project) Allocated Amount 2021-22 \$500,000	Sewerage	Infrastructure Planning, (Project Delivery)	Completion of concept designs and construction contract award
	4.1.3.49 CW	Preconstruction works for upgrade of Port Macquarie Sewerage Pumping Station #64	Sewerage	Infrastructure Planning, (Project Delivery)	Deliver project to approved project plan
	4.1.3.50 CW	Replacement of 2nd Effluent Pump at Port WwTP Allocated Amount 2021-22 \$150,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Completion of programmed works
4.1.3.51		Undertake asset revaluations for sewer network	Sewerage	Infrastructure Planning	Completion of revaluation to the required standard for external audit
	4.1.3.52 CW	Thrumster STP Area 13 Augmentation Pre-Construction	Sewerage	Infrastructure Planning	Deliver project to approved project plan

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, ,	Delivery Program 2017- 2022 Objective 4.1.4 Develop and implement annual maintenance and preventative works program for sewerage assets.								
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures				
	4.1.4.1 CW	Programmed replacement of Sewer Pumps and Electrical switchboards at Sewage Pump Stations	Sewerage	Infrastructure Planning (Infrastructure Operations)	Completion of programmed works				
		Sewer Pump Replacement Program							
		Allocated Amount 2021-22 \$150,000							
		Annual Switchboard Replacement							
		Allocated Amount 2021-22 \$150,000							
		Lid Replacement Program							
		Allocated Amount 2021-22 \$250,000							
	4.1.4.4 CW	Carry out programmed replacement of Sewer Treatment Plant (STP) electrical and mechanical assets	Sewerage	Infrastructure Planning (Infrastructure Operations)	Completion of programmed works				
		Allocated Amount 2021-22 \$200,000							
		Annual Electrical STP Asset Replacement							
		Annual Mechanical STP Asset Replacement							

	4.1.4.5 CW	Conduct sewer assets replacement and maintenance programs for Sewer Rehabilitation and Relining Works Annual Sewer Rehabilitation Allocated Amount 2021-22 \$350,000 Annual Sewer Relining Works Allocated Amount 2021-22 \$650,000	Sewerage	Infrastructure Planning (Infrastructure Operations)	Completion of programmed works
	4.1.4.7 CW	Installation of Dedicated Generators for backup power supply to Sewerage Pumping Stations CH SPS #9 Allocated Amount 2021-22 \$80,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Completion of installation
	4.1.4.8 CW	Installation of Dedicated Generators for backup power supply to Sewerage Pumping Stations PM SPS#64 Allocated Amount 2021-22 \$120,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Completion of installation
	4.1.4.9 CW	Develop Urban Sewer Management Plans Allocated Amount 2021-22 \$240,000	Sewerage	Infrastructure Planning, (Infrastructure Operations)	Completion of programmed works
, ,	m 2017- 2022 Objec ards planning, inves	ctive tigation, design, construction of stormwater ass	sets		
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	4.1.5.06 CW	Continue design and construction for Black Swan Terrace - Stormwater detention facility (multi-year project) Allocated Amount 2021-22 \$158,186	Stormwater and Drainage	Infrastructure Planning	Completion of detailed designs.
	4.1.5.12 CW	Stormwater Remediation - 35 Hart Street Allocated Amount 2021-22 \$40,000	Stormwater and Drainage	Infrastructure Planning	Completion of detailed designs.
	4.1.5.14 CW	Undertaken Panorama Drive/Binbilla Drive stormwater remediation works Allocated Amount 2021-22 \$420,000	Stormwater and Drainage	Infrastructure Planning	Completion of construction works

4.1.6 Develop a	and implement annu	al maintenance and renewal programs for storn	nwater assets		
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.1.6.2		Undertake annual canal maintenance for Settlement Shores and Broadwater canals as required	Stormwater and Drainage	Infrastructure Planning, (Infrastructure Operations)	Undertake canal maintenance works in accordance with the rolling priority program and risk management processes
4.1.6.2		Undertake maintenance for Settlement Shores Canals	Stormwater and Drainage	Infrastructure Planning, (Infrastructure Operations)	Undertake canal maintenance works in accordance with the rolling priority program and risk management processes
4.1.6.2		Undertake canal maintenance for Broadwater Canals	Stormwater and Drainage	Infrastructure Planning, (Infrastructure Operations)	Undertake canal maintenance works in accordance with the rolling priority program and risk management processes
	4.1.6.7 CW	Carry out the Stormwater Renewal Program Allocated Amount 2021-22 \$250,000	Stormwater and Drainage	Infrastructure Planning, (Infrastructure Operations)	Completion of listed renewal works
4.1.6.18		Carry out stormwater maintenance program including inspections, monitoring and repair works.	Stormwater and Drainage	Infrastructure Planning, (Infrastructure Operations	In accordance with adopted programs and reactive maintenance requirements based on risk
4.1.6.19		Carry out storm water engineering investigations in response to identified issues from both reactive and proactive inspections	Stormwater and Drainage	Infrastructure Planning,	Engineering Investigations completed for all complex issues raised.
4.1.6.20		Stormwater Asset Management Condition Rating of stormwater assets via CCTV inspections, including of newly constructed works	Stormwater and Drainage	Infrastructure Planning, (Infrastructure Operations	In accordance with adopted programs and proactive maintenance requirements based on risk
4.1.6.21		Develop Urban Stormwater Catchment Management Plans for each of the 62 sub- catchments	Stormwater and Drainage	Infrastructure Planning,	Approval of detailed project plan and commencement of investigations

	4.1.6.22 CW	North Haven Flood mitigation Works - investigation and concept design of remedial works Allocated Amount 2021-22 \$55,000	Stormwater and Drainage	Infrastructure Planning (Project Delivery)	Approval of detailed project plan and commencement of investigations
, ,	n 2017- 2022 Object nd implement effecti	tive ve waste management strategies			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.1.7.6		Ensure effective recycling and recovery of domestic waste through Material Recovery Facility (MRF)	Waste Management	Commercial Business Units	 Recovery of domestic waste through Material Recovery Facility in accordance with regulatory requirements Finalise options analysis and commence planning for new Material Recovery Facility by 30 June 2022
4.1.7.7		Ensure effective recycling and recovery of organic waste through Organic Resource Recovery Facility (ORRF)	Waste Management	Commercial Business Units	 Recovery of domestic waste through Organic Resource Recovery Facility (ORRF) in accordance with regulatory requirements Capture and report carbon credit data associated with organic waste diversion from landfill
	4.1.7.9 CW	Deliver efficient waste and recycling services at all Waste Transfer Stations - New transfer bins x 4 Allocated Amount 2021-22 \$100,000	Waste Management	Commercial Business Units	Operation of all Waste Transfer Stations undertaken in accordance with regulatory requirements
4.1.7.13		Address illegal dumping and provide clean-up programs	Waste Management	Commercial Business Units, Environment and Regulatory Services	Illegal dumping and clean-up programs delivered by 30 June 2022

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	4.1.7.26 CW	Caimcross Waste Management Facility (WMF) Expansion / Improvements - (multi- year project)	Waste Management	Commercial Business Units Project Delivery	Finalise construction of new landfill cell by 30 June 2022
		Finalise construction of new landfill cell (1A, 1C, 1E) in accordance with Development Consent		,,	
		Allocated Amount 2021-22 \$3,165,000			
4.1.7.27		Waste Strategy - Develop new Waste Strategy following NSW 20yr Waste Strategy	Waste Management	Commercial Business Units	Development of new Waste Strategy commenced by 30 June 2022
4.1.7.28		Deliver efficient waste collection, disposal and resource recovery services, including domestic, public place, commercial, and construction and demolition waste	Waste Management	Commercial Business Units	Waste collection, disposal and resource recovery services delivered in accordance with industry best practice
4.1.7.29		Ensure effective operation of Cairncross waste management facility including effective disposal of waste to landfill	Waste Management	Commercial Business Units	1. Operation of Cairncross waste management facility undertaken in accordance with regulatory requirements
					2. Capture the tonnage of waste to landfill for environmental reporting
4.1.7.30		Undertake Waste Education and Marketing campaigns	Waste Management	Commercial Business Units	Waste Education and Marketing campaigns delivered according to approved programs
				Community	
4.1.7.31		Operate and maintain waste facilities in accordance with regulatory requirements and industry best practice	Waste Management	Commercial Business Units	Undertake landfill gas monitoring in accordance with regulatory requirements
	4.1.7.34 CW	Kingfisher Waste Transfer Station Upgrade - Kingfisher Road Rehabilitation	Waste Management	Commercial Business Units	Finalise construction of Kingfisher Road Rehabilitation by 30 June 2022
		Allocated Amount 2021-22 \$650,000		Project Delivery	

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4.2 Aim to minimise the impact of natural events and climate change, for example, floods, bushfires and coastal erosion

Delivery Program 2017- 2022 Objective

4.2.1 Develop and implement Coastal, Estuary, Floodplain, and bushfire management plans.

Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.2.1.01		Deliver the annual bushfire risk mitigation program for PMHC	Natural Resource Management	Environment and Regulatory Services	 90% of program delivered RFFF estimates are submitted to the RFS for the following financial year grant application
4.2.1.02		Implement actions from the Flying Fox Camp Management Plan for Kooloonbung Creek	Natural Resource Management	Environment and Regulatory Services	 National flying-fox census count x 4 (quarterly) Community meetings x 2 annually Delivery of on-ground management actions (including subsidy program and operation of sprinkler system)
4.2.1.05		Continue to ensure development compliance to achieve sound environmental outcomes - Vegetation Management Plans (VMP's), Koala Plans of Management (KPoM) developed, registered, implemented and monitored	Natural Resource Management	Environment and Regulatory Services	Register new plans and continue to undertake audits of VMP's and KPoM's to ensure on-ground works are completed.
4.2.1.06		Development of environmental program of works (registration, implementation, monitoring) to ensure compliance to Airport bio diversity certification	Natural Resource Management	Environment and Regulatory Services	 Undertake scoping, briefing preparation, and requisition of appropriate services. Ongoing monitoring, generate reports, manage budgets for payment of contracts and write final report pertinent to on-ground environmental management actions.
4.2.1.07		Provide subject matter expert advice on DA and landuse application referrals	Natural Resource Management	Environment and Regulatory Services	Referrals to DA applications is undertaken within 21 days
4.2.1.09		Continue to develop a Flood Study for the North Brother Local Catchments - facilitation of stormwater remediation	Stormwater and Drainage	Infrastructure Planning	Development of a North Brother Flood Study underway to facilitate future stormwater remediation

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	4.2.1.11 CW	Commence construction of new stormwater system at Illaroo Road Allocated Amount 2021-22 \$867,124	Natural Resource Management	Environment and Regulatory Services	Construction of Illaroo Road stormwater system has commenced
4.2.1.14		Coast and estuary management committee	Natural Resource Management	Environment and Regulatory Services	Four committee meetings held per year
4.2.1.16		Development of a Coastal Management Program for Lake Cathie - Tidal Improvement - Lake Innes/Lake Cathie Estuarine	Natural Resource Management	Environment and Regulatory Services	Continue to undertake studies and engagement for Coastal Management Program stage 2 specific to Lake Cathie including funding for hydrodynamic model associated with Kenwood Drive Bridge, continued discussion regarding Illaroo revetment wall and acid sulphate studies associated with holistic estuary management.
4.2.1.17		Develop the annual bushfire risk mitigation program for Council lands prepared in accordance with the Bush Fire Risk Management Plan.	Natural Resource Management	Environment and Regulatory Services	The annual Bushfire Risk Management Program is adopted by 30 September 2021
4.2.1.18		Flood mapping - Update LEP Flood Maps to incorporate new knowledge and latest flood model results across LGA.	Land Use Planning	Environment and Regulatory Services	Complete updates to LEP Flood Mapping by June 2022.
	4.2.1.19 CW	Installation of in-line monitoring instruments at Koree Island River intake Allocated Amount 2021-22 \$100,000	Water Supply	Infrastructure Planning, (Infrastructure Operations)	Deliver project according to approved project plan
4.2.1.20		Commence development of the Coastal Management Plan - CMP Stage 2 - Hastings River Drive	Natural Resource Management	Environment and Regulatory Services	Begin studies and engagement for Coastal Management Program stage 2 specific to Hastings River area

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	n 2017- 2022 Object trategies to mitigate				
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.2.2.1		Undertake development of a Sustainability and Climate Change Strategy	Sustainability	Environment and Regulatory Services	Climate Change Response Strategy & Action Plan developed and adopted
4.2.2.2		Implement sustainability and climate change strategy actions	Sustainability	Environment and Regulatory Services	Commence action in accordance with Climate Change Response Strategy
-	egic Plan Strategy velopment that is co	mpatible with the natural and built environme	nt		
	n 2017- 2022 Object transparent and effi	ive cient development assessment in accordance	with relevant legislation		
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.3.1.1		Ensure development assessment, building certification and subdivision certification activities are completed efficiently and in accordance with legislation	Development Assessment	Development Assessment	Capture the number of applications and processing times, together with a commentary on development trends and report to Council quarterly
4.3.1.2		Ensure the Development Assessment Panel operates in accordance with their charter and all applications are accurately determined	Development Assessment	Development Assessment	Have no successful legal appeals relating to process errors
-	egic Plan Strategy grated transport syst	ems that helps people get around and link our	communities		
,	n 2017- 2022 Object stigate, design and co	ive onstruct transport assets which address pedes	strians, cyclist and vehicula	ar needs to cater for the fu	iture growth of the region.
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.4.1.02		Regional Integrated Transport Strategy	Strategic Land Use Planning	Strategy	Production of a draft RITS for exhibition before 30 June 2022
	4.4.1.03 CW	Footpath, Cycleway and Pedestrian works	Roads, Bridges and Transport	Infrastructure Planning, (Project Delivery)	Complete construction of listed works

4.4.1.32		Continue corridor planning - Kendall Road – Ocean Drive – Hastings River Drive regional road corridor (MR538 / MR600) - (multi-year project)	Roads, Bridges and Transport	Infrastructure Planning	Council adoption of the completed road corridor plan
	4.4.1.34 CW	Kew Main Street Upgrade Construction Allocated Amount 2021-22 \$1,211,316	Roads, Bridges and Transport	Infrastructure Planning, (Project Delivery)	Completion of the Kew Main Street Upgrade
	4.4.1.37 CW	Local Roads Proactive Renewal Program Allocated Amount 2021-22 \$365,000	Roads, Bridges and Transport	Infrastructure Planning (Infrastructure Operations)	Completion of listed road renewals and resurfacing works
4.4.1.48		Development Contribution Plan (Roads) Review and Update	Roads, Bridges and Transport	Infrastructure Planning	Deliver project according to approved project plan
	4.4.1.51 CW	Installation of new bus shelters as per the Community Passenger Transport Infrastructure Grant Scheme (CPTIGS) Program Allocated Amount 2021-22 \$176,000	Roads, Bridges and Transport	Infrastructure Planning (Infrastructure Operations)	Completion of listed bus shelter installations
4.4.1.52		AUS-SPEC Review - Undertake a comprehensive review of Councils full suite of design and construction specifications (multi-year project)	Roads, Bridges and Transport	Infrastructure Planning	Council adoption of the updated AUS_SPEC specifications
	4.4.1.55 CW	Beach to Beach - shared path project at Camden Haven – Detailed design, environmental approvals and commencement of construction Allocated Amount 2021-22 \$367,245	Roads, Bridges and Transport	Infrastructure Planning, (Project Delivery)	Completion of the Detailed design and environmental approvals.
	4.4.1.63 CW	Pembrooke Road - Stoney Creek Bridge Upgrade - Detailed Design Allocated Amount 2021-22 \$350,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of detailed design
	4.4.1.66 CW	Kindee Bridge Upgrade - Detailed Design Allocated Amount 2021-22 \$30,000	Roads, Bridges and Transport	Infrastructure Planning, (Project Delivery)	Detailed project plan approval and commencement of the detailed design

4.4.1.67		Transport Network Improvement Planning Project - Options Assessment and Strategic Business Case (Including Review of Orbital Road Options) - (Multi Year Project)	Roads, Bridges and Transport	Infrastructure Planning	Completion of a Transport Network Plan and Strategic Business Case for the highest priority improvements
	4.4.1.80 CW	Boundary Street Upgrade - Detailed Design and Environmental Approvals Allocated Amount 2021-22 \$429,000	Roads, Bridges and Transport	Infrastructure Planning,	Completion of the detailed design and environmental approvals.
	4.4.1.83 CW	Gordon Street Upgrade - Road Resurfacing, Stormwater flood mitigation and water main renewal works (multi-year project) Allocated Amount 2021-22 \$6,380,152	Roads, Bridges and Transport	Infrastructure Planning (Project Delivery)	Completion of works
	4.4.1.88 CW	Lorne Road Sealing - Commence detailed design and approvals Allocated Amount 2021-22 \$250,000	Roads, Bridges and Transport	Infrastructure Planning (Project Delivery)	Approval of the detailed project plan and commencement of the detailed design.
4.4.1.89		Undertake a detailed traffic analysis of the Sancrox/Thrumster/Fernbank growth area to identify high priority works	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the investigations.
	4.4.1.96 CW	Design and Construction of the Pappinbarra Bridge Replacement Allocated Amount 2021-22 \$2,808,140	Roads, Bridges and Transport	Infrastructure Planning	Completion of construction
4.4.1.97		Review of Car Parking Strategy	Roads, Bridges and Transport	Infrastructure Planning	Completion of draft Car Parking Action Plan and Parking Policy for public exhibition
	4.4.1.98 CW	The Hatch Rd - Reseal Sections - Investigation and design for sealing 3kms of Rd	Roads, Bridges and Transport	Infrastructure Planning, (Project Delivery)	Completion of construction
		Allocated Amount 2021-22 \$1,359,290			

4.4.1.113		Laurieton Town Centre Master Plan COVID PROJECTS Roads and Community Infrastructure Program	Roads, Bridges and Transport	Strategy	Completion of draft Master Plan for public exhibition
	4.4.1.119 CW	Commence Construction of the Lake Road / Jindalee Road / Fernhill Road Intersection upgrade (Multi Year) Allocated Amount 2021-22 \$250,000	Roads, Bridges and Transport	Infrastructure Planning, (Project Delivery)	Approval of Detailed project plan and completion of construction tender documents
	4.4.1.120 CW	Commence detailed design and approvals for Lake Road Duplication (Ocean Drive to Chestnut Road) (Multi- year) Allocated Amount 2021-22 \$200,000	Roads, Bridges and Transport	Roads, Bridges and Transport	Approval of Detailed project plan and commencement of detailed design
	4.4.1.123 CW	Maria River Road Allocated Amount 2021-22 \$300,000	Roads, Bridges and Transport	Infrastructure Planning,	Construction contract award
4.4.1.124		Ocean Drive (South of Port Macquarie) Soil Remediation	Roads, Bridges and Transport	Infrastructure Planning	Completion of works
	4.4.1.125 CW	Pembrooke Road Beechwood Rd & Stoney Creek Rd Allocated Amount 2021-22 \$2,980,000	Roads, Bridges and Transport	Infrastructure Planning,	Completion of works
4.4.1.127		Undertake sealed road network condition survey	Roads, Bridges and Transport	Infrastructure Planning	Completion of condition survey

	4.4.2.1 CW	Undertake road resurfacing and rehabilitation works throughout the local government area in accordance with the rolling priority program and Pavement Management System	Roads, Bridges and Transport	Infrastructure Planning (Infrastructure Operations)	Completion of listed road renewal and resurfacing works
		Continuation of High Traffic Road Resurfacing Allocated Amount 2021-22 \$994,245			
		Continuation of Pavement Rejuvenation Treatments Allocated Amount 2021-22 \$248,560			
		Road Resealing Works Allocated Amount 2021-22 \$2,099,395			
4.4.2.2		Carry out annual unsealed road maintenance program including gravel resheeting, grading, and rural roadside vegetation clearing	Roads, Bridges and Transport	Infrastructure Planning (Infrastructure Operations)	In accordance with adopted programs and reactive maintenance requirements based on risk
4.4.2.3		Carry out annual sealed road maintenance program including resurfacing, jetpatching, heavy patching and installation of roadside furnishings.	Roads, Bridges and Transport	Infrastructure Planning (Infrastructure Operations)	In accordance with adopted programs and reactive maintenance requirements based on risk
4.4.2.4		Undertake bridges, stormwater and culvert maintenance program including inspections, monitoring and repair works.	Roads, Bridges and Transport	Infrastructure Planning (Infrastructure Operations)	In accordance with adopted programs and reactive maintenance requirements based on risk
4.4.2.5		Carry out reactive maintenance to Koala Food Trees and Koala Fencing on Link Rd (Ocean Dr)	Roads, Bridges and Transport	Infrastructure Planning (Environmental Services)	In accordance with adopted programs and reactive maintenance requirements based on risk
4.4.2.6		Bridgeworks and Road Rehabilitation Program - Undertake regular bridge and geotechnical road pavement tests to inform and develop program	Roads, Bridges and Transport	Infrastructure Planning	Completion of required testing and assessments
	4.4.2.7 CW	Crowe Roods Bridge, Bellangry - Upgrade Allocated Amount 2021-22 \$102,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design

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4.4.2.8 CW	Bridge 178, Mundays Lane - Upgrade Allocated Amount 2021-22 \$66,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.9 CW	Bridge 179, Mundays Lane - Upgrade Allocated Amount 2021-22 \$66,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.13 CW	Bridge 181, Mundays Lane - Upgrade Allocated Amount 2021-22 \$76,500	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.14 CW	Donkins Flat Bridge, Comboyne - Upgrade Allocated Amount 2021-22 \$540,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.15 CW	Old School Road Bridge, Herons Creek - Upgrade Allocated Amount 2021-22 \$570,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.16 CW	Cowal Creek Bridge, Bellangry - Upgrade Allocated Amount 2021-22 \$51,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.17 CW	Culvert - Thone River Rd, Byabarra Allocated Amount 2021-22 \$51,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.18 CW	Culvert - Farrawells Road, Telegraph Point Allocated Amount 2021-22 \$55,500	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.19 CW	Tower Rd Bridge, Pembrooke - Upgrade Allocated Amount 2021-22 \$141,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.20 CW	Bottlebrush No 1 Bridge Upgrade Allocated Amount 2021-22 \$439,500	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.21 CW	Cutty Creek Bridge, Herons Creek Upgrade Allocated Amount 2021-22 \$115,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.22 CW	Joes Bridge Upgrade Allocated Amount 2021-22 \$115,500	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.23 CW	Langdons Bridge (Bottlebrush No 2) Upgrade	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design

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	Allocated Amount 2021-22 \$256,500			
4.4.2.24 CW	Bril Bril Bridge Upgrade Allocated Amount 2021-22 \$500,000	Roads, Bridges and Transport	Infrastructure Planning	Commence the rehabilitation of Bril Bril Bridge
4.4.2.25 CW	Little Mortons Creek Bridge Upgrade Allocated Amount 2021-22 \$390,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.26 CW	King Creek Bridge Upgrade Allocated Amount 2021-22 \$543,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of the detailed project plan and commencement of the detailed design
4.4.2.27 CW	Beechwood Pedestrian Upgrades Allocated Amount 2021-22 \$480,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of Pedestrian Upgrade
4.4.2.28 CW	New Footpath on the North Shore Allocated Amount 2021-22 \$960,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of footpath at Northshore
4.4.2.29 CW	Kerb and Gutter Installation - Wesley Avenue, Petit St, Hart St and Anderson St Allocated Amount 2021-22 \$550,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of Kerb and Gutter Installation
4.4.2.30 CW	Bago Rd final seal Allocated Amount 2021-22 \$200,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of works
4.4.2.31 CW	Resurface Council Carparks - Cameron St, Wauchope & Bridge St, Port Macquarie Allocated Amount 2021-22 \$150,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of Resurfacing
4.4.2.10 CW	Timber Bridges – Renewals & Repairs for Logans Crossing Bridge Allocated Amount 2021-22 \$100,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of detailed project plan and commencement of detailed design
4.4.2.11 CW	Ocean Drive Preliminaries Allocated Amount 2021-22 \$250,000	Roads, Bridges and Transport	Infrastructure Planning	Approval of detailed project plan and construction contract documentation
4.4.2.12 CW	Kindee Bridge - Immediate Structural Repairs Allocated Amount 2021-22 \$530,000	Roads, Bridges and Transport	Infrastructure Planning	Completion of structural repairs

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Delivery Program	2017- 2022 Object	ive			
4.4.3 Develop an	d implement traffic	and road safety programs			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.4.3.1		Develop a Road Safety Action Plan and undertake associated safety education and awareness programs identified in the plan	Roads, Bridges and Transport	Infrastructure Planning	Undertake road safety educational program
4.4.3.2		Install and maintain street lights in accordance with identified priorities	Roads, Bridges and Transport	Infrastructure Planning	Deliver street lighting program according to approved schedule and prioritisation
Community Strate	egic Plan Strategy				
4.5 Plan for integ	rated and connecte	d communities across the Port Macquarie-Has	tings area		
Delivery Program	2017-2022 Objecti	ve			
4.5.1 Carry out st	trategic planning to	manage population growth and provide for co-	ordinated urban developn	nent	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.5.1.7		Review Local Environmental Plan (LEP) and Development Control Plan (DCP) provisions to promote development of the Yippin Creek Urban Release Area (UGMS Action 4)	Strategic Land Use Planning	Strategy	Report to Council regarding draft planning outcomes to Council by 30 June 2022
4.5.1.8		Progress Council led precinct planning for the proposed Health and Education Precinct (UGMS Action 13)	Strategic Land Use Planning	Strategy	Continue support studies for a planning proposal in consultation with stakeholders by 30 June 2022
4.5.1.10		Review LEP and DCP provisions to promote appropriate housing choice options (UGMS Action 1)	Strategic Land Use Planning	Development Assessment	 Complete Local Housing Strategy Report to Council by December 2021 on local housing priorities and timeframes for delivery of action
4.5.1.11		Investigate the capacity of land at the intersection of Ocean Drive with Houston Mitchell Drive for light industrial use and at the intersection of Ocean Drive with Bonny View Drive for light industrial development or for use as a school (UGMS Action 17)	Strategic Land Use Planning	Development Assessment	Final report to Council re Local Environmental Plan (LEP) and Development Control Plan (DCP) amendments by 30 June 2022

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4.5.1.15		Port Macquarie Civic Precinct - Commence investigations for strategic landuse planning of Council owned property	Strategic Land Use Planning	Strategy	Planning investigations commenced for Council owned property in the Port Macquarie Civic Precinct
4.5.1.17		Fernbank Creek/Sancrox - Support Studies	Strategic Land Use Planning	Strategy	NSW DPIE endorsement of Structure Plan by December 2021
4.5.1.18		Sustainability Strategy	Strategic Land Use Planning	Strategy	Production of a draft Sustainability Strategy by 30 June 2022.
4.5.1.19		Conduct a strategic review of development contributions plans	Strategic Land Use Planning	Strategy	Strategic Review the DSP and Contributions Plans and recommendations by 30 June 2022.
4.5.1.20		Finalise and implement the Port Macquarie Regional City Action Plan in collaboration with DPIE	Strategic Land Use Planning	Strategy	Implement and advocate for actions by 30 June 2022
Delivery Program	2017- 2022 Object	ive			
4.5.2 Plan for inf	frastructure that sup	ports population growth			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.5.2.2		Continue the review of the Major Roads Contributions Plan for completion in 2020 - 2021 (UGMS Action 23)	Strategic Land Use Planning	Strategy	Complete review of draft works program for review of the Major Roads Contributions Plan by 30 June 2022

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	<mark>d protect natural area</mark> m 2017- 2022 Object				
, ,		e of programs for the environmental manager	nent of lands within the loc	al government area	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.6.1.1		Undertake strategic biosecurity (weed management) program to restore and conserve the natural environment of the Mid North Coast	Natural Resource Management	Environment and Regulatory Services	 600 high risk species inspections undertaken 2,000km of high risk/priority pathways inspected 210ha of control for high risk species 200km of high risk pathways treated for high risk species Extension events conducted on weed biosecurity (field days, workshops, presentations) Develop Local Weed Management Pla
4.6.1.5		Implement the Ecological Restoration program and collaborate with various	Natural Resource Management	Environment and Regulatory Services	in accordance with the North Coast Regional Strategic Weed Managemer Plan framework 1. Undertake management of Council reserves that are deemed of significa
		community groups (e.g. Landcare)			 ecological value following guidance a direction from the Ecological Restoration report 2. Develop a roles and responsibilities charter for Landcare and Bushcare groups in PMHC that work on Council land

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4.6.1.6	Inform and educate residents, industry and community groups about Council's tree management requirements within the Port	Parks and Reserves	Recreation, Property and Buildings	1. Provide advice in accordance with service standards and industry best practice
	Macquarie-Hastings 2013 Development Control Plan (DCP)			2. Develop and implement educational material and delivery program
				3. Undertake Investigations in relation to all reported illegal tree works
4.6.1.15	Environmental Compliance Auditing of Part 5 Review of Environmental Factors	Natural Resource Management	Environment and Regulatory Services	1. Toolbox talks have been held with relevant teams to relay information regarding environmental management procedures
				2. Undertake 10 site inspections throughout the year to audit environmental compliance
4.6.1.16	Commence the Natural Resources asset and maintenance register	Natural Resource Management	Environment and Regulatory Services	Continue to carry out priority maintenance actions within the budget allocated
4.6.1.17	Review of Environmental Factors (REF) Training and process/capability improvement	Natural Resource Management	Environment and Regulatory Services	1. Provide internal staff with 4 environmental training opportunities during the year
				2. Create new training and process documents where relevant (Environmental induction, environmental awareness training, Environmental Work Method Statement (EWMS), procedures, environmental inspection checklist)
4.6.1.18	Partridge Creek Acid Sulfate Soils Wetland Management	Natural Resource Management	Environment and Regulatory Services	6 site inspections of weir conducted each year
4.6.1.20	Wild deer management in PMHC	Natural Resource Management	Environment and Regulatory Services	Implement PMHC specific management actions generated from the Hastings Wild Deer Program.
4.6.1.21	Tip Gravel Road Environmental Protect Biodiversity Conservation (EPBC) Act Koala Management Plan	Natural Resource Management	Environment and Regulatory Services	Undertake actions as required which include camera monitoring, fence inspections and submission of compliance report

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4.6.1.24		Undertake feasibility study for	Natural Resource	Environment and	Feasibility study report completed
		engineering solutions and initial concept design work to combat koala road strike	Management	Regulatory Services	
4.6.1.25		Undertake development of workflow procedure for identification and reporting of unauthorised vegetation clearing	Natural Resource Management	Environment and Regulatory Services	Development of workflow procedure
4.6.1.26		Undertake development of Illegal Tracks identification and prioritization plan	Natural Resource Management	Environment and Regulatory Services	lllegal tracks plan created
4.6.1.27		Review the Koala Population Recovery Strategy 2018	Natural Resource Management	Environment and Regulatory Services	Review of strategy completed with opportunities for improvements identified
4.6.1.28		Contract management of invasive weed and ecological restoration on Council owned land as per Vegetation Management Plans	Natural Resource Management	Environment and Regulatory Services	New contracts are scoped, briefs are prepared and services are secured for work for 2021 - 2022 operational year
4.6.1.29		Investigation of stewardship sites establishment on Council owned lands.	Natural Resource Management	Environment and Regulatory Services	1. Investigation of feasibility of bushland reserves becoming stewardship sites
					2. Investigation for new lands for Development Control Plan (DCP) offsets
	n 2017-2022 Object enewable energy out	ive tcomes within Council			
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
	4.7.1.11 CW	Install solar energy PV systems at Port Macquarie (Kingfisher Road), Wauchope and Kew waste transfer stations Allocated Amount 2021-22 \$395,000	Waste Management	Commercial Business Units (Project Delivery)	Finalise construction of solar energy PV systems by 30 June 2022
	tegic Plan Strategy areness of issues af	fecting our environment, including the preserv	ation of flora and fauna		·
	n 2017-2022 Object Council operations o	ive comply with environmental standards and reg	ulations		
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.8.1.1		Operate and maintain water treatment plants in accordance with adopted maintenance programs and scheme requirements	Water Supply	Infrastructure Planning (Infrastructure Operations)	Monitor plants continuously with plant breakdowns attended to within 24 hours

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4.8.1.2		Operate the water supply network to ensure public health and safety	Water Supply	Infrastructure Planning (Infrastructure Operations)	Ensure any public health and safety issues in relation to water supply are responded to in line with service standards
4.8.1.3		Maintain and operate storage dams in accordance with Australian National Committee On Large Dams (ANCOLD) guidelines	Water Supply	Infrastructure Planning (Infrastructure Operations)	Ensure any issues in relation to the operation and maintenance of storage dams are handled in line with ANCOLD guidelines
4.8.1.4		Operate and maintain sewerage treatment plants in accordance with environmental licenses, adopted maintenance programs and scheme requirements	Sewerage	Infrastructure Planning (Infrastructure Operations)	Monitor plants continuously with plant breakdowns attended to within 24 hours
4.8.1.5		Operate the sewerage network to ensure service delivery meets public health and safety requirements	Sewerage	Infrastructure Planning (Infrastructure Operations)	Ensure any public health and safety issues in relation to sewerage network are responded to in line with service standards
, ₁	n 2017-2022 Objectiv ommunity awareness	ve and enable access to the natural environmen	t	-	
Operational Action	Capital Works Project	Description	Council Service	Lead Responsibility	Performance Measures
4.8.2.1		Deliver bushfire preparedness and planning programs to the community	Natural Resource Management	Environment and Regulatory Services	1. Attendance at BFMC and bushfire risk meetings - 90 % participation
					2. Attendance at relevant industry training

Delivery Program 2017-202 4.8.3 Promote Biodiversity F	·			
4.8.3.1	Continue implementation of identified actions from the adopted Biodiversity Strategy	Natural Resource Management	Environment and Regulatory Services	Implementation of actions from the following plans (as outlined in specific OP plan goals): - Koala Recovery Strategy - Flying-fox Camp Management plan - Ecological restoration report
				- Mid-North Coast Feral Deer Strategy
4.8.3.2	PMHC in partnership with JO, KPS, and DPIE support the ongoing work of the Koala Recovery Partnership Program	Natural Resource Management	Environment and Regulatory Services	Operational objectives of the Koala Recovery Partnership Program are met



Contact

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Item 09.10 Attachment 2

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Submissions response Summary Draft Operational Plan 2021/2022

Council Service:	
Sports and Recreation	n
Submitter's name	Submitter's issue
Karen Stasukaitis 4886/2021	We would like to make a suggestion to have cement put in at the "Dog Bath" At Lighthouse beach. The faucet where the camels come down. As currently either to rinse feet or dogs, you are standing in mud.
Response	
the community. We th	reviewing potential future works in this area to improve beach access for nank you for your suggestion and will consider this dependant on wing the access improvement works.
Council Service:	
Sports and Recreation	n
Submitter's name	Submitter's issue
Julie Daniell 2971/2021	1. Our group shared a concern that a lack of council mowing of grassed areas for a prolonged period all through the North Haven area.
	2. One other matter I'd like to mention is the lack of Doggy Bag poop stations in North Haven
Response	
to manage when we e warm conditions resu parks and gardens ed with storms in 2020 a program. The Bunny's through the growing s the area. Staff will con community complaint rostering over the Chr operational team on o	and reserves across our network is a challenge and an even harder one experience a high growth season like we did in 2020/21. The wet and lited in grass growing faster than we could get to at times. In addition, juipment and staff had to be diverted to emergency works associated and floods this year which resulted in interruptions to our routine mowing corner reserve at North Haven is on a 4-6 week mowing program leason in accordance with the Council adopted maintenance schedule for ntinue to review mowing schedules against weather trends and s to ensure we keep our public spaces looking their best. In regards to istmas period the service will always have a minimum of 50% of the duty supplemented by contract services as required. With the wet weathe 2020 there was some mowing delay related to machinery not being able
community with off le responsible pet owne and bins within dog p responsibility to comp areas, this includes in	ties under the Companion Animals Act (CAA) 1998 is to provide the ash exercise areas for dogs, regulatory services and education on rship. We offer a courtesy service of providing dog waste bag dispensers arks and other designated Off Leash areas only. Dog owners have a bly with the CAA, and should be considerate of other users in any public nmediately cleaning up and disposing of dog droppings. To this end,

Council is not in a position to install new waste dispensers in public areas where dogs are permitted On Leash, such as at North Haven River Bath. Rangers will continue to educate the

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community on responsible pet ownership, respond to complaints and issue fines where appropriate when an owner does not pick up their dog's droppings.

Council Service:			
Sports and Recreation			
Submitter's name	Submitter's issue		
Brian Seymour 2969/2021	My wife & I are both retired & enjoy walking especially along the river near the town green which we think is one of the best areas in the east coastHOWEVER there are seats BUT NO SHADE .		
	We like to think we could have some picnic tables with covers on the town green & along the sea wall seats with shade.		

Response

Thank you for your submission. We note your comments in relation to your concern around the lack of shade options along the Port Macquarie Breakwall and Town Green. In recent years, Council and the Town Centre Master Plan Committee have invested significantly in the upgrade of sections of the Town Green. As part of the Town Green East upgrade new tree plantings were undertaken with the objective of providing more shade along this area of the foreshore once they are established. More recently, we have completed an upgrade to the Town Green West section where new picnic tables have been installed in close proximity to the existing trees to maximise shade opportunities in this area. Further, concept designs were presented by Council to the state government in 2016 which recommended improvements to the Port Macquarie Breakwall. The concept design included the addition of new shaded areas along the Breakwall pathway which is a state government asset. The Minister for Transport and Roads recently made a funding announcement which will see the state government undertake public consultation in mid-2021 on the proposed design. Refer to further details here: https://www.portnews.com.au/story/7029365/5-million-green-light-for-port-macquarie-breakwall-upgrade

Council Service:

Sports and Recreation		
Submitter's name	Submitter's issue	
Janette Hyde 2968/2021	Many years ago, Port Macquarie consistently won awards for being a Tidy Town. Sadly, this is not the case now.	
	The Historical Cemetery display at the end of Horton Street used to be admired by all. Now the display of green is not even maintained and the surrounding weeds in the lawn framing the Cousin Its, are so prolific.	
	Recently a new entrance sign was installed at the Oxley/Pacific Highway. It is surrounded by weeds.	

Response

Thank you for your submission regarding the maintenance service levels across our LGA. Our Open Spaces team of approximately 22 staff maintain over 385 open space areas across our region including parks, reserves, sports fields, cemeteries, buffer zones, natural areas and roadsides. Maintaining these spaces is a constant challenge, and there are many factors that contribute to our scheduling. These factors include: - Level of use; Regional and local significance; Location; Growing conditions; Weather and storm events; Preparation for community events; Resource availability; Flexibility due to emergencies or competing priorities; Other environmental factors.

As a general rule, we use a hierarchy approach to set our mowing program. We also look at event bookings for community spaces to ensure our maintenance works are aligned where possible. As our open space network continues to expand, our teams are endeavouring to continue with the prioritisation of our maintenance schedule that will allow for visually appealing and vibrant spaces for our community and visitors to enjoy. Undertaking roadside vegetation work on Transport for NSW (TfNSW) assets is not something we can do with our current budget and priorities. Council will continue to refer concerns to TfNSW and advocate for our community in this regard with Local Members.

Council Service:

Sports	and	Recreation
Sports	anu	Neercation

Submitter's name	Submitter's issue
Sandra Pepper 6935/2021	I am writing to you on behalf of Wauchope Soccer Club about the lights at Fairmont. We not so long ago had them tested and were sent a quote on how much it would cost to upgrade the lights.
	The lights structure and system is outdated. They do not give the amount of light that they used to. The whole system and structure needs an upgrade. It has ongoing costs that could be reduced with the upgrade and it would also make it more economical not only for the club but the community as well.
	With that being said we use the fields 5-6 days a week and with the amount of people that require the use of the fields each season and year it becomes a safety issue as we require certain lux levels set by our association to play.
	With the cost of the upgrade we are not in a financial position to be able to do so and we are asking that the council could please put the upgrade of Fairmont lights to be considered into the 2021/2022 budget.

Response

Thank you for your submission requesting the inclusion of a lighting upgrade at Fairmont Gardens Sports Fields. Council recognises the importance of sport and recreation as an integral part of our social fabric with wide ranging benefits for our community members. The continual review and upgrading of our sporting infrastructure supports this and as such we prioritise on annual budget works to be completed that will enhance our facilities for our sporting user groups. Regrettably, we do not have sufficient budget to be able to commit to a lighting upgrade at Fairmont Gardens Sports Fields. However, we would welcome the opportunity to work with and support the Wauchope Soccer Club on any grant funding applications that you may wish to apply for to secure funding for this project in the coming year. Our team can assist with providing letters of support, reviewing drafted applications and providing guidance on approval pathways for such works to be undertaken. I would also encourage the Club to regularly check our dedicated Port Macquarie-Hastings Grant Finder website where all available grant funding programs are listed throughout the year as they become available.

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Council Service: Sports and Recreation	
Submitter's name	Submitter's issue
Nicholas Taylor 8179/2021	I'm inquiring about the council putting into their budget sealing of the carpark in front of the Port Macquarie Rowing club and Hibbard Park. This carpark is currently unsealed with gravel and is in poor condition due to the floods and heavy traffic usage. This carpark is used every single day from both members of the rowing club and the general public for fishing or park. On weekends the carpark is full with cars from members of the rowing club and once a month the horse-riding club. I have attached photos of the number of cars parked in the carpark. Also, during rainy weather, the carpark becomes very wet and muddy, making a mess of the carpark. Furthermore, the rain washes away the gravel over time and leads to holes and runts in the carpark.
	The residents of the apartment complex east of the rowing club have complained to us and the council previously about the high amount of dust that blows off when windy. The dust travels into their homes and becomes a health hazard. Dust does lead to exacerbation of respiratory diseases and does have a chronic health impact. The residents have been wanting the carpark sealed for many years. In the best interest of the general public and the Rowing club the carpark should be sealed. The carpark receives many vehicles every single day and is the only carparking for rowing club and general public.

Response

Council will in 21/22 be preparing Plans of Management (POM) for the parks and reserves we manage to prioritise asset protection, maintenance and upgrade works for each area. While your request to seal the gravel carpark in front of the Rowing Club, Port Macquarie cannot be funded this year unfortunately, it is a request staff can consider in developing the POM for this area. The first priority for Council is to prepare the POM's to ensure operational works are meeting regulatory obligations for example in bushfire protection, asset lifecycle management and development standards. A POM will also identify resident and user priorities and utilise the information we have gained from other contributing studies such as the Economic Development Strategy and Recreational Needs Study. A POM will then drive the operational programs, design work and budget and grant applications we make to deliver the community's priorities.

Community groups are encouraged to apply for their own funding to undertake works without the need for a financial contribution from Council. Staff are very happy to assist community groups with their grant applications and can provide letters and application support for any community lodgements. You might find our Grant Finder tool a helpful resource to understand the funding opportunities available. https://www.pmhc.nsw.gov.au/Community-Culture/Grants/Grant-Finder

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Council Service:	
Sports and Recreatio	n
Submitter's name	Submitter's issue
Paul Dirago 8616/2021	In June 2020, Council decided that an amount of \$35,000 be provided in the 2020/21 Operational Plan to develop detailed design and approval for the Tacking Point Accessible Ramp and Viewing Platform. That design and approval work is due to be completed by Council very soon.
	This Submission requests that the 2021/2022 Operational Plan/Budget continue to include the Project, with construction during 2021/2022.
	Construction will provide a significant facility for people with disabilities, benefit the local population and enhance future tourism. It will be of value to a wide range of community members.
	As our Coastal walk gains national and international recognition, an Accessible Ramp and Viewing Platform at the Lighthouse will be seen as a thoughtful and forward thinking facility.
	I request that the project remain active by inclusion in the 2021/2022 Operational Plan/Budget along with a commitment to actively seek grant funding.
Response	
Accessible Ramp and Operational Plan due unlikely that complete component will be un	in the 20/21 budget for the design of the proposed Tacking Point Viewing Platform will be required to be brought into the $21/22$ to difficulties in Council securing a design consultant. Accordingly, it is on of the design will be finalised by June 2021. Therefore the design dertaken in $21/22$ with construction to be considered as part of a future prough available grant funding.
Council Service:	
Sports and Recreatio	n
Submitter's name	Submitter's issue
Emilie Cooper 8723/2021	I am making this submission on behalf of the Port Macquarie Astronomical Association in relation to the budget for Councils Operational Plan for the 2021-2022 financial year. I have previously made a brief submission through Council's website and provide this email and the attached information by way of support for our submission that Council allocate \$1.2 million over two years to the upgrade of Rotary Park to include a substantial contribution to the construction of the proposed Astronomical Science Centre.
	Our submission, in general terms, is that Council commit \$1.2 million to the upgrade of Rotary Park to include sewer and public toilets and the construction of other public facilities associated with the proposed Astronomical Science Centre such as:
	 the relocation of the public pathway, car parking bays, upgrade of the children's play area into a children's space centre and

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	outdoor areas of the Centre accessible to the public
Response	1
done to apply for gran Council met with the A	s this important community asset and the work the Association have nt funding to improve the serviceability and visitation of the facility. Association on 18 May 2021 and we look forward to this further ne how Council may be able to assist.
Council Service:	
Roads, Bridges, Trans	sport
Submitter's name	Submitter's issue
Dianne Sales 3130/2021	I, and other residents in the street, are concerned about the potential for a serious incident as the result of increased use of Settlement Point Road and in particular once Maria River Road is sealed giving easy access between Crescent Head and Port Macquarie.
	It would be good to know that Council have plans to address the safety concerns now and also the potential for increased traffic once Maria River Road is sealed.
Response	L
Additional Response	from Director 9/2/21:
particularly after wet v accidents prevent veh seal this road in order however the Liberal/N	vell known for its safety concerns, sharp bends and fluctuating condition, weather events. As this road is regularly used as a detour route when hicles using the Old Highway, a joint funding application was submitted to r to address these safety concerns. This application was unsuccessful; Nationals Coalition committed \$17 Million towards the project as an he lead up to the March 2019 election.
assist us in delivering Settlement Point Roa	lookout for State and Federal Government grant funding opportunities to grojects across the region and will certainly consider upgrades to d for future programs. However, it is often difficult to find suitable grant projects due to the funding program criteria.
hours for the Hibbard	to present options to Councillors regarding the extension of operating ferry, along with other alternatives that would address community nly ask staff to consider your suggestion of swapping the ferries to allow r for heavy vehicles.
Response from Direct	tor 3/2/21:
	nail regarding Settlement Point Road (internal ref: CRM 1954/2021). I or and Councillors into this response for their information.
l appreciate your cond	cerns for road safety in this area.
prioritise road repairs every three years, whi	roads within the Local Government Area and finite funding, we have to and upgrades. We conduct an independent detailed survey of our roads ich assists in determining the planning of maintenance and repairs that results for our community.

Our current Operational Plan does not identify any immediate upgrade plans for this road, including the provision of footpaths or widening, however we will continue to monitor the road condition and treat it with reactive maintenance as required.

The Mayor presented a Mayoral Minute to the 9 December 2020 Ordinary Council Meeting proposing an extension to the hours of operation on the Hibbard Ferry for a trial period.

Unfortunately, this motion was not carried and an amendment was resolved to provide a briefing to Councillors in February 2021 on the options available. We are unable to make any changes to the service until Councillors have been briefed.

With the Hibbard Ferry operating only mornings and afternoons, it is not practical to redirect all heavy vehicles to this location. The Settlement Point Ferry also has a more capacity for larger vehicles.

I understand this may not be the outcome you were hoping for, but I trust it explains our position.

Submitter's name	Submitter's issue
Jillian Turbill 6971/2021	I have just had spinal surgery and walking is an important part of my rehabilitation. As I am not yet able to manage Mrs York's Walk due to the stairs and levels I have started walking up Grant St to Stewart Street and along the roadside footpath on the Northern side of the roa Unfortunately there isn't a footpath on the Southern side.
	Although it is narrow and would not be suitable for those using walkers or wheelchairs, as the trunks of the pine trees impinge in some places find it a good walk with lovely views etc. My concern is that the footpat doesn't extend all the way to the Gaol Point Lookout and ends abruptly The only option for pedestrians is to leave the cement footpath and traverse across the uneven grass and dirt to the lookout, binoculars et As a senior citizen I think others who are not fully mobile may have similar concerns.
	As the cement footpath recommences on the other side of the lookout and car park it would be ideal if the gap could be connected particular as a safety measure.
	I realise this wouldn't have a high priority given the floods etc but perhaps it could be considered in next year's budget.
	Thanks for your consideration.

We recognise the importance of providing pedestrian access for our community and we aim to achieve a high level of appropriate access by identifying areas where we can create or link paths. Thank you for raising this missing link. We will add this to our prioritisation list for footpaths across the region."

Council Service: Roads, Bridges, Transport	
Submitter's name	Submitter's issue
Michael Mowle 8192/2021	I write to request that PMHC include on its upcoming Operational Plan 2021/22 an item for the development of a Masterplan for the Gordon Street Business Precinct (Ocean Drive to Horton Street).
	The Chamber of Commerce recognises this precinct as the "Gateway" to Port Macquarie and an important and emerging commercial precinct. The roadworks and drainage upgrades currently underway will provide the necessary structural improvements needed. However, the

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	streetscape, parking and pedestrian access and safety aspects of this precinct require a masterplan with staged delivery.
	On behalf of property owners and business operators Chamber submits that an upgraded streetscape in this precinct has great potential to increase business opportunities, tourism accommodation, pedestrian safety, shopper visitation and provide an aesthetically appealing "gateway" experience for visitors. For context I have attached a recent Focus Article and Gordon Street Business Precinct plan.
	Port Macquarie Chamber of Commerce requests that PMHC include the Gordon Street Masterplan on the Operational Plan 2021/22 with a budget to develop a "concept Masterplan" which includes streetscape upgrades, pedestrian access and a parking strategy.
Response	
stormwater, watermai	plans to complete a Master Plan for Gordon Street further to the current n and pavement works. This may be reconsidered following the Transport I be considered for the 2022/23 Operational Plan.
Council Service:	
Roads, Bridges, Trans	port
Submitter's name	Submitter's issue
Roger Barlow 8956/2021	Following our recent meeting with you, answers to our queries about various elements in the OP were provided and are attached for the record. This has provided a short cut to our response here.
	We are pleased to note that there are several projects for capital expenditure directly relevant to Bonny Hills, and of course considerable expenditure proposed elsewhere that will benefit all who reside in our LGA.
	But as discussed at our meeting, a priority for Bonny Hills residents identified in our Community Plan is path works, with five prioritised for immediate attention back in 2019. To date nothing has been delivered and it seems there is no guarantee of delivery in the upcoming year. If correct this is most disappointing – see item 7 in the attached email response.
Response	
Comments noted and	meeting for BHPA and Director Infrastructure being arranged.
Council Service:	
Roads, Bridges, Trans	port
Submitter's name	Submitter's issue
Paul Dirago 8941/2021	Comments on: Port Macquarie Transport Network Planning; Odours from Councils' Water Treatment Plants Koala Street; Maintenance of Local Roads – State Government Responsibility; Breakwall Improvements; Tidal Pool Port Macquarie
Response	
	ant amount of information available on our website regarding this project rsay.pmhc.nsw.gov.au/transport-network

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We are expecting the first report on the wider network to be available late 2021. We had originally hoped to complete the report prior to the elections, however we are now awaiting the preliminary outcomes of the Oxley Highway Corridor Strategy engagement so we can ensure our Business Case is aligned with the wider road network plans.

Preliminary consultation was undertaken in September 2020 and has been ongoing through meetings with key stakeholders groups, representing the broader community, including the Port Macquarie Transport Network Community Consultative Committee and our Have your Say webpage. Further workshops are expected later in 2021.

2. The EPA have implemented a Pollution Reduction Program (PRP) as part of our EPA licence for the Port Macquarie Wastewater Treatment Plant in Koala Street in response to odour concerns. This PRP has a number of milestones and requirements that we need to address including trail chemical dosing to reduce odours, which is currently underway. In addition, we are completing further modelling of the system with a focus on predicting the generation of sulphide to determine the most appropriate points to address odours. We are also looking into the costs associated with undertaking works at the plant to limit the ability of odours to escape.

We are required to report on these actions under our EPA licence conditions. The actions associated with the PRP are required to be completed by the end of 2022 and we are hopeful that we will see a reduction in odour emissions in line with this program objectives.

Further to this, as part of the Integrated Water Cycle Management Strategy currently under review, there will be a sewer strategy which will identify any capacity and capability issues and propose options for addressing these to better serve our community.

3. There is currently a Regional Road Transfer and Road Classification Review being undertaken by Transport for NSW. More information is available on their website: https://yoursay.transport.nsw.gov.au/regional-road-transfer-and-road-classification-review. This review will examine the current road network managed by the NSW Government and Councils, along with submissions from local Council's regarding the classified road network. No outcomes from this review have been provided to Council as yet.

We are also working with Transport for NSW regarding the transfer of other roads as a result of the construction of the Pacific Highway upgrades between the Oxley Highway and Kundabung bypass. This specifically refers to the status of Telegraph Point Road and Hastings River Drive between Winery Drive and the Blackmans Point interchange. This may see these roads become classified as Regional Roads, which would be managed by Council with financial support from the State.

At this stage, it is too early to determine what, if any, impacts this will have as the outcomes of the review and timing of any transfers are currently unknown.

4. Improvements to Port Macquarie's breakwall are a Crown Lands project. Council had provided Concept designs to the State Government in 2016, and plan to engage with Crown Lands once they commence design for this project. Port News published an article regarding this project on 26 November 2020 (https://www.portnews.com.au/story/7029365/5-million-green-light-for-port-macquariebreakwall-upgrade/) which identified the following: Minister for Transport and Roads Andrew Constance announced a \$5 million funding for the breakwall in Port Macquarie on November 26, 2020. The funding has come from the NSW government's \$205 million maritime stimulus package. Member for Port Macquarie Leslie Williams confirmed the works will commence after feedback is sought from the community on design options and discussions have been had with major events stakeholders who utilise the breakwall including Ironman Australia. The proposed upgrade is consistent with the adopted Crown Lands Plan of Management for the Port Macquarie foreshore. Some of those design elements include the widening of the concrete footpath, more lighting and signage, a three metre wide accessible ramp at either end of the breakwall path, fishing platforms with seating, retaining walls, more rocks to widen the breakwater and a lookout concrete

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platform. The design and public consultation for the breakwall project, which is a state government asset, is expected to start by mid-2021. For further information it is best that you contact Crown Lands directly Email: cl.enquiries@crownland.nsw.gov.au or phone 1300 886 235.

5. A Tidal Pool for Port Macquarie is not a Council project however Council is assisting the Tidal Pool Committee with their enquiries on the completed Feasibility Study which identified Oxley Beach Port Macquarie as the preferred site. Staff provided the Tidal Pool Committee with a presentation in December 2020 on the main issues that will impact on the development of a tidal pool from a planning perspective at the committee with their pre-development proposal enquiries. For further information it is best that you contact the Tidal Pool Committee directly https://pmqtidalpool.wordpress.com/ or https://www.facebook.com/pmqtidalpool.com.au/. Email: pmqtpc7@gmail.com or phone 6581 5287.

Council Service:	
Stormwater & Draina	age
Submitter's name	Submitter's issue
Patricia Lenehan 14850/2020	This is a long standing issue. Ms Lenehan made a submission for funding for stormwater works in the 2019-2020 Operational Budget but was not successul. The work is identified for completion within Council's register of pending stormwater upgrades and improvements, however no funding is currently allocated due to other higher priority stormwater improvement works. Ms Lenehan was responded to via letter dated 11/07/19 as part of the Operational Plan process advising this. Leslie Williams MP has now made contact with the Mayor in August 2020 on behalf of Ms Lenehan requesting an update on the issue. The Mayor has now requested that the Director of Infrastructure look into the history of this matter and that the Mayor would be grateful for his assistance in seeing this work included in the 2020-2021 Operational budget. The Mayor again requests that this work be given serious consideration for funding in 2020-2021.
Response	
Response from Dire	ctor Infrastructure 20/4/2021
Thank you for your e	mail and apologies for the delay in coming back to you.
We are aware of sto	rmwater and flooding issues in Leighton Close North Haven and regular

We are aware of stormwater and flooding issues in Leighton Close North Haven and regular inspect the drains in this location both prior to and following heavy rains to undertake proactive and reactive maintenance.

We have received a petition from residents to be presented at this month's Council meeting regarding this matter. The Mayor and Councillors will determine any further action at this meeting.

In addition, we are continuing work on the North Haven Flood Catchment Study which will assist in identifying and prioritising remediation works across the area.

Response to Leslie Williams 8/9/2020:

Thank you for your recent correspondence to the Mayor on behalf of your local constituent, Patricia Lenehan, regarding stormwater issues in Leighton Crescent, North Haven.

The Mayor has asked me to respond to you to provide an update on her behalf.

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Ms Lenehan made representations to the Mayor in 2018 regarding the stormwater issue and Ms Lenehan was encouraged to make a submission on the draft 2019-2020 Operational Plan. Ms Lenehan did make a submission, however funding was not forthcoming. The advice provided to Ms Lenehan in the response letter from Council dated 11/07/2019 regarding her submission was as follows:

"Council is aware of the historic stormwater issues impacting residents in Leighton Close. The issues at this location form part of a larger known stormwater "hotspot" (or problem area), located generally between Coral Street and the open drain downstream of the Beachfront Caravan Park on the south-eastern side of The Parade. Given the sale of the issue, and in order to efficiently and correctly identify, prioritise and address these longstanding issues, a whole of catchment stormwater management plan is required to identify solutions to the large scale stormwater issues impacting the locality.

This plan has been previously identified for completion within Council's register of pending stormwater upgrades and improvements, however no funding is currently allocated due to other higher priority stormwater improvement works.

Whilst Council has no short term solution to these ongoing stormwater issues, Council will continue to monitor and maintain the existing drainage system to ensure that this this system is functioning at maximum capacity. Catchment management plans provide assessment of a broader area, allowing Council to prioritise identified hotspot locations for future planning of staged upgrades, which has proven to be the most effective and efficient use of resources to address known issues. Prioritisation of works is determined based upon risk factors to safety, property (private and public), social and economic impacts to the area, and the extent of area affected."

Further to the above information provided to Tricia in 2019, I have been advised by staff that Council has allocated funding in this financial year for a study to carry out a North Haven Catchment Management Plan for the area to the east of Ocean Drive to North Haven Beach and Leighton Crescent will be included in the scoping area.

The Mayor has also re-submitted Tricia's request for the stormwater issues to addressed as a new submission on the Draft 2021-22 Operational Plan, which will be placed on placed on public exhibition in March 2021. This submission has been created as CRM 14850/2020.

Council Service:		
Stormwater & Drainage		
Submitter's name	Submitter's issue	
Melissa Best 6658/2021	We are residents and own homes in Leighton Close, North Haven NSW 2443.	
	I'm emailing in reference to Patricia Lenehan's submission 14850/2020.	
	We completely support and agree with her submission 14850/2020. There is inadequate drainage in Leighton Close, North Haven 2443.	
	As demonstrated this week with the flooding there is grossly inadequate drainage in Leighton Close. In addition there is substantially insufficient cleaning/maintenance/supervision of the storm water drain behind 25 and 26 Leighton Close and the storm water gates.	
	Adequate drainage and regular cleaning/maintenance/supervision of the storm water drain and gates could have resulted in much less flooding and damage to properties in Leighton Close.	
	We request that this is addressed and at the very least the cleaning/maintenance/supervision of the storm water drain behind 25	

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and 26 Leighton Close and the storm water gates is reviewed and
addressed immediately.

Response

We are aware of stormwater and flooding issues in Leighton Close North Haven and regularly inspect the drains in this location both prior to and following heavy rains to undertake proactive and reactive maintenance. We have received a petition from residents to be presented at this month's Council meeting regarding this matter. The Mayor and Councillors will determine any further action at this meeting. In addition, we are continuing work on the North Haven Flood Catchment Study which will assist in identifying and prioritising remediation works across the area.

Council Service:

Stormwater & Drainage

Submitter's name	Submitter's issue
Heather & Robert Mansfield	Just wanting to "Have Our Say"
8765/2021	Regarding the proposed new charge relating to the Settlement Shores Maintenance Fee. Why have we not been contacted as well as everybody else who is subject to this fee to consider its implications. Surely leaving this open for a legal debate

Response

At the September 2020 Council meeting it was resolved to consult with property owners in the Settlement Shores Canal Estate to determine an equitable fee/charge to be added to the draft 2021/22 Fees and Charges to fund maintenance activities as outlined in the 2017 Canal Maintenance Plan. We have targeted engagement planned (this was delayed due to the March Floods) with all impacted residents on this proposed fee and are currently working with a small resident group to finalise the messaging for these engagement sessions.

2021/22 Operational Plan - Capital Works Program Changes from Draft

<u>Bridges</u>

Bridge 178 Mundays Lane 66,000 Bridge 179 Mundays Lane 66,000 Bridge 181 Mundays Lane 76,500 Culvert - Thone River Rd Byabarra 51,000 Culvert - Farrawells Road, Telegraph Point 55,500 Cowal Creek Bridge 51,000 Crowe Roads Bridge Upgrade 102,000 Cutty Bridge Upgrade 115,500 Donkins Creek Bridge 540,000 Joes Bridge Upgrade 115,500 Langdons Bridge 256,500 King Creek Bridge 543,000 Little Mortons Creek Bridge 390,000 Old School Rd Bridge 570,000 Tower Rd Bridge Upgrade 141,000 Pappinbarra Right Arm Bridge 808,140 Kindee Bridge 127,122 Bril Brilge Bridge 500,000	Bottlebrush No 1 Bridge Upgrade	439,500
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Tower Rd Bridge Upgrade141,000Pappinbarra Right Arm Bridge808,140Kindee Bridge127,122Bril Bril Bridge500,000	Little Mortons Creek Bridge	390,000
Pappinbarra Right Arm Bridge808,140Kindee Bridge127,122Bril Bridge500,000	Old School Rd Bridge	570,000
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Bril Bridge 500,000	Pappinbarra Right Arm Bridge	808,140
	Kindee Bridge	127,122
5,014,262	Bril Bridge	500,000
		5,014,262

<u>Other</u>

Town Green West Playgound	133,500	
Bago Rd Stage 3	200,000	
Library Kiosk - Sovereign Hills	128,000	
Marbuk Motorised Valve Relocation	600,000	
Trunk Main Augmentation - Sancrox Reservoir to Wauchope (Property acq)	100,000	
Wauchope WTP Chemical Dosing	1,500,000	
The Hatch Road Reseal	1,309,290	
Upgrade to PM EOC	148,968	
Park & Reserve Upgrades	250,000	
Kendall Tennis Club Upgrade	150,000	
Lake Cathie Community Reserve Basketball	70,000	
Beechwood Pedestrian Upgrades	480,000	
Bonny Hills Reserve Master Plan Improvements - Landscaping Improvements	59,640	
Resurface Council Carparks - Cameron St Wauchope & Bridge St Port Macquarie	150,000	
Town Beach Reserve Basketball Courts	70,000	
Wayne Richards Park Bike Track Upgrade	350,000	
New footpaths on the North Shore	960,000	
Kerb & Gutter Installation - Wesley Avenue, Petit St, Hart St & Anderson St	550,000	
Vince Imon	650,000	
O'Neills Bridge	-81,977	
Property Management System	-80,000	
Depot Rationalisation	-400,000	
Artwalk	31,763	
Kew/Kendall STP Mid-Scale Solar	-50,000	
New Water Supply Services	-300,000	
Wauchope WTP	-800,000	
Hastings Regional Sporting Complex	-712,637	
Design of Ampitheatre Bain & Westport Park	450,000	
Develop Urban Sewer Catchment Plans	240,000	
		6,156,547

Re-prioritisatios Due to Floods

Plant Purchases	-500,000
Bril Bril Road Sealing	-385,390
Horton St Pedestrain Facilities	-32,604
School to School	-211,869
Timber Bridges Renewal Program	-200,000
Logans Crossing Bridge	-400,000
Pathway Renewals	-237,000
Camden Haven Surf Club - Contribution to Design	-40,000
Charlie Watt Reserve - playground equipment	-85,000
Moroko Park Playground Equipment	-85,000
Deliver Park Furniture	-78,000
	-2,254,863

8,915,946

TOTAL

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2 Your Community Life

What we are trying to achieve

A healthy, inclusive and vibrant community.

What the result will be

We will have:

- Community hubs that provide access to services and social connections
- A safe, caring and connected community
- A healthy and active community that is supported by recreational infrastructure
- A strong community that is able to identify and address social issues
- Community participation in events, programs, festivals and activities

How we will get there

- 2.1 Create a community that feels safe
- 2.2 Advocate for social inclusion and fairness
- 2.3 Provide quality programs, community facilities and public spaces, for example, community halls, parks and vibrant town centres
- 2.4 Empower the community through encouraging active involvement in projects, volunteering and events
- 2.5 Promote a creative and culturally rich community



PORT MACQUARIE-HASTINGS BICENTENARY EVENT REPORT

Port Macquarie's Bicentenary event innovatively showcased the region, its people and stories and successfully delivered activities across 9 days for approximately 18,000 total attendees between 10 - 18 April 2021.

The Bicentenary event program, and consequent marketing and promotional collateral, supported the event vision *"To walk our place and discover the stories that weave together to make Port Macquarie-Hastings the region it was, it is, and will be"*. The cultural diversity of the region was reflected, and inclusion and multi-generational storytelling was actively supported with consideration to the region's future generations.

The role of Port Macquarie-Hastings Council was to coordinate the program streams and promote the event locally and out of region.

Outcomes of key program streams

Wakulda: Weaving Our Stories As One:

Utilising funded secured through the Australian Government Regional Bushfire Recovery Fund, a worldclass immersive story-telling experience on the façade of the Port Macquarie Historic Courthouse was launched by Her Excellency the Honourable Margaret Beazley AC QC, Governor of New South Wales, on Saturday 10 April 2021. The project delivered a nightly program of free community engagement activities centred around the new sound and light installation between 10–18 April 2021 (inclusive). This program attracted over 7,400 attendees across the participating venues of the Historic Courthouse, Port Macquarie Museum, Mid North Coast Pilot Boatshed, Glasshouse Regional Gallery and Sunset Sessions at Little Shack. These live performances were funded from a separate Regional Bushfire Recovery grant to support local musicians.

Hastings River Aquatic and Visiting Vessels Activities, including a visit by the Tall Ship Coral Trekker

In consultation with community aquatic stakeholders, the impacts of the flood event resulted in the postponement of the planned foreshore activities on Sunday 18 April. The traditional canoe making workshops by Fred and John Kelly were relocated to Sea Acres National Park and able to continue as planned with the construction of 4 canoes utilising traditional materials and techniques and attended by Birpai community members and local residents. The Mid North Maritime Museum Pilot Boatshed achieved a mammoth clean up to open throughout the event days and participate in the Wakulda Nightly Program. The visiting Sydney Harbour Tall Ship, Coral Trekker, was able to safely undertake the passage to Port Macquarie and was in residence on Lady Nelson Wharf for the duration of the event. The vessel acted as a classroom on Monday 12 and Tuesday 13 April for a Council-coordinated delivery of Port Macquarie TAFE's General Purpose Hand Course to 8 local students. The Coral Trekker also offered public access tours and sailing trips.

Art Exhibitions

With support from the Glasshouse Regional Gallery, the Bicentenary event period was host to a range of local and visiting art exhibitions. All residents of the Port Macquarie-Hastings region were invited to create an artwork in any medium to interpret the <u>Community Art Exhibition</u> theme: 'Creating a visual response to the varied natural, cultural, historic and popular elements that make positive contributions

to our local community'. A total of 68 artworks were submitted and exhibited, with the youngest artist under the age of one. Over 480 entries were submitted in the People's Choice Vote which was awarded to **Julie Mills** 'Rainbow Beach Bonny Hills'. A judging panel consisting of the Regional Gallery Curator and representatives of the Cultural Steering Group and Bicentenary Working Group awarded Best response to theme to Lorraine Neill's 'Magic of the Hastings', Best application of technique to Mikayla Harrison's 'Pacific Waves and the Emerging Artist Encouragement Award to Lucas Tubergan's 'Safety First at Bartlett's Beach'. Other exhibitions included <u>David Martin: Port Macquarie 200 Exhibition</u> which profiled the contributions of seven artists who chose Port Macquarie as their subject during the course of two centuries, and the <u>Saltwater Freshwater Arts 2021</u> featuring the Aboriginal Art award and Contemporary Cultural Objects.

Food, Wine, Art and Cultural Trails

Utilising COVID Recovery funding support, visitors and residents joined for a **one-off event** to launch the <u>Artists' Open Studio Weekend</u> on 10 and 11 April, where 20 of the region's most talented creatives opened their studios and workshops to the public, many for the very first time. Participants spanned a diverse range of artistic pursuits from painting, drawing, sculpture, glass, pottery, ceramics, jewellery, metalwork, textiles and more. Over 1,000 attendees visited creatives in the region, stimulating local economy with purchases made onsite and as a result of sustained travel through outlying townships. The new <u>Taste Trails</u> digitally launched during the Bicentenary event as five specially curated 'Insider's Guides' providing local insights into some of the region's best experiences. Online resources including maps and drive times were available so both locals and visitors could assemble their own self-drive trail and maximise visits. From the success of this launch, the printed collateral will go into production in May/June, and will include an illustrated map of the region with each of the Trail locations marked, as well as signage and digital assets for participating businesses.

Launch of the Wauchope Bicentenary Riverside Sculpture Trail

In consultation with the Bunyah Local Aboriginal land Council and the Djuyalga Wakulda, via the Muurrbay Aboriginal Language and Culture Co-operative, this sculptural trail was gifted the Gathang name; <u>Yapang Bila Yapun, yapun</u>. However, due to the flood damage across Rocks Ferry Reserve, a number of the installed and partially-installed sculpture pieces either sustained major damage or were washed away. The Destination and Cultural Development team are now working with the selected artists and broader Council infrastructure teams to create a revised production schedule for delivery. Once finalised, NSW Government as the key funder under the Stronger Communities Fund will be notified and a new launch date mutually agreed.

Community days

Due to the fluid, and challenging, regulatory environment for COVID Safe delivery of major events, the focus of the Council-led community initiatives moved to the Wakulda Nightly Program and support of the events controlled by community organisations. The last minute addition of the Royal Australian Air Force Roulette Sevens aerial display on Sunday 18 April did offer a strong platform for community participation, with several thousand attending Town Green and foreshore to watch the 15 minute display. This could only be safely facilitated due to updated changes to the NSW Public Health Order, and in consultation with local authorities.

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Satellite events

An Expression of Interest process invited community groups to undertake their own activations to 'tell their story' as part of the Bicentenary commemorations. These formed over 40 'satellite events' that were actively promoted for community and visitor participation. Activities ranged from facility and club open days, art exhibitions, film screenings, book launches, historical and cultural tours, performances, river cruises and workshops. Approximately 2,000 attendees participated in the satellite events with the majority of initiatives being delivered by volunteers. Highlights included the launch of the viewing platforms at Mrs York's Garden by the Governor of NSW, the Hastings District Flying Club Open Day attended by the Royal Australian Air Force Roulette Sevens, and performances by the Dance Mid North Coast Association of 'Gumba', choreographed by Ryan Pearson from Bangarra Dance Theatre and featuring 25 Indigenous and non-Indigenous dancers from the local region.

Noting, *Education resources* for local schools remain under development with Council's Education team and are scheduled to launch at the commencement of Term 3 as a free, online education pack consisting of cultural and historical resources to assist a holistic primary-level curriculum delivery.

Event Engagement with Local Aboriginal Land Councils

The Bicentenary Working Group (BWG) engaged with the Local Aboriginal Land Councils throughout the event planning, and welcomed Birpai Local Aboriginal Land Council (BLALC) Members Roger Munro and Sandra Hindman as members of the working group in 2019.

In September 2020 BWG members undertook a facilitated discussion with Rhonda Radley, Birpai Elder and Birpai Local Aboriginal Land Council Board member, where Rhonda led attending BWG members and Council staff through a planning process to ensure the continued development of the Bicentenary program was inclusive of the cultural diversity of the region, actively supported inclusion and multigenerational storytelling, and considered the region's future generations.

Engagement on key program activities included:

- <u>Wakulda: Weaving Our Stories As One</u>: Council's Cultural Development staff met and spoke with Land Council representatives to develop a title for the project in Birpai language that referenced the project's theme of 'the weaving of our stories, and of walking this place together'. The Muurrbay Aboriginal Language and Culture Co-operative consulted and reviewed local language incorporated in development and creation of this project, as well as other First Nations cultural consultants, artists and performers.
- <u>Welcome to Country for Launch of Wakulda on Saturday 10 April</u>: A Birpai Local Aboriginal Land Council booking request form was completed and Uncle Bill O'Brien delivered the Welcome to Country, with invited Birpai representative, Latoya Smith, delivering an address as one of the key speakers. The local Dhoongang Dancers conducted a smoking ceremony and performance as part of the COVID safe event which marked the first screening and launch of the identified event period.
- Acknowledgement of Country in selected printed and online marketing collateral / printed booklet: The acknowledgement provided to Council was updated to include 'seas and rivers' and included across multiple platforms. The Muurrbay Aboriginal Language and Culture Co-operative and the Local Gathang Language Group Djuyalgu Wakulda also provided a translation of 'Rescue

of 7 sailors from the Alligator by 7 Aboriginal men, *Guri Wakul Gagil - Together As* One featured in the Bicentenary event booklet.

- <u>Lighting of Town Square Acknowledgement Garden for Wakulda Nightly Program</u>: Investigations were made to source and install a permanent solar lighting around the pillars of the Acknowledgement Garden and the Birpai Local Aboriginal Land Council consulted as to the appropriateness of recognising this site during the identified event period. Whilst this proposed recognition did not proceed for the Wakulda Nightly Program, this enquiry has initiated further discussion regarding appropriate cultural recognition on Town Green.
- <u>Coral Trekker Residency</u>: Engagement on cultural appropriateness of proposed activity prior to contract negotiations was undertaken with the BLALC, including determination of Birpai welcome on port arrival. Community engagement with TAFE General Purpose Hand Course on Coral Trekker was undertaken by Council's Economic Development team and an introduction facilitated between a local Aboriginal historian and the vessel to advise on local stories relating to the Hastings River.
- <u>Identified Satellite Events</u>: During the Bicentenary date period engagement with Local Aboriginal Land Councils included the completion and premiere of short film 'Blackmans Point Massacre' screened in collaboration with Charles Sturt University. Created by local film makers, Big Mob Productions, the free screening and facilitated panel discussion was approached as healing through truth telling and was attended by invited community members, Land Council representatives, Port Macquarie-Hastings Council representatives and general attendees. In addition., a two day traditional canoe making workshop with Kempsey-based craftsman was relocated to Sea Acres National Park following the flood event. Following own cultural protocols, the engaged craftsman contacted the BLALC Chairperson directly and an onsite meeting conducted to determine attendees prior to delivery.

Event External Funding

The Bicentenary event secured a \$20,000 grant in late 2019 from the Building Better Regions Fund to support activities for a one-day community event. As a consequence of the COVID-19 gathering restrictions, funding was renegotiated to support the delivery of a nightly program of free, COVID Safe community engagement activities centred around the new immersive sound and light installation, Wakulda: Weaving Our Stories As One.

In addition, Council was invited in late 2020 by the Regional Tourism Bushfire Recovery Fund to submit an application to support Bicentenary activities which met the grant's objectives. A total of \$5,000 was secured to deliver live music by local musicians as the Sunset Sessions featured in the Bicentenary's Wakulda Nightly Program to ensure the diversity and abundance of creative talent in local affected communities was celebrated.

Event Destination Attraction Activity

Whilst overseas travel was not viable during the event period, the Bicentenary destination attraction activity pivoted to target intrastate visitors within a 2-4hr travel radius and focused on short-lead digital marketing assets and local print media and radio to maximise reach within an ever-changing COVID-19 regulatory environment. The paid social campaign via our Greater Port Macquarie Destination channels over a four-week period delivered 35,238 website visits and 5515 goal completions with a conversion rate of 15.7%.

Event utilisation of Council-owned assets

The Bicentenary event program worked collaboratively with the Glasshouse to host the Community Art Exhibition and participate in the Wakulda Nightly Program by opening the Regional Gallery and Glasshouse Studio to attend programmed exhibitions, artist talks as satellite events, and offer commercial activities such as the Glasshouse Shop and bar facilities. Glasshouse Technical Services were engaged to assist with offsite event delivery such as the Wakulda Launch and the lighting of the Pilot Boatshed attraction whilst Glasshouse Marketing Services were utilised for digital and onsite promotions.

The Port Macquarie Historic Courthouse was a central location for the Wakulda Nightly Program with the looped screening between 6.00pm-9.00pm each evening of the sound/light installation as well as the Bicentenary information stall. Courthouse Volunteers enabled the venue to open and hosted the <u>Governing Change</u> exhibition by local artist, Leanne Prussing.

Lady Nelson Wharf was utilised for the residency of Sydney Harbour Tall Ship, Coral Trekker, for the duration of the Bicentenary event. Despite some initial hesitation concerning structural integrity following the flood event, the centrally located wharf was safe to use and provided a strong visual focus for attendees.

Event results

The event was designed to commemorate Port Macquarie's Bicentenary of European settlement while generating awareness and knowledge of the heritage, culture and natural assets of the local area.

The event results are based upon surveys collected on-line from a sample of 400+ event attendees and 30 participants that were analysed by the staff at Destination Research and Development. The following key results were found:

The event attracted a wide range of attendees from local regions and intrastate which included:

- 86% from Greater Port Macquarie.
- 4% came from neighbouring LGAs .
- 5% from Sydney
- 60% of event attendees over 40 years of age.
- 28% of attendees with children at the event.
- 41% are couples or 25% in family groups of three or four.
- 11% of event attendees stayed overnight or longer.
- The average night stay of event visitors was 5 nights.
- 56% of attendees came for the event, while 24% came for the event and other reasons
- 77% of attendees felt an enhanced image of the town to visitors
- The event generated an estimated \$333,830 from visitor spending.

Attendees used a variety of sources to access information about the event:

- 36% used social media to access information about the event.
- 20% used word of mouth to access information about the event.

• 19% used 2 or more sources to find event information.

Attendees felt the event had a positive cultural impact on the community through improved:

- heritage awareness (80%)
- vibrancy of the CBD (77%)
- indigenous awareness (67%)

Participants felt the event had a strong impact on:

- Community cohesion feeling part of the community (89%)
- Social inclusion feeling part of the community, and pride in the community (89%)
- Network development (76%)

While the majority of attendees perceived the event as a success, the timing of the event was unfortunate, with COVID-19 restrictions affecting the likelihood of attracting interstate travellers, and major flooding impacting the entire LGA just days before the event. Despite these limitations, the research shows a high level of satisfaction the event itself from both visitors and the community, with 78% of attendees rating the overall event experience as very good or excellent.

2021 BICENTENARY

& WAKULDA PROJECTION PROJECT

EVENT REPORT

Authored by: Jo Mackellar 13 May 2021

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Item 10.01 Attachment 2 Page 356

Executive Summary

The 2021 Bicentenary & Wakulda Projection Project (the Event) was held in the Port Macquarie-Hastings area over ten days in April, 2021. The event was designed to celebrate the Bicentenary of Port Macquarie while generating awareness and knowledge of the heritage, culture and natural assets of the local area. This report provides a summary of the research conducted at the event, assessing the characteristics and expenditure patterns of visitors as well as the cultural value of the event to the local community. The research is based upon surveys collected on-line from a sample of 400+ event attendees and 30 participants which were analysed by the staff at Destination Research and Development. The following key results were found:

The event attracted a wide range of attendees from local regions and intrastate which included:

- 86% from Greater Port Macquarie.
- ➢ 4% came from neighbouring LGAs .
- > 5% from Sydney
- ➢ 60% of event attendees over 40 years of age.
- > 28% of attendees with children at the event.
- > 41% are couples or 25% in family groups of three or four.
- > 11% of event attendees stayed overnight or longer.
- > The average night stay of event visitors was 5 nights.
- > 56% of attendees came for the event, while 24% came for the event and other reasons
- > 77% of attendees felt an enhanced image of the town to visitors
- The event generated an estimated \$333,830 from visitor spending.

Attendees used a variety of sources to access information about the event:

- > 36% used social media to access information about the event.
- > 20% used word of mouth to access information about the event.
- > 19% used 2 or more sources to find event information.

Attendees felt the event had a positive cultural impact on the community through improved:

- heritage awareness (80%)
- vibrancy of the CBD (77%)
- indigenous awareness (67%)

Participants felt the event had a strong impact on:

- Community cohesion feeling part of the community (89%)
- Social inclusion feeling part of the community, and pride in the community (89%)
- Network development (76%)

While the event was perceived as a success by the majority of attendees, the timing of the event was unfortunate, with Covid_19 restrictions impacting on the likelihood of attracting interstate travellers, and major flooding impacting the entire LGA just days before the event. Despite these limitations, the research shows a high level of satisfaction the event itself from both visitors and the community, with 78% of attendees rating the overall event experience as very good or excellent.

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Research methods

The event was spread over 10 days in Port Macquarie CBD and towns such as Wauchope and Laurieton, with the focal event of the program (Wakulda) screening artistic projections nightly on the heritage buildings in the Port Macquarie CBD. Wakulda was a uniting theme of the program, meaning 'As One' in the Gathang language of the local Birpai people. In addition to the nightly activities, the Bicentenary event featured an Artist Open Studio Weekend to 21 studios, a visit by the Coral Trekker Tall Ship and 40 satellite events throughout the LGA encouraging attendees to discover new pockets of local culture, heritage and nature.

A visitor survey was conducted to establish the profile of attendees, their expenditure and perceptions of the event. As the event was spread across the LGA, the data collection was also designed to collect data from a wide range of event attendees including visitors and residents. Tally sheets were used in event venues including museums and artist studios to record the number of attendees and their place of residence. A longer survey was designed to collect data from the main event venues over the 10 days of the event, distributed at the CBD town green and via social media. A total of 406 surveys were collected representing a random sampling error of +/- 5 per cent at the 95 per confidence level within a population of 18,000.



https://www.portnews.com.au/story/7195233/wakulda-will-bring-together-the-past-and-present-in-visual-bicentenary-display/

The total attendance measured by the tally sheets indicated an estimated **18,000 total audience**, with most of these attending the Wakulda nightly program around the Port Macquarie CBD. Survey data indicated that visitors attended an average of 1.9 events during the 10 day program, resulting a **unique visitor estimate of 9,474.** While this is a substantial result, it should be noted that attendance at the event was severely interrupted by major flooding which impacted the entire LGA, and the wider North Coast region just days before the event. Furthermore, Covid_19 travel restrictions also affected the likelihood of attracting interstate travellers from Victoria and Queensland at this time.

EVENT ACTIVITY	ATTENDANCE
MNMC PILOT BOATSHED	100
PORT MACQUARIE MUSEUM (10-18 APRIL)	1048
CORAL TREKKER (10 - 18 APRIL)	1155
PEOPLE'S CHOICE VOTES	488
ART EXHIBITION SUBMISSIONS	68
WAKULDA NIGHTLY PROGRAM COURTHOUSE, ART EXHIBIT, SUNSET SESSIONS	7572
ARTS STUDIOS	2310
SATELLITE EVENTS	2000
TASTE TRAILS	260
TOTAL	18,001

Data was analysed to establish the demographic profile of attendees, and the ratio of visitors at the event from outside the LGA. Data also assessed the level of visitor satisfaction and their perceptions of the event experience. Expenditure of visitors was assessed using a 'Direct Visitor Expenditure' method to determine the direct economic impact of an event to its host community. Expenditure is calculated using an event tourism ratio identifying visitors to the event who are bringing new money to the region. It does not include "(D) local residents" from the local region (Port Macquarie) as that money is assumed to exist in the local economy. Nor does it include "(C) holiday-makers" or others not in the area for the event, as their money is assumed to be spent in the LGA regardless of the event.

A participant survey was also undertaken to further understand the impact of the event on those who presented artworks or assisted in the creation of feature presentations. This survey was completed by 30 participants in art, heritage and music activities, with the results are also presented in this report.



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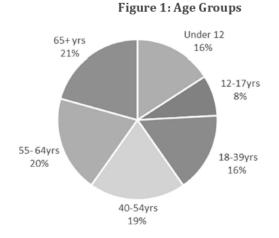
VISITOR SURVEY RESULTS

Demographic Profile

The profile of attendees attending the event indicates the appeal to a wide range of age groups. Most attendees attended in couples or small family groups of 2-4 people. Attendees are predominantly from the greater Port Macquarie area, and yet the event also extended its reach to a range of visitors from other parts of NSW.

Age profile

Survey respondents were asked the number of people in their group and the ages of those people with the results highlighting the wide appeal of the event to all age groups. Adults over 40 years make up slightly more than half of the audience (60%), and 24% of the audience were under 18 years - highlighting the engagement of younger people.



Group Size and children

The average group size was **3.4** with attendees mainly attending in couples. As shown in the table below most respondents attended in couples (41%) and groups of three/four people (25%). Groups with more than four people (22%) were also noted, including many larger community groups (12+ people). Family groups including children under 12 years were prominent with 28% of attendees bringing children with them.

-				0	
Ľ	ab	le	1:	Group	size

Group size	single	couple	three	four	> four
% of respondents	12%	41%	11%	14%	22%

Place of Residence

Postcode data was collected from the main Visitor Survey and compared to visitor Tallies from the Open Art Studios and the Port Macquarie Museum. The analysis suggests the majority of attendees at all activities resided in the Greater Port Macquarie area. However, attendees also come from other geographical markets including:

- Sydney (5%)
- Neighbouring regions such as Greater Taree (1%) and Kempsey (3%).
- NSW regional areas such as Newcastle (1%) and Central Coast (1%).
- Interstate (0.8%).

It is interesting to note the variance in the ratio of locals to visitors at each venue, with the museum attracting more visitors from Sydney and Interstate and the Open Art Studios attracting more locals.

Region	Main venue survey (#400 responses)	Open studios (#1202 tallies)	MUSEUM (#558 tallies)
NSW	99%	96 %	91%
Greater Port Macquarie	86%	90%	50%
Sydney	5%	4%	17%
Kempsey	3%	1%	1%
Newcastle/Hunter	1%	0%	7%
South Coast	1%	0%	2%
Greater Taree	1%	1%	2%
Coffs Coast	1%	0%	1%
Central Coast	1%	0%	5%
North Coast	0%	1%	1%
Central West	0%	1%	3%
Interstate	1%	4%	9%
QLD	<1%	1%	5%
VIC	<1%	2%	3%
ACT	0%	1%	1%
Overseas		>1%	
Total	100%		

Table 2: Postcode analysis

While international travelers have been previously captured in Port Macquarie event surveys only two were recorded at this event - reflecting the impacts of COVID_19 travel restrictions on local tourism.

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Annular region analysis

An annular region analysis is undertaken by plotting postcodes on an annular map that allows calculation of the distances travelled. The results of this analysis illustrate that approximately 14% of the audience reside more than 50 kilometres away. This result is useful for interpreting the tourist value of the event; as many tourism organisations such as Tourism Research Australia define a tourist as coming from over 50kms¹.

No.	Annular Analysis	2021
1	Less than 50 kms	86%
2	51 - 100 kms	4%
3	101 - 200 kms	3%
4	201 - 400 kms	6%
6	400kms + interstate	2%
	Total	100%

Table 3: Annular analysis

Figure 2: Annular regions



Figure is for illustrative purposes and is not to exact scale.

¹ See http://www.tra.gov.au/statistics/domestic-travel-by-australians.html

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Accommodation and Length of Stay

Survey respondents were asked about their accommodation choice and their length of stay. A large majority of attendees attend the event on a day trip from within Port Macquarie-Hastings or the local regions, with 87% returning to their own home. A further 8% of event attendees used local commercial accommodation including hotels/motels (5%), rented apartments (2%), caravan parks (1%). However, 5% used private accommodation, Air BnB (1%) and timeshare/own property (2%), houses of friends and family (2%), thus reducing the economic contribution of the event.

For those who did stay overnight or longer specifically for the event the average night stay was **5 nights**, longer than the average stay for domestic visitors (3 nights²).

Purpose of Visit

The 'purpose of visit' was analysed in order to determine the ratio of the audience that attended the destination specifically for the event, and those who attended for 'other' reasons. In total, 56% of respondents came specifically for the event, 24% came for the event and other reasons, and 20% were in the area for other reasons.

As shown below, the event attracted visitors from within 50kms, as well as from other LGAs including Sydney, Kempsey, Newcastle and the North Coast. In total **5% of visitors from outside the LGA** came for the event.

	just for the	for the event and other	no I was there for other
	event	reasons	reasons
Port Macquarie-Hastings	53%	19%	14%
Sydney	1%	1%	3%
Kempsey	1%	1%	1%
Newcastle	1%	0%	1%
North Coast	0%	1%	0%
Greater Taree	0%	1%	0%
South Coast	0%	0%	1%
Coffs Coast	0%	0%	1%
QLD	0%	0%	0%
VIC	0%	0%	1%
Grand Total	56%	24%	20%

Table 4: Purpose of visit



Direct Visitor Expenditure

The direct visitor expenditure is estimated using an event tourism ratio (Figure 3) which examines the ratio of visitors versus residents at the event. Table 5 shows the expenditure for "event specific visitors" determined though analysis in previous sections to be 7% of all visitors. These visitors have come into the region, for the purpose of visiting the event. It includes (A) overnight event visitors and (B) day trippers not from the local area.

As a point of reference, Table 5 also shows the expenditure of "all visitor expenditure" which includes holiday makers (C).



	Event specific expenditure	All visitor expenditure
Overnight Visitors		
unique visitors	9,500	9,500
overnight tourist %	7.1%	11.0%
overnight tourist #'s	675	1045
Per person average spend per event	\$415	\$299
Total Overnight Visitor expenditure	\$279,918	\$312,455
Day Trippers (B)		
unique visitors	9,500	9,500
day trippers %	3.0%	3.0%
# day trippers	285	285
Per person average spend per event	\$75	\$75
Total Day trippers	\$21,375	\$21,375
Total Overnight and Daytrips	\$301,293	\$333,830

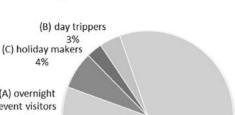
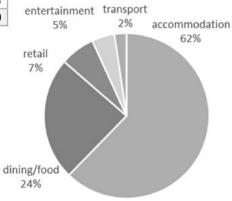


Figure 3: Event tourism ratio

(A) overnight event visitors 7%
(D) local residents 86%

Figure 4: Expenditure sectors



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The majority of All Visitor expenditure, described above, is expended in the local area/s on accommodation (62%), dining (24%) and retail (7%) as well as entertainment (including event tickets) (5%) as shown in the chart.

Promotion Analysis

Attendees were asked how they heard about the event, and allowed to choose more than one source (multiple response). As shown in the table below, over half of respondents reported using just one source (59%) of information, while others used two or more sources. This reinforces the importance of using a strong mix of promotional materials and mediums to reach a wide audience.

Table 6: Number of information sources used

1 source	2 sources	3 sources	4+ sources
59%	26%	11%	4%

Social media (36%) was reported as the highest source of information (however it should be noted that the survey responses were mainly collected via this media which may bias the result). Other traditional sources of event information remain relevant such as 'word of mouth' (20%) and Magazine (11%) all provided good response rates.

A further 6% simply 'walked into the event' with several comments around "not knowing it was on", or not seeing any advertising. This, however is understandable given the volume and importance of news and information around the river floods across the LGA at the same time, making it difficult to cut through the news with event advertising and publicity.

Other responses included:

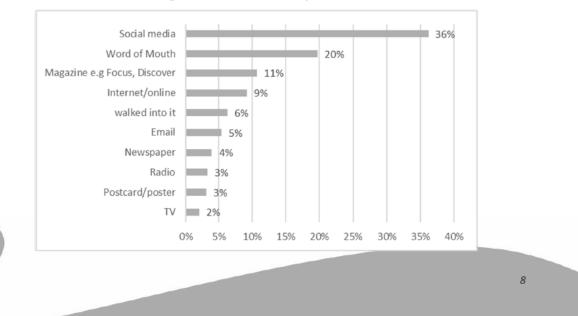
- Booklet
- Brochures
- Council advert
- Dance company
- FB sponsored ad Roulettes.
- Inside Outside Management
- Interested in history used to
- live in PortLibrary
- Marine Rescue member
- Museum

•

- NPWS
- PM Bicentenary Brochure
 - PMHC Bicentenary email

Figure 5: Promotion analysis

- Port News Facebook post
- Port News website
- Printed brochure
- Rotary club
- Tourism association
- Wauchope Library, Brochure
- Council brochure



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Social and cultural impacts

Survey respondents were asked for their opinion on the cultural value of the event to the local area. They were asked to rate the impact of the event across seven different dimensions as shown in Figure 5. While all areas received positive impact ratings, the highest response was for **heritage awareness** where 80% of respondents felt there was a positive impact toward their awareness of heritage of the area. This was closely followed by **vibrancy of the CBD** where 77% of respondents felt the event had a positive impact, and the **enhanced image of the town to tourists** with 77% of respondents feeling a positive impact. There was marginally less impact reportedly felt for **inclusion with community (71%) and indigenous awareness** (67%).

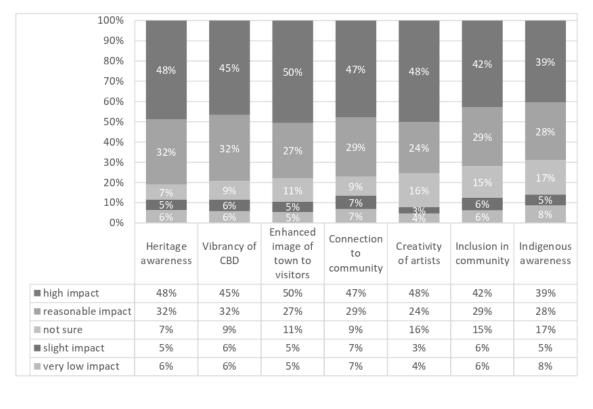


Figure 6: Ratings of event aspects

Positive impact rating	80%	77%	77%	76%	72%	71%	67%

Opinions of the event experience

Attendees were asked to rate and describe their event experience, both on a rating scale and by open ended responses. On the rating scale from one to ten, the results suggest an overall high satisfaction with all aspects of the event.

Respondents rated the overall experience 8.4/10

Poor	Not good	average	Very good	excellent
4%	4%	13%	25%	53%

As shown the majority of attendees (53%) rated the experience as excellent, with many finding the event inspiring and enjoying the range of activities.

- > Fell in love with port Macquarie all over again and I've lived here for 15 years!
- Beautiful, educational, inspiring.

Respondents provided an open response comment about the event which was then coded into groups as shown below. While the majority of **general comments** were very positive about the organisation and the activities, many were concerned with the lack of **advertising/information** (38%). Some responses were mixed about the Indigenous program and the Coral Trekker, while being very positive about the Open Art Studios and Wakulda. The full list of themed comments can be found in the appendix document.

Category	% of responses
Advertising/information	38%
General positive comments	28%
Indigenous program	9%
Coral Trekker comments	7%
Open Art Studios	5%
Wakulda	3%

Table 7: Open comments

Some respondents added further comments to share their positive perceptions of the event.

- > Love love loved Wakulda. So important in truly weaving our connection with and on Birpai land.
- Brilliantly done, made me proud to live here
- Great week of events. Good variety. Positive comments by many people. Well organized
- It was excellent and just what we needed after fire, COVID and flooding
- Loved the music artists too on Town Green we should do that every Friday / Saturday.

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In one word...

Respondents were asked to encapsulate their event experience in one word. The clear majority of words were positive based around the ideas of being proud and happy, that the event was enjoyable and interesting. The word cloud shows the frequency of these and other words.

Figure 7: Word frequency chart

Amazed Awesome Impressed Relaxing Macquarie Amazing Excited Inspired Good Happy Wonderful Connected Lovely Surprised Underwhelmed Interested like about all Exciting tall Positive Proud Exciting Roulettes Moved Enlightening Fun day Enjoyment events Enjoyed Educational informative Inspiring ship Creative know experience Interesting Excellent Great

Suggestions for improvement

Survey respondents were asked for suggestions to improve the event. Many suggested that no improvements were needed, and the event was great as it is. Other responses were categorised into themes where most responses related to:

- Advertising (53%) e.g. "Much greater publicity would be good. We saw the art gallery signs in Lake Innes but maybe have a billboard at Coles announcing up coming council events/happenings".
- Event content (25%) e.g. Requests for more music, indigenous displays, free tours and markets.
- Operations (11%) e.g. issues around ticketing, commentary, seating in particular activities.
- Future events (5%) Some respondents also provided encouragement to create **future events** within the same theme or sense of inclusion; suggesting the Indigenous Program could be part of a wider program or 'included in more events throughout the year'. Some comments were made as to the presentation of the impact of colonization while others found it well done.
- A full list of comments can be found in the appendix document.

Table 7: Suggested improvements

Category	% of responses
Advertising/information	53%
Event content	25%
Operational issues	11%
Future events	5%
Timing	4%
Other issues	2%

PARTICIPANT SURVEY RESULTS

Beyond the economic and tourism impacts created by the event other important community and social impacts are clearly demonstrated. The event offered people from the local area (and beyond) a range of creative and other opportunities to be involved in the production process. It is estimated that 291 people were involved as event participants and over 306 were volunteers. As shown the opportunities were spread across different types of arts, cultural and heritage organisations and productions over the ten days.

Table 8: Participants and volunteers

PROGRAM	Participant tally	Volunteer tally
Wakulda	30	72
Courthouse	8	18
Community Art Exhibition	68	0
Sunset Sessions	24	0
MNMC Pilot Boatshed	4	18
Port Macquarie Museum (10-18 April)	4	36
Coral Trekker (10 - 18 April)	5	0
Open Art Studios (#21)	21	42
Roulette Sevens Display	7	0
Satellite events (#40)	120	120
	291	306

An Event Participant Survey was undertaken by 30 participants to understand their involvement and the outcomes for themselves as artists and/or community organisations. Most respondents were from the open arts studio (80%), with others from community art exhibition, heritage exhibition, music performers, and Satellite Events.

Respondents were asked to rate aspects of the event on a scale of 1 to 10 with 1 being poor and 10 being excellent. The average rating scores, shown below demonstrate a high perception of the organisation, atmosphere and overall experience.

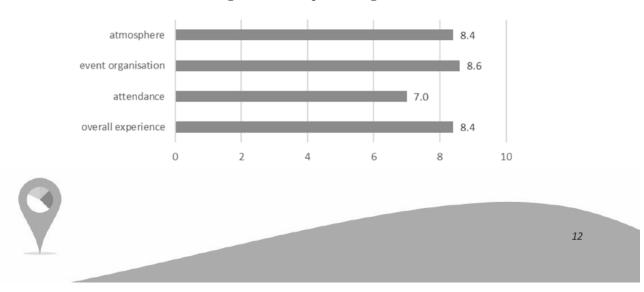


Figure 8: Participant ratings

Attendance comments

It is interesting that the attendance did not rate as well as other aspects, when the comments around attendance shown below indicate most respondents were mostly happy with the attendances at their studios.

Table 9: Attendance comments

- > A lot of locals from the Hastings Valley Art Society, just looking for ideas.
- Far beyond my expectations. Congratulations to the organizers
- > I anticipated maybe 5-10 people on both days had 123 all together which was unreal
- > Over 100 visitors signed in at my studio during this two day event.
- Pretty steady flow most of the day. Could have been more IO expect but my studio missed out on being included in the flyer but thats my fault.,
- Saturday was quite for us here at Beechwood as I think there was so much going on in Port. But Sunday was really busy and had great energy and great community support.
- The amount of visitors was a little overwhelming at times and not all of them were interested in my work.
- There is no way I could have gotten this kind of exposure to my studio and classes without your help, thank you!
- Very happy with the amount of attendees
- ➢ We didn't stop all day.
- We thought we would be sitting reading a book for a lot of the day!.. 15 min after 10am and we were into it...yet not swamped with too many to manage covidly!
- The feedback we received from visitors was positive but the numbers were disappointing. Most commented that they found out about our participation second hand or from the sign we had outside our building, as we were not included in other published brochures.
- A nice size crowd but open to many more attending.
- > Again we only had passers by stopping to listen
- > Almost too many visitors in such a short time, I felt like I was on 'loop'

Community impacts

Previous studies both in Australia and internationally have demonstrated the significant social and community impacts that can result from participation in cultural events and festivals³. Analysis of the survey results reflected and expanded these previous studies, especially in the following aspects:

- 1. Community cohesion feeling part of the community and its heritage
- 2. Learning and skills development learning new skills
- 3. Social inclusion feeling part of the community, and pride in the community
- 4. Network development meeting new contacts and getting to know people better
- 5. Creativity and inspiration exploring creative practice
- 6. Appreciating heritage, culture and arts appreciation of arts and cultural practice
- 7. Promotion of artistic practice finding new ways to promote artist endeavours.

Based on their involvement in the event as artists and heritage, participants were asked to rate the impact of the Bicentenary event on these aspects. The results show the positive impacts toward social inclusion and pride of place, as well as meeting new people. Interestingly these social benefits were felt more than personal benefits such as learning new skills and exploring creativity.

100%	4%							
90%		12%						
80%	27%							
70%		36%						
60%	23%		27%					
50%				43%	31%			
40%		16%	19%	4570	5170	31%		
30%	35%		1570		10%		41%	41%
20%		32%	19%	4%	10%	-0%-		
10%	12%			21%	17%	24%	4%	100/
0%	1270	4%	8%	0%	3%	0%	7%	10%
	learning new skills	appreciating community heritage	exploring your own creativity	socialising - getting to know people better	promotion of your artistic practice	meeting new people	pride in our place	feeling part of the community
a great deal	4%	12%	27%	32%	38%	45%	48%	48%
■ quite a lot	27%	36%	27%	43%	31%	31%	41%	41%
unsure	23%	16%	19%	4%	10%	0%	4%	0%
a little	35%	32%	19%	21%	17%	24%	7%	10%
■ not at all	12%	4%	8%	0%	3%	0%	0%	0%

³ Delamere, T. A. (2001). Development of a scale to measure resident attitudes toward the social impacts of community festivals, Part II: verification of the scale. *Event Management*, 7(1),

Small, K, Edwards, D, & Sheridan, L. (2005). A flexible framework for evaluating the socio-cultural impacts of a (small) festival. International Journal of Event Management Research, 1(5),

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When asked to further describe the social/cultural outcomes of the event, the artists in particular made comments around the importance of these aspects to the event experience. Most comments related to networking and promotion, and toward the appreciation of the community for their work. These are important aspects to the further development of the artists in the community. selection of these comments are shown below, with the full list in the appendix.

Table 10: Community impact from participation

	% of
Community impact from participation	responses
Network development	28%
The event created a great network of who is out there in our community ready to share their works	
I connected with a photographer with whom I may be able to work with in the future.	
I loved developing working relationships with both Indigenous and nom-Indigenous artists who do not have their own space to hold open studios and offering them a venue and event to promote their passion.	
Meeting a lot of new people, along with friends who came to visit	
Meeting and appreciating others' interests/skills/experience	
Sharing a passion for art	
the sense of idenity, and being part of something bigger than my own little circle!this was helped by the great support from the event organisers	
Promotion and sales	26%
Able to show my art work in my studio	
Actually made me sort out my studio to have some gallery space!	
Advertising on a hand out map	
Being able to show my work and talk about the various techniques	
Exposure and promotion of my artistic practice	
It shows how many creatives are in our community, and identified those who are prepared to 'open' their studios	
Showcasing local works to a much broader sector of the community	
Showing of our home galleries and our way we work	
The exposure my business had	
The opportunity to show off what I do and how I have built my creative practice.	
I had one person interested in tuition. One definite commission to do a specific art work, and another who is considering a commission	
Appreciating heritage, culture and arts	20%
Having a good response from a small crowd at our gig.	
Seeing how interested the visitors were in seeing the studio & having a chat	
I loved that the public really appreciated us opening up our studios for the weekend.	
Putting my work 'out there' and getting some great feedback and supportive comments.	
Recognising & raising awareness of our shared history with our Indigenous community.	
Talking about my art to people who were interested.	
The excitement of the visitors to the studio from all over the valley and beyond, and interacting with some neighbors I didn't really know	
The fabulous response from the community. So many people out doing the trail and spending time with the artists.	

Community cohesion	10%
Community involvement	
Feeling part of our community	
Genuine one on one connection with the public	
Interaction with the community	
The community coming to the Garden to enjoy.	
Learning and development	6%
Feedback about artwork and studio	
Learning how to hold an event-hanging painting	
Seeing my work through others eyes	
Other comment	4%
Council support	
It is wonderful to work with a PMHC team that have a vision, that are passionate about this vision and allow us as creatives to thrive and fly Thanks Guys !!	
Social inclusion	4%
Seeing my work being seen by others and gauging their responses. Mostly very positive and interested.	
Seeing that locals DO appreciate the arts	

The sentiment of artists may be best summed up by this quote.

- I loved the enthusiasm and passion shown by the many visitors, they attended this event and many of the other art studios because of their interest and excitement about this type of event and their enthusiasm and support both in conversations and attending workshops and purchasing from our artists and pop up cafe were fabulous.
- I think the weekend really opened the eyes of the public to how many lovely talented artists we have in our area. The visitors loved the concept of the artists inviting them into their own private creative space and were loving the fact that the Art Trail was something new and different. Many people spent the entire weekend doing the trail. It has given us as artists the opportunity to showcase our work to a broad selection of the community. To have the council as the creators and the driving force behind the weekend has been fantastic.

Other heritage attractions also felt the importance of the event to the community.

It showcased the beautiful space we have created at Mrs York's Garden, for the Community to enjoy. Many more residents and visitors are now aware of the Gardens and the facilities available there for leisure and recreation. It raised awareness of the heritage value of the area with many sites within or bordering the Garden



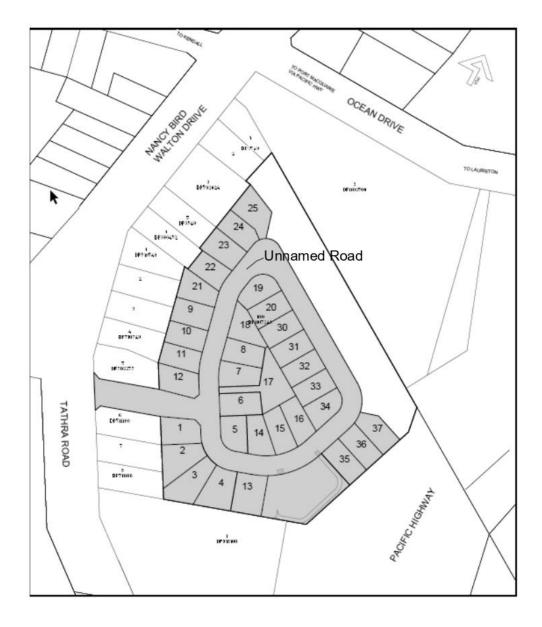
06 MAYOR'S SPORTING FUND - APPLICATIONS RECEIVED

CONSENSUS:

That the Mayor's Sporting Fund Sub-Committee recommend to Council the following allocations:

- 1. Phoebe Hempsell \$350 to compete at the Australian Youth Beach Volleyball Championships in Coolangatta, Queensland which were held on 28-29 April 2021.
- 2. Jasmine Hyde \$750 to compete at the 2021 Under 19s NSW State Team Netball Squad in the 2021 National Netball Championships which are being held in Hobart, Tasmania in July 2021.
- Naykisha Little-Legend \$250 to compete at the 2021 NSW Country Rugby Union Championship events in Sydney, Camden and Bathurst across June and July 2021.

DA2019/914 - 12 Tathra Road Kew



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2/26/2021

NSW Department of Planning, Industry and Environment

Home > About us

Kew Police Station and Official Residence

Item details

Name of item:	Kew Police Station and Official Residence
Other name/s:	Kew Police Station and Lockup Residence
Type of item:	Built
Group/Collection:	Law Enforcement
Category:	Police station
Primary address:	Old Pacific Highway, Kew, NSW 2439
Local govt. area:	Port Macquarie-Hastings

All addresses

Street Address	Suburb/town	LGA	Parish	County	Туре	
Old Pacific Highway	Kew	Port Macquarie-Hastings			Primary Address	

Owner/s

Organisation Name	Owner Category	Date Ownership Updated
NSW Police Force	State Government	

Statement of significance:

The Kew Police Station & Official Residence date from the late 19th century. This building contributes to the historic streetscape of the town, with its attractive form and character. The Kew Police Station & Official Residence is important for its long and continuous association with the provision of law and order, including police services in the local area.

Date significance updated: 02 May 06

Note: The State Heritage Inventory provides information about heritage items listed by local and State government agencies. The State Heritage Inventory is continually being updated by local and State agencies as new information becomes available. Read the OEH **copyright and disclaimer**.

Description

 Designer/Maker:
 W.L. Vernon, Government Architect

 Physical description:
 The original Police building was built as a Courthouse, Police Station and Lockup Residence and is now used as a Police Station and Lockup Residence. Two cells and an exercise yard are contained within the building. The building is constructed in brick with a corrugated iron roof.

Date condition updated:02 May 06

https://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4180086

1/3

ORDINARY COUNCIL 16/06/2021

2/26/2021	Kew Police Station and Official Residence NSW Environment, Energy and Science
Further information:	Materials Exterior: Brick, Corrugated Iron Roof.
Current use:	Police Station & Residence
Former use:	Courthouse, Police Station & Residence
History	

Historical notes: The original Police building was built as a Courthouse, Police Station and Lockup Residence, which was designed by the Government Architect, WL Vernon in 1899. It is now used as a Police Station and Lockup Residence. Two cells and an exercise yard are contained within the building.

Historic themes

Australian theme (abbrev)	New South Wales theme	Local theme
7. Governing-Governing	Law and order-Activities associated with maintaining, promoting and implementing criminal and civil law and legal processes	(none)-

Assessment of significance

SHR Criteria a) [Historical significance]	*
SHR Criteria b) [Associative significance]	*
SHR Criteria c) [Aesthetic significance]	*
SHR Criteria d) [Social significance]	*
Assessment criteria:	Items are assessed against the 🔁 State Heritage Register (SHR) Criteria to determine the level of significance. Refer to the Listings below for the level of statutory protection.

Recommended management:

Further Investigation needed. Any proposed building works should respect the existing building fabric, including the interior of the building.

Listings

Heritage Listing	Listing Title	Listing Number	Gazette Date	Gazette Number	Gazette Page
Heritage Act - s.170 NSW State agency					

https://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4180086

ORDINARY COUNCIL 16/06/2021

2/26/2021	Kew Police Station and Official Residence NSW Environment, Energy and Science	
heritage register		

References, internet links & images

Туре	Author	Year	Title	Internet Links
Written	Port Macquarie District Accommodation Survey, September 1990			

Note: internet links may be to web pages, documents or images.



(Click on thumbnail for full size image and image details)

Data source

The information for this entry comes from the following source:

Name: State Government

Database 4180086 number:

Every effort has been made to ensure that information contained in the State Heritage Inventory is correct. If you find any errors or omissions please send your comments to the **Database Manager**.

All information and pictures on this page are the copyright of the Heritage Division or respective copyright owners.

https://www.environment.nsw.gov.au/heritageapp/ViewHeritageItemDetails.aspx?ID=4180086

From: Geographical Names Board <<u>ss-gnb@customerservice.nsw.gov.au</u>> Sent: Friday, 16 April 2021 6:01 PM To: Simon Welch <<u>Simon.Welch@pmhc.nsw.gov.au</u>> Subject: Road Name Proposal Activity: 16 Apr 2021

Port Macquarie-Hastings Council Road Name Proposal Activity: 16 Apr 2021

Pre-Approved Road Name Proposals				
Reference	Road Name	Locality		
	Vernon Circuit	KEW	View	

The relevant parties, as per the Roads Regulations 2018 have been notified. If you have any questions please contact the GNB Secretariat. Regards,

GEOGRAPHICAL NAMES BOARD SECRETARIAT

Spatial Services | Department of Customer Service

346 Panorama Avenue, Bathurst NSW 2795

p 02 6332 8214

e SS-GNB@customerservice.nsw.gov.au

w www.gnb.nsw.gov.au | www.customerservice.nsw.gov.au



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3 Your Business and Industry

What we are trying to achieve

A region that is a successful place that has vibrant, diversified and resilient regional economy that provides opportunities for people to live, learn, work, play and invest.

What the result will be

We will have:

- A strong economy that fosters a culture supportive of business and ensures economic development of the region
- Townships, villages and business precincts that are vibrant commercial, cultural, tourism, recreational and/or community hubs
- A region that attracts investment to create jobs
- Partnerships that maximise economic return and create an efficient and effective business environment

How we will get there

- 3.1 Embrace business and a stronger economy
- 3.2 Create vibrant and desirable places
- 3.3 Embrace opportunity and attract investment to support the wealth and growth of the community
- 3.4 Partner for success with key stakeholders in business, industry, government, education and the community



12-Month Update

Priority 1 - Destination Management	Establish an effective Destination Management Framework for Council and Port Macquarie- Hastings tourism stakeholder organisations to work cooperatively to grow the local visitor economy.		
Actions for Review	Action to date	Future Priorities	Non-Council project Funding
1.1 Council consider the integration of the DMP 2020 to 2024 into Council's Delivery Program and annual Operational Plans.	Included via the Economic Development Strategy reference.	Ensure destination is considered and represented within the updated Economic Development Strategy	
1.2 Consider Council's budget for tourism and events to adequately resource the management, development and marketing actions of the DMP 2020 to 2024.	Annual allocation ongoing, additional funds sourced through Bushfire and COVID Recovery funding/grants.	Advocate for ongoing support for destination activities.	
1.3 Work cooperatively with relevant Council departments to ensure the priorities and actions of the DMP 2020 to 2024 are communicated and the visitor economy is an important consideration of Council's strategies and plans (see also Strategic Priority 2).	Ongoing through project delivery.	Advocate for the Rural Land Management Strategy to inform activation of the visitor economy in our rural areas and hinterland.	
1.4 Support the GPMTA Board and other tourism stakeholder organisations to review their Strategic Plan, including governance arrangements, to ensure the integration of priorities and actions of the DMP 2020 to 2024.	Complete - GMECD led a governance and strategy session with the GPMTA Board focused on succession planning, individual and group vision and goals setting, roles and responsibilities. The Board has continued this work.	Continued partnership with GPMTA.	

3/06/2021

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ORDINARY COUNCIL 16/06/2021

PMHC Destination Management Plan 2020 - 2024

June 2021

12-Month Update

	Regular meetings with the Marketing Sub-Committee and project related engagements are ongoing.	Ongoing	
1.6 Work with stakeholder partners to improve operator understanding and skills in key areas including digital marketing, sustainable business best practice, story-telling, cross-selling and packaging, and international ready product development.		Destination Eco Certification process has commenced. This will include delivery of sustainable tourism sessions for industry. Destination North Coast industry training program to roll out soon.	WWF bushfire recovery funding for Destination Eco Certification
1.7 Meet with Council's Economic Development and Cultural Steering Groups as required to update members on strategic tourism plans and initiatives and leverage cross-Council work.	On-going - updates on Destination Management Update, Destination Brand Review and COVID recovery projects provided.	Continued engagement on the Destination Brand Review and Marketing Strategy.	
1.8 Continue to engage tourism research consultants to provide detailed data for decision-making including tourism visitation, visitation perceptions and satisfaction. A better evidence-based understanding of occupancy is also required to plan improvements in seasonality and event timing.	Annual tourism monitor, plus new research including in-depth focus groups for the Destination Brand Review. Implementation of Uber Media mobile location data provider to track visitation trends.	Visitor profiling and satisfaction survey due in 2022. Review of Uber media platform.	

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PMHC Destination Management Plan 2020 - 2024

June 2021

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ORDINARY COUNCIL
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1.9 Develop an effective long-term approach to review and understand the community desires, social impact of the visitor economy and the 'pinch points' that may require action.	Medium term priority.	Future project scoping required.	
1.10 Work with key partners to monitor air access and develop approaches to increase the frequency, capacity and access to the region as appropriate.	Post COVID resulted in multiple discussions with airlines and the addition of REX and Air Pelican carriers.	Ongoing	
1.11 Provide input to improve planning around potential risks related to the local visitor economy including natural disasters, threats to natural and economic environments and ensure visitor safety and security is assessed and integrated into current and future Council plans and policies.	Many learnings from bushfires and floods to input to recovery processes and planning. Industry support and visitor messaging processes refined.	Ongoing	
1.12 Evaluate annual implementation of DMP 2020 to 2024.	Underway	Ongoing	
1.13 Work with industry on crisis preparedness including further developing the Emergency Management Communications Toolkit developed during the fires. Provide support and links to information, training and development to assist tourism businesses to plan for, respond to, and recover from crisis events both man–made and natural.	Emergency Management Communications Toolkit refined and redeployed for flood recovery, Industry Update communications embedded as process. Participation in Flood Recovery - Community Meetings to support business and understand emerging needs.	Ongoing	

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PMHC Destination Management Plan 2020 - 2024

June 2021

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	WWF bushfire
s will Opgoing	recovery funding
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stry.	Eco Certification
e	Ongoing

Priority 2 - Destination Experience Development	Further develop and package experiences across Port Macquarie-Hastings to provide a diversity of contemporary experiences to attract visitors year round.		
Actions for Review	Action to date	Future Priorities	Non-Council project Funding
2.1 Support the Koala Preservation Society in their major re- development project to become a world class attraction and conservation facility and to leverage significant visitor economy outcomes	Support delivered through PR and advocacy.	Ongoing	
2.2 Ensure Council continues to work with Local Aboriginal Land Councils (LALCs) to support LALC projects to connect visitors to Aboriginal culture and heritage.	Recent examples include Bicentenary event, Wakulda and Destination Brand Review.	Ongoing	Australian Government Regional Bushfire Recovery Fund
2.3 Create better linkages to strategies within the Cultural Plan including enhancing Council facilities, such as the Glasshouse, as hubs for cultural tourism activity (see also Strategic Priority 3).	Cultural Plan reviewed and new draft on exhibition. Includes strategies and actions to support linkages. Cultural Precinct Planning Project commenced.	Ongoing	
2.4 Work with National Parks and Wildlife Service and the Koala Preservation Society to develop and promote the Coastal Walk as an integrated experience linking Sea Acres and the Koala Hospital.	Yet to commence - medium term priority. Coastal Walk is currently undergoing repair works from flood damage.		

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2.5 Support the further development of Sea Acres as a visitor experience, including its capacity to act as an incubator for growing a wide range of indigenous experiences.	Medium term priority, initial focus has been support and promotion of Cultural Tours and workshop during Bicentenary Event. Focal point during LGNSW conference site visit.	Funding application submitted includes continued support of Cultural Tours at Sea Acres. Planning collaboration for future local aboriginal art and story telling opportunities.	
2.6 Work with NSW Forestry Commission and National Parks and Wildlife Service to identify new opportunities for nature based tourism product and experience development in State Forests (e.g. eco-accommodation, trails, an enhanced mountain bike hub and event management including motorsports) and to develop appropriate locations enabling commercial operator access to the hinterland sites.	Completed site investigation for eco cabin activation. Cross organisation (NPWS, FC & Kempsey Council) collaboration to explore Banda Banda Adventure Drive product development. Work interrupted by bushfires and now floods.	Aiming to reinvigorate in future.	Multi-agency funding
2.7 Support stakeholder organisation submissions to leverage government grant funding for product and experience development.	Ongoing and includes bushfire/flood recovery grants .		
2.8 Continue to improve the visual appeal of towns and villages across the Port Macquarie-Hastings area through ongoing Council beautification works including the Town Centre Master Plan and improve physical access for visitors, including inclusive tourism.	Development of a night-time projection onto the façade of the Historic Courthouse - Wakulda, weaving our stories as one.	Development of curatorial guidelines as addendum to Public Art guidelines.	
2.9 Link Council's place based community planning and local tourism work across the visitor sub-regions including Wauchope and Camden Haven and create cross regional collaborative themes and trails to grow regional dispersal.	Launch of Taste Trails on website.	Ongoing engagement to refine and improve Taste Trails including development of a touring map, in-store promotional items and cross promotion.	

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PMHC Destination Management Plan 2020 - 2024

June 2021

12-Month Update

2.10 Ensure tourism is considered as an important part of Council strategic land use, community and infrastructure planning to attract quality tourism and hospitality investment and provide exceptional experiences for visitors and the community across towns, villages and hinterland.	Ongoing	Advocate for the Rural Land Management Strategy to inform activation of the visitor economy in our rural areas and hinterland.	
2.11 Support the heritage sector to collaborate and to enable the growth and development of contemporary and integrated heritage experiences including the proposed development of the Port Macquarie Museum site.	Bicentenary Event planning and delivery focused on heritage activations, supported planning and delivery. Inclusion in Destination Brand and Cultural Plan Review processes.	Potential inclusion of heritage in Taste Trails.	
2.12 Encourage and support stakeholder initiatives that identify and develop opportunities to protect, enhance and showcase our natural assets, and elevate the importance of our natural environment and sustainability values.	As per 2.6 action to date, plus Council support for Beach to Beach Walkway.		

Priority 3 - Destination Event Development	destinati	Establish Port Macquarie-Hastings as a vibrant destination for year-round festivals and events that positively impact the visitor economy across the region.		
Actions for Review	Action to date	Future Priorities	Non-Council project Funding	

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PMHC Destination Management Plan 2020 - 2024

3.5 Support events to have strong environmental best practices.

June 2021

12-Month Update

3.1 Review Council's existing Event Plan and resourcing to enhance the strategies and actions to attract, procure and support the delivery of a vibrant and contemporary event calendar that appeal to key visitor markets and align with the seven experience platforms.	Complete - the 4 year Event Action Plan is due for review in 2024. Each year objectives within the plan are targeted. In the last year we have done such things as Develop a standard post-event acquittal for organisers to fill in, to ascertain how events are achieving their KPI's and keep results all in one place.	Reviewing the event application process to	
and the visitor economy including industry development activities for event and experience packaging.	Encouraging events to reach out to industry and allow offers/ promotions etc through to their event participants.	Updating destination information, images and assets that event owners can use to easily promote the destination offerings.	
3.3 Work with the Destination North Coast business events team to grow and attract mid-scale conferences to Port Macquarie-Hastings.	LG tourism conference events budget contributed to this	Continue to look for and support bids for conferences to be held in the region	
	Use of social media channels and website listings	Connecting events with each other so they can cross promote each other.	

The Council Waste Team to work with local

materials that can be collected in bulk from

contractors to allow commercial

events.

composting of some compostable

Copy of Document_Destination Management Plan Reporting Matrix_020621

Introduced the Events Waste

Guidelines for held on Council

Management Plan template and

property or sponsored by Council

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PMHC Destination Management Plan 2020 - 2024

June 2021

12-Month Update

3.6 Support event owners/organisers and local events developers by providing connections to community, volunteer groups and visitors.	team suggests local connections that may be able to assist with marketing, volunteer roles and other event owners that may be	Encourage event owners to create assets that Council socials can share and pass on the lists of Facebook groups in the region should they wish to reach out to the community	
---	--	--	--

Priority 4 - Destination Marketing	Review destination brand and marketing strategies to best position Port Macquarie-Hastings and its experiences to key visitor markets.		
Actions for Review	Action to date	Future Priorities	Non-Council project Funding
4.1 Evaluate results of marketing campaigns to ensure these provided effective results and to inform future campaign development.	Comprehensive evaluation and reporting include regular updates to EDSG and GPMTA.	Ongoing	
4.2 Engage specialist destination marketing consultants to undertake a brand review that considers consumer perceptions (current and potential visitors), key visitor markets, and tourism stakeholder views to clearly articulate a destination brand identity/positioning story for the area including creative brand communication messages and imagery.	In progress - consultation underway.	Presentation of draft brand strategy October 2021	
4.3 Prepare a 3-year Marketing Strategy and Marketing Implementation Plan based on brand review findings to guide Council promotional and visitor information initiatives to effectively package and promote drawcard experiences, the experience platforms and ensure 'cut through' to appeal to and attract key domestic and international markets visitor markets.	Initiating procurement in June.	Tender or RFQ process.	

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ORDINARY COUNCIL 16/06/2021

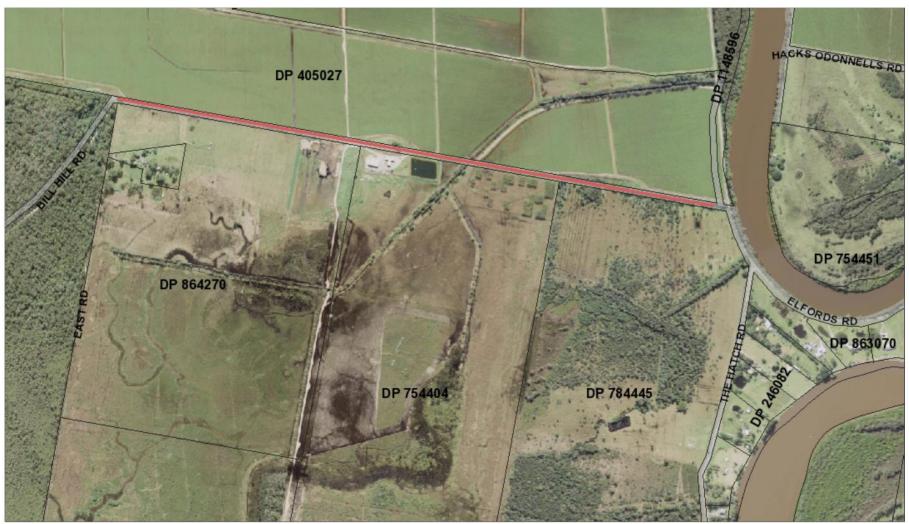
PMHC Destination Management Plan 2020 - 2024

June 2021

12-Month Update

4.4 Investigate opportunities to partner with Destination North Coast in relevant cooperative marketing campaigns (e.g. the currently proposed North Coast Caravan and Camping campaign).	Destination North Coast secured funding for digital trails development and industry development. Will partner with these projects.		
4.5 Investigate opportunities to partner with Destination New South Wales to leverage State Government marketing programs and initiatives.	Partnered with DNSW for LoveNSW cooperative marketing Sept-Nov 2020.	Funding application submitted for matched co-op Tourism Industry Marketing Support - Grants Program for marketing campaign during July and August. Outcome expected by June 30. Ongoing PR support and collaboration.	DNSW Co-Op Marketing matched funding
4.6 Work cooperatively with North Coast stakeholders in adjoining LGAs on cooperative marketing initiatives that provide good opportunities to generate awareness of Port Macquarie- Hastings to key domestic and international visitor markets (e.g. cruise tourism opportunities ex South West Rocks, international product ready itinerary development).	An example is Banda Banda Adventure Drive. More opportunities will be explored after the Brand Review is complete.		Multi-agency funding
4.7 Create a VFR program that highlights key experiences across Port Macquarie-Hastings to the community and their visitors that aligns with brand review findings.	For delivery second half of 2021. Project scoping next FY and rolled into new Brand implementation.		
4.8 Prepare a Tourism Signage Strategy that aligns with findings of the brand review and Marketing Strategy to create distinctive signage to encourage dispersal and promote the diversity of experiences across Port Macquarie-Hastings.	Medium term priority - yet to commence.		

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Item 11.04 Attachment 1

4 Your Natural and Built Environment

What we are trying to achieve

A connected, sustainable, accessible community and environment that is protected now and into the future.

What the result will be

We will have:

- Effective management and maintenance of essential water, waste and sewer infrastructure
- A community that is prepared for natural events and climate change
- Sustainable and environmentally sensitive development outcomes that consider the impact on the natural environment
- Accessible transport network for our communities
- Infrastructure provision and maintenance that meets community expectations and needs
- Well planned communities that are linked to encourage and manage growth
- Accessible and protected waterways, foreshores, beaches and bushlands
- An environment that is protected and conserved for future generations
- Renewable energy options that are understood and accessible by the community

How we will get there

- 4.1 Provide (appropriate) infrastructure and services including water cycle management, waste management, and sewer management
- 4.2 Aim to minimise the impact of natural events and climate change, for example, floods, bushfires and coastal erosion
- 4.3 Facilitate development that is compatible with the natural and built environment
- 4.4 Plan for integrated transport systems that help people get around and link our communities
- 4.5 Plan for integrated and connected communities across the Port Macquarie-Hastings area
- 4.6 Restore and protect natural areas
- 4.7 Provide leadership in the development of renewable energy opportunities
- 4.8 Increase awareness of issues affecting our environment, including the preservation of flora and fauna





PORT MACQUARIE-HASTINGS

Kindee Bridge Replacement Investigations

COMMUNITY ENGAGEMENT REPORT

May 2021



Item 12.01 Attachment 1 Page 393

Project name	Kindee Bridge Replacement Investigation	
Project manager	Stephen Wood	
Consultant	NA	
Engagement Officer	Oceana Kovacs	

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PORT MACQUARIE-HASTINGS

1. BACKGROUND

Kindee Crossing Suspension Bridge, built in 1936 is approaching 100 years old in 2036 and Council is investigating options for the bridge's replacement to continue to provide safe and reliable access across the Hastings River to the Kindee Valley from the Oxley Highway, west of Long Flat. The bridge currently has a 10 tonnes vehicle load limit and heavy vehicles are required use Pipeclay Road, a long and winding unsealed road to access the valley.

The existing bridge is considered an extremely rare and unusual form of bridge construction and is protected as a local heritage item in the *Port Macquarie-Hastings Council Local Environment Plan 2011*. A Conservation Management Plan was prepared in 2019 to guide the ongoing maintenance and management of the structure, and is an important record of the heritage significance of the structure.

Various investigations into the condition of the existing bridge have occurred between 2016 and 2020. These have identified a number of critical maintenance and rehabilitation activities. Although repairs and maintenance activities have progressed over recent years with an average actual expenditure in the order of \$140,000 per annum over the past 5 years, the current condition of the bridge remains poor with several critical elements still requiring rehabilitation. To ensure the bridge remains a safe and reliable crossing, funding of the rehabilitation and repair of key structural elements is critical. Due to the form and materials of original construction, the existing bridge may still reach its end of useful life as a vehicular bridge in the next 10 to 15 years, requiring consideration of alternative crossing options.

Council has identified key limitations with the existing bridge, and the strategic need for an alternate bridge crossing at this location including:

- Existing bridge is in poor condition with key structural elements requiring rehabilitation.
- High risk of closure due to misuse or overload, deterioration of critical elements, or natural disaster.
- Increasing cost and time associated with maintenance activities, including ongoing impacts to the community due to bridge closures.
- Heavy vehicle access and emergency vehicles cannot use the existing Kindee Bridge, limiting
 growth and economic opportunity for residents of the Kindee Valley.

Council has prepared a Strategic Options Report that has identified five (5) alternative crossing route options to continue to ensure access across the Hastings River to the Kindee Valley. These options were assessed taking into consideration current transport needs, long-term maintenance, topography and flooding, environmental heritage constraints, and impacts on the community. The preferred option was identified as Option B - New single-lane high-level bridge located immediately upstream of the existing bridge utilising the existing road approaches.

Council consider consultation with the community an important part of the project planning, with the aim of keeping the community well informed and inviting feedback throughout the project. Council spoke with the community and stakeholders between 18 March - 20 May 2021 to introduce the project and seek feedback on potential locations for a new bridge. As well as proposed bridge replacement options, this engagement asked the community for feedback relating to the existing bridge, and ideas to preserve and honour the significance heritage of the Kindee Bridge.

The respondents clearly show a strong preference for Option B, as the preferred option of the replacement bridge, and noted that there was a strong expectation that the project should look to preserve the existing bridge.

A report will go to the Council June meeting 2021 to make a recommendation on the future of the preferred option and the existing bridge.

PORT MACQUARIE-HASTINGS

This Consultation Report outlines the communication and engagement activities carried out around the proposed investigation options for the Kindee Bridge to better understand what the community value, their concerns and suggestions relating to the preferred proposed options and heritage significance of the bridge.

It also provides a summary of the comments and ideas received during this period.

The feedback has been grouped into categories and sub-categories and is presented with a more extensive summary in Appendices.

2. ENGAGEMENT APPROACH AND COMMUNICATIONS

The consultation for this project was initially scheduled from 18 March to 8 April 2021. The engagement period was extended to 20 May 2021 due to the March 2021 floods to allow residents sufficient time to comment on this project.

The aim of the community and stakeholder consultation was to:

- Inform the community and stakeholders of relevant detailed information to allow for informed submissions and feedback
- Provide a range of opportunities and avenues for the community and stakeholders to provide feedback
- Help gain a better understanding if, and why, the existing Kindee Bridge is/is not important to the community and stakeholders
- Give the community and stakeholders an opportunity to provide ideas for the future use of the existing Kindee Bridge
- To be open and transparent on how this feedback will be used in the decision-making process.

When "n = XXX" is referenced in the report this refers to number.

The community engagement process included:

Engagement activities

Councillor Briefing

A Councillor briefing was held on 10 March 2021, to inform Councillors of the outcomes of investigations to date and the proposed engagement process.

Letters to residents and email correspondence

On the 22 February, letters were mailed to 8 directly and indirectly potentially affected property owners; and 11 community stakeholders. The letter outlined the upcoming consultation and invited stakeholders to attend an initial information session on 16 March 2021 before wider consultation with the community was undertaken.

On 22 April 2021 an information pack including a letter, hard copy of the survey and supporting documents was sent to all (n=97) residents in Kindee Valley, Long Flat and Ellenborough.

**Some residents did not received the letter via mail due to postal service delays.

Regular emails were sent to key community and government stakeholders throughout the duration of the consultation.

Have Your Say

A page was used for the community to access the documentation, interactive online map, taking the survey and community drop-in sessions.

COMMUNITY ENGAGEMENT REPORT - Kindee Bridge Replacement Investigations

Online survey and written feedback	49 surveys completed 4 submissions
Hard copy survey and supporting documents Available at the Long Flat Shop and mailed to all residents in Kindee, Long Flat and Ellenborough	Over 200 copies distributed
Community Drop-in sessions	50 people
A presentation of the Kindee Bridge Replacement Investigations was delivered. Following the presentation, there was an open forum and Q&A session.	

A total of 53 people participated in the community engagement

- A total of 53, people provided feedback via the online survey, hard copy and submissions
- Approximately 80, Have Your Say A5 flyers were distributed
- We spoke with approximately 50 people at the different Community Sessions.

2.1. Stakeholders

Identified stakeholders were:

- · Kindee Valley directly impacted property owners
- Kindee Valley, Long Flat and Ellenborough residents
- The broader community
- Government stakeholders
- Hastings Hinterland and surrounds Council Community Action Groups
- Wauchope Chamber of Commerce and Industry
- Wauchope Historical Society

3. COMMUNICATIONS

During the consultation period, a communication campaign was implemented to raise awareness of the survey and project. The aim of the campaign was to encourage active participation of residents in the Kindee, Long Flat and Ellenborough areas.

A range of communications were developed to enable the community and stakeholders to give informed feedback. This included providing information on the structural condition and heritage significance of the existing Kindee Bridge, the possible outcomes being considered and the likely cost implications for each.

Channel	Reach	Date
Dedicated Have Your Say	389 visitors	18 March to 20 May 2021
webpage	49 engaged	
	Document Downloaded 77	
Social media	Seven Facebook posts on the	Throughout the course of the
	main page and Group	consultation period at key
	potential reach 3000	dates
Flyers and corflute	80+ A5 flyers and	A different stages during the
	5 x A4 Posters distributed	consultation
	2 x corflute	Kindee Bridge, Long Flat Shop
		and Ellenborough Hall
Enewsletter	2000 potential reach	March - clicks 353 people
		April - clicks 239 people

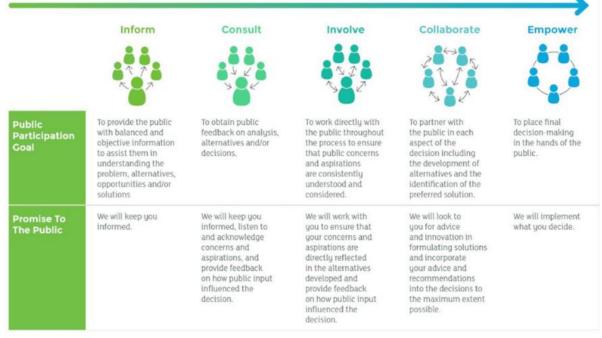
COMMUNITY ENGAGEMENT REPORT - Kindee Bridge Replacement Investigations



Social media commentary shows that the Kindee Bridge is of significance value and heritage importance to the surrounding residents.

*see Appendix 1.





4. ENGAGEMENT ACTIVITIES & RESULTS

4.1. What we heard

Community members were encouraged to offer an opinion on:

- What was their preferred access option
- How they feel about the existing bridge
- What they thought should happen to the existing bridge in the future
- If they had any ideas for its future preservation

Key feedback raised by the community during the consultation included:

- Option B as the preferred option
- Build the new bridge soon rather than later
- · Keep the bridge rather than demolish and consider alternate uses
- Retain the existing bridge as a footbridge
- Register the Kindee Bridge on the State Heritage List
- Upgrade Pipeclay Road if Option E4 is selected

The preferred route option for a replacement bridge was Option B, however a number of respondents thought the bridge should be retained in some form. Respondents would like to see the bridge promoted as a footbridge to encourage tourism to the Hasting Hinterland. Popular suggestions for the future use of the bridge included a shared path/cycle ways, markets, or other entertainment uses such as festivals.

There was a small proportion of respondents that understand the bridge has a limited life cycle.

A number of respondents identified the financial resources required to maintain the existing bridge would be better spent building a replacement bridge sooner.

4.2. Survey

The survey was open from 18 March until 20 May 2021. During that time, 49 responses where collected from community members within the Hastings Hinterland postcode area.

To capture feedback, a short online survey on the potential outcomes of the Kindee Bridge was developed. The survey had a mixture of qualitative and quantitative questions.

The following quantitative questions were, asked during the consultation process.

- 1. Which option do you prefer?
- 2. How important to you are the particular features and functions of a new bridge?

Please note that due to the nature of the survey, not all respondents answered every question. The number of respondents answering each question is marked as "n = XXX" accompanying that question.

A summary of the information provided through the feedback forms is detailed below.

Key finding of survey results:

When asked which option do you prefer there was a clear preference for Option B (n=34) new bridge immediately upstream of the existing bridge. This was followed by Option E4 (n=8) Oxley Highway to Pipe Clay Road Alignment C east of Long Flat.

The remaining six respondents allocated the following: Neither (n=4)Option C - Boat Harbour Lane to Kindee Road (n=1)Option C2 - Boat Harbour Lane to Kindee (n=1).

The feedback received indicated Option B was preferred as it followed a similar alignment to the existing bridge, provided direct access to the Kindee Valley, and had less impact on the surrounding environment, minimal impact on property owners and flood immunity.

While some respondents indicated **Option E4** as the preferred route, the support was not as strong for this preference due to the following factors;

- it is not the original access point to the Kindee Valley
- a detour for residents of the Kindee Valley
- Pipeclay Road is an unsealed, windy road that people feel is not safe
- Pipeclay Road would have to be upgraded.

In question 2, respondents were asked to tick a box for nine options and to indicate how important or not important they felt about each of the nine features and functions to a new bridge.

Of the 49 respondents, the majority felt the following factors were either very important or important:

- Reduce the impact on the surrounding landscape and waterway
- the new bridge provides flood immunity
- provides access for heavy vehicles
- new bridge to occupy the existing road
- to improve access for road users accessing the Kindee Valley
- to minimise the long-term maintenance costs for the new bridge
- to minimise private property impacts on potential property acquisition.

From the responses there was an equal spilt when asked to consider the following:

to minimise impacts on the environment including existing vegetation, and habitats

- to recognise existing heritage aspects of the original suspension bridge in a new concrete bridge
- to opt for lower construction costs.

The table below provides the responses for question 2 "How important to you are particular features and functions of a new bridge (please tick one option for each feature)".

			,	
Option	Not important	Somewhat important	Important	Very important
The new bridge to occupy the existing road to reduce the impact on surrounding landscape and waterway?	7	5	11	26
Provide for Heavy vehicle access to the Kindee Valley?	6	5	10	28
To recognise the existing heritage aspects of the original suspension bridge in a new concrete bridge?	19	11	5	14
To minimise impacts on the environment including existing vegetation, and habitats?	7	12	15	15
To opt for the lowest construction cost?	18	11	14	5
To minimise the long-term maintenance costs for the new bridge?	3	9	16	21
To minimise private property impacts and potential property acquisition?	5	10	13	21
To provide high-level flood free access to the Kindee Valley in lieu of a low level crossing of the Hastings River?	1	3	11	34
To provide improved access for road users accessing the Kindee Valley?	4	6	12	27

*see Appendix 2.

Other comments

Respondents had the opportunity to provide further comments in the survey, via three open-ended questions. A total of 102 comments were received from 53 respondents and these comments were coded to capture the sentiment of the feedback. While the questions were different, the responses were not always specific to those particular questions. As many of the topics/opinions were common across the three questions, the verbatim comments were categorised and themed under one code frame.

The following questions were asked during the survey:

- Is there anything else the bridge should have?
- We understand the cultural significance of the Kindee Bridge and the historical importance to the local community. In preparing for the long term (10-15 years), we would also like to hear your ideas on an acknowledgement of the original timber bridge. Including physical and digitals means to showcase the heritage value.

Below is a summary of the verbatim comments in the survey and submissions. These comments reflect the sentiment of feedback received at the in-person community drop-in sessions.

COMMUNITY ENGAGEMENT REPORT - Kindee Bridge Replacement Investigations

*see Appendix 3.

The number of respondents that made a comment is marked as "n = XXX" in the graph.

	Number of comments
Heritage significance and value	45
 one of the last bridges of its kind still in use in NSW, and the Southe Hemisphere described the importance of its place in the history of the area respondents mentioned that many Port Macquarie-Hastings heritage have been lost over the years and it is important that heritage items like the old brid maintained for future generations local icon landmark gateway to the Kindee valley features of the bridge including its rare and unique design good example of our engineering past, while others described its favourable visual appeal and beauty State Heritage listing required (n=4). 	e items Ige are
Retain as a footbridge	25
some would like to see the bridge retained as a footbridge opportunity to promote the bridge as a tourism destination there is a perceive notion that as a footbridge the ongoing cost will no keep the scenic views/new bridge not to block old bridge (n=5). "The bridge could be used as a foot bridge so tourist and local comm bridge. Also consider a picnic table close by to sit and enjoy a meal."	nunity can enjoy the
"Keep the bridge as walking bridge or heritage landmark" survey	
Tourism opportunity - promote the heritage unique opportunity to promote the bridge as a tourism destination ra make it a destination for day trips market stalls, a unique event space Council does not do anything to promote the Hasting Hinterland some responses suggested leasing arrangements in an attempt to re of the on-going maintenance. include as part of the walking trail map have a viewing platform	
do not block the view of the existing bridge with the new bride many memories and stories associated with it.	a sultural and tourist
do not block the view of the existing bridge with the new bride	e, cultural and tourist

	State Heritage Listing Required 4
	 one of the last bridges of its kind still in use in NSW, and the Southern
	 Hemisphere Council has previously demolished heritage items under the LEP
ľ	uncil should work with the community in getting the existing bridge itage listed with state or commonwealth significance so that it can remain a historical draw card for the local area" survey
	Heritage interpretation and remembrance 11
	remember the existing Kindee Bridge with some sort of exhibition at a local hall/pub or tourist information display virtual or visual story telling (mobile app) a plaque either on the replacement bridge would be suitable reviewing platform and story boards
	New Bridge 49
	access to allow for heavy and emergency vehicles (=n5) one lane bridge (n=1) new bridge - two lanes (n=6) flood immunity (n=5) build a replacement bridge sooner (n=5) Pipe Clay Road needs to be sealed if it will be option E4 progresses (n=4) new bridge to reflect the design of the existing bridge (n=4) water security to cope with population growth (n=2) limit access to 10 tone (n=2) keep the scenic views/new bridge not to block existing Bridge (=9) maintain the quiet character and atmosphere of the Kindee Valley (n=2) existing bridge to remain as a footbridge - during construction of a new bridge (n=4). *The key items have been grouped under the main theme "new bridge" and the "n=XXX" is the number of comments for each specific topic/issue. "Access for Heavy Vehicles is important. Bridge to be built on a side angle so that the corners are not as sharp. Can't use pipe clay road as it is very dangerous" survey "The residents of Kindee need a new bridge as soon as possible. I feel the timeframe of 10- 15 years is overly excessive."- survey
	Road safety 16
	general road safety was a safety for respondents (crossing the river and making sure there is good visual and the turns are not too sharp) seal Pipe Clay Road new bridge after O'Neil's corner - flood resistant (connects key roads).
	Ongoing maintenance and repair of the existing bridge 14

- cost should not be a factor
- maintain both bridges ongoing
- safety standards have changed and it does not meet these new standards
- these responses talked mostly about the high cost to refurbish the existing Kindee Bridge and about the need and cost for on-going maintenance
- some suggested that the costs outweighed potential benefits of keeping the existing bridge
- becoming unsafe as it ages either with or without the proper maintenance.

"It will cost a lot to maintain the existing bridge and be a cost to rate payers". - survey

4.3. Community sessions

Multiple consultation activities were carried out to provide a range of opportunities for the community and stakeholders to understand and provide feedback on the project.

Project staff were available at these sessions to answer questions and provide further information.

Approximately 55 people attended the consultation events. Through these conversations, people had the opportunity to express the community value of the bridge and understand the costs involved for the different options.

Community information sessions	Attended
16 March from 6pm to 8pm @ Long Flat Community Hall	10 people
Thursday 6 May, 6pm to 7:30pm @ Ellenborough Hall	30 people
Thursday 13 May, 5pm - 6pm @ online "zoom"	1 people
Thursday 12 May, 11am - 12pm @online "zoom" Government stakeholders	6 government stakeholders
Tuesday 18 May, 4pm - 7pm @ Ellenborough Hall	2 people
Thursday 19 May, 9:30am - 10am @ Wauchope RLS	8 Wauchope Historical Society people

A summary of the key points raised by the community is outlined below:

Existing Bridge

- the bridge is of significance heritage importance to the community
- strong preference to keep the existing bridge and consider alternate uses (pedestrian / tourism opportunity)
- existing bridge is an important community asset and preference shown to retain, instead of demolition and interpretation
- preference to leverage existing structure as a tourist attraction to encourage tourism in the hinterland
- acceptance that existing bridge may reach its end of useful life in the next 10 to 15 years assuming ongoing maintenance

New Bridge

- need for reliable and safe crossing of the Hastings River
- preference to construct a new structure with no load limits sooner rather than later
- · Pipe Clay Road is not suitable as an alternate access, unless it was sealed
- · crossing required to improve access of heavy vehicles
- single lane bridge to match current level of service appropriate
- · preference to spend money required for maintenance on the new bridge

- agree with Council's approach to proactively address access over the Hastings River at this location
- ensure limited impact to access during construction to prevent impacts on community and local schools.

4.4. Formal/Written Submissions

During the consultation period, Council received four written submissions via email. The below reflects the written submission and feedback from community meetings.

Both submissions referred to the historic significance of the existing Kindee Bridge and voiced their opposition to any option that would see the existing Kindee Bridge demolished. All also outlined the importance of the bridge to be kept in use.

Respondents stated that every effort should be made to conserve the Bridge for other re-use options. The feedback acknowledges there may be costs involved in the retention of the existing Kindee Bridge, but emphasised that cost should not decide its future. Community sentiment stated that a price cannot be placed on heritage and Council has an obligation to conserve its heritage.

All discussions/submissions confirmed that the existing Kindee Bridge is not currently listed on the State Heritage Register but indicated the bridge may be worthy of inclusion and invited Council to discuss the possibility of listing the bridge on the State Heritage Register.

The Wauchope Historical Society submission agreed with the high historic significance and heritage values of the existing Kindee Bridge. The submission also identified the existing bridge as an important Kindee Valley icon. The submission detailed the demand for heritage and cultural tourism and identified an area for economic growth within this sector. It stated that the closure of the Kindee Bridge to traffic presented a unique opportunity to use the bridge as a footbridge for promoting the Hastings Hinterland.

The submissions also indicated that:

- Preferred Option E4 Oxley Highway to Pipe Clay Road east of Long Flat connect to Beachwood and other areas of the Hinterland (2 submissions)
- Option B, preferred preference and people will have easier access to the review the existing bridge (1 submission)
- Pipe Clay and Kindee Valley have potential as an agricultural and farm stay region
- Include the Kindee Bridge as part of a scenic trail
- Opportunity for tourist attraction and hinterland economy
- Suggest that a Friends of Kindee Bridge be formed to preserve the bridge
- Suggest to work in collaboration with Council to develop a restoration plan for the existing bridge
- Two lane bridge to be constructed to provide for future development and safety
- The replacement Bridge to have manual river height gauge plates to be as back up when the automatic flood warning gauge is down.

*see Appendix 3.

5. RESULTS SUMMARY

Results show that the respondents' preference for a replacement bridge is Option B, to be built sooner rather than later. The majority of respondents indicated that it is important for any new bridge to have access for a heavy vehicles and have a high-level flood immunity. Respondents identified the existing road alignment of Option B would improve access to the Kindee Valley. While some respondents preferred Option E4, the majority expressed that for this option to progress, Pipeclay Road would need to upgraded.

There was a small proportion of respondents that understand the bridge has a limited life cycle and indicated the ongoing maintenance and repair costs would be better spent on building the replacement bridge sooner.

The heritage value of the bridge is of importance to respondents, with some requesting Council take steps to have the bridge listed on the State Heritage Register. Some respondents indicated that they may take their own actions to do this or to form a "Friends of the Kindee Bridge" Group to help preserve the heritage of the bridge. Respondents would like to see the Bridge promoted as a footbridge to encourage tourism to the Hastings Hinterland.

Further consultation with the community is important to continue to hear the community's ideas about potential heritage interpretation of the existing bridge including opportunities for enhanced recreational facilities, such as tourism, education, heritage studies, continue use and potential adaptive reuse.

While not directly linked to the Bridge, a majority of respondents expressed that they would like to see more support from Council to promote and encourage tourism in the Hastings Hinterland by encouraging tourists to take day trips to the local different local villages.

6. NEXT STEPS

- Report to Council: June 2021
- Report to back to community: July 2021 on the outcome of the June Council meeting.

APPENDIX 1: COMMUNCIATIONS AND COLLATERAL

Have your say page

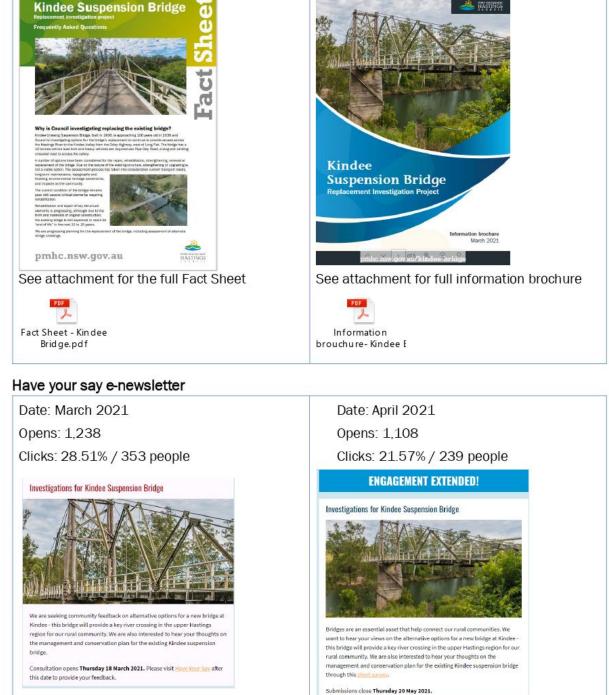


Visit <u>www.pmhc.nsw.gov.au/kindee-bridge</u> to view the Have Your Say page, factsheet and information brochure.

Poster



Fact Sheet and Information Brochure Kindee Suspension Bridge



16

Letter to residents

22 February 2021 to:	Refers	26 April 2021	Refers to:
Dear Resident, Long term planning for Kindee suspension bridge Kindee Crossing Suspension Bridge, built in 1936, is approaching 100 year well past its original design life. Council has invested significant funds in rec complete oritical maintenance work at Kindee Bridge, and we will continue t bridge as required to ensure a safe and reliable oroscing of the Hastings RM As a valued member of the Kindee community, we are inviting you to attend to learn more about our management for the Kindee Suspension Bridge and planning to provide a safe and secure river crossing for the community. When: Luceday 16 March, 6pm to 7.30pm Where: Long Flat Community Hall If you are unable to attend the briefing session, we will be running broader of consultation from late March to mid-April 2021. This will provide a turther of with our project officers at a indocine measure or an online information session	ent years to o maintain the er at this looation. a briefing session t the long-term ommunity oportunity to speak	Deer Resident. Long term planning for Kindsa suspension bridge. Middee Crossing Suspension Bridge, tuilt in 1930, is to cryptal design its: Council her instate significan ensure a serie and values crossing of the Hasting D We understand that the is a difficult time for extro- vorting there to reacres access to properties, repair working there to reacres access to properties, repair working there to reacres access to properties, repair working there to reacres access to the suspension are proper for the future. As a value member of the <u>kindee</u> community, we as provide a set and access the crossing for the <u>kindee</u> 30 provide a set and access the crossing for the <u>kindee</u> 30 provide a set and access the crossing for the <u>kindee</u> 30 When: Thursdey Kind, Spin to Tyme When: Churcher Schlegen to 	It funds in recent years to complete critical bitwar at the bidge as required to bitwar at this location. When at this location. In the second second second second second or community, and our cognitisation. Our staff are assessed to assess and bidges, whill take to bidge it significations who are providing descater relief enter to progressing this important project so we re initing you to attend a briefing session to spencing Didge and the long-term planning to mumby.
an opportunity to share your feedback on Council's proposed solutions. Please RSVP your attendance by email to <u>engagement@pmhc.nsw.gov.au</u> or 4cm. Monday 15 March.	6581 8111 by	fill this in and either post it back or drop it in the box received by Thursday 20 May, 2021. If you have any questions, please don't hesitate to o Dosana Koveso via email ensagement@emitc.naw g	at the Long Plat shop. All feedback to be
If you have any questions, please don't hesitate to contact Council's Commu Officer, Oceana Kovacs on the above.	inity Engagement	Yours sincerely,	A CAN BEEL CITY
Yours sincerely,		Comeron Hewkins	
Cameron Hawkins Group Manager Infrastructure Planning		Group Managar infrastructura Pienning	
Letter and email sent on the 22 Feb to 8 directly and indirectly potential property owners and 11 community government stakeholders.	ly affected		2021 to all residents in , Ellenborough (n=26) and

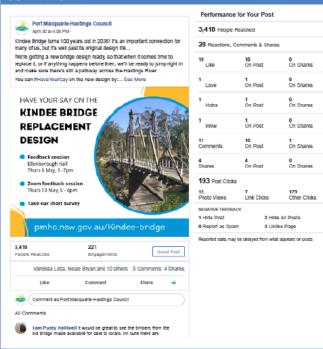
Social media

SOCIAL MEDIA	
30 April 2021	https://www.facebook.com/pmhcouncil/photos/a.168926823158807/41741 81929299923/
1 May 2021	https://www.facebook.com/events/782344639087235/?post_id=78234465 5753900&view=permalink
11 May 2021	https://www.facebook.com/pmhcouncil/photos/a.168926823158807/42065 09592733823/
12 May 2021	https://www.facebook.com/events/605253160869694/?post_id=60525318 7536358&view=permalink
12 May 2021	https://www.facebook.com/groups/Wauchopewire/permalink/28312390671 38874/
13 May 2021	https://www.facebook.com/events/789996934975455/?post_id=79631103 7677378&view=permalink
19 May 2021	https://www.facebook.com/events/499876947822015/?post_id=49987696 1155347&view=permalink
	https://twitter.com/pmhcouncil/status/1391976062737805315
MEDIA RELEASE	
17 April 2021	https://www.pmhc.nsw.gov.au/Lists/News-Listing/2021-Media- Releases/March-2021/Working-with-the-community-to-design-for-the-future

COMMUNITY ENGAGEMENT REPORT - Kindee Bridge Replacement Investigations

MEDIA COVERAGE				
18 March 2021	https://portapp.com.au/news-sport/news/future-of-kindee-suspension-bridge- up-for-community-discussion?id=6052a5178a55dc04223145af			
17 May 2021	<u>https://www.portnews.com.au/story/7252632/approach-to-kindee-bridge- replacement-options-planning/</u>			

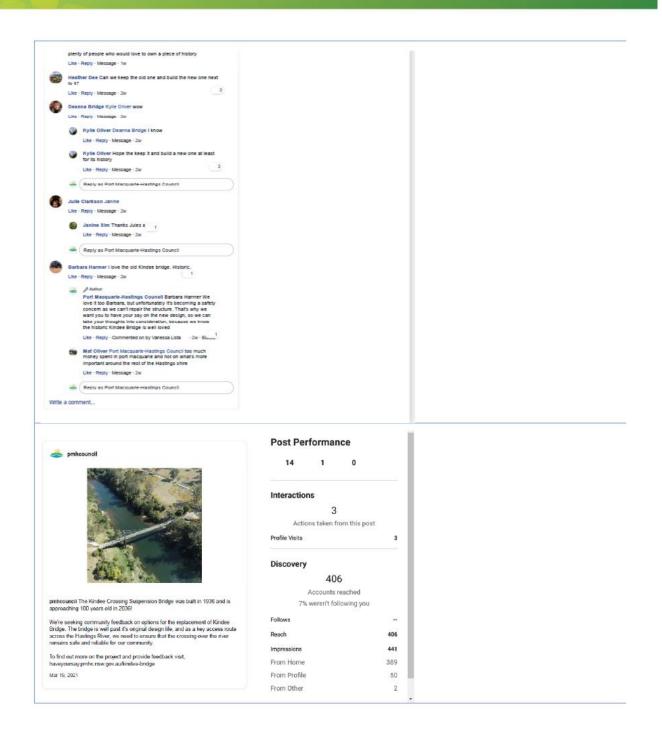
Social Media



COMMUNITY ENGAGEMENT REPORT - Kindee Bridge Replacement Investigations

ATTACHMENT

PORT MACQUARIE-HASTINGS



Port Macquari	c-Hastings Council			5,253 People Re	ached	
	d the historical Kindee Bridg n, as a preparation measure			37 Likes, Comme	nts & Shares	
	old bridge as it nears it's dea			28 Likes	18 On Post	10 On Shares
	get this right, so we've organ laveYourSay and learn more			1 Comments	1 On Post	0 On Shares
Joining us online vi Visiting Ellenboroug	a Zoom on Thurs 13 May at 5 h Hall on Tucs 18 May, 4 - 8 ion with us on Wed 19 May, 1	pm	m - 50m	8 Shares	8 On Post	0 On Shares
the Wauchope Libra	y Is and survey are available a			152 Post Clicks		
	g and provide feedback visit	https://fal.cr	V3ih1Vn	22 Photo Views	3 Link Clicks	127 Other Clicks
Real						
			-	1 Hide Post 0 Report as Spam		e All Posts ike Page
	EEE CONTRACTOR					
253 cople Reached	189 Engagements	Во	ost Post			
			STREET, STREET, ST			
Ella Jenkins, Christi	ne Leamond and 16 others	1 Comment	8 Sharea			

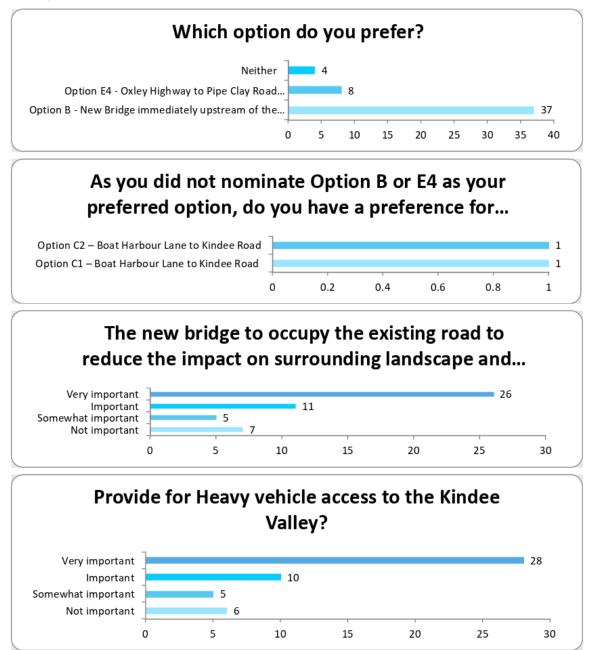
COMMUNITY ENGAGEMENT REPORT - Kindee Bridge Replacement Investigations

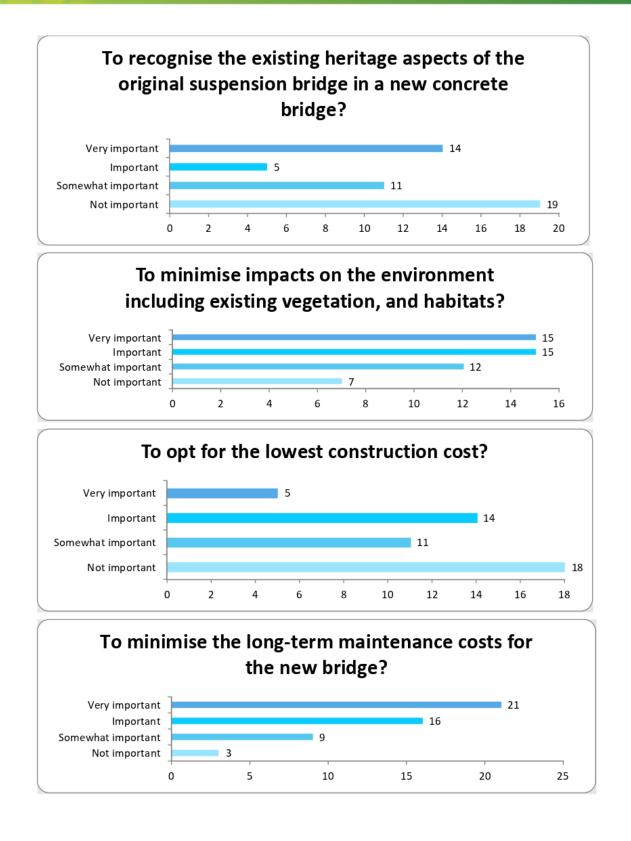
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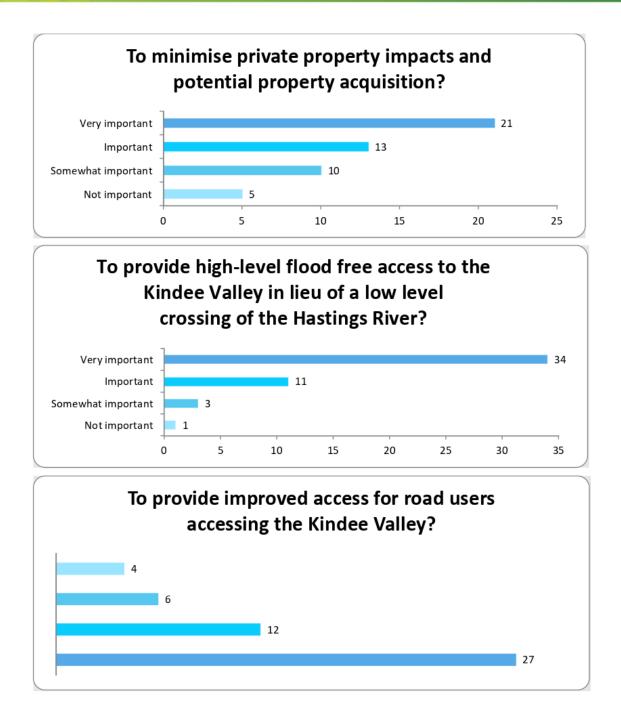
Item 12.01 Attachment 1

APPENDIX 2 - ENGAGEMENT FEEDBACK

Survey







Feedback and PMHC Responses

Theme/Feedback	PMHC Response
Heritage significance and value	
 one of the last bridges of its kind still in use in NSW, and the Southern Hemisphere described the importance of its place in the history of the area respondents mentioned that many Port Macquarie-Hastings heritage items have been lost over the years and it is important that heritage items like the old bridge are maintained for future generations local icon landmark gateway to the Kindee valley features of the bridge including its rare and unique design good example of our engineering past, while others described its favourable visual appeal and beauty State Heritage listing required. 	 Noted Heritage significance is recognised via the National Trust Register, PMHC LEP 2011 and the Kindee Bridge Conservation Management Plan. Provided protection under the Heritage Act 1977 via listing in the PMHC LEP 2011. Not listed in the State Heritage Register, but notification to the Heritage council is required for any change of use. It is unlikely that State Heritage listing would provide sufficient to funding for the rehabilitation, interpretation and adaptive re-use, and on-going maintenance of the existing bridge, although may be considered to be a viable option for funding in the future.
Retain as a footbridge	
 some would like to see the bridge retained as a footbridge opportunity to promote the bridge as a tourism destination there is a perceive notion that as a footbridge the ongoing cost will not be as high keep the scenic views/new bridge not to block old bridge. Tourism opportunity - promote the heritage	 Noted Council implemented the CMP in 2019 and will continue to maintain and repair the bridge At this time no decision has been made on the future of the existing Bridge
unique opportunity to promote the bridge as a	Noted
 tourism destination rather than pass by make it a destination for day trips market stalls, a unique event space Council does not do anything to promote the Hasting Hinterland some responses suggested leasing arrangements in an attempt to regain some of the costs of the ongoing maintenance. include as part of the walking trail map have a viewing platform do not block the view of the existing bridge with the new bride many memories and stories associated with it. 	 The bridge is unique and appeals to a section of visitors to our region. We promote dispersal through suggested day trips and itineraries; we will look to include Kindee Bridge where relevant To become a destination for day trips additional supporting visitor experiences or activities are required to attract visitors. Unique and immersive experiences are very attractive to visitors now. For example the Pappinbarra community runs the Grown & Gathered markets. It is key that the offering is local, quality and consistently available. Would there be interest in leasing the asset? Who would potentially do this? Dispersal is key to the promotion and attraction of our region as a whole. Council promotes businesses and experiences in the hinterland that are open and accessible to visitors. These can be seen in the Official Visitor Guide, the destination website and social media platforms.

		 Council promotes walking trails through a variety of platforms; these are very popular with locals and visitors. Please contact Council and advise it there are specific existing walking trails in Kindee that should be included? It would be key to understand why this would be valuable and what visitor numbers it would attract. A potential project for the community to develop a vision and seek funding to develop and build. The bridge is certainly attractive and maintaining the visual amenity would be important if retained as a tourist attraction. It is clear the Kindee Bridge has significance to many and this is an opportunity for the community and Council to collect and share stories.
	leritage interpretation and remembrance	
of di vi a si	emember the existing Kindee Bridge with some sort f exhibition at a local hall/pub or tourist information isplay irtual or visual story telling (mobile app) plaque either on the replacement bridge would be uitable eviewing platform and story boards	 Further discussions and consultation with the community will be undertaken.
	lew Bridge	
O N FI E K Li K M K K E	ccess to allow for heavy and emergency vehicles ine lane bridge lew bridge - two lanes lood immunity duild a replacement bridge sooner tipe Clay Road needs to be sealed if it will be Option 4 progresses lew bridge to reflect the design of the existing bridge Vater security to cope with population growth imit access to 10 tone teep the scenic views/new bridge not to block old ridge Maintain the quiet character and atmosphere of the tindee Valley xisting bridge to remain as a footbridge - during onstruction of a new bridge	 Noted Council aims to progress preferred design route options Replacement bridge to cater for heavy load limits and emergency vehicles Replacement bridge to have high flood immunity Ensure limited impact to access during construction to prevent impacts on community and local schools Council will continue to engage with the community through design of the alternative crossing, on-going management of the existing structure, and options for heritage interpretation
R	load safety	
(c vi se	eneral road safety was a safety for respondents crossing the river and making sure there is good isual and the turns are not too sharp) eal Pipe Clay Road ew bridge after O'Neil's corner - flood resistant connects key roads).	 Noted Pipe Clay Road would require significant upgrade and would have to be costed into Option E4 Permanently segregates communities of Kindee, Long Flat and Ellenborough
0	ngoing maintenance and repair of the existing bridge	
	ost should not be a factor naintain both bridges ongoing	 Noted Costs will continue to be required for ongoing maintenance of the structure under light vehicle access only.

Item 12.01

- safety standards have changed and it does not meet these new standards
- these responses talked mostly about the high cost to refurbish the existing Kindee Bridge and about the need and cost for on-going maintenance
- some suggested that the costs outweighed potential benefits of keeping the existing bridge
- becoming unsafe as it ages either with or without the proper maintenance.

 In the future, structure may deteriorate or be damaged resulting in requirement for closure and demolition from a public safety perspective.

Item 12.01 Attachment 1

APPENDIX 3 - VERBATIM COMMENTS

COMMUNITY ENGAGEMENT REPORT - Kindee Bridge Replacement Investigations

27

Date of						
ontributi on	Is there anything else the bridge should have?	We understand the cultural significance of the Kindee Bridge and the historical Importance to the local community. In preparing for the long term (10-15 years), we would also like to hear your ideas on an acknowledgement of the original timber bridge. Including physical and digital means to showcase the heritane value.	is there anything else you would like to mention?	Suburb	Age	Gender
pr 30 21 4:18:30 m		Turn the old bridge into a pedestrian and bike bridge.	Do not let the old bridge become un-repairable. It must be maintained to a working standard.	KING CREEK, NSW	36-45	male
lay 12 21	Please keep the existing bridge and leave it as a community facility for people to work across, have priorics on etc. It should not be demolished. Please build a new weir type bridge upstream and then a weir would be perfect for future water shortage in times of drought. We need another reservoir to cope with the growth in our Region. It's a no brainer. Thanks.		We don't have too much heritage in our area and to demolish a heritage structure is just crazy.	NORTH HAVEN, NSW	46-55	male
Mar 31 21 2:01:34 m		Make it a pedestrian bridge, make a pionic spot (possibly camping) and install a poster explaining outtural significance.	Population is growing faster on the Beechwood side of Pipeclay road. Please consider building a bridge and connection to Oxley Highway close to 7-8 km mark of Pipeclay road.	PIPECLAY, NSW	46-55	male
pr 30 21 7:45:33 m	Save the existing bridge	Keep the bridge as walking bridge or heritage landmark	Survey does not give option to save existing bridge	WAUCHOPE, NSW	46-55	female
lay 0121 3:54:19 m	Safe passage for pedestrians and cyclists.	This bridge is iconic. I would like to see a new bridge with the same elegance and "lighthess" as the old one. Make engineers to be creative - we are building a bridge for the next 100 years. It should be as iconic as the old one. I would be disappointed if it would look like any other concrete bridge	Thanks!	KING CREEK, NSW	Over 65	male
pr 1521 3:19:13 11	Limitation of weight and long vehicle access so that the quiet character and atmosphere of the Kindee Valley is maintained.	Under no oircumstances should the current bridge be demolished.	The current bridge is one of, if not THE oldest Suspension Bridges in Australia. How can demolishing it be even considered. I would have thought that in 2021 we would be trying to conserve our history. NOT destroying it.	LONG FLAT, NSW	Over 65	male
lay 02 21 3:52:04 11	Have not answered 'To got for the lowest construction cosf. What is needed is a bridge that meets the needs of the community and it should not be built to a cost.	Inote that the bridge is considered a Heritage item of local importance. That should mean that it will be preserved, but with Council's track record (Wauchope Primary Schod) it would be a waste of time & effort to attempt to save it. I wonder why we both to have any heritage items when the rating can be change to zero.		BEECHWOOD, NSW	Over 65	male
pr 30 21 6:45:58 m	The old bridge, as is, renovated as necessary	The whole faming of this survey makes it clear that you do not "understand the cultural significance of the Kinde Bidge" which is far move momentous than it's meer "historical importance to the local community". It is of national, and I imagine international importance, as among the earliest examples of it's kind. It's resilience is also of significance, in this age where resilience is now a buzzvord, but CAD all too often ensures contemporary structures are under engineered, to meet economics tructures, rather than over engineered as with examples from earlier in the Industrial Age.		WAUCHOPE, NSW	Over 65	male
lay 0121 3:17:19 11		The existing suspension bridge should remain in some form. It could have restoration work done on it but closed to all but pedestrian traffic.		WAUCHOPE, NSW	Over 65	male
lay 1021 1:54:24 n			For greater utilization of a high level bridge, the Kindee Road could be spread up to join up with the Forbes River Road	PORT MACQUARIE, NSW	28-35	female
lay 1021 1:56:12 11	Kaye Mullot - 42 Ellenborough road - 0285874345			PORT MACQUARIE, NSW	28-35	female
lay 1021 1:59:28 11	William John Lyon - 179 Rocky Falls Road, Kindee - 0417 39 548		Would not want the old bridge to be closed while the new on being built.	PORT MACQUARIE, NSW	26-35	female
lay 12 21 1:34:48 11	Ian and Joy Lewis. 227 Pine Scrub Road, Kindee 0419 938 969 joylewis@internode.on.net	We think keeping the original bridge as a walking bridge, maintaining it with the budget it would require to demolish it would be an outcome that would enable all to be satisfied for the foreseeable future. When the budget runs out fence it off and allow people to continue to observe and photograph it until it no longer stands		PORT MACQUARIE, NSW	28-35	female
lay 19 21 4:42:50 11	Raymond and Islet McKinnon 310 Pine Sculo Road, Kindee 02 6867 4388 isletmckinnon@gmail.com			PORT MACQUARIE, NSW	26-35	female
lay 24 21	2 Ianes, high enough to cross in a flood. To withstand heavy loads 30 tonne plus Access to old bridge during construction High guard safety rails	Still use a footbridge so tourists can walk across it to take photos	As residents of Kindee Road we would like to have a detour down pipeolay roads as it is extremely dangerous. Also very hard on @WD cars and tyres not to mention bumpy for young babies. Adding another 30+ minutes onto an already long journey to Port Macquarie where we work. Michelle and Aaron Aafes	PORT MACQUARIE, NSW	28-35	female
ay 24 21 ::57:17 1		The existing bridge should stay. Pedestrian traffic. Tourist attraction.	The cost of maintaining the existing bridge - compare the off set cost of demolition. Pipe day road needs to be sealed for option E4.	PORT MACQUARIE, NSW	26-35	female
av 24 21	I'd prefer (if possible) to keep our existing bridge in use indefinitely BUT understand the oulvert bridge is sadly nearing the end of its life span.	Let's see the current bridge preserved in physical form as a heritage, cultural and tourist attraction.	I am not able to comment on any other options until info is available, such as atternative options would be via sealed roads. I brought my property based on having a sealed road to my door, as is currently the case with the existing bridge. The bridge surface and Kindee Road are in desperate need of repairs as a result of damage from the recent floods.	PORT MACQUARIE, NSW	26-35	female
ay 24 21 :20:12		Why are we have to wait 10-15 years for a new bridge. Built is ASAP and maintain the old one while you do it. Yes we love the old bridge and would be really happy to see it preserved if possible.	We see visitors on the old bridge regulatly taking photos. We see wedding photos taken on it. It is very semimental in value to the local community. We understand the cost involved in this, however, we also see many other icons maintained. We feel it is of huge tourism value and could be of great importance in this tourism markening of the valley for the many groups that travel the Oxley highway. Motorcycle dubs, car dubs, caravamers, individual travelers, oplisits clubs, and locals exploring their own backgrads. It should be on postartors and does more the PMQ Halsing Valley	PORT MACQUARIE, NSW	26-35	female
ay 24 21	To help keep costs down to replace the bridge with new as soon as possible to lower maintenance costs on Historical and Tourist Bridge so the community can benefit as long as possible as a walk bridge Tourist attraction also special events e.g. weddings are very popular now.		Wayne and Wendy Bell 459 Kindee Road, Kindee 0428 552 301 wbkindee8@outlook.com	PORT MACQUARIE, NSW	28-35	female
ay 24 21 I:49:32	Michelle O'Hearn 2897 Pipeday Rd, Kindee 0408 231 945	While its a positive thing that PMHC are wanting to build a new bridge, its very disappointing that Council want to demolish the original Kindee Bridge. Just because something is "OLD" doesn't mean you should get rid of it. PMHC should conserve, promote and look after our Heritage.	When I first moved to the area 20 years ago, I went to the visitor's information centre in Port Macquaries to get a map so I could find my way around. That map had the Kindee Bridge on the cover. Somebody obviously thought it was worth promoting back then, what has changed. I don't believe you can just say its too expensive to keep, and looking at the costings provided on the info night if you take away the 12 million to demolish and add in this amount it seems possible to fund this bridge into the future. Please take another look, once its gone its gone forever.	PORT MACQUARIE, NSW	28-35	female
ay 24 21	Michael Ulrick 2897 Pipeday Rd, Kindee 0488 490 667 6872/02	I have lived here in Kindee for 20 years. I have travelled over the Kindee Bridge at least 13,000 times. The maintenance that I have seen has been limited and slow for all of this time. There is not another timber bridge that is more beautiful in Australia than the Kindee Bridge. It makes a proud statement over the Hastings River. It should have been State Heritage Listed many years ago, I don't know why it was not. We need to pull out all stops and	As for a new bridge, I don't have any issues with, only, if built near the Kindee Bridge the issues of congestion around the oldiexisting bridge. Also, the 'pimple on the end of the nose effect't would take away from the existing bridge. I would much rather the new bridge be built on the Eastern end of Long Flat, this would allow work to proceed earlier than later, with less interruption. The Kindee Bridge (existing) could have a ton limit lower than it is now, says 5ton, this would allow wre isidents to travel	PORT MACQUARIE, NSW	28-35	female

May 24 21 02:43:43 pm	The old bridge should be left in place and converted to foottraffic access only to mailtain the heritage aspect. It should not be knockec down.	Don't knock it down!	Kim and Brendon Lost 225 Kindee Road, Kindee 0410 413 279 kalastbooks@gmail.com	PORT MACQUARI NSW
May 24 21 03:02:10 pm	Clear vision if it is 1 lane. 2 lane bridge would be preferable.		As the longest single-span bridge offits kind in the southern hemisphere, it has special significance, and to have been built when it was without the loss of many lives, it was a significant point of pride in the area. Both Hastings Shire Coundi and Port Macquarie Council prior to and after amalgamation have promoted Kindee Bridge as a point of interest in the area and to tourists of the area. Myself, family, and friends in the community would strongly like the existing bridge to be kept as a pedestrian bridge with parking on both sides where the existing approaches are that will be losed off.	PORT MACQUARI NSW
May 25 21 02:39:03 pm				PORT MACQUARI NSW
May 25 21 02:43:52 pm		The old bridge should remain as a tourist attraction for the area. Pictures of the construction should be displayed along the bridge. People like to walk along the old bridge, even now. Please don't distory the beautiful historical bridge its lovely.		PORT MACQUARI NSW
May 25 21 02:47:55 pm		The bridge has sentimental significance to use local upper Hastings residents, however I understand the bridge is nearing its lifespan and shall pose as a significant safety risk if not replaced in near future. It would be good if it could remain until it reaches 100years and this fact be celebrated prior to its removal. It wouldn't matter what you did with design of new bridge it will never represent old bridge. New bridge needs to be functional for farmers, wide and stong enough hof trucks and heavy machinery.		Port Macquari NSW
May 25 21 02:49:24 pm				PORT MACQUARI NSW
May 25 21 02:54:10 pm	Access for Heavy Vehicles is important. Bridge to be built on a side angle so that the corners are not as sharp. Can't use pipe clay road as it is very dangerous.	It will cost a lot to maintain the existing bridge and be a cost to rate payers.		PORT MACQUARI NSW
May 25 21 02:55:58 pm			Hi Peta and Councilors of PMHC. Kindee Bridge is a unique timber suspension bridge which was constructed during the Great Depression. It is one of only two left in NSW. It's streamline modern lines hark back to the Art Deco period of design.	PORT MACQUARI NSW
May 25 21 02:57:21 pm			Attr: Oceana Kovaos, Stephen Wood, CEO Dr Clare Allan, Mayor, Peta Pinson All Councillors. 24th May, 2021	PORT MACQUARI NSW
Mar 18 21 12:17:55 pm	Pedestrian use is important.			KINDEE, NS
Mar 29 21 10:55:06 am	No	It would be very sad to see the bridge dismantled as it is a significant heritage asset for Hastings - Port Macquarie region.	I would like to see speed and surveillance camera's installed on the bridge. As we have witnessed on countiess occasions multiple vehicles crossing at the same time and speeding across the bridge. This is a daily occurrence. And sadly these are some of the residence in Kindee Valley. I don't believe speed bumps would be effective, they would likely cause more damage.	LONG FLA NSW
	(no tight turns) and allow for pull off area for viewing of virtual Bridge. access needs to allow for longer vehicles as long as load limits and access to bridge allows residents to get deliveries	Iliked the ideas presented by council about information available at the Pub, the virtual phone use at the site. I also liked the replica idea provided from a community member on the night and Donna Lloyds recent feedback re celebration to mark the occasion. A viewing platform to use the virtual bridge app would make the area a focal point. Maybe this could be where a replica is	community. I believe that the least impact as possible to the environment & waterways is essential and whilst the cheapest option from memory is Option B, the overall cost and impact to the community is more important. Any dosure of the ourrent bridge due to construction should be limited as much as possible with the view to allowing pedestrian access when the bridge is dosed and forward planning to maintain school bus services in the area.	LONG FLA NSW
	Both suggestions put the new bridge after O'NEILS corner and during the last rains that area was flooded for a long period and is regularly there for incorporating a solution that connects the high sides of O'NEILS, long Flat and Pipe Clay would be beneficial and connect all these areas in times of disaster.	Speaking purely from a personal not we were married on that bridge so keeping it for foot traffic only may be a option as was done in Wagga Wagga with there original timber bridge.	Connecting pipe Clay could have economic opportunities if some of the area could be re-zoned for housing creating an are not run-similar to King Creek and Beadwood bringing development to the hinterland and supporting the existing infrastructure in the surrounding areas eg Long Flat with sympathetic and modest upgrades eg. Dedicated grocery and fuel supply including new Zero emissions H2 fuels. (There is private sector investment looking at these options so the time is now to with landholders and other interested parties).	LONG FLA NSW
Apr 29 21 12:00:55 am	A foot traffic lane separate to vehicle access	Build a pionic pullover for tourists and winnebagos with a walk around display of photos and a story.	Whilst lovely, the cost to maintain and safety aspects can t be ignored. It is time to go.	ELLENBORC H, NSW
Apr 30 21 06:24:59 pm		I take students and visitors to see this bridge every year. It is a valuable historical bridge and if possible should remain as a pedestrian / light access bridge. It would be great to see the existing bridge stay alongside the new bridge. A digital representation of the bridge is not a substitute for the bridge itself so if you are thinking of a sign or the like as a memorial to it, you are trying to pay for a valuable as set with a worthless substitute. By all means promote the historical aspect of the bridge through.	road corridor, but no road ourrently exists, and a new road will be required. While the proposed connecting link to Pipe Clay road is not as substartial as the upgrades needed to Pipeolay Road (which are extremely significant) they need to be mentioned in the supporting statements. This omission gives a false impression of the challenges involved in construction. The impact of construction and road noise, dissection of access to the residence and disturbance of pastures are all	WAUCHOP NSW
May 02 21 06:04:51 pm	The new bridge should not impact on the scenic value of the current bridge or impose on the rural vistas up or down stream from the bridge.		No where was here a question addressing the eye sore of a modern bridge being built next to the existing bridge. The heritage bridge is iconic any new structure built in sight will impact the landscape and scenic attributes of the area. Keep the current bridge the way it is, perhaps make it a walking bridge only and tourism will surely follow.	LONG FLA NSW
May 08 21 08:03:17 am	two lanes, obviously - why bother going to all the expense if you don't improve people's lives and access?	Has to have State Heritage listing and must NEVER be removed - essential part of our Valley's history, ingenuity, beauty		TOMS CREE NSW
May 12 21 12:58:03 pm				ELLENBORC H, NSW
May 13 21 08:09:47 pm	A View of the existing bridge !!!	The Bridge needs to remain intact, preserved safely as a pedestrian bridge for historical and tourism valve. Nothing else is acceptable !!	I cannot believe PMHC is trying to encourage the demolition of this bridge, a notable tourist iconic structure.	MOUNT SEAVIEW, N
May 13 21 09:21:31 pm		Leave existing bridge, Close bridge off to vehicles and maintain as historic foot bridge		ELLENBORC H, NSW
May 14 21 12:52:06 am		Leave it there.	You say heritage value but you are saying you're going to take the old bridge down??? You obviously don't care about what the local community wants because if you did you would leave it up. I bet if it was in gon Macquarie it would be left up. One thing I'm starting to notice is port Macquarie council don't care about outer communities and only care about port Macquarie it's set. You only have to look at wauchope boatramp and the park to see you don't care about outer communities.nothing has	ELLENBORC H, NSW
May 19 21 08:24:15 pm		The best thing that could happen for the long standing hard working existing bridge, is for it to be kept in place but purely for foct traffic at most. A burist attraction & fantastic photographic opportunity. Local icon & engineering masterpiece! Please please do not relegate her to demolition!!		LONG FLA NSW
May 19 21 08:46:35 pm		A new bridge design that reflects aspects of the original structure would provide a form of continuity in infrastructure development.	Would prefer a reduced timeframe for provision of new infrastructure	KINDEE, NS

RΕ,	26-35	female
RΕ,	28-35	female
RΕ,	28-35	female
RE,	26-35	female
RE,	28-35	female
SW	Over 65	male
AT,	56-65	male
AT,	46-55	female
AT,	46-55	male
OUG	46-55	female
PE,	56-65	male
AT,	36-45	female
ΞK,	46-55	female
oug	36-45	female
vsw	46-55	female
OUG	26-35	male
OUG	26-35	male
AT,	46-55	female
SW	46-55	male

May 19 21 10:15:07 pm	Dual carriage way and safe continuous access for residents	Everybody I tell about the bridge is amazed about its significance. All means need to be taken to ensure that it is well marketed and appreciated by all that come to Kindee.	Please balance cost and convenience with historical and future value. We don't need a massive amount of tourism but to preserve the historical significance of this bridge is truly important for all of us.	KINDEE, NSW	46-55	female
May 20 21 08:27:23 am				KINDEE, NSW	28-35	male
May 20 21 08:53:48 am				PORT MACQUARIE, NSW	26-35	female
May 20 21 11:05:26 am		Council should work with the community in getting the existing bridge heritage listed with state or commonwealth significance so that it can remain as a historical draw card for the local area	The state & federal government should be approached for help in maintaining the existing bridge once a new bridge is built	TOMS CREEK, NSW	46-55	male
May 20 21 03:31:07 pm	Adequate railings and safeguards	Allow bridge to remain open for foot traffic for locals and as tourist attraction		KINDEE, NSW	18-25	female
May 20 21 03:47:51 pm	Access to Kindee during floods.	The bridge could be used as a foot bridge so tourist and local community can enjoy the bridge. Also consider a picnic table dose by to sit and enjoy a meal.	I work in Port Macquarie and use the bridge frequently (twice a day up to 7 days a week). Creating a safe and reliable bridge is imperative for the many community members, schools and medical professionals who require access to the Kindee Valley.	KINDEE, NSW	56-65	female
May 20 21 04:05:16 pm	Safety aspects such as guide rails with reflectors, visible speed limit signs and not to impacted by foods. Car park access for tourist/visitors to view did Kindee bridge (e.g. Pionic table). Appropriate and sustainable lighting (solar lighting).	Create instagram page for the old Kindee Bridge. Onsite images displayed of the construction and engineering of old Kindee Bridge. Old Kindee Bridge to remain at ourrent location as a foot bridge.	I regularly use the bridge to access essential services (e.g. work, groceries, medication and medical practitioners) and need to do so in timely matter.	KINDEE, NSW	Over 65	male

No.	Submission/Email
1	My thoughts: 1. Bridge Location Options. I believe that Option B is the most suitable. Follows the existing road corridor. People will have easier access to the old bridge if it is not demolished. Options C1, C2, E1 and E4 all require more ground disturbance which will require an Aboriginal Heritage Impact Permit (AHIP). There will be the potential to uncover Aboriginal sites. I recall one job I undertook. Salvage nine recorded stone artefact and excavate a test pit to see if sub surface deposits existed. Three months later, 4000+ artefact and considerable cost to the developer the job could continue.
	 Future of existing Kindee Bridge Prime objective should be the preservation of the bridge. To achieve this the heritage status of the bridge should at least be of State importance, but I believe it would make World heritage listing.
2	All Councillors. 24th May, 2021
	Dear Council Team,
	Re: Kindee Suspension Bridge Replacement Investigation Report.
	Thank you for the opportunity to have our say on this matter. Firstly, thank you for attending our monthly meeting last Wednesday and expressing the options of the above investigations. Since the meeting our President, has inspected the Pipe Clay Road and Kindee Bridge and makes the following observations.
	Kindee Bridge History Built in 1936 this single lane steel-trussed cable and timber suspension structure, 308 feet 308 feet long designed for moderate loads and built at a cost of 2,800 pounds. It is one of the few suspension bridges remaining in NSW. Twin braced trussed cables reinforce the bridge, it does not rely on the surfaces of the deck for strength, Kindee Bridge is light and flexible with limited sway from wind and traffic loads. The bridge was designed by V. Karmalsky and A. T. Britton of the Dept of Main Roads. Morrison supplied steelwork, local timber by R. McMillan of Long Flat. Mr B. E Coombes and Mr Neil McKinnon carted the sand and gravel in drays for the concrete foundations. Kindee Bridge was officially opened on19th December, 1936 by Member for Oxley, Mr L. O. Martin before a large crowd, after which the gathering adjourned to Long Flat for lunch and further celebrations.
3	Observations from Council Reports Having scrutinised the above reports we acknowledge the Councils staff with the extent of the reports. It would be our preferred Option of E4 Oxley Highway to Pipe Clay Road east of Long Flat with Key advantages as follows: 1. Utilising existing public road corridor between Oxley Highway and Pipe Clay Road. 2. Minimum impacts on private property requiring property acquisition. 3. Favourable geotechnical conditions for pier construction. 4. High level flood immunity. 5. Better constructions access directly adiacent Oxley Highway III retia and Councilous of PMITO,
	Kindee Bridge is a unique timber suspension bridge which was constructed during the Great Depression. It is one of only two left in NSW. It's streamline modern lines hark back to the Art Deco period of design.
	Being a timber and steel structure it needs specialist care. It is vital that a professional person is employed to do an assessment on the timber's and steel cables. This should be a specialist that is chosen by the community to ensure transparency. If the correct sandblasting and painting to the steel work, and waterproof paint was applied to the timber it would extend the life of the structure significantly.
	The present bridge could be turned into a walking bridge and a new bridge built away from the historical structure. After visiting the site it is very clear that no modern structure should be built anywhere near the historically significant bridge. One of the options that council staff identified is the E4 option. This option crosses the river east of the village of Long Flat. The distance from that new site would be a four minute journey back to the present bridge.
	The new bridge must be a two lane structure .From a safety point of view it would be very unwise to have a single lane bridge. PMBH has promoted for many years the concept of growing our community. This includes the hinterland. The tar sealing of the Pipe Clay road would greatly improve the Dailey drive of the many residents that live along the route. The road is in very good condition and could be tared in sections over the next four to five years.
	Just as importantly for the future, it would allow visitors to travel from Beechwood to Long flat in 20 minutes, opening up that part of the valley to investment in boutique farming and value added industries centered around healthy lifestyle, and connecting future 'village hubs' to one another. Residents from Port Macquarie and Wauchope would be more likely to take day trips due to improved infrastructure. Visitors would be impressed by the stunning scenery, and to have improved infrastructure it would make a more attractive destination.
	The highlight of this drive would be a restored Kindee bridge painted in a spectacular Art Deco colour scheme re splendid with flags. This work could be a combination of volunteers, 'Friends of Kindee Bridge', an expert in timber bridge construction, and timber craftsmen.
	The Hastings Valley has lost too much of its heritage, and this bridge played an important role in helping farming to thrive from the mid 20th century.
4	
	Thanks for the opportunity for SES to be involved.
	You guys are probably across this already but SES's main requirement would be for the manual river height gauge plates to be integrated into any replacement bridge.
1	

They would be required for SES volunteers to do manual river height reads of the Hastings River at Kindee when the automatic flood warning gauge is down.



Kindee Bridge Conservation Management Plan

Prepared for Port Macquarie-Hastings Council / 21 June 2019

Revision 3

181989

Taylor Thomson Whitting (NSW) Pty Ltd (ABN 81 113 578 377) as trustee for the Taylor Thomson Whitting NSW Trust (ABN 59 514 956 558) I Consulting Engineers Level 3, 48 Chandos Street, St Leonards NSW 2065

Your Partner in Engineering

Port Macquarie-Hastings Council
Kindee Bridge Conservation Management Plan

21 / 06 / 2019 181989

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1.0 Document Register

AUTHOR	REVIEWED	APPROVED	ISSUE	STATUS	DATE
Angus Busuttil	Paul Connett	-	Draft	Rev. 1	22/03/19
Angus Busuttil	Garth Miller	Paul Connett	Draft	Rev. 2	24/05/19
Angus Busuttil	Paul Connett	Paul Connett	Final	Rev. 3	21/06/19

2.0 Introduction

2.1 Context of the Conservation Management Plan

This Conservation Management Plan (CMP) has been prepared for Port Macquarie-Hastings Council to assess the significance of the Kindee Bridge, based on historical and social contexts, and to identify the issues, constraints and opportunities of future proposals for the bridge.

The Kindee Bridge, opened on 19 December 1936, allowed the farmers of the Kindee Valley a safe method of moving their produce to the railway hub of Wauchope to the east. The bridge is now over 80 years old and is an ongoing safety risk and maintenance liability to Port Macquarie-Hastings Council. The bridge is listed in the Port Macquarie-Hastings Council Local Environment Plan 2011 under Listing Number 1011.

Considering its current condition, two options have been presented to Port Macquarie-Hastings Council in a strategic management report, written by GHD; which are to:

- Retain the bridge and apply for State Heritage status and a commensurate level of funding to overhaul and upgrade the bridge
- Replace the bridge with a modern structure capable of carrying modern vehicles.

The purpose of this assessment and Conservation Management Plan is to assess the heritage significance of the Kindee Bridge, to form part of the Port Macquarie-Hastings Council's structure management strategy for the bridge.

2.2 Site Location

Kindee Bridge is located at the intersection of Kindee Road and the Hastings River, between Ellenborough and Long Flat, approximately 51km west of Port Macquarie and 33km west of Wauchope, along the Oxley Highway. Figures 1 and 2 below show the location of the bridge and its immediate surroundings and its position in relation to Port Macquarie and Wauchope.



Figure 1 – Maps Showing location of Kindee Bridge (Source: left: 6 Maps, right Google Maps)

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Port Macquarie-Hastings Council
Kindee Bridge Conservation Management Plan

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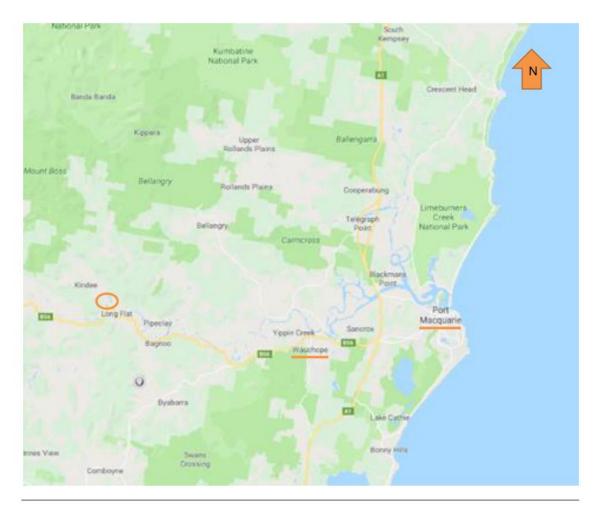


Figure 2 - Location Relative to Port Macquarie and Wauchope (Source: Google Maps)

2.3 Setting and Views

The Kindee Bridge crosses the Hastings River, which flows west to east (south to north beneath the bridge), in a narrow, deep channel with steep banks at either side. Floods are known to be common in the river, carrying driftwood, with recorded flood levels reaching up to 1.8m beneath the centre of the bridge deck and 0.9m beneath the abutments¹. The bridge forms part of the Kindee Road spur road, which diverts from the Oxley Highway, serving farms in the fertile Kindee Valley region, which is surrounded by mountains. The site is zoned as 'Rural Landscape' in the Local Environmental Plan for the area², but has not been identified as an Aboriginal Place of Heritage Significance or an Archaeological Site Item.³

² Port Macquarie-Hastings Local Environmental Plan 2011, Land Zoning Map - Sheet LZN_006A, 2010

¹ Port Macquarie-Hastings Council - Kindee Bridge - Strategic Options Report Rev. A, 2018

³ Port Macquarie-Hastings Local Environmental Plan 2011, Heritage Map - Sheet HER_006A, 2010

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Figure 3 – Drone Survey Photograph Showing Surroundings of Kindee Bridge (Source: Stephen Wood, Port Macquarie-Hastings Council, 2018)

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2.4 Methodology

This Conservation Management Plan has been prepared in accordance with guidelines in the Australian ICOMOS Charter for the Conservation of Places of Cultural Significance, known as The Burra Charter, and the NSW Heritage Manual. Paul Connett of TTW undertook an inspection on 25 February 2019, accompanied by Stephen Wood of Port Macquarie-Hastings Council.

2.5 Technical Terminology

Technical terminology used in this CMP in relation to timber bridge structures is defined below, in accordance with the Road and Maritime Services' (formerly RTA) Timber Bridge Manual Series⁴:

ltem	Description
Abutments	Substructure components at the ends of a bridge providing support to the superstructure and retaining the approach fill.
Bearing	The contact surface or element supporting a component.
Bracing	Component providing stability to a member, or group of members, such as the timber cross bracing on piles.
Capwales	Pair of horizontal timber components (typically 300 mm x 150 mm) at the top of piles or posts, providing bearing for the superstructure.
Components	General term referring to members forming part of a structural assembly.
Composite	A member or system with two components, or materials structurally joined together to form one member.
Corbel	A longitudinal timber bearing member which provides support and some continuity between bending members in adjacent spans.
Decking	Closely spaced sawn timbers up to 125 mm deep (200 mm to 250 mm wide) supported on cross girders. ⁵
Design Life	The desired life expectancy of the bridge or component.
Deterioration	A general term referring to either decay or insect attack in timber, corrosion in steel and general wear of components.
Durability (Class)	A rating for hardwood timbers indicating the susceptibility to deterioration from decay and/or insect attack.
Engineered / Engineering	Referring to design and/or evaluation by an engineer certified by the Institution of Engineers Australia.

⁴ RTA, Timber Bridge Manual – Section One to Eight, Timber Bridges - General, 2008

⁵ In the case of Kindee Bridge, the original decking has been replaced with plywood decking.

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Flexural Member	A component primarily subjected to bending between these supports.
Fungal (Fungi)	Deterioration in timber caused by a fungal growth.
Girders	Typically referring to longitudinal bending members (round or rectangular) of fairly large cross section.
Hazard (Level)	A rating relating to susceptibility to deterioration for timber which is to be preservative treated.
Headstock	Single horizontal timber component (typically 300 mm x 300 mm) at the tops of piles, or posts, providing bearing for the superstructure.
Insect (Attack)	Deterioration in timber caused by insect attack.
Longitudinal	A component oriented parallel to the roadway.
Moisture Content (%)	A measure of the amount of moisture in timber as a percentage of its oven dry weight.
Piers	Intermediate substructure components providing support to the superstructure.
Piles	Round timber poles driven into the ground to provide support for a structure.
Preservative	A chemical used to treat timber which inhibits deterioration.
Protective Treatment	General term referring to the protection applied to components to provide resistance to deterioration.
Protective Systems	All types of protection including preservative treatments, flashing and sealants.
Seasoned	A timber component which has been air dried to remove some moisture.
Sheeting	Timber plank running surface, generally 50 mm to 75 mm thick, running parallel to the roadway and supported on the timber decking.
Stress Laminated Timber	A structural system formed from small timber elements stressed together using prestressing tendons.
Stringers	Typically referring to longitudinal bending members (usually sawn) of relatively small cross section.
Structural Defects	A general term referring to damage in timber such as splits, checks, fractures, pipes and crushing.
Substructure	The timber components supporting the superstructure.

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Superstructure	All components above the substructure.
Sway Brace	Diagonal member located outside the truss extending between the top chord and the extended cross girder designed to resist sway of the truss.
Timber Cross Girders (Beams)	Typically, large section and high-quality timbers of up to 400 mm x 400 mm cross section oriented transversely as part of a larger structure and supporting traditional timber deck systems.
Transverse	A component oriented perpendicular to the roadway.
Wearing Surface	The top coating on a bridge deck provided to resist wear due to traffic.

2.6 Documentary and Photographic Sources

The following documentation has been gathered from the following sources to write this Conservation Management Plan for Kindee Bridge:

- Paul Connett of TTW visually inspected and photographed the bridge on 25 February 2019
- Stephen Wood of Port Macquarie-Hastings Council (PMHC) supplied photographs and previous documentation of the bridge
- Veronica Norrie of Port Macquarie Library supplied additional documentation and oral information on the bridge
- Lillian Andrews of Port Macquarie Museum supplied additional documentation and oral information on the bridge
- Nathan Ross of Aboriginal Participation & Engagement Advisory Services orally supplied information on indigenous heritage.
- Other documentation referenced accordingly.
- 2.7 Authorship and Acknowledgements

This CMP has been written by Paul Connett and Angus Busuttil of Taylor Thomson Whitting (TTW) Consulting Engineers. Historical documentation and photographs have been provided by Stephen Wood of Port Macquarie-Hastings Council and other independently referenced sources.

3.0 Documentary Evidence

3.1 Early History of the Site

The traditional custodians of the Port Macquarie land area are the Birpai or Biripi people (spelt differently in Port Macquarie and Taree District)⁶, who lived along the Hastings River⁷. After European settlement the local Birpai people suffered a huge decline in population. Between 1840 and 1900 the Birpai people were forcibly removed from the Hastings area to reserves at Purfleet, Taree and Kempsey. Indigenous river crossings over the Hastings River by the Birpai peoples are known to have been constructed over time⁸. Archaeological investigations indicate that Aboriginal persons have been living along the east coast of Australia for over 30,000 years. These studies also indicate that they enjoyed living in riverine and marine locations.⁹

Although James Cook in 1770 and Lieutenant Mathew Flinders in 1802 documented basic topographic features of the Port Macquarie region, the existence of what is now known as the Hastings River was not known until discovered by John Oxley on his expedition east⁹. The European settlement of coastland from Sydney to the Queensland border was staged slowly by Government Order. In 1830 the Port Macquarie area was added to the limited permissible settlement locations. In 1830 the Deputy Surveyor General, Samuel Perry, wrote in his journal of, 'an aborigine in the Hastings Valley who exhibited a dexterity in carpentry and in the use of tools which a white man could not acquire unless he practised for a long time'⁶. The use of timber as a structural material was particularly popular in the Hastings region by the local Birpai people.

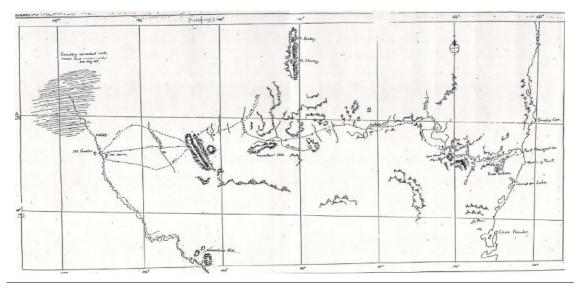


Figure 4 – Surveyor General Oxley's Plan 18189

⁶ Van Kempen E., An Excerpt from Some Timber Stories of the Hastings, 2003

⁷ W.J. Enright, B.A., Mankind [Vol. 2, No.9], 1940

⁸ Ross N., Aboriginal Participation & Engagement Advisory Services, 19/03/2019

⁹ Suters Architects Snell, Municipality of Hastings Heritage Study, 1991

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The existing Oxley Highway follows the route of John Oxley in 1818, when he was commissioned as Surveyor General of New South Wales by Governor Macquarie to make a route east from the New England region. The explorer moved north east by bullock wagon over the Liverpool Plains. After slow progress his party eventually crossed the Dividing Range near the head of the Hastings River.¹⁰ He continued down to the Hastings River Valley and reached Port Macquarie. By 1828 Governor Darling asked permission from London to close down the penal settlement at Port Macquarie and open the region to free settlement. The closure of the penal settlement came down to the requirement of a penal settlement to be isolated, to reduce the capacity for escape, and additionally due to rumours that the camp was not fulfilling its role as a deterrent to crime, with freed prisoners in Sydney pleading to return up north.¹¹

After the introduction of the Great Northern Railway line in 1878 the Oxley Highway ceased to be a significant travel route. The eastern portion served the Hastings Valley as a local and critical road to Port Macquarie, the western portion served as a route to transport sheep to the railway line west. The central portion between Yarras and Yarrowitch became unused, unmaintained and eventually impassable.¹²

The Kindee region was further developed from the mid-19th century with the land being used for agriculture, which consisted mainly of dairying and logging. The area was mostly in isolation due to the difficulty associated with crossing the river, however several sections of the river were known to be forded at Double Crossing, Ellenborough and at Kindee Crossing. During flooding, the transportation of produce by hand across the Hastings River was impossible, resulting in considerable loss.¹³

The first Kindee school was built in 1907 out of locally sourced timber from Long Flat.¹⁴ In the 1920's the school at Kindee was removed and components were used to build a new school at Forbes River. As the school-age population increased in Kindee there was renewed need for a school. In 1933 the department of education decided that the only plausible solution was to have two schools, one at Long Flat (south of the Hastings River) and one in Kindee (North of the Hastings River). The separation of schools was partly due to no safe crossing over the Hastings River being available at the time.¹⁴

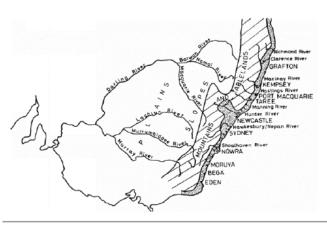


Figure 5 - Settlement of Early Colonial NSW 1788-18381

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¹⁰ Sydney Morning Herald (NSW: 1842 - 1954), Friday 13 February 1931, page 10

¹¹ Suters Architects Snell, *Municipality of Hastings Heritage Study*, 1991, page 6

¹² Main Roads, Oxley Highway Reconstruction between New England Plateau and Port Macquarie notes for Tourists, 1971

¹³ Kindee Bridge Options Report and Management Strategy

¹⁴ Walker. A., Barefeet and Blackboards, 2006

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The Great Depression in the 1930's resulted in increased government investment in infrastructure projects, as they attempted to stimulate a stagnant economy. One outcome of this was the construction of infrastructure to boost the economy and reduce unemployment. This triggered a short revival in bridge building in Australia¹⁵.

A bridge further west crossing the Hastings River had previously been constructed at Yarras, known as the Double-Crossing Bridge. However, due to the difficulties of the local terrain, this bridge was not accessible for those in Kindee, who still needed to take alternate routes. A decision was made by Council that a permanent crossing over the Hastings River was required to connect the Kindee Valley to the Oxley Highway, providing a safe and efficient route for the transportation of produce and during emergencies, across the otherwise hazardous Hastings River, particularly during flooding.

Commissioned in 1936, the Kindee Bridge is locally listed in the Port Macquarie-Hastings Council Local Environmental Plan under Item Number 1011. Feasibility studies for keeping the bridge have been continually carried out over time, however conclusions have been much the same as originally acted on when constructing the bridge; it is required in its capacity to allow safe passage across the Hastings River during floods to this date.

3.2 Due Diligence Assessment for the Protection of Aboriginal Objects

As part of a CMP an assessment of potential Aboriginal objects is required. The purpose of this assessment is to exercise due diligence and to assess whether carrying out maintenance work on the Kindee Bridge will harm Aboriginal objects. The purpose is also to determine whether the proposed maintenance works for the Kindee Bridge should apply for consent in the form of an Aboriginal Heritage Impact Permit (AHIP). It was previously recommended in the Hastings Heritage Study that an evaluation of Aboriginal Heritage be carried out,¹⁶ however it is unclear if an evaluation progressed.

The procedure is a step by step process which identifies by Step 5 (if reached) if an AHIP needs to be carried out.¹⁷ The context of this assessment is based on the repair and maintenance of fabric to the existing bridge only (not construction of a new bridge). Assumptions made for this assessment assume that the bridge will remain, and no secondary bridge will be constructed. If ground disturbance is proposed, or an additional bridge is planned to be constructed, a separate Aboriginal Heritage Information Management System (AHIMS) assessment will need to be carried out.

A copy of our assessment has been included in the appendix of this CMP where we have assessed that there is a low probability of Aboriginal objects occurring in the area of the proposed repair and maintenance activity. This assumes that the only ground disturbance will be at abutment pier locations where limited but more extensive excavation has already taken place. Following this evaluation, the project for maintenance on the bridge can proceed without applying for an Aboriginal Heritage Impact Permit (AHIP). We advise however that this will not be the case for bridge demolition

¹⁵ Nolan G., The Forgotten Long Span Timber Structures of Australia, 1994

¹⁶ Suters Architects Snell, *Municipality of Hastings Heritage Study*, 1991

¹⁷ Environment Climate Change and Water NSW, Due Diligence Code of Practice for the Protection of Aboriginal Objects in New South Wales, 2010

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or constructing a new bridge. An AHIP will have to be carried out by an appropriately qualified consultant if these options are selected.

3.3 The Bridge Design

The bridge design consists of a 3-span trussed cable suspension system, spanning 27m from each abutment and 67m centrally between piers. The bridge deck is supported by steel hangers, which are fixed alternatively to the top and bottom chords of the cable truss system, suspended by cables between the bridge's 24m high piers. The bridge piers consist of round timber piles (10 in total) with planks nailed in a lateral formation and secured by timber diagonal braces to form braced trestles, which bear onto concrete footings.

The deck comprises a bitumen pavement laid on retrofitted plywood in place of the original timber planks. The decking bears onto timber stringers, which run longitudinally along the bridge, supported by timber cross girders, which are suspended by the steel hangers. Diagonal timber bracing is built into the bridge deck in each bay.

The bridge was designed by Department of Main Roads Engineers V. Karmalsky and A. T. Britton, who were well respected Engineers of the time.¹⁸ The contractor for the construction, R.B. Haydon, was killed in a site accident in April 1936, prior to the bridge being opened. Timber for the bridge was sourced locally from R McMillan of Long Flat and the steel truss members sourced from Messr Morison and Bearby¹⁹ of Newcastle²⁰.

At the time of design, the main stakeholders were communities on either side of the Hastings River, in Kindee and Long Flat, who wished to cross the river for social, education and business reasons. Farmers north of the river needed to cross the river to move their produce to Wauchope via the Oxley Highway. The design for the bridge made the assumption that, "traffic will always be light", and the loading adopted for design was as follows:

- a 10-ton (gross) vehicle (10.16 metric tonnes), seven tons on rear axle, three tons on front axe, 11-foot wheelbase, with 25% impact for deck and stringers, 20% for cross girders and 15% for cables, etc., or
- b) a distributed load of 60lb per square foot (293kg/m²) with no impact"²¹

Two proposals for the bridge were investigated, which consisted of both a low and high option, in reference to their relative height above the normal water level, for the crossing of the Hastings River:

 The low-level option involved a bridge deck approximately a meter above the normal water level, allowing a safe crossing during periods of low water level or marginal increases in water level. The bridge design philosophy was for the bridge to be submerged during high flooding, this would allow driftwood to float over during flood waters and prevent impact damage to the

¹⁸ Maxwell K., Gladesville Bridge 50th Anniversary, Engineers Australia

¹⁹ Main Roads, Oxley highway Reconstruction between New England Plateau and Port Macquarie notes for Tourists,

¹⁹⁷¹

²⁰ NRMA, Kindee NRMA Plaque, 1988

²¹ Department of Main Roads - NSW Lower North Coast Division Municipality of Hastings, *Bridge over Hastings River at Kindee Crossing*, 1984 (letter referencing original design at an earlier date)

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bridge. During high flood levels, the bridge would be impassable. To achieve such a low-lying bridge, significant civil work was required to construct long approaches down steep riverbanks. The additional cost of the significantly longer approaches made the low-level approach approach approximately equal in cost to the high-level bridge proposal.

 The high-level option involved a bridge deck 6ft (1.8m) above the highest predicted flood level in the centre and 3ft (0.9m) at abutments, which allowed traffic to cross safely at all times²². Considering the threat of impact damage from driftwood floating downstream in heavy flooding, bridge spans were required to be large to reduce the area of potential impact. As such it was deemed most economic to construct a cable truss bridge with two pylons on each riverbank. Pylons were cladded in timber planks to prevent driftwood becoming stuck between piers and prying them apart.

In the early 1900's iron, steel and concrete bridges were generally constructed in preference to timber bridges in many Australian States, however timber was still used for economic bridge construction²³. This was particularly relevant in the case of the Kindee Bridge where transportation of materials was difficult. The highly rigid trussed cable suspension system additionally provided an advantage during construction, high above the river, allowing the bridge deck to be erected and supported from the trussed cables without the assistance of scaffolding below.²⁴

²² Karmalsky V., Britton A.T., Transaction of the Institution, Vol. XVIII, Three Span Light Suspension Bridge over Hastings River at Kindee Crossing, NSW, 1937

²³ Nolan G., The Forgotten Long Span Timber Structures of Australia, 1994

²⁴ Karmalsky V., Britton A.T., Transaction of the Institution, Vol. XVIII, Three Span Light Suspension Bridge over Hastings River at Kindee Crossing, NSW, 1937

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4.0 Physical Evidence

4.1 Introduction

The inspection of the physical condition of the Kindee Bridge included visual observation of elements visible from road level, under the bridge and from an elevated platform. Non-destructive testing was not carried out as part of this Conservation Management Plan; however, Wood Research and Development has produced a report which identifies the current condition of timber piers of the bridge.

4.2 Original Fabric

The choice of materials for the bridge was dictated by cost, weight and local availability. Situated in the Kindee Valley region, the bridge utilised local timber, and limited its use of imported steel which was expensive at the time. The bridge underwent significant maintenance in 1980 and 1990. With minor exception, fabric that has been replaced has mostly matched the original design, proportion and general appearance.

4.3 General Condition

The bridge is in a good condition considering its age and use; however, the bridge is in need of maintenance, which should be carried out in accordance with relevant engineering report recommendations. The NSW Office of Environment and Heritage describes the bridge's condition as, 'in excellent state of repairs'²⁵, under the statement of significance of the bridge dated 23 February 2011. Based on our visual survey, this condition assessment is no longer the case. Sections of timber have begun to decay, a steel hanger is broken, and the weight limit has been temporarily reduced to 5 tonnes (at the time of our inspection).

The condition of timber elements has been assessed by Wood Research and Development on 25 February 2019 and the condition of steel elements has been investigated and reported on by Noble Engineering Services on 25 May and 14 October 2016. A further summary of the general condition of individual bridge elements has been included in Section 7.0 of this CMP, Grading of Significant Elements.

²⁵ NSW Office of Environment & Heritage, Kindee Road Suspension Bridge (part), 2011

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4.4 Technical Terminology Illustration

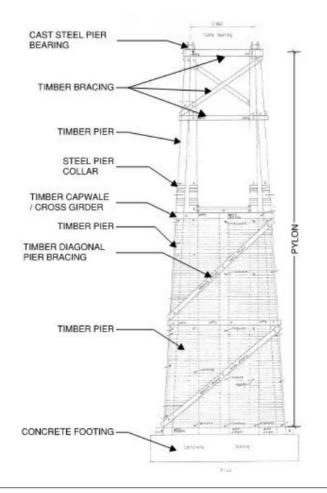


Figure 6 – Illustration of Technical Terminology Used in This CMP – Abutment

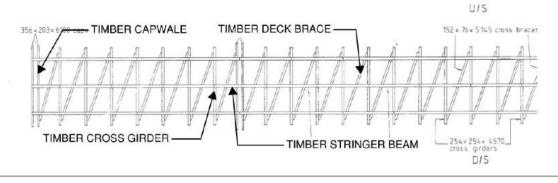


Figure 7 – Illustration of Technical Terminology Used in This CMP – Bridge Deck

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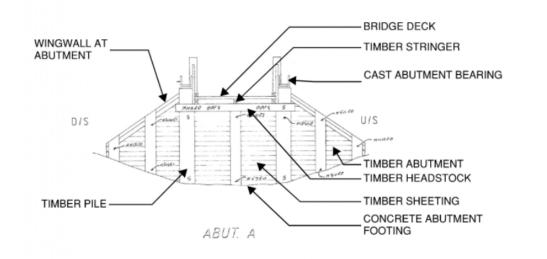


Figure 8 – Illustration of Technical Terminology Used in This CMP – Bridge Deck Plan and Elevation

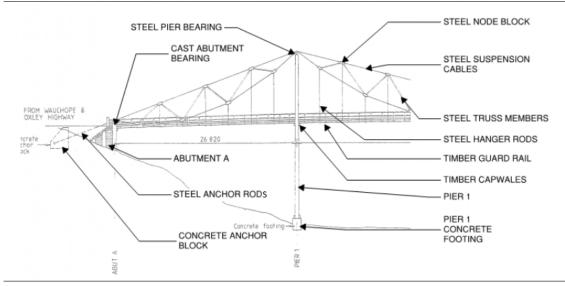


Figure 9 - Illustration of Technical Terminology Used in This CMP - Bridge Elevation

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4.5 Overview of the Existing Bridge Components



Photograph 1 - Overview of The Bridge

Photograph 2 - Overview of The Bridge26



Photograph 3 – Overview of The Bridge²⁶

Photograph 4 – Overview of The Bridge



Photograph 5 - Overview of The Bridge Approach West



Photograph 6 - Overview of The Bridge Approach East

²⁶ Wood S. Photograph by Stephen Wood, 2018

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Photograph 7 – Existing Bridge Interpretation²⁷

Photograph 8 – Steel Pier Bearing



Photograph 9 – Timber Pier Head and Brace



Photograph 11 – Timber Pier and Bearing



Photograph 10 - Timber Pier Bracing



Photograph 12 – Steel Pier Collar

²⁷ NRMA. Kindee NRMA Plaque, 1988

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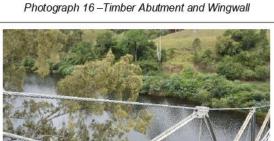
Photograph 13 – Timber Pylon



Photograph 14 - Timber Pylon & Diagonal Pier Bracing

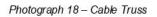


Photograph 15-The Bridge Deck





Photograph 17 - General view of Pylon Concrete Footing



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Photograph 19 – Truss Members

Photograph 20 - Suspension Cable



Photograph 21 – Overview of Steel Cable Node Block



Photograph 22 - Steel Hanger / Timber Cross Girder



Photograph 23 – Timber Cross Girder and Stringer



Photograph 24 – Steel Anchor Rods

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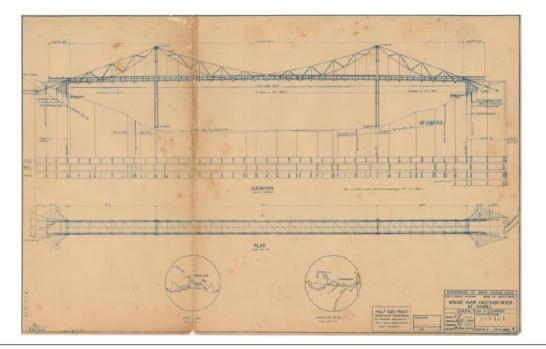


Photograph 27 – Timber Kerbs

Photograph 28 - Steel Guardrail

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4.6 Overview of Original Drawings – Full Size in Appendix E

Figure 10 - Original Drawing 1

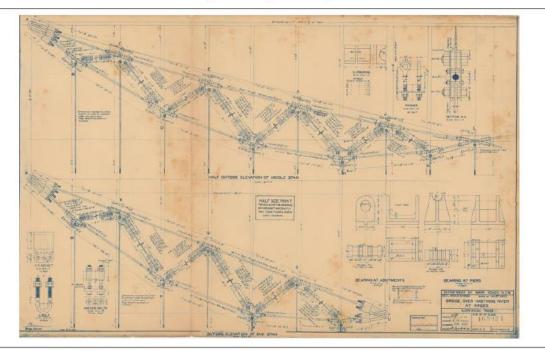


Figure 11 - Original Drawing 2

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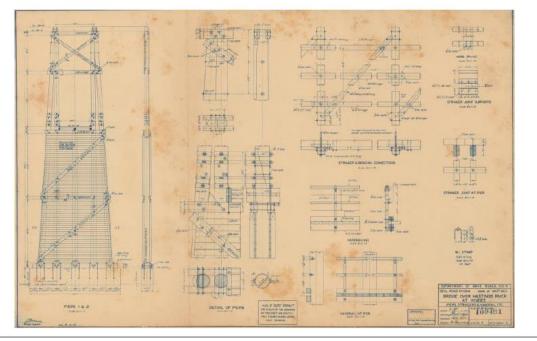


Figure 12 - Original Drawing 3

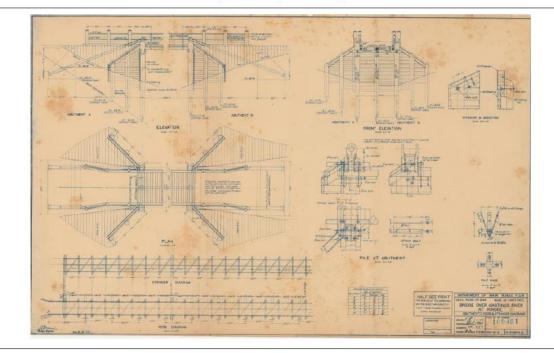


Figure 13 - Original Drawing 4

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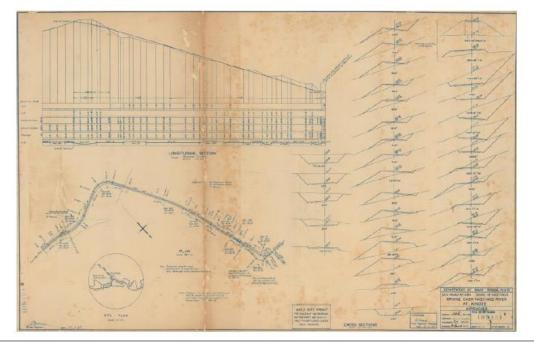


Figure 14 - Original Drawing 5

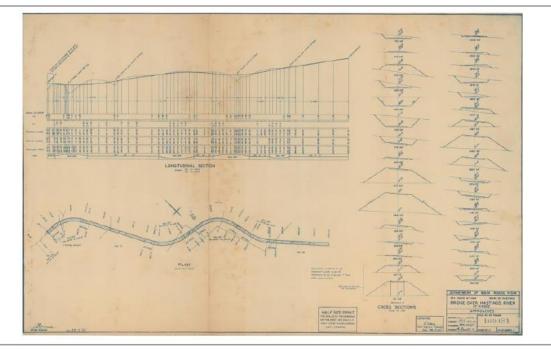


Figure 15 - Original Drawing 6

5.0 Comparative Analysis

5.1 Introduction

As part of the heritage assessment for a Conservation Management Plan structures of similar design are considered. This section compares the Kindee Bridge with other works of engineering, which share common key characteristics with the Kindee Bridge, in accordance with the following broad criteria:

- Bridges primarily constructed from timber with a trussed cable suspension system (High Similarity)
- Bridges that use a suspension system in the general region of the Kindee Bridge which are designed along similar principles (Moderate Similarity)
- Bridges that have a trussed cable suspensions system from top of bridge piers to abutments (Moderate Similarity)
- Suspension bridges which incorporate trusses for stiffening (Low Similarity).

Chronology of Bridges Considered Similar to The Kindee Bridge:

Year	Summary	Similarity
1885	The Seventh Street Bridge, Pittsburgh, USA - Opened	Moderate Similarity
1894	London Tower Bridge - Opened	Moderate Similarity
1895	Mill Creek Park Suspension Bridge, Ohio, USA- Opened	Moderate Similarity
1925	The Seventh Street Bridge, Pittsburgh, USA - Removed	Moderate Similarity
1927	Murrurundi Suspension Footbridge	Moderate Similarity
1926	Hercilio Luz Bridge, Santa Catarina, Brazil:	Low Similarity
1928	The Silver Bridge, Ohio, USA - Opened	Low Similarity
1933	Double Crossing Bridge - Opened	High Similarity
1936	The Walter Taylor Bridge, QLD - Opened	Low Similarity
1936	Kindee Bridge - Opened	-

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1940's	Double Crossing Bridge - Collapsed in a flood.	High Similarity
1955	The Allan Bridge, Segenhoe, Scone Vicinity – Re-opened	High Similarity
1967	The Silver Bridge, Ohio, USA - Collapsed	Low Similarity
1991	Hercilio Luz Bridge, Santa Catarina, Brazil – Removed from Service	Low Similarity

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5.2 Comparative Analysis of Highly Similar Bridges

The Allan Bridge, Segenhoe, Scone Vicinity (Rebuilt)

The Allan Bridge is locally listed in the Upper Hunter Shire Council Local Environmental Plan (under the name Allan Bridge) as Item Number 167. Originally built in 1937²⁸ and later rebuilt in 1955 downstream due to flooding, the bridge is similar in design to the Kindee Bridge. The Allan Bridge consists of timber piers, timber stringer beams and cross girders supported by steel cable and angle trusses. The Allan bridge is the only other remaining bridge of similar design to the Kindee Bridge. Having been rebuilt and having shorter spans, lower level clearance above water level and replaced abutments of unsympathetic design, the Allan Bridge is not as original as the Kindee Bridge. The Allan Bridge has a 4-tonne gross weight restriction posted for traffic, rendering its use solely for cars and small commercial vehicles.



Photograph 29 - Overview of the Allan Bridge²⁹

Photograph 30 - Overview of the Allan Bridge Deck²⁹



Photograph 31 – Allan Bridge Similar Cable Truss Design²⁹

Photograph 32 – Allan Bridge Non-Original Abutments²⁹

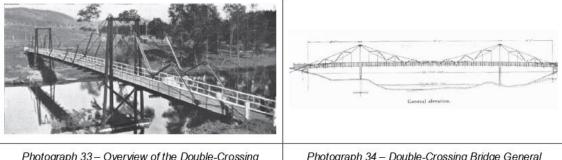
²⁸ Port Macquarie-Hastings Council - Kindee Bridge - Strategic Options Report RevA, 2018

²⁹ Chookman, Other bridges of Note, <http://bridges.chookman.id.au/others/others.html>

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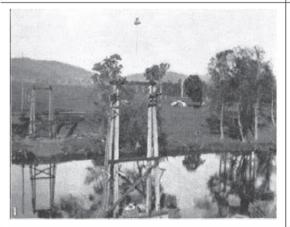
Double Crossing Bridge (No Longer in Existence)

The Double-Crossing Bridge was constructed across the Hastings River at Double Crossing near Yarras, and officially opened on 1 July 1933 by Hon. L. O. Martin, M.L.A., Minister of Justice. The requirement for a light, cheap bridge over the Hastings River at Double Crossing was required for a load capacity of no more than 10 tons (metric equivalent is 10.16 tonnes)³⁰. The bridge was lost in a flood in the late 1940's because of a miscalculation in the flood height. Flood water breached the bridge superstructure and destroyed the bridge.³¹ Along with the Allan Bridge, the bridge at Double Crossing is the only other trussed cable bridge known to have been constructed and was likely the most similar to the Kindee Bridge prior to its collapse. It is noted that the construction of the Double Crossing Bridge proceeded the construction of Kindee Bridge by three years.

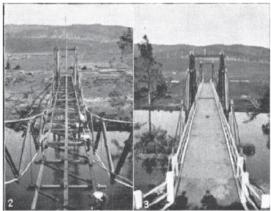


Photograph 33 – Overview of the Double-Crossing Bridge³⁰





Photograph 35 – Double-Crossing Bridge Stage 1 of Construction³⁰



Photograph 36 – Double-Crossing Bridge Stage 2 and 3 of Construction³⁰

³⁰ Main Roads, Suspension Bridge over the Hastings River Vol. V, No. 1, 1933

³¹ National Trust of Australia, Ellenborough - Bridge at Kindee Crossing, 1984

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5.3 Comparative Analysis of Moderately Similar Bridges

Tower Bridge London – Approach Spans

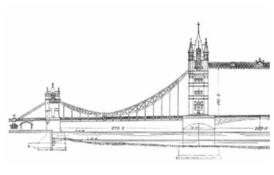
Tower Bridge in London was opened in 1894 and crosses the Thames in the London Borough of Tower Hamlets. The bridge was first listed with Historic England in 1973, under Listing Entry Number 1357515³². Tower Bridge consists of two distinct designs, the 270 ft (82m) suspended approaches and the bascule (moveable with counterweights) mid span. The bascule lifting section was designed to allow boats to pass under and is still functioning today.

The suspended approaches are relevant for comparison to the Kindee Bridge, where each chain comprises of two links, with the shorter link dipping from the abutment tower to the roadway, and the longer link rising from the roadway to the crest of the main tower. Links have an upper and lower chord, connected by diagonal braces to form a stiffened section.³³

Of the known remaining bridges in the world, the stiffened cable trusses of the Tower Bridge are the most similar in engineering design to the Kindee and Allan Bridges in NSW. The cable links converge at pier and deck connections and diverge between, with a truss assembly in the diverged section. This style of stiffening is not known to be used in any other remaining bridges. The suspension cable stiffening is economic because it increases the depth / capacity of the trusses at areas of critical stress. The stiffening of the cable section for Tower Bridge may have been utilised because it offers more rigidity than a typical suspension system, which is similar to the Kindee Bridge design concept.



Photograph 37 – Overview of the Tower Bridge³⁴



Photograph 38 – Elevation of the Tower Bridge Approach Span³⁵

³² Historic England, Tower Bridge (That Part in London Borough of Tower Hamlets) <

https://historicengland.org.uk/listing/the-list/list-entry/1357515>, 2019, Accessed 27/03/2019

³³ Wonders of World Engineering, Building the Tower Bridge < https://www.wondersofworldengineering.com/tower-

bridge.html>, 2017, Accessed 27/03/2019

³⁴ University of Bristol, *Tower Bridge – London*

https://www.bristol.ac.uk/civilengineering/bridges/Pages/NotableBridges/TowerBridge.html, Accessed 27/03/2019

³⁵ Fandom, Tower Bridge, https://london.fandom.com/wiki/Tower_Bridge, Accessed 27/03/2019

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Photograph 39 – Tower Bridge During Construction 1893³⁶



Photograph 40 – Tower Bridge Newly Complete 1894³⁶



Photograph 41 – Comparison of Trussed Chains (Tower Bridge)



Photograph 42 – Comparison of Trussed Cables (Kindee Bridge)

³⁶ Mashable, 1881-1895 – Building Tower Bridge < https://mashable.com/2016/01/21/london-towerbridge/#n4WajJBQIGqk>, Accessed 28/03/2019

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Mill Creek Park Suspension Bridge, Ohio, USA

The Mill Creek Park Suspension Bridge was opened in 1895 and services one-way vehicular traffic and two-way pedestrian traffic with a 5-ton load limit as of 2015³⁷. The bridge was listed on the National Register of Historic Places in the United States of America in 1976, under Item Reference Number 76001482.³⁸ The bridge consists of a deck supported by suspended eyebar chain cables hanging from steel piers. The bridge is similar to the Kindee Bridge because of the trussed cable suspension design which provides stiffness to the suspending elements. The bridge however does not use timber as a material, making the Kindee Bridge different.



Photograph 43 – Overview of the Mill Creek Park Suspension Bridge³⁷

Photograph 44 – Overview of the Mill Creek Park Suspension Bridge³⁷



Photograph 45 – Overview of the Mill Creek Park Suspension Bridge³⁷



Photograph 46 – Overview of the Mill Creek Park Suspension Bridge³⁷

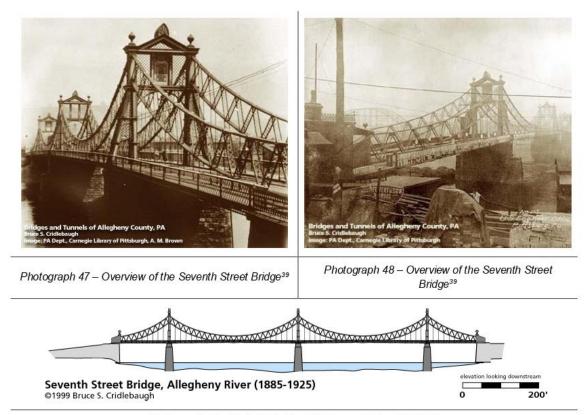
³⁷ Google Maps, *Mill Creek Park, Youngstown, OH, USA* < https://www.google.com/maps/@41.0731054,-80.6887984,3a,75y,20.6h,71.64t/data=!3m6!1e1!3m4!1sbQGnwnccSn46KoSU1lj_9A!2e0!7i13312!8i6656> Accessed 27/03/2019

³⁸ National park Service U.S. Department of the Interior, National Register of Historic Places, <https://www.nps.gov/maps/full.html?mapId=7ad17cc9-b808-4ff8-a2f9-a99909164466>Accessed 27/03/2019

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The Seventh Street Bridge, Pittsburgh, USA (No Longer in Existence)

The Seventh Street Bridge, or Irvine (Irwin) Street Bridge as it was otherwise known, was designed by Gustav Lindenthal and constructed in 1885. The bridge consisted of two main spans and two side spans, which were supported from braced suspension eyebar chain cables. The trussed cable system is similar to that at Kindee Bridge, however designed to carry larger loads. The bridge was replaced in 1925 by a new suspension bridge. It is noted that, unlike the Kindee Bridge, this bridge did not use timber as decking or for piers, nor do the trussed cable sections converge.³⁹



Photograph 49 – Scaled Sketch of the Seventh Street Bridge³⁹

³⁹ Pghbridges.com, Seventh Street Bridge 1885-1925, < http://pghbridges.com/pittsburghW/0584-4477/seventh1885.htm>, 2000, Accessed 27/03/2019

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Murrurundi Suspension Footbridge

The Murrurundi Suspension Footbridge, located at the intersection of Murulla Street and The Pages River, Murrurundi NSW, was built in 1927 to replace the original bridge which was washed away in a flood.⁴⁰ The bridge is locally listed in the Upper Hunter Shire Council Local Environmental Plan under Item Number I190. The bridge is located in a rural setting similar to the Kindee, Allan and Double-Crossing Bridges and has been included in this assessment because it is of similar materials and structural / aesthetic design, aside from the principal key characteristic trussed cable system to the aforementioned bridges, being of single cable design.



Photograph 50 - Overview of the Murrurundi Bridge⁴¹

Photograph 51 - Overview of the Murrurundi Bridge42



Photograph 52 – Overview of the Murrurundi Bridge⁴¹



Photograph 53 – Overview of the Murrurundi Bridge⁴¹

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⁴⁰ Murrurundi Community Portal, *Swinging Bridge*, https://murrurundi.nsw.au/attractions/swinging-bridge/>, 2019, Accessed 28/03/2019

⁴¹ Google Maps. Munulla St Murrurundi, New South Wales < https://www.google.com/maps/@-

^{31.7623183,150.8311574,3}a,75y,234.06h,84.59t/data=!3m6!1e1!3m4!1szau5KA3qt6PzThdojF9khA!2e0!7i13312!8i6656 >, 2010, Accessed 20/03/2019

⁴² Google Maps. Munulla St Murrurundi, New South Wales < https://www.google.com/maps/@-

^{31.7623183,150.8311574,3}a,75y,234.06h,84.59t/data=!3m6!1e1!3m4!1szau5KA3qt6PzThdojF9khA!2e0!7i13312!8i6656 >, 2010, Accessed 20/03/2019

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5.4 Comparative Analysis of Other Notable Bridges of Low Similarity

Several other bridges have been constructed around the world, which showcase similar design characteristics. These bridges have not undergone a thorough analysis in this comparative study; however, they have been included in this section of the assessment to appreciate similar design approaches.

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			1.000	-	and the second	

Bridge Name

The Hercilio Luz Suspension Bridge was the first bridge constructed which linked the island of Santa Catarina to the mainland. The bridge was the longest suspension bridge in Brazil and one of the longest in the world at the time. It is also one of the last bridges possessing eyebar cable trusses as part of its structural form. The bridge was removed from service in 1991 due to high corrosion levels and structural impairment. The design is different from the Kindee Bridge design because the truss stiffened cable portion of the bridge is inverted and only relevant to half the central span of the bridge. Photograph⁴³

The Walter Taylor Bridge, QLD: 1936

Also known as the Indooroopilly Toll Bridge, The Walter Taylor Bridge spans across the Brisbane River from Indooroopilly to Chelmer, Queensland. The bridge is listed on the Queensland Heritage Register under Item Number 600181. Officially opened in 1936, the design is different from the Kindee Bridge design because the truss stiffened cable portion of the bridge is inverted and only relevant to half the central span of the bridge.

Photograph44



Photograph



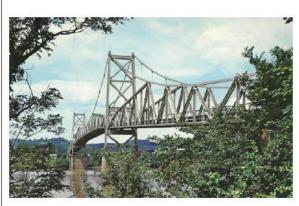
⁴³ Carvalho H. et al., The Challenges of Rehabilitating the Hercilio Luz Suspension Bridge,

http://www.scielo.br/scielo.php?script=sci_arttext&pid=S1679-78252017000400700>, 2017, Accessed 27/03/2019
⁴⁴ Visit Brisbane, *Walter Taylor Bridge*, https://www.visitbrisbane.com.au/brisbane/things-to-do/arts-and-entertainment/heritage-and-architecture/walter-taylor-bridge?sc_lang=en-au>, 2018, Accessed 27/03/2019

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The Silver Bridge, Ohio, USA: 1928

The Silver Bridge crossed the Ohio River to connect Point Pleasant, West Virginia with Kanauga, Ohio. The bridge consisted of a stiffened suspension eyebar structural system. The bridge collapsed in 1967 due to a minute crack which had formed in the original casting of the steel eyebar and grew over time with fatigue.⁴⁵ The bridge was different from the Kindee Bridge because the truss stiffened cable portions of the bridge were inverted. Photograph⁴⁶



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⁴⁵ Corrosion Doctors, Silver Bridge Collapse, < https://www.corrosion-doctors.org/Bridges/Silver-Bridge.htm>, 2019, Accessed 27/03/2019

⁴⁶ Laura Hollow Park, *The Silver Bridge*, <http://www.laurelhollowpark.net/chester/silverbridge.html>, 2017, Accessed 27/03/2019

6.0 Assessment of Significance

6.1 Introduction

Assessing the significance of items and preparing concise statements of significance allows asset managers to consider opportunities for using an item's heritage significance in a positive way. It also identifies constraints and what would be inappropriate development intentions for the item.

6.2 Relationship to Historical Themes

The significant elements of the Kindee Bridge relate to a number of key historical themes, as identified below. The NSW State themes have been taken from and numbered in accordance with the historical thematic framework developed for the NSW State Heritage Inventory.⁴⁷

Australian Theme	NSW State Theme	Associated Elements
1 Tracing the natural evolution of Australia	Environment - naturally evolved	The Hastings River
2 Peopling Australia	Convict	Spur road from the convict-built Oxley Highway
	Migration	River crossing
3 Developing local, regional and national economies	Commerce	Trade routesDistribution Points
	Communication	Tracks connecting Kindee to Long Flat linked the two communities
	Environment – cultural landscape	 Reshaping the landscape to allow safe passage over the Hastings River Key link between isolated communities
	Events	Flood lines and markings
	Exploration	Spur road from the explorer John Oxley's original route east
	Forestry	 Region known for using local timber in construction The Kindee Bridge's defining timber elements
	Transport	Made movement across the Hastings River possible for economic and social purposes
4 Building settlements, towns and cities	Towns, suburbs and villages	Connected the Kindee Valley region to Long Flat and Wauchope
5 Working	Labour	Bridge built during the Great Depression

⁴⁷ Heritage Office NSW, Assessing Historical Importance - A Guide to State Heritage Register Criterion A, 2006

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7 Governing	Law and Order	History of bridge weight limit exceedance
8 Developing Australia's cultural life	Creative Endeavor	 Bridge is of unusual design Bridge incorporates economic local materials.
9 Marking the phases of life	Birth and Death	R. B. Haydon, the contractor building the bridge was killed during its construction.

6.3 Curtilage Analysis

The NSW Heritage Office (now the NSW Office of Environment and Heritage – Heritage Division) publication, Heritage Curtilages⁴⁸, defines 'heritage curtilage' as the area of land surrounding an item or area of heritage significance which is essential for retaining and interpreting its heritage significance. Heritage curtilage can be classified as one of four types:

- Lot Boundary Heritage Curtilage: For places where the legal boundary of the allotment is defined as the heritage curtilage. The allotment should, in general, contain all significant related features, for example outbuildings and gardens, within its boundaries.
- Reduced Heritage Curtilage: For places where an area less than the total allotment is defined as the heritage curtilage. Applicable where not all parts of a property contain places associated with its significance.
- Expanded Heritage Curtilage: For places where the heritage curtilage is larger than the allotment. Particularly relevant where views to and/or from a place are of significance.
- Composite Heritage Curtilage: For larger areas that include a number of separate related places, such as heritage conservation areas based on a block, precinct or whole village.

The curtilage of the Kindee Bride as indicated in the Port Macquarie Hastings Local Environmental Plan 2011 Heritage Map Sheet HER_006A⁴⁹, is limited to the Council owned roadway corridor immediately adjacent to the bridge and bridge approach. The bridge allows single lane traffic and forms part of the spur road, Kindee Road, from the Oxley Highway. Kindee Road a sealed road which is understood to be owned and maintained by Port Macquarie-Hastings Council. The surrounding area has been defined as 'Rural Landscape' in the Port Macquarie Hastings Local Environmental Plan 2011 Heritage Map Sheet LZN_006A⁵⁰

⁴⁸ Warwick Mayne-Wilson - NSW Heritage Office and the Department of Urban Affairs and Planning, NSW, *Heritage Curtilages*, 1996

⁴⁹ Port Macquarie-Hastings Local Environmental Plan 2011, Heritage Map - Sheet HER_006A, 2010

⁵⁰ Port Macquarie-Hastings Local Environmental Plan 2011, Land Zoning Map - Sheet LZN_006A, 2010

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Figure 16 - Heritage Map - Sheet HER_006A

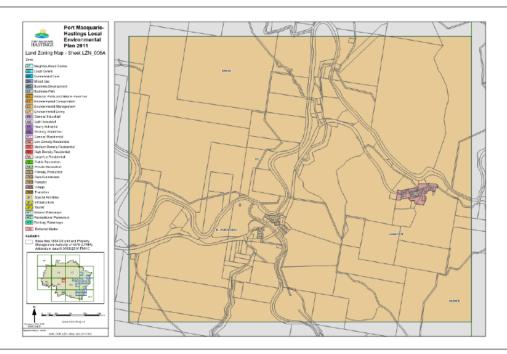


Figure 17 - Heritage Map - Sheet LZN_006A

6.4 Assessment Criteria

The heritage significance of an item is examined based on four values in the Australian ICOMOS Burra Charter⁵¹, which are:

- Historical significance
- Aesthetic significance
- Scientific significance
- Social significance

The NSW Heritage Manual, by the NSW Heritage office and the NSW Department of Urban Affair and Planning, provides a matrix for assessing the significance of an item by evaluating against the following criteria:

Criterion (a)

An item is important in the course, or pattern, of NSW's cultural or natural history (or the cultural or natural history of the local area);

Criterion (b)

An item has strong or special association with the life or works of a person, or group of persons, of importance in NSW's cultural or natural history (or the cultural or natural history of the local area);

Criterion (c)

An item is important in demonstrating aesthetic characteristics and/or a high degree of creative or technical achievement in NSW (or the local area);

Criterion (d)

An item has strong or special association with a particular community or cultural group in NSW (or the local area) for social, cultural or spiritual reasons; an item has potential to yield information that will contribute to an understanding of NSW's cultural or natural history (or the cultural or natural history of the local area);

Criterion (e)

An item has potential to yield information that will contribute to an understanding of NSW's cultural or natural history;

Criterion (f)

An item possesses uncommon, rare or endangered aspects of NSW's cultural or natural history (or the cultural or natural history of the local area);

⁵¹ Australia ICOMOS, The Burra Charter, The Australia ICOMOS Charter for Places of Cultural Heritage, 2013

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Criterion (g)

An item is important in demonstrating the principal characteristics of a class of NSW's:

- cultural or natural places; or
- cultural or natural environments.

or a class of the local area's:

- cultural or natural places;
- or cultural or natural environments.

6.5 Kindee Bridge Assessment of Significance

Criterion (a)

The Kindee Bridge is of historical significance because it provided a route of passage over the Hastings River from the Oxley Highway to the Kindee Valley region. It showcases the movement of European settlement in the region, following on from John Oxley's original expedition through the area.

Criterion (b)

The Kindee Bridge does not have a strong or special association with the life or works of a person.

Criterion (c)

The Kindee bridge showcases an example of the extensive use of local timber products during the interwar period. The bridge is an extremely rare example of a timber trussed cable bridge. Only 3 timber trussed cable bridges are known to have been constructed, and now only one other bridge remains, which is not at its original location and is of lesser span and height. The concept of the bridge's timber and trussed cable design is thought to be completely unique to this region of the world and shows creativity and ingenuity.

Criterion (d)

The region of the Hastings River is of significance to the traditional custodians of the Port Macquarie land area, the Birpai People. River crossings over the Hastings River by the Birpai Peoples are thought to have been constructed over time and relics from this time are thought to be in the vicinity of the bridge. The bridge itself is of high economic and social importance for the communities living in Kindee, where it provides access to communities across the Hastings River.

Criterion (e)

The Kindee Bridge is an important benchmark of a type of bridge design that is no longer used. The timber and trussed cable structural system can only be found on one other smaller scale bridge.

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Criterion (f)

Kindee Bridge is the only known remaining original timber trussed cable bridges in the world, other than Allan Bridge at Segenhoe, which was reconstructed downstream from its original location in 1955. Kindee Bridge provides physical evidence of an extremely rare form of bridge design.

Criterion (g)

The Kindee Bridge has characteristics of an important and very rare form of bridge class. It is one of two remaining timber trussed cable bridges, which also demonstrate a history of a local timber industry and a history of European settlement adapting to local geographical flooding conditions through lightweight, long span bridge designs.

6.6 Statement of Significance

The Kindee Bridge is an extremely rare example of a timber trussed cable bridge, which at the time of construction in 1936 provided a critical link between communities within the Kindee Region. Kindee Bridge is the only known remaining original timber trussed cable bridges in the world, other than Allan Bridge at Segenhoe, which was reconstructed downstream from its original location in 1955. Only three timber trussed cable bridges are known to have been constructed, with only one other (reconstructed) bridge now remaining, at Segenhoe NSW.

The use of steel angles as truss members within the cable suspension structural system is unique to Kindee Bridge and the Allan Bridge at Segenhoe NSW. The locally sourced timber piers are of high significance because they identify as a key characteristic of the regions self-sufficiency. The design of the bridge reflects an economic approach to allowing passage for farmers and their produce to pass from Kindee to Wauchope in the 1930's, ensuing its high economic value to the community. The Kindee Bridge is the highest and longest known timber trussed cable bridge in existence⁵². The bridge was a novel design when built, however similar structures are understood to have been built in France.

Historical bridges are a vital part of Australian heritage, as they tell the story of colonial movement and progression. The Kindee Bridge demonstrates the high durability of local timber and, in tangible terms, what was possible at the time in terms of the length of span possible using timber and light steel composite structures.

⁵² National Trust of Australia, Ellenborough - Bridge at Kindee Crossing, 1984

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7.0 Grading of Significant Elements

7.1 Introduction

The various elements of the bridge and surrounding site have been assessed, to determine their relative and independent grading of significance. When reviewing the significance of components relating to the bridge the following factors have been considered:

- Age
- Original quality of design
- General condition
- Extent of alterations from original design
- Ability to demonstrate rare design

This preliminary assessment of the various parts of the bridge uses the hierarchy of heritage significance assessment criteria of the NSW Heritage Office publication Assessing Heritage Significance⁵³.

Grading	Justification	Status
Exceptional (E)	Rare or outstanding element directly contributing to an item's local and State significance.	Fulfils criteria for local or State listing.
High (H)	High degree of original fabric. Demonstrates a key element of the item's significance. Alterations do not detract from significance.	Fulfils criteria for local or State listing.
Moderate (M)	Altered or modified elements. Elements with little heritage value, but which contribute to the overall significance of the item.	Fulfils criteria for local or State listing.
Little (L)	Alterations detract from significance. Difficult to interpret.	Does not fulfil criteria for local or state listing.
Intrusive (I)	Damaging to the it e m's heritage significance.	Does not fulfil criteria for local or State listing.

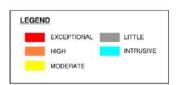
⁵³ NSW Heritage Office, Assessing Heritage Significance, 2001

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7.2 Grading of Significant Elements

Aspects of the bridge and surrounding site have been graded and are shown in the table and colour coded drawings below. A larger copy of Bridge Grading Drawing below is included in Appendix B of this CMP.



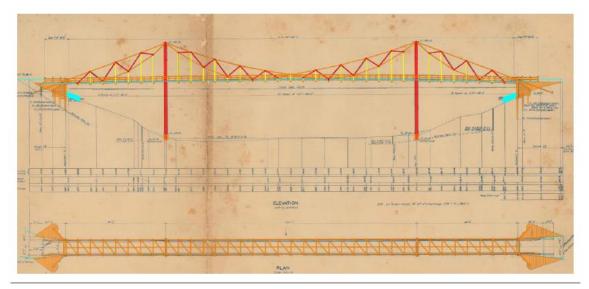


Figure 18 – Heritage Significance Gradings of Kindee Bridge Elements

7.3 Archaeological Potential

It has been noted that the site of Kindee Bridge is highly likely to be of Aboriginal archaeological significance, by Nathan Ross, Managing Director of Aboriginal Participation & Engagement Advisory Services.⁵⁴ If ground disturbance is proposed (beyond repair and maintenance that does not disturb the ground) a separate assessment and consultation will be required. This will need to be coordinated with local representatives of the Aboriginal Heritage Offices in the Northern Region, and in accordance with the Roads and the Maritime Services Procedure for Aboriginal Cultural Heritage Consultation and Investigation manual.⁵⁵ Considering the bridge would have been built in the most economically feasible location, it is expected that the site was originally an indigenous river crossing, because of its relatively narrow width and shallow banks at this point.

⁵⁴ Ross N., Aboriginal Participation & Engagement Advisory Services, 19/03/2019

⁵⁵ RMS, Roads and Maritime Services Procedure for Aboriginal Cultural Heritage Consultation and Investigation, 2011

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7.4 Grading of Individual Elements

The elements of the Kindee Bridge have been individually graded below in terms of their significance. The significance grading for each element is broadly based on:

- Age
- Original quality of design
- General condition
- Extent of alterations from original design
- Ability to demonstrate rare design

Condition State Descriptions

The following condition state definitions have been used to describe the condition of individual elements of the bridge from visual inspection. The condition state categories are taken from the RTA Bridge Inspection Procedure Manual⁵⁶, and are used within this CMP to comment on the existing condition of bridge elements for the purpose of maintenance scheduling. Relevant engineering reports should be reviewed to determine the actual condition of the structure.

Condition State:	Description:
1	Steel: No evidence of section loss, damage or decay. Timber: May have cracks splits and checks which have no effect on strength or serviceability.
2	 Steel: Surface rust or minor pitting has formed or is forming however there is no measurable loss of section. There may be minor deformations that do not affect the integrity of the element. There are no cracks in the steel or welds and all bolts and rivets are in sound condition. Timber: Minor decay, insect infestation, splitting, cracking, checking or crushing may exist, but none is sufficiently advanced to affect serviceability.
3	Steel: Heavy pitting may be present, and some measurable section loss is present locally, but not critical to structural integrity and / or serviceability of the element. There may be some loose or missing bolts or rivets. Defects have been assessed as not sufficient to impact on the ultimate strength and / or serviceability of the element. Timber: Medium decay, insect infestation, splitting, cracking or crushing has produced loss of strength of the element but not of a sufficient magnitude to affect the serviceability of the bridge.
4	 Steel: Section loss is sufficient to warrant analysis to ascertain the impact on the ultimate strength and / or serviceability of either the element or the bridge. There may be cracks and / or deformations in the steel or welds or there may be numerous failed / missing bolts or rivets. Defects may impact on the ultimate strength and / or serviceability of the element. Timber: Advanced deterioration. Heavy decay, insect infestation, splits, cracks or crushing has produced loss of strength that affects the serviceability of the bridge

⁵⁶ RTA, Bridge Inspection Procedure Manual – Second Edition, 2007

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Timber Piers

The timber piers supporting the cables are original and unique to this style of bridge, which is the largest of the 2 known remaining bridges in NSW.

Condition State:	3
Modifications:	Lower sections of downstream piles were replaced like-for-like in 1990. Base of posts in Pier 2 were epoxy filled in 1990 also.
Significance:	Exceptional



Photograph 54 – Timber Pier Cable Connection



Photograph 55 – Timber Pier Scarf Joint



Photograph 56 - Timber Piers Strapped with Steel Bands

Photograph 57 – Timber piers Covered in Timber Planks

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Timber Capwales

Timber capwales form the horizontal components of the bridge piers and are flashed in exposed sections. The timber capwales are of high significance to the bridge because they are of original design and showcase the historical use of timber in the region.

Condition State:	2
Modifications:	No known modifications
Significance:	High



Photograph 58 – Overview of Flashed Timber Capwale

Photograph 59 – Overview of Timber Capwale



Photograph 60 – Overview of Timber Capwale



Photograph 61 – Overview of Timber Capwale

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Timber Bridge Deck Cross Girders and Stringers

Timber cross girders are supported by steel hangers and support three perpendicular timber stringers which support the sealed ply deck. Cross girders are flashed at their connection to the steel hangers to reduce deterioration. The timber deck cross girders and stringers are of high significance to the bridge because they are of original design and showcase the historical use of timber in the region.

Condition State:	2
Modifications:	All sections have been replaced like-for-like in 1990
Significance:	High





Photograph 62 – Overview of Timber Stringers and Cross Girders

Photograph 63 – Overview of Timber Stringers and Cross Girders



Photograph 64 – Biological Growth / Bee Nests to Timber Cross Girders



Photograph 65 – Typical Timber Cross Girder to Hanger Connection

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Timber Abutments and Wingwalls

Timber Abutments and wingwalls consist of timber planks, retained by timber piles. The abutments and wingwalls are of high significance to the bridge because they are of original design and showcase the historical use of timber in the region.

Condition State:	Planks 1-3, Piers: 2-3
Modifications:	Abutment piles and timber boarding were replaced like-for-like in 1990. Timber piles beneath ground are original
Significance:	High



Photograph 66 - Timber Abutment

Photograph 67 - Timber Abutment



Photograph 68 - Timber Abutment

Photograph 69 - Timber Wingwall Piles

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Timber Guardrails

Timber guardrails along the bridge deck on the north and south sides are of high significance because they are mostly original. The rails are however typical of bridges of the period, and therefore not unique.

Condition State:	1-2
Modifications:	Sections to the eastern rail have been replaced in 2018 and 2019 after a car damaged the rails in 2018
Significance:	High



Photograph 70 - Timber Guardrails

Photograph 71 - Timber Guardrails



Photograph 72 – Timber Guardrails



Photograph 73 – Repaired Timber Guardrails57

57 Wood S. Photograph by Stephen Wood, 27/09/2018

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Timber Kerbs

Painted timber kerbs along the bridge deck on the north and south sides are of high significance because they are of original design. However, they are typical of bridges of the period, and therefore not unique.

Condition State:	1-2
Modifications:	Kerbs were replaced like-for-like in 1990
Significance:	High



Photograph 74 – Overview of Timber Kerbs

Photograph 75 - Overview of Timber Kerbs



Photograph 76 – Overview of Timber Kerbs



Photograph 77 – Overview of Timber Kerbs

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Steel Hanging Rods

The steel hanging rods for the timber deck are mostly original, or original by design and in a reasonable condition for their age. The steel deck hanging rods are of moderate significance to the bridge because they are of original design (replaced in the 1990s) but not unique to this bridge.

Condition State:	2
Modifications:	All hanger rods were replaced in the 1990s, with several also replaced in 2017 and 2019.
Significance:	Moderate



Photograph 78 - Hanging Rod Connection to Cable Truss

Photograph 79 – Hanging Rod Connection to Cable Truss



Photograph 80 - Hanging Rod Weld (Replacement Rod)



Photograph 81 - Hanging Rod Connection to Timber Cross Girder

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Steel Angle Truss Members

The Steel equal angle truss members are unique to this style of bridge, which is the largest of 2 known remaining bridges of this style in the world. Trusses have been carefully laid out to ensure no compressive forces are applied to cables in the truss system. The trusses are of exceptional significance to the bridge because they are original and are unique to this and only one other remaining bridge in the world. The early use of bolts alongside rivets in steel conjunction is also considered significant.

Condition State:	2
Modifications:	Sections of the trusses have been replaced after the car crash in 2018.
Significance:	Exceptional



Photograph 82 - Steel Angle Truss Members

Photograph 83 – Steel Angle Truss Members



Photograph 84 – Steel Angle Truss Members



⁵⁸ Wood S., Photograph by Stephen Wood, 21/11/2018

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Steel Suspension Cables

The Steel cables are geometrically positioned so they don't take compressive forces. The cables are an integral part of the lightweight design, which allowed for the bridge to be constructed with minimal temporary works from below. The cables are of high significance to the bridge because they are original and form part of the exceptional trussed cable suspension form unique to this design of bridge.

Condition:	2
Modifications:	No known modifications
Significance:	High



Photograph 86 – Top Chord Cable

Photograph 87 - Top Chord Cable



Photograph 88 – Top and Bottom Chord Cables



Photograph 89 - Top and Bottom Chord Base Pin

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Steel Cable Node Blocks

The Steel cable node blocks transfer load between the steel trusses, cables and hangers. The cable node blocks are of exceptional significance to the bridge because they are original and form part of the exceptional and unique trussed cable suspension form.

Condition:	2 (Block), 3-4 (Bolts)
Modifications:	No known modifications
Significance:	Exceptional



Photograph 90 - Overview of Cable Node Block

Photograph 91 - Overview of Cable Node Block



Photograph 92 – Overview of Cable Node Block



Photograph 93 - Overview of Cable Node Block

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Steel Pier Bearings

The Steel pier bearings transfer load between the steel suspension cables and the timber piers. The pier bearings are of exceptional significance to the bridge because they are original and utilise a central pin, contributing to the unique trussed cable suspension form.

Condition:	2
Modifications:	No known modifications
Significance:	Exceptional



Photograph 94 - Overview of Steel Pier Bearings





Photograph 96 – Overview of Steel Pier Bearings⁵⁹



Photograph 97 – Overview of Steel Pier Bearings59

59 Wood S., Photograph by Stephen Wood, 16/04/2015

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Steel Abutment Bearings

The Steel abutment bearings transfer load between the steel suspension cables and the abutments. The abutment bearings are of high significance to the bridge because they are original and contribute to the unique trussed cable suspension form.

Condition:	2
Modifications:	No known modifications
Significance:	High





Photograph 98 – Overview of Steel Abutment Bearings60

Photograph 99 – Overview of Steel Abutment Bearings®



Photograph 100 – Steel Abutment Bearings60



Photograph 101 – Steel Abutment Bearings60

60 Wood S., Photograph by Stephen Wood, 16/04/2015

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Steel Anchor Rods

The steel anchor rods transfer load between the steel abutment bearings and ground anchors. The anchor rods are of high significance to the bridge because they are original and form part of the exceptional and unique trussed cable suspension form.

Condition:	3-4
Modifications:	No known modifications
Significance:	High



Photograph 102 - Overview of Steel Anchor Rods

Photograph 103 – Overview of Steel Anchor Rods61



Photograph 104 - Overview of Steel Anchor Rods



Photograph 105 – Overview of Steel Anchor Rods61

⁶¹ Wood S., Photograph by Stephen Wood, 16/04/2015

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Steel Guardrails

Steel guardrails along the east and west approaches provide vehicles safe access to the bridge. The steel guardrails are intrusive to the significance of the bridge because they are not original and not unique.

Condition:	2
Modifications:	New in 1990, no known modifications
Significance:	Intrusive



Photograph 106 - Guardrail Overview

Photograph 107 - Guardrail Overview



Photograph 108 - Guardrail Overview

Photograph 109 - Guardrail Overview

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Steel gates, Markers and Miscellaneous Items

Steel gates, flood level markers and miscellaneous items have been added to the bridge over time and restrict access / provide important flood level information. The gates, markers and miscellaneous items are intrusive to the significance of the bridge because they are not original and not unique.

Condition:	2
Modifications:	Not part of original design
Significance:	Intrusive



Photograph 110 - Gate Near Abutment

Photograph 111 - Gate Near Abutment



Photograph 112 – Flood Markers Near Abutment

Photograph 113 – Flood Markers Near Abutment

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Bridge Deck

The bridge deck consists of bitumen laid over plywood decking, bearing on timber stringers which are supported by the cross girders below. The bridge deck is intrusive to the significance of the bridge because it not original and not unique.

Condition:	3
Modifications:	Original timber decking planks have been replaced with plywood and a bitumen topping
Significance:	Intrusive



Photograph 114 – Bituminous Bridge Deck Surface

Photograph 115 - Bituminous Bridge Deck Surface



Photograph 116 - Bituminous Bridge Deck Surface



Photograph 117 – Deteriorated Plywood and Bituminous Bridge Deck Surface

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Concrete Pier Footings

The concrete pier and abutment footings at each side of the Hastings River, transfer load from the timber piers to the ground below. The concrete pier footings are of high significance to the bridge because they are original but not unique to this bridge.

Condition:	1
Modifications:	No known modifications
Significance:	High



Photograph 118 - Overview of Concrete Pier Footings

Photograph 119 - Overview of Concrete Pier Footings



Photograph 120 – Overview of Concrete Pier Footings



Photograph 121 - Overview of Concrete Pier Footings

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8.0 Constraints and Opportunities of Proposed Actions

8.1 Introduction

This section outlines various issues involved in the preparation of the conservation guidelines for the site. It takes into consideration constraints and opportunities arising from the assessment of significance given in Section 7.0 and procedural constraints imposed by cultural conservation methodology such as the Australian ICOMOS Burra Charter. Other constraints and opportunities that may be due to legislative compliance are also briefly outlined. Potential constraints and opportunities will arise from the following:

- Ownership
- Heritage values / Significance
- Physical condition and maintenance
- Operational requirements
- Security, safety and access
- Statutory requirements and
- Interpretation
- 8.2 Owners Requirements

At this time of this CMP the Kindee Bridge is owned and maintained by the Port Macquarie-Hastings Council. The needs of the owner and stakeholders of the bridge need to be considered when formulating guidelines for the conservation, use and management of the bridge. The owning authority, Port Macquarie-Hastings Council, understands that a need to cross the Hastings river at this point is as necessary now as it was when originally constructed. The potential loading on the bridge however has changed over time, with a general increase in truck tonnage. There have been several instances in the past with overweight trucks passing over the bridge.

It is understood that the cost of maintaining the bridge is a continual issue for Council, which is becoming more expensive. Unsuccessful attempts have been made in the past to hand over the care and maintenance of the bridge to Roads and Maritime Services.⁶²

⁶² Port Macquarie Hastings Council, *Kindee Bridge - Maintenance Planning Report*, 2018

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8.3 Proposed Actions

As part of our brief, provided by the Port Macquarie-Hastings Council, 5 proposed options have been outlined as solutions to the future of the Kindee Bridge. The options from the 'Options Report and Management Strategy Report¹⁶³, have been summarised below with potential positives and negatives listed. The options provided have been ordered by the Port Macquarie-Hastings Council in terms of level of intervention.

Option 1 – Do Nothing

This option involves taking on minimal maintenance, allowing the bridge to fall into a state of disrepair, with inevitable natural or physical demolition.

Positives	 Less initial cost to Council Will not initially impact or trigger requirements for an Aboriginal Heritage Impact Permit (AHIP).
Negatives	 No alternative for crossing the Hastings River, the only alternate route being Pipeclay Road. This will isolate the local communities socially and economically Loss of the last remaining timber trussed cable bridge in its original location in the world The significant heritage value of the bridge will be lost.

Option 2 – Minor Structural Repairs

This option involves carrying out key structural repairs as required and within a specified budget. Works would typically include, replacing damaged anchor bolt sheathing at abutments, replacing node block clamp bolts, replace hanger base plates, and inspection / replacement of timber abutments. Painting is also considered to be part of this minor repair scope. The bridge is anticipated to remain in service for 5-10 years before becoming unfit for service and requiring demolition, because the key structural elements (piers) will continue to degrade. (as assessed by Wood Research and Development Pty Ltd).

Positives	 Low initial cost to Council Will not initially impact or trigger requirements for an Aboriginal Heritage Impact Permit (AHIP) The significance of the bridge can be preserved and interpreted for future years An additional bridge will not need to be constructed in the near future (next 5 to 10 years) A crossing will exist across the Hastings River.
Negatives	• The service life expectancy of the bridge is predicted to reach a maximum of 10 years. After this time significant costs will be associated with demolition and construction of a new bridge.

⁶³ Port Macquarie-Hastings Council - Kindee Bridge - Strategic Options Report Rev A, 2018

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- After the predicted 5-10-year service life the significant heritage value of the bridge could be lost.
- The eventual planned loss of the last remaining timber trussed cable bridge in its original location in the world.

Option 3 – Full Remediation of the Existing Bridge

This option requires rehabilitation, reconstruction and protection of the bridge. The intended service life to be achieved from this level of repair would be 30-50 years. The remediation would include all components of the bridge.

Positives	 Will likely not impact or trigger requirements for an Aboriginal Heritage Impa Permit (AHIP) The significance of the bridge can be preserved and interpreted for future yea An additional bridge will not need to be constructed in the near future Preservation of an extremely rare and the last known remaining timber trusse cable bridge in its original location in the world. Bridge will be returned to its 10-tonne limit 	
Negatives	 Moderate initial cost and moderate continual maintenance costs. The bridge's 10 tonne load limit may be too low for some trucks and emergency vehicles. 	

Option 4 – Strengthen Existing Bridge

This option involves strengthening the existing bridge by supplementing structure with modern designs to cater for larger loads. Strengthening however has been acknowledged by Council to be unsuitable for the Kindee Bridge and has been removed as an option.

Positives	 The bridges load capacity may be increased A crossing will continue to exist over the Hastings River at Kindee Crossing The bridge can be preserved and interpreted to some extent for future years An additional bridge will not need to be constructed in the near future Adaption of an extremely rare and the last known remaining timber trussed cable bridge in its original location in the world.
Negatives	 The heritage significance of the bridge will be compromised by the alteration and undermining of the bridges significant structural form Significant cost Will likely trigger requirements for an Aboriginal Heritage Impact Permit (AHIP).

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Option 5 – Replace with New Bridge on Current Alignment

This option involves demolishing the existing bridge and replacing the bridge with a modern single lane bridge. The new bridge would follow a similar alignment, also be high level and need to span large distances between piers (same as existing).

Positives	 The bridges load capacity may be increased Fewer ongoing maintenance costs in the short term.
Negatives	 The heritage significance of the bridge will be lost Significant cost Will trigger requirements for an Aboriginal Heritage Impact Permit (AHIP). Loss of the last remaining timber trussed cable bridge in its original location in the world.

Option – Conclusion Based on Significance

In terms of the bridge's heritage significance, an approach to maintenance and repair of 'as much as necessary and as little as possible' should be maintained. Following from the heritage assessment of the bridge, the bridge has been identified as the last remaining known timber trussed cable bridge in the world in its original location. From a heritage viewpoint and considering the bridges significance, Option 3 – Full Remediation of the Existing Bridge is the most appropriate option.

8.4 Current Heritage Listings

List	Listing
World Heritage List	No
National Heritage List	No
NSW State Heritage Register	No
PMHC LEP 2011	Yes

8.5 Issues Arising from the Statement of Significance

The assessment of significance provides a framework of the extent to which the heritage item should be conserved and the extent to which it can continue to function to support future use. The overall conservation policy framework is based on the following recognition of heritage significance:

- The Kindee Bridge is an exceptionally rare example of a timber trussed cable bridge. It is the only known remaining example of a timber trussed cable bridge in its original location in the world, which provided a critical link between communities within the Kindee Region in 1936.
- The design of the bridge reflects an economic approach to allowing passage for farmers and their produce to pass from Kindee to Wauchope in the early 1930's, ensuing its high economic value to the community.

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- The Kindee Bridge is the highest and longest of two remaining known trussed cable bridges in the world⁶⁴.
- The Kindee Bridge demonstrates the high durability of local timber, and in tangible terms, what was possible at the time, in terms of the length of span possible using timber and light steel composite structures.

The future conservation, development and ongoing management of the Kindee Bridge should consider constraints arising from the identified heritage values of the bridge and its setting. Constraints that arise from the assessed heritage values of the place include:

- Original elements of the bridge should be retained in their original form wherever possible
- Structural components that require replacement should be replaced to match the original fabric in size, proportion and general appearance as closely as possible.
- The trussed cable suspension structural form should be retained.

While the heritage significance of the Kindee Bridge poses constraints on management, it creates opportunities for enhanced recreational facilities, such as tourism, education, heritage studies, continual use and potential adaptive reuse.

8.6 Physical Condition and Maintenance of the Bridge

The bridge is in a reasonable to poor condition in parts, with repairs having periodically been carried out as required over time, as summarised in Appendix A. Significant structural repairs were carried out in 1990 and emergency repairs were carried out in September 2018, to address damage from a car which collided with the south-eastern guardrail and truss.

The maintenance approach for the bridge to date, on review of correspondence and documentation, has been carried out in an ad hoc manner. It is unclear if implementation of all said repairs has been carried out, which has allowed the existing fabric of the bridge to reach a deteriorated condition in parts. We are aware that Wood Research and Development Pty Ltd has investigated the integrity of the bridge fabric. As such this CMP will not provide further specific review of material condition. Additionally, Noble Engineering Services has provided a report on observations and non-destructive testing (NDT) results of steel bridge components.

Funding opportunities should be sought to undertake the catch-up maintenance works as well as refurbishment identified in the Conservation Policies and Schedule of Conservation Works in this CMP, as well as other structural engineering requirements. This will extend the life of the fabric and reduce the risk to public safety.

⁶⁴ National Trust of Australia, Ellenborough - Bridge at Kindee Crossing, 1984

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8.7 Operational Requirements

The bridge was originally designed and constructed under the design criteria of; 'traffic will always be light', and the loading adopted for design was as follows:

- a 10-ton (gross) vehicle (in metric units this would equate to 10.16 tonne), seven tonnes on rear axle, three tons on front axe, 11-foot wheelbase, with 25% impact for deck and stringers, 20% for cross girders and 15% for cables, etc., or
- b) a distributed load of 60lb per square foot with no impact"65

In 2018 the bridge was closed to traffic due to a car crash and consequent repairs. The original design of 10 ton (10.16 metric tonne) is reportedly exceeded occasionally due to a lack of other reasonable access roads being available. Reports on the integrity of the bridge have been previously carried out. In 2010 it was recommended that the loading capacity be reduced to 7 tonne fixed axle vehicles only.⁶⁶ A structural analysis and extensive repairs may increase the load capacity of the bridge and return it to a reasonable level of service for the local community, however load capacity is unlikely to increase above the original design loads.

The bridge owner should consult the certifying authority for the bridge, for advice regarding the loading requirements for emergency vehicles such as ambulances and fire trucks. If the bridge is considered incapable of providing required emergency access, suitable precautions or arrangements should be made.

8.8 Safety

Remaining in the control of Port Macquarie-Hastings Council and on the LEP Heritage Register may potentially reduce other safety requirements on the bridge, required under AS5100, such as traffic safety barriers. The bridge was constructed before present Code Standards, and heritage objectives, to conserve the bridge, may argue against requirements to upgrade the bridge to current Code Standards. However, any reconstruction or strengthening of the bridge would require compliance with AS5100, which would add considerable cost.

8.9 Road level

The existing Kindee Bridge was designed for a particular flood level which is not known to have been exceeded to date. It should be advised that to reconstruct the bridge may require more conservative flood level requirements to be apply, which would impact the approaches. It should be considered that the original design of the bridge was selected to reduce the significant costs of the approaches⁶⁷.

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⁶⁵ Department of Main Roads - NSW Lower North Coast Division, *Municipality of Hastings. Bridge over Hastings River at Kindee Crossing*, 1984 (letter referencing original design at an earlier date)

⁶⁶ Port Macquarie Hastings Council, *Kindee Bridge - Maintenance Planning Report*, 2018

⁶⁷ Karmalsky V., Britton A.T., Transaction of the Institution, Vol. XVIII, Three Span Light Suspension Bridge over Hastings River at Kindee Crossing, NSW, 1937

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8.10 Interpretation

Existing interpretation at Kindee Bridge consists of a metal plaque fixed onto a concrete bollard at the eastern approach. The plaque was erected to celebrate Australia's Bicentenary in 1988 by the Department of Main Roads and the National Roads & Motorists' Association (N.R.M.A.).⁶⁸ There is an opportunity to improve interpretation through a comprehensive interpretation plan, which could include highlighting its significance and rarity through digital media, for example.

9.0 Conservation Policies

9.1 Introduction

The conservation policies set out in this document are a guide for the restoration and future maintenance of Kindee Bridge and associated elements. The purpose of the conservation policies is to retain and protect the heritage significance of the bridge.

9.2 Application of the Burra Charter

The Australia ICOMOS Charter for the Conservation of Places of Cultural Significance (known as the Burra Charter)⁶⁹ is widely accepted in Australia as the underlying methodology by which all works to structures and sites that have been identified as having national, State and regional significance are to be undertaken.

9.3 Terminology

The following terminology will be used to describe the historical fabric of the site and are included here to assist in understanding the intent of the conservation requirements in this section. For further clarrification and explanation of the below definitions, refer to the Engineering Heritage & Conservation Guidelines⁷⁰:

Place

Means a geographically defined area. It may include elements, objects, spaces and views. Place may have tangible and intangible dimensions.

Cultural significance

Means aesthetic, historic, scientific, social or spiritual value for past, present or future generations. *Cultural significance* is embodied in the *place* itself, its *fabric*, *setting*, *use*, *associations*, *meanings*, records, *related places* and *related objects*.

Fabric

Means all the physical material of the *place* including elements, fixtures, contents and objects.

Conservation

Means all the processes of looking after a place so as to retain its cultural significance.

⁶⁹ Australia ICOMOS, The Burra Charter - The Australian ICOMOS Charter for Places of Cultural Significance, 2013

⁶⁸ NRMA, Historic Bridges of New South Wales – Suspension Bridge at Kindee Crossing, 1988

⁷⁰ Engineers Australia, Engineering Heritage & Conservation Guidelines, 2014

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Maintenance

Means the continuous protective care of a *place*, and its setting. *Maintenance* is to be distinguished from repair which involves *restoration* or *reconstruction*.

Preservation

Means maintaining a *place* in its existing state and retarding deterioration

Restoration

Means returning a *place* to a known earlier state by removing accretions or by reassembling existing elements without the introduction of new material.

Reconstruction

Means returning a *place* to a known earlier state and is distinguished from restoration by the introduction of new material.

Adaptation

Means changing a *place* to suit the existing use or a proposed use.

Use

Means the functions of a *place*, including the activities and traditional and customary practices that may occur at the place or are dependent on the place.

Compatible Use

Means a use which respects the *cultural significance* of a *place*. Such a use involves no, or minimal, impact on cultural significance.

Setting

Means the immediate and extended environment of a *place* that is part of or contributes to its *cultural significance* and distinctive character.

Related Place

Means a *place* that contributes to the *cultural significance* of another place.

Related Object

Means an object that contributes to the cultural significance of a place but is not at the place.

Associations

Mean the connections that exist between people and a place.

Meanings

Denote what a *place* signifies, indicates, evokes or expresses to people.

Interpretation

Means all the ways of presenting the *cultural significance* of a *place*.

9.4 Development of Conservation Policy

The conservation management policies identified herein, has the protection and conservation of the fabric of the Kindee Bridge as its guiding principle. The conservation works described are in reference to their assessed level of significance, as set out in Section 6.0. It is accepted that in some

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circumstances, considering the condition, social context of the bridge and the intended use of the bridge, some loss and modification of fabric may be acceptable.

9.5 Schedule of Conservation Policies – Kindee Bridge

9.5.1 General Policies

Policy	Number	Policy
9.5.1	A	Conservation Approach – The conservation approach of the historical bridge fabric should be based on respect for the existing fabric. Competent direction and work should be carried out at all levels of maintenance. Work should be carried out by professionals / tradespeople of appropriate specialist experience in traditional building with specific knowledge in timber structures.
9.5.1	В	Schedule of Conservation Works - The schedule of conservation works for the Kindee Bridge in this CMP relies on the graded assessment of significance for each element. Conservation of the original fabric should be a priority in future work, following by conservation of fabric which is of original design.
9.5.1	С	Structural Form – The structural form of the bridge should retain its architectural character, including materials, scale and style. Works to areas and elements of high significance should generally be restricted to preservation, restoration and maintenance with some reconstruction as required.
9.5.1	D	Use – The bridge should remain in use as a bridge for vehicles or as a cycling / footbridge if structural assessment will not permit vehicle loads.

9.5.2 Role of the Conservation Management Plan and Review of Policies

Policy	Number	Policy
9.5.2	A	Review of Conservation Management Plan – This CMP proposes a framework for the ongoing use and maintenance of the bridge; however, it is acknowledged that circumstances change over time. Conservation policies must progressively adapt to changing situations to remain relevant. Any reviews of this document should be based on the Burra Charter and other endorsed documents by the Heritage Division of the NSW Office of Environment and Heritage. Reviews are to consider relevant legislation, appropriate literature and recognised conservation practices and procedures.
9.5.2	В	Regular Maintenance Schedule – A regular maintenance schedule should be implemented to ensure ongoing conservation of significant fabric. Maintenance schedules for individual components of the bridge have been included in this Conservation Management Plan. The Maintenance Schedule should be updated every 5-10 years to coincide with the Conservation Management Plan review. The Maintenance Schedule should reflect and document all works carried out, and no works should negatively impact on the significance of the fabric.
9.5.2	С	Professional Advice on Policies – The conservation management of the bridge must be undertaken in consultation with heritage practitioners with relevant expertise as required.

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9.5.3 Conservation Methodology

Policy Nu	ımber	Policy
9.5.3	A	Elements of Exceptional Significance – Any works that affects fabric with Exceptional Significance should be limited to preservation, restoration, reconstruction and adaptation as defined in The Burra Charter, and should be carefully maintained. It is essential that elements of Exceptional Significance remain visible and appreciable.
9.5.3	В	Elements of High Significance – Any works that affects fabric with High Significance should be limited to preservation, restoration, reconstruction and adaptation as defined in The Burra Charter, and should be carefully maintained. It is essential that elements of High Significance remain visible and appreciable, however it is accepted that minor impacts may be acceptable in the context of continual use / reuse.
9.5.3 (с	Elements of Moderate Significance – Any works that affects fabric with Moderate Significance should be limited to preservation, restoration, reconstruction and adaptation as defined in The Burra Charter, and should be carefully maintained. Work involving reduction / removal, may be acceptable where it is necessary for the proper function of the bridge and is beneficial to, or does not reduce, the overall significance of the bridge.
9.5.3	D	Elements of Little Significance – Any works that affects fabric with Little Significance can include both retention and removal.
9.5.3	E	Elements of Intrusive Significance – These elements detract from the significance of the bridge, despite demonstrating continual use. The preferred long-term approach is to remove these elements and replacement with compatible items which help retain the overall significance of the bridge. This can only be done where appropriate.
9.5.3	F	Repair of Damage – Wherever possible, damage caused by use or previous works should be repaired to match the original fabric and not detract from the significance of the bridge.
9.5.3	G	Reveal Concealed Elements – Wherever possible, significant / original elements which have been concealed from view by later works or environmental conditions should be revealed.
9.5.3	н	Avoid Defective Practices – The intention of reconstruction and reinstatement is to return elements to their original condition, however building practices or products which are known to be defective should not be used. Reinstated fabric should be discretely date stamped to indicate this work.
9.5.3	I	Modern Techniques - As a rule, repair and maintenance of significant fabric shall employ traditional materials and methodologies. Modern techniques and materials which offer substantial conservation benefits may be appropriate in certain circumstances.
9.5.3 ,	J	Determine Causes of Deterioration – The causes of original deficiency for damaged or deteriorated elements shall be determined to establish whether the repairs will be susceptible to the same problems.

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9.5.3	К	Structural Elements – Any required structural strengthening should be discrete and not diminish from the overall significance of the bridge. Any works should be sympathetic to the original design and not undermine the structural intention and form of the bridge.
9.5.3	L	Colour – Repainting of steel and timber elements of the bridge should be carried out to match the original colour. Any requirements for colour outside this scope require colour schemes, details and finishes to be sent to a suitably experienced Heritage Consultant for endorsement.
9.5.3	Μ	Management of Archaeological Findings – In the encounter of archaeological findings on / near the site, management of the artefacts shall be undertaken in accordance with the Local Aboriginal Land Council (Birpai) recommendations and consent condition of any excavation permit that is required under the provision of the Heritage Act 1977.

9.5.4 Maintenance

Policy	Number	Policy
9.5.4	A	Inspections - Annual inspections and regular maintenance are required to keep the bridge in a safe operating condition.
9.5.4	В	Management – The Local Council is responsible for the maintenance of the bridge and shall prepare a thorough maintenance manual that follows the intent of this CMP.
9.5.4	С	Maintenance Guide – Guidance for maintenance planning may be taken from the RTA (RMS) Timber Bridge Manual series.

9.5.5 Public Access, Engagement and Interpretation

Policy	Number	Policy
9.5.5	A	Interpretation – An existing plaque exists near the bridge which explains the brief history of the bridge's construction. A separate Interpretation Strategy Plan should be prepared and implemented to describe the significance of the bridge.
9.5.5	В	Distribution of the CMP – Copies of the final CMP should be lodged with all relevant administrative, maintenance, heritage and archival bodies / agencies, as well as being held by the Port Macquarie-Hastings Council, and be readily available for public reference.

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9.5.6 Relationship to Commonwealth, State and Local Authorities

Policy Number		lumber	Policy
	0	А	Code Compliance – The bridge is to comply with current Codes and Standards where practical. If early or original elements require removal or concealment in order to achieve compliance, the alteration should be reversible.
	0	В	Consultation – The Local Aboriginal Land Council is to be consulted on all proposed developments regarding the bridge and its curtilage.

9.6 Schedule of Conservation Works – Kindee Bridge

9.6.1 Annual Maintenance - General Works

Work	ltem	Policy
9.6.1	A	Removal of fire hazards from the proximity of the bridge with emphasis on grass and brush near the abutments and near bridge piers.
9.6.1	В	All accessible bolted connections should be checked and retightened with particular emphasis on the attachment of capwales and node blocks.
9.6.1	С	Preservative protection should be reapplied where practical and spot checks should be performed on abutment components to see whether further work is needed.
9.6.1	D	Minor collision or flood damage should be repaired. Major damage should be reported, and remedial repairs carried out if possible.
9.6.1	E	 Visual inspection to be conducted which should include the following and be thoroughly documented: All timber components including the substructure Observation of the bridge under transient loading Identification of any obvious defects and damage Identification of any obvious deterioration Specific attention to previously reported problem areas

9.6.2 Three-Year Maintenance - General Works

Work	Item	Policy
9.6.2	А	All bolted connections including timber bracing are to be retightened.
9.6.2	В	Timber piles and other ground contact members are to be exposed for retreatment with preservative protection.
9.6.2	С	Difficult areas which cannot be exposed easily can be flooded with preservative (this must be carried out with proper attention to protecting the environment).

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9.6.2 D All flashing should be removed to expose the members for inspection, cleaning and retreatment.

9.6.3 Timber Piers

Work	ltem	Policy
9.6.3	А	Timber testing to identify internal voids to be carried out by an appropriately qualified contractor.
9.6.3	В	If required, replacement timber should match the original timber in grade and species as much as possible, should be of unobtrusive design and not detract from the significance of the bridge.
9.6.3	С	Deteriorated sections of timber to be replaced where required, removing as much as necessary but as little as possible.
9.6.3	D	Missing timber horizontal planking across the piers are to be reinstated with sympathetic timber which matches the existing and does not detract from the significance of the bridge.
9.6.3	Е	Steel pier collars at the pier scarf joint should be cleaned, corrosion removed and repainted to match the original colour.
9.6.3	F	Cast steel cable bearing to be cleaned, corrosion removed and repainted to match the original colour.
9.6.3	G	Timber bracing to timber piers to be inspected for capacity and retained where practical. Bolted connections to be inspected and corrosion removed, blast cleaned and repainted to match original colour.
9.6.3	н	Steel bearing pier to be corrosion treated and repainted as part of the annual maintenance plan for the bridge. Paint to match original colour and texture.
9.6.3	I	Steel hold down bolts and plates connecting the piers to concrete footings are to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.
9.6.3	J	Redundant fixings are to be removed and appropriately patched

9.6.4 Timber Capwales

Work	Item	Policy
9.6.4	А	Timber testing to identify internal voids to be carried out by an appropriately qualified contractor.
9.6.4	В	If required, replacement timber should match the original timber in grade and species as much as possible, should be of unobtrusive design and not detract from the significance of the bridge.
9.6.4	С	Deteriorated sections of timber to be replaced where required, removing as much as necessary but as little as possible.

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9.6.4	D	Flashing above timber capwales to be inspected as part of the annual maintenance plan for the bridge. Flashing to be lifted and covered timber is to be inspected closely for damage. Damaged flashing to be sympathetically replaced to match existing.
9.6.4	Е	Steel hold down bolts and plates to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.

9.6.5 Timber Bracing

Mark	ltana	Belley
Work	Item	Policy
9.6.5	А	Timber testing to identify internal voids to be carried out by an appropriately qualified contractor.
9.6.5	В	If required, replacement timber should match the original timber in grade and species as much as possible, should be of unobtrusive design and not detract from the significance of the bridge.
9.6.5	С	Deteriorated sections of timber to be replaced where required, removing as much as necessary but as little as possible.
9.6.5	D	Flashing above timber horizontal bracing s to be inspected as part of the annual maintenance plan for the bridge. Flashing to be lifted and covered timber is to be inspected closely for damage. Damaged flashing to be sympathetically replaced to match existing.
9.6.5	Е	Steel hold down bolts and plates to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.

9.6.6 Timber Bridge Deck Cross Girders and Stringers

Work	Item	Policy
9.6.6	A	Timber testing to identify internal voids to be carried out by an appropriately qualified contractor.
9.6.6	В	If required, replacement timber should match the original timber in grade and species as much as possible, should be of unobtrusive design and not detract from the visual impact of the bridge.
9.6.6	С	Deteriorated sections of timber to be replaced where required, removing as much as necessary but as little as possible.
9.6.6	D	Organic matter beneath the bridge deck along deck cross girders and stringers should be removed to prevent moisture deterioration.
9.6.6	Е	Steel hold down bolts and plates to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.

9.6.7 Timber Abutments and Wingwalls

Work	ltem	Policy
9.6.7	A	Timber testing to identify internal voids to be carried out by an appropriately qualified contractor.
9.6.7	В	If required, replacement timber should match the original timber in grade and species as much as possible, should be of unobtrusive design and not detract from the visual impact of the bridge.
9.6.7	С	Deteriorated sections of timber to be replaced where required, removing as much as necessary but as little as possible.
9.6.7	D	Steel hold down bolts and plates to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.
9.6.7	Е	Flashing over abutment and wingwall piers to be inspected and removed to reveal the condition of the timber beneath. Damaged flashing to be repaired / replaced with sympathetic materials which do not detract from the significance of the bridge.
9.6.7	F	Redundant fixings to be removed and appropriately repaired.

9.6.8 Timber Guardrails

Work Item		Policy
9.6.8	A	Timber testing to identify internal voids to be carried out by an appropriately qualified contractor.
9.6.8	В	If required, replacement timber should match the original timber in grade and species as much as possible, should be of unobtrusive design and not detract from the visual impact of the bridge.
9.6.8	С	Deteriorated sections of timber to be replaced where required, removing as much as necessary but as little as possible.
9.6.8	D	Timber guardrails are to be repainted as part of the annual maintenance plan for the bridge, to match the original colour and appearance.
9.6.8	Е	Steel brackets securing timber guardrails to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.
9.6.8	F	Steel hold down bolts and plates to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.

9.6.9 Timber Kerbs

Work Item		Policy
0	A	Timber testing to identify internal voids to be carried out by an appropriately qualified contractor.
0	В	If required, replacement timber should match the original timber in grade and species as much as possible, should be of unobtrusive design and not detract from the visual impact of the bridge.
0	С	Deteriorated sections of timber to be replaced where required, removing as much as necessary but as little as possible.
0	D	Timber kerbs are to be repainted as part of the annual maintenance plan for the bridge, to match the original colour and appearance.
0	E	Steel hold down bolts and plates to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.

9.6.10 Steel Deck Hanging Rods

Work Item	Policy
9.6.10 A	Original fabric to remain where practical
9.6.10 B	Maintain original engineering behaviour, size, proportion and general appearance as closely as possible.
9.6.10 C	If required, replacement steel hanger rods should match the original as much as possible, should be of unobtrusive design and not detract from the visual impact of the bridge. Welds and other non-original connections of fabric should be unobtrusive.
9.6.10 D	Match existing fixings such as nuts and bolts.
9.6.10 E	Existing hangers to be cleaned, corrosion removed (if any) and painted to match original colour.
9.6.10 F	All welds to be inspected using the magnetic particle method as part of the regular maintenance.

9.6.11 Steel Angle Truss Members

Work Item		Policy
9.6.11	А	Original fabric to remain where practical
9.6.11	В	Maintain original engineering behaviour, size, proportion and general appearance as closely as possible.
9.6.11	С	Replacement truss members are to replicate the original size, proportion and general appearance as closely as possible and not detract from the significance of the bridge.
9.6.11	D	Existing steel member trusses to be routinely inspected for corrosion. Corrosion to be removed and the members are to be repainted to match the original design. Only when corrosion is beyond reasonable levels should the member be replaced to match existing as closely as possible and not detract from the significance of the bridge.
9.6.11	E	Steel truss members to be inspected and repainted as part of the annual maintenance plan for the bridge.

9.6.12 Steel Suspension Cables

Work Item	Policy
9.6.12 A	Original fabric to remain where practical
9.6.12 B	Monitor and assess the corrosion of cables, particularly inside node blocks where corrosion is likely due to dissimilar metals.
9.6.12 C	Appropriately treat corrosion, particularly at water build-up points such as node blocks.
9.6.12 D	Cables to be inspected and repainted as part of the annual maintenance plan for the bridge.
9.6.12 E	Steel anchor U-Bolt and cable attachment to be corrosion treated and repainted as part of the annual maintenance plan for the bridge.

9.6.13 Steel Cable Node Blocks

Work Item	Policy
9.6.13 A	Original fabric to remain where practical
9.6.13 B	Maintain original engineering behaviour, size, proportion and general appearance as closely as possible.
9.6.13 C	Existing steel cable node blocks are to be routinely inspected for corrosion. Corrosion to be removed and the blocks are to be repainted to match existing. Only when corrosion is beyond reasonable levels should the node blocks be replaced to match existing as closely as possible. Severely corroded clamping bolts are to be replaced urgently to match original.

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9.6.13	D	Replacement cable node blocks are to replicate the original size, proportion and general appearance as closely as possible.
9.6.13	Е	Steel cable node blocks are to be inspected and repainted as part of the annual maintenance plan for the bridge. Paint or debris is not to cover natural weep holes in the blocks.
9.6.13	F	Bitumen to be removed from node blocks.

9.6.14 Steel Pier Bearings

Work Item		Policy
9.6.14	А	Original fabric to remain where practical
9.6.14	В	Maintain original engineering behaviour, size, proportion and general appearance as closely as possible.
9.6.14	С	Existing steel pier bearings are to be routinely inspected for corrosion. Corrosion to be removed and the members are to be repainted to match existing. Only when corrosion is beyond reasonable levels should the bearings be replaced to match existing as closely as possible.
9.6.14	D	Replacement bearings are to replicate the original size, proportion and general appearance as closely as possible.
9.6.14	E	Steel bearings are to be inspected and repainted as part of the annual maintenance plan for the bridge. Paint is not to undermine the integrity of the bearings and not to be built up to a level where water drainage is impeded.

9.6.15 Steel Abutment Bearings

Work Item		Policy
9.6.15	А	Original fabric to remain where practical
9.6.15	В	Maintain original engineering behaviour, size, proportion and general appearance as closely as possible.
9.6.15	С	Existing steel pier bearings are to be routinely inspected for corrosion. Corrosion to be removed and the members are to be repainted to match existing. Only when corrosion is beyond reasonable levels should the bearings be replaced to match existing as closely as possible.
9.6.15	D	Replacement bearings are to replicate the original size, proportion and general appearance as closely as possible.
9.6.15	E	Steel bearings are to be inspected and repainted as part of the annual maintenance plan for the bridge. Paint is not to undermine the integrity of the bearings and not to be built up to a level where water drainage is impeded.

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9.6.16 Abutment Anchor Rods

Work Item		Policy	
9.6.16	А	Original fabric to remain where practical	
9.6.16	В	Maintain original engineering behaviour, size, proportion and general appearance as closely as possible.	
9.6.16	С	Existing steel anchor rods are to be routinely inspected for corrosion and internal defects. Corrosion to be removed and the members are to be repainted to match existing. Only when corrosion is beyond reasonable levels should the node blocks be replaced to match existing as closely as possible.	
9.6.16	D	Replacement anchor rods are to replicate the original size, proportion and general appearance as closely as possible.	
9.6.16	Е	Steel anchor rods are to be inspected and repainted as part of the annual maintenance plan for the bridge. Paint is not to be 'built up', but removed, cleaned treated and repainted.	
9.6.16	F	Steel sheath over anchor rods to be removed where damaged is beyond repair. Anchor rods to be adequately corrosion treated and sheath to be replaced to match the original. Sheath to be grout filled to fully encapsulate rods.	

9.6.17 Dead Man Anchors

Work Item	Policy
9.6.17 A	Original fabric to remain where practical
	Maintain original engineering behaviour, size, proportion and general appearance as closely as possible.

9.6.18 Steel Guardrails

Work Item	Policy
	Steel guardrails along both approaches to the bridge can be replaced with a guardrail system that is more sensitive to the natural surroundings and timber construction of the bridge where practical and Code compliant, such as steel cable guarding in place of the present Armco-type guardrails.

9.6.19 Steel Fencing, Posts & Miscellaneous Items

Work Item		Policy
0	А	Modern steel fencing and posts installed along the riverbanks can be removed if required.
0	В	Required fencing and signposts are to be sympathetic to the overall design of the bridge.
0	С	Flood level tapes to be straightened and deteriorated sections to be sympathetically replaced to match existing. Replacement should not be more distinct than the existing, nor detract from the significance of the bridge.

9.6.20 Bridge Deck

Work Item	Policy
9.6.20 A	Bituminous surface coating to be patch repaired, to match the existing bridge deck surface.
	Damaged sections of bridge deck plywood are to be repaired if practical, otherwise replaced and resurfaced with bitumen to match existing bridge deck surface.

9.6.21 Concrete Pier Footings

Work Item	Policy	
9.6.21 A	Footings to be inspected as part of the bridge's annual maintenance.	
	Cracks and spalled sections of concrete are to be sympathetically replaced, so that they do not stand out or detract from the significance of the bridge.	

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ATTACHMENT

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Wood S. Photograph by Stephen Wood, Port Macquarie-Hastings Council, 2018

Appendix A – Chronological Summary

Year	Summary
1936	The contractor for the construction R.B. Haydon was killed in a site accident in April prior to the bridge being opened. The bridge was officially opened on 19 th December 1936 at the total cost of 10,000 pounds. ⁷¹
1937	Allan Bridge at Segenhoe was completed. ⁷²
1940	Care and Control of the bridge was passed to the then Hastings Shire Council in August 1940. ⁷³
1940's	The suspension bridge at Double Crossing near Yarras collapsed in a flood due to a miscalculation of the height of the flood which reached the bridge deck and pushed it over. ⁷⁴
1950	The collapsed bridge at Double Crossing was replaced by a low-level bridge. ⁷⁴
1955	Allan Bridge at Segenhoe washed away during severe flooding. The bridge was rebuilt shortly after downstream. ⁷²
1980	Considerable amount of decking was renewed. 2No. cross girders and bolting planks were replaced. All steelwork and timber handrail repainted. ⁷³
1984	Kindee bridge was listed in by the National Trust of Australia, falling under the trust's definition of: "Those places which are components of the natural or cultural environment of Australia, that have aesthetic, historic, scientific or social significance or other special value for future generations, as well as for the present community." ⁷⁴
1984	Request sent to Department of Main Roads, for DMR to assume responsibility for care and control of the bridge along with a request to consider upgrading the bridge. ⁷³
1984	DMR commented in correspondence declaring the unlikelihood that any significant upgrades to the bridge could be carried out without substantial modification. This modification may destroy historical interest in the bridge. ⁷³
1985	Bridge inspection was carried out for the purpose of estimation. Boring records were additionally carried out in this year. ⁷³
1988	Application for the responsibility of Kindee Bridge to be assume by the Department of Main Roads was refused. ⁷⁵

⁷¹ NSW Environment & Heritage, Kindee Road Suspension Bridge (part), 2011

73 Port Macquarie Hastings Council, Kindee Bridge - Maintenance Planning Report, 2018

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⁷² Port Macquarie-Hastings Council - Kindee Bridge - Strategic Options Report RevA, 2018

⁷⁴ National Trust of Australia, Ellenborough - Bridge at Kindee Crossing, 1984

⁷⁵ Department of Main Roads, File Reference - R5-20 & 196.1223, 1988

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1988	A member of the public was caught but not prosecuted for exceeding the bridges 10 tonne limit and causing damage on the grounds that other significantly overloaded vehicles have crossed the bridge. ⁷⁶		
1989	Bridge underwent temporary daily closures between 9am and 3pm in order carry out required repair works. ⁷⁷		
1990	 Major Bridge Maintenance was carried out which included: All abutment piles and timber boarding were replaced, and piles spliced into ground piles All timber cross girders were replaced All timber stringers were replaced All timber wind cross braces replaced All timber bridge decking replaced with Bridgewood 125mm deep ply decking, with spray seal 7 /10mm seal All timber kerbs replaced Pier 1 – Downstream outside lower pile up to deck level replaced. Timber piers filled with epoxy resin. Timber cap wales at base of pier potentially replaced (unknown) Pier 2 - Downstream outside lower pile up to deck level replaced. Timber piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers filled with epoxy resin. Timber cap wales at base of pier piers piers filled with epoxy resin. Timber cap wales at base of pier piers pier piers filled with epoxy resin. Timber cap wales at base of pier piers piers filled with epoxy resin. Timber cap wales at base of pier piers piers pier piers filled with epoxy resin. Timber cap wales at base of pier piers piers piers filled with epoxy resin. Timber cap wales at base of pier piers piers filled with epoxy resin. 		
1991	Upgraded approach guardrailing. ⁷³		
1991	Timber guard posts along the bridge were repaired. ⁷⁸		
1992	Ultrasonic inspection carried out to steel suspension components of the bridge ⁷⁹ : Primary Anchor Bolts (8 off) – No cracking detected Secondary anchor bolts (8 off) – No cracking detected Anchor mount pins (4 off) – No cracking detected Tower apex pins (4 off) – No cracking detected Cable truss bolts – No cracking detected Suspension rod eye bolts – No cracking detected (visual inspection)⁷³ 		
2005	Test bores were carried out to the bridge piers by a contractor. ⁸⁰		
2010	 Integrity Assessment which advised: Recommend restricting access to 7 tonne fixed axle vehicles only. Defective timbers in span 1 and 2 including section loss, and fungicidal growth. Defective timbers in pier 2 including evidence of white ant attack. Unknown state of cable anchorages and corrosion No fail-safe mechanism if the cables were to give way.⁷³ 		

78 Wood S. Letter in Reference to Ian Anderson - Ref 145783, 1984

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⁷⁷ Wood S. Kindee Bridge Correspondence, 1989

⁷⁸ Hastings Council, Request for Work by Drawings Office Design Brief, 1991

⁷⁹ C.W. Pope & Associates Pty. Ltd, Radiographic Examination, 1992

⁸⁰ Unknown, Kindee Bridge test bores, 2005

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2011	Downstream wing collapsed after significant flood. All piles and boarding on this downstream abutment wing were replaced at this time. ⁷³	
2011	All steel work re-painted prior to 75th anniversary celebrations. ⁷³	
2016	 Suspension system condition assessment by Noble Engineering Services finding: All steel components, other than cables should be cleaned back to bare metal and repainted. All clamping bolts at node blocks (131 at a minimum) should be replaced. Hangar backing plates to be checked as replaced. (30 No. noted to be heavily corroded). Some noted to be poor condition. Investigate zinc-based anodes to extend remnant life of steel components. Cables, hangars, anchor rods, U-bolts, node blocks. Section loss of abutment anchor rods noted to be in the order of 19%, however may not be limiting factor in load limit. Anchor rods to be protected against further corrosion.⁷³ 	
2017	2.No steel hanger rods were replaced as recommended by Noble Engineering Services. ⁷³	
2018	A passenger vehicle crashed in the southern side of the bridge truss. The bridge was repaired. ⁸¹ 2 No. Braces and 2 No. hangers damaged. ⁷³	
2019	Hangar failure was reported.82	

⁸¹ Wood S. Photo catalogue, 13/09/2018

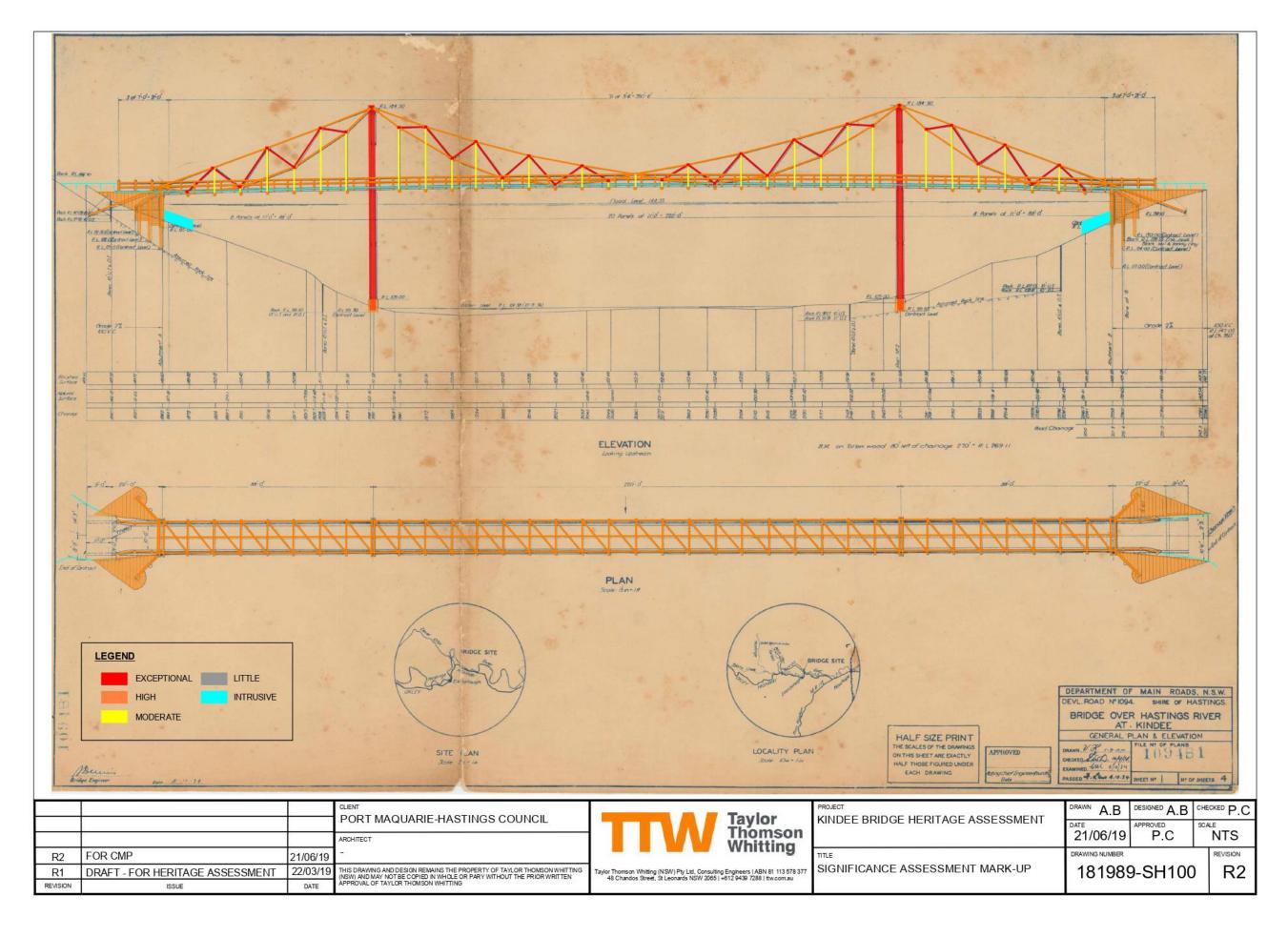
82 Port Macquarie Hastings Council, Kindee Bridge - Hanger Failure, 2019

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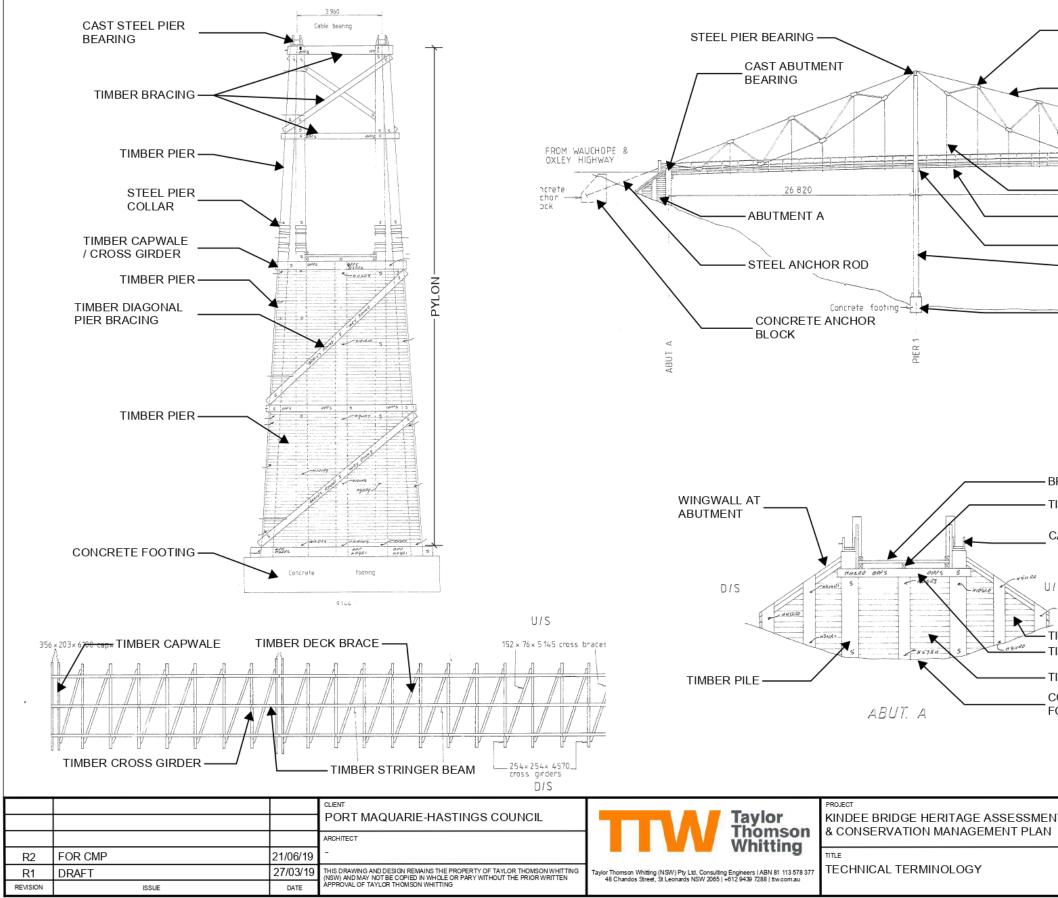
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Appendix B – Significance Assessment Mark-Up



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Appendix C – Technical Terminology



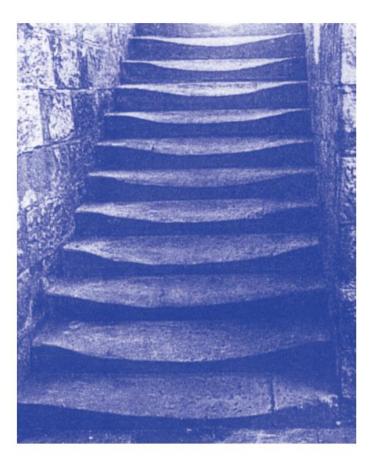
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Appendix D – Burra Charter

THE BURRA CHARTER

The Australia ICOMOS Charter for Places of Cultural Significance 2013





Australia ICOMOS Incorporated International Council on Monuments and Sites

ICOMOS

ICOMOS (International Council on Monuments and Sites) is a non-governmental professional organisation formed in 1965, with headquarters in Paris. ICOMOS is primarily concerned with the philosophy, terminology, methodology and techniques of cultural heritage conservation. It is closely linked to UNESCO, particularly in its role under the World Heritage Convention 1972 as UNESCO's principal adviser on cultural matters related to World Heritage. The 11,000 members of ICOMOS include architects, town planners, demographers, archaeologists, geographers, historians, conservators, anthropologists, scientists, engineers and heritage administrators. Members in the 103 countries belonging to ICOMOS are formed into National Committees and participate in a range of conservation projects, research work, intercultural exchanges and cooperative activities. ICOMOS also has 27 International Scientific Committees that focus on particular aspects of the conservation field. ICOMOS members meet triennially in a General Assembly.

Australia ICOMOS

The Australian National Committee of ICOMOS (Australia ICOMOS) was formed in 1976. It elects an Executive Committee of 15 members, which is responsible for carrying out national programs and participating in decisions of ICOMOS as an international organisation. It provides expert advice as required by ICOMOS, especially in its relationship with the World Heritage Committee. Australia ICOMOS acts as a national and international link between public authorities, institutions and individuals involved in the study and conservation of all places of cultural significance. Australia ICOMOS members participate in a range of conservation activities including site visits, training, conferences and meetings.

Revision of the Burra Charter

The Burra Charter was first adopted in 1979 at the historic South Australian mining town of Burra. Minor revisions were made in 1981 and 1988, with more substantial changes in 1999.

Following a review this version was adopted by Australia ICOMOS in October 2013.

The review process included replacement of the 1988 Guidelines to the Burra Charter with Practice Notes which are available at: australia.icomos.org

Australia ICOMOS documents are periodically reviewed and we welcome any comments.

Citing the Burra Charter

The full reference is *The Burra Charter: The Australia ICOMOS Charter for Places of Cultural Significance,* 2013. Initial textual references should be in the form of the *Australia ICOMOS Burra Charter,* 2013 and later references in the short form (*Burra Charter*).

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The Burra Charter consists of the Preamble, Articles, Explanatory Notes and the flow chart.

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Cover photograph by Ian Stapleton.

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http://australia.icomos.org/

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The Burra Charter

(The Australia ICOMOS Charter for Places of Cultural Significance, 2013)

Preamble

Considering the International Charter for the Conservation and Restoration of Monuments and Sites (Venice 1964), and the Resolutions of the 5th General Assembly of the International Council on Monuments and Sites (ICOMOS) (Moscow 1978), the Burra Charter was adopted by Australia ICOMOS (the Australian National Committee of ICOMOS) on 19 August 1979 at Burra, South Australia. Revisions were adopted on 23 February 1981, 23 April 1988, 26 November 1999 and 31 October 2013.

The Burra Charter provides guidance for the conservation and management of places of cultural significance (cultural heritage places), and is based on the knowledge and experience of Australia ICOMOS members.

Conservation is an integral part of the management of places of cultural significance and is an ongoing responsibility.

Who is the Charter for?

The Charter sets a standard of practice for those who provide advice, make decisions about, or undertake works to places of cultural significance, including owners, managers and custodians.

Using the Charter

The Charter should be read as a whole. Many articles are interdependent.

The Charter consists of:

•	Definitions	Article 1
•	Conservation Principles	Articles 2–13
•	Conservation Processes	Articles 14–25

- Conservation Practices Articles 26–34
- The Burra Charter Process flow chart.

The key concepts are included in the Conservation Principles section and these are further developed in the Conservation Processes and Conservation Practice sections. The flow chart explains the Burra Charter Process (Article 6) and is an integral part of

The Burra Charter, 2013

the Charter. Explanatory Notes also form part of the Charter.

The Charter is self-contained, but aspects of its use and application are further explained, in a series of Australia ICOMOS Practice Notes, in *The Illustrated Burra Charter*, and in other guiding documents available from the Australia ICOMOS web site: australia.icomos.org.

What places does the Charter apply to?

The Charter can be applied to all types of places of cultural significance including natural, Indigenous and historic places with cultural values.

The standards of other organisations may also be relevant. These include the Australian Natural Heritage Charter, Ask First: a guide to respecting Indigenous heritage places and values and Significance 2.0: a guide to assessing the significance of collections.

National and international charters and other doctrine may be relevant. See australia.icomos.org.

Why conserve?

Places of cultural significance enrich people's lives, often providing a deep and inspirational sense of connection to community and landscape, to the past and to lived experiences. They are historical records, that are important expressions of Australian identity and experience. Places of cultural significance reflect the diversity of our communities, telling us about who we are and the past that has formed us and the Australian landscape. They are irreplaceable and precious.

These places of cultural significance must be conserved for present and future generations in accordance with the principle of inter-generational equity.

The Burra Charter advocates a cautious approach to change: do as much as necessary to care for the place and to make it useable, but otherwise change it as little as possible so that its cultural significance is retained.

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Article 1. Definitions

For the purposes of this Charter:

- 1.1 *Place* means a geographically defined area. It may include elements, objects, spaces and views. Place may have tangible and intangible dimensions.
- 1.2 *Cultural significance* means aesthetic, historic, scientific, social or spiritual value for past, present or future generations.

Cultural significance is embodied in the *place* itself, its *fabric*, *setting*, *use*, *associations*, *meanings*, records, *related places* and *related objects*.

Places may have a range of values for different individuals or groups.

- 1.3 *Fabric* means all the physical material of the *place* including elements, fixtures, contents and objects.
- 1.4 *Conservation* means all the processes of looking after a *place* so as to retain its *cultural significance*.
- 1.5 *Maintenance* means the continuous protective care of a *place*, and its *setting*.

Maintenance is to be distinguished from repair which involves *restoration* or *reconstruction*.

- 1.6 *Preservation* means maintaining a *place* in its existing state and retarding deterioration.
- 1.7 *Restoration* means returning a *place* to a known earlier state by removing accretions or by reassembling existing elements without the introduction of new material.
- 1.8 *Reconstruction* means returning a *place* to a known earlier state and is distinguished from *restoration* by the introduction of new material.
- 1.9 Adaptation means changing a *place* to suit the existing *use* or a proposed use.
- 1.10 *Use* means the functions of a *place*, including the activities and traditional and customary practices that may occur at the place or are dependent on the place.

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Explanatory Notes

Place has a broad scope and includes natural and cultural features. Place can be large or small: for example, a memorial, a tree, an individual building or group of buildings, the location of an historical event, an urban area or town, a cultural landscape, a garden, an industrial plant, a shipwreck, a site with in situ remains, a stone arrangement, a road or travel route, a community meeting place, a site with spiritual or religious connections.

The term cultural significance is synonymous with cultural heritage significance and cultural heritage value.

Cultural significance may change over time and with use.

Understanding of cultural significance may change as a result of new information.

Fabric includes building interiors and subsurface remains, as well as excavated material.

Natural elements of a place may also constitute fabric. For example the rocks that signify a Dreaming place.

Fabric may define spaces and views and these may be part of the significance of the place.

See also Article 14.

Examples of protective care include:

- maintenance regular inspection and cleaning of a place, e.g. mowing and pruning in a garden;
- repair involving restoration returning dislodged or relocated fabric to its original location e.g. loose roof gutters on a building or displaced rocks in a stone bora ring;
- repair involving reconstruction replacing decayed fabric with new fabric

It is recognised that all places and their elements change over time at varying rates.

New material may include recycled material salvaged from other places. This should not be to the detriment of any place of cultural significance.

Use includes for example cultural practices commonly associated with Indigenous peoples such as ceremonies, hunting and fishing, and fulfillment of traditional obligations. Exercising a right of access may be a use.

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Articles

- 1.11 *Compatible use* means a *use* which respects the *cultural significance* of a *place*. Such a use involves no, or minimal, impact on cultural significance.
- 1.12 *Setting* means the immediate and extended environment of a *place* that is part of or contributes to its *cultural significance* and distinctive character.
- 1.13 *Related place* means a *place* that contributes to the *cultural significance* of another place.
- 1.14 *Related object* means an object that contributes to the *cultural significance* of a *place* but is not at the place.
- 1.15 *Associations* mean the connections that exist between people and a *place*.
- 1.16 Meanings denote what a place signifies, indicates, evokes or expresses to people.
- 1.17 Interpretation means all the ways of presenting the *cultural* significance of a place.

Conservation Principles

Article 2. Conservation and management

- 2.1 Places of cultural significance should be conserved.
- 2.2 The aim of *conservation* is to retain the *cultural significance* of a *place*.
- 2.3 *Conservation* is an integral part of good management of *places* of *cultural significance*.
- 2.4 *Places* of *cultural significance* should be safeguarded and not put at risk or left in a vulnerable state.

Article 3. Cautious approach

- 3.1 *Conservation* is based on a respect for the existing *fabric*, *use*, *associations* and *meanings*. It requires a cautious approach of changing as much as necessary but as little as possible.
- 3.2 Changes to a *place* should not distort the physical or other evidence it provides, nor be based on conjecture.

Article 4. Knowledge, skills and techniques

4.1 *Conservation* should make use of all the knowledge, skills and disciplines which can contribute to the study and care of the *place*.

Explanatory Notes

Setting may include: structures, spaces, land, water and sky; the visual setting including views to and from the place, and along a cultural route; and other sensory aspects of the setting such as smells and sounds. Setting may also include historical and contemporary relationships, such as use and activities, social and spiritual practices, and relationships with other places, both tangible and intangible.

Objects at a place are encompassed by the definition of place, and may or may not contribute to its cultural significance.

Associations may include social or spiritual values and cultural responsibilities for a place.

Meanings generally relate to intangible dimensions such as symbolic qualities and memories.

Interpretation may be a combination of the treatment of the fabric (e.g. maintenance, restoration, reconstruction); the use of and activities at the place; and the use of introduced explanatory material.

The traces of additions, alterations and earlier treatments to the fabric of a place are evidence of its history and uses which may be part of its significance. Conservation action should assist and not impede their understanding.

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4.2 Traditional techniques and materials are preferred for the conservation of significant fabric. In some circumstances modern techniques and materials which offer substantial conservation benefits may be appropriate.

Article 5. Values

- 5.1 Conservation of a place should identify and take into consideration all aspects of cultural and natural significance without unwarranted emphasis on any one value at the expense of others.
- 5.2 Relative degrees of *cultural significance* may lead to different *conservation* actions at a place.

Article 6. Burra Charter Process

- 6.1 The *cultural significance* of a *place* and other issues affecting its future are best understood by a sequence of collecting and analysing information before making decisions. Understanding cultural significance comes first, then development of policy and finally management of the place in accordance with the policy. This is the Burra Charter Process.
- 6.2 Policy for managing a *place* must be based on an understanding of its *cultural significance*.
- 6.3 Policy development should also include consideration of other factors affecting the future of a *place* such as the owner's needs, resources, external constraints and its physical condition.
- 6.4 In developing an effective policy, different ways to retain *cultural significance* and address other factors may need to be explored.
- 6.5 Changes in circumstances, or new information or perspectives, may require reiteration of part or all of the Burra Charter Process.

Article 7. Use

- 7.1 Where the *use* of a *place* is of *cultural significance* it should be retained.
- 7.2 A place should have a compatible use.

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Explanatory Notes

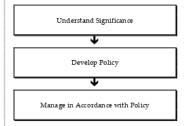
The use of modern materials and techniques must be supported by firm scientific evidence or by a body of experience.

Conservation of places with natural significance is explained in the Australian Natural Heritage Charter. This Charter defines natural significance to mean the importance of ecosystems, biodiversity and geodiversity for their existence value or for present or future generations, in terms of their scientific, social, aesthetic and life-support value.

In some cultures, natural and cultural values are indivisible.

A cautious approach is needed, as understanding of cultural significance may change. This article should not be used to justify actions which do not retain cultural significance.

The Burra Charter Process, or sequence of investigations, decisions and actions, is illustrated below and in more detail in the accompanying flow chart which forms part of the Charter.



Options considered may include a range of uses and changes (e.g. adaptation) to a place.

The policy should identify a use or combination of uses or constraints on uses that retain the cultural significance of the place. New use of a place should involve minimal change to significant fabric and use; should respect associations and meanings; and where appropriate should provide for continuation of activities and practices which contribute to the cultural significance of the place.

Article 8. Setting

Conservation requires the retention of an appropriate *setting*. This includes retention of the visual and sensory setting, as well as the retention of spiritual and other cultural relationships that contribute to the *cultural significance* of the *place*.

New construction, demolition, intrusions or other changes which would adversely affect the setting or relationships are not appropriate.

Article 9. Location

- 9.1 The physical location of a *place* is part of its *cultural significance*. A building, work or other element of a place should remain in its historical location. Relocation is generally unacceptable unless this is the sole practical means of ensuring its survival.
- 9.2 Some buildings, works or other elements of *places* were designed to be readily removable or already have a history of relocation. Provided such buildings, works or other elements do not have significant links with their present location, removal may be appropriate.
- 9.3 If any building, work or other element is moved, it should be moved to an appropriate location and given an appropriate use. Such action should not be to the detriment of any place of cultural significance.

Article 10. Contents

Contents, fixtures and objects which contribute to the *cultural significance* of a *place* should be retained at that place. Their removal is unacceptable unless it is: the sole means of ensuring their security and *preservation*; on a temporary basis for treatment or exhibition; for cultural reasons; for health and safety; or to protect the place. Such contents, fixtures and objects should be returned where circumstances permit and it is culturally appropriate.

Article 11. Related places and objects

The contribution which *related places* and *related objects* make to the *cultural significance* of the *place* should be retained.

Article 12. Participation

Conservation, interpretation and management of a *place* should provide for the participation of people for whom the place has significant *associations* and *meanings*, or who have social, spiritual or other cultural responsibilities for the place.

Article 13. Co-existence of cultural values

Co-existence of cultural values should always be recognised, respected and encouraged. This is especially important in cases where they conflict.

Explanatory Notes

Setting is explained in Article 1.12.

For example, the repatriation (returning) of an object or element to a place may be important to Indigenous cultures, and may be essential to the retention of its cultural significance.

Article 28 covers the circumstances where significant fabric might be disturbed, for example, during archaeological excavation.

Article 33 deals with significant fabric that has been removed from a place.

For some places, conflicting cultural values may affect policy development and management decisions. In Article 13, the term cultural values refers to those beliefs which are important to a cultural group, including but not limited to political, religious, spiritual and moral beliefs. This is broader than values associated with cultural significance.

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Conservation Processes

Article 14. Conservation processes

Conservation may, according to circumstance, include the processes of: retention or reintroduction of a *use*; retention of *associations* and *meanings*; *maintenance*, *preservation*, *restoration*, *reconstruction*, *adaptation* and *interpretation*; and will commonly include a combination of more than one of these. Conservation may also include retention of the contribution that *related places* and *related objects* make to the *cultural significance* of a *place*.

Article 15. Change

- 15.1 Change may be necessary to retain *cultural significance*, but is undesirable where it reduces cultural significance. The amount of change to a *place* and its *use* should be guided by the *cultural significance* of the place and its appropriate *interpretation*.
- 15.2 Changes which reduce *cultural significance* should be reversible, and be reversed when circumstances permit.
- 15.3 Demolition of significant *fabric* of a *place* is generally not acceptable. However, in some cases minor demolition may be appropriate as part of *conservation*. Removed significant fabric should be reinstated when circumstances permit.
- 15.4 The contributions of all aspects of *cultural significance* of a *place* should be respected. If a place includes *fabric*, *uses*, *associations* or *meanings* of different periods, or different aspects of cultural significance, emphasising or interpreting one period or aspect at the expense of another can only be justified when what is left out, removed or diminished is of slight cultural significance and that which is emphasised or interpreted is of much greater cultural significance.

Article 16. Maintenance

Maintenance is fundamental to *conservation*. Maintenance should be undertaken where *fabric* is of *cultural significance* and its maintenance is necessary to retain that *cultural significance*.

Article 17. Preservation

Preservation is appropriate where the existing *fabric* or its condition constitutes evidence of *cultural significance*, or where insufficient evidence is available to allow other *conservation* processes to be carried out.

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Explanatory Notes

Conservation normally seeks to slow deterioration unless the significance of the place dictates otherwise. There may be circumstances where no action is required to achieve conservation.

When change is being considered, including for a temporary use, a range of options should be explored to seek the option which minimises any reduction to its cultural significance.

It may be appropriate to change a place where this reflects a change in cultural meanings or practices at the place, but the significance of the place should always be respected.

Reversible changes should be considered temporary. Non-reversible change should only be used as a last resort and should not prevent future conservation action.

Maintaining a place may be important to the fulfilment of traditional laws and customs in some Indigenous communities and other cultural groups.

Preservation protects fabric without obscuring evidence of its construction and use. The process should always be applied:

- where the evidence of the fabric is of such significance that it should not be altered; or
- where insufficient investigation has been carried out to permit policy decisions to be taken in accord with Articles 26 to 28.

New work (e.g. stabilisation) may be carried out in association with preservation when its purpose is the physical protection of the fabric and when it is consistent with Article 22.

Article 18. Restoration and reconstruction

Restoration and *reconstruction* should reveal culturally significant aspects of the *place*.

Article 19. Restoration

Restoration is appropriate only if there is sufficient evidence of an earlier state of the *fabric*.

Article 20. Reconstruction

- 20.1 *Reconstruction* is appropriate only where a *place* is incomplete through damage or alteration, and only where there is sufficient evidence to reproduce an earlier state of the *fabric*. In some cases, reconstruction may also be appropriate as part of a *use* or practice that retains the *cultural significance* of the place.
- 20.2 *Reconstruction* should be identifiable on close inspection or through additional *interpretation*.

Article 21. Adaptation

- 21.1 *Adaptation* is acceptable only where the adaptation has minimal impact on the *cultural significance* of the *place*.
- 21.2 *Adaptation* should involve minimal change to significant *fabric*, achieved only after considering alternatives.

Article 22. New work

- 22.1 New work such as additions or other changes to the *place* may be acceptable where it respects and does not distort or obscure the *cultural significance* of the place, or detract from its *interpretation* and appreciation.
- 22.2 New work should be readily identifiable as such, but must respect and have minimal impact on the *cultural significance* of the *place*.

Article 23. Retaining or reintroducing use

Retaining, modifying or reintroducing a significant *use* may be appropriate and preferred forms of *conservation*.

Article 24. Retaining associations and meanings

- 24.1 Significant *associations* between people and a *place* should be respected, retained and not obscured. Opportunities for the *interpretation*, commemoration and celebration of these associations should be investigated and implemented.
- 24.2 Significant *meanings*, including spiritual values, of a *place* should be respected. Opportunities for the continuation or revival of these meanings should be investigated and implemented.

Explanatory Notes

Places with social or spiritual value may warrant reconstruction, even though very little may remain (e.g. only building footings or tree stumps following fire, flood or storm). The requirement for sufficient evidence to reproduce an earlier state still applies.

Adaptation may involve additions to the place, the introduction of new services, or a new use, or changes to safeguard the place. Adaptation of a place for a new use is often referred to as 'adaptive re-use' and should be consistent with Article 7.2.

New work should respect the significance of a place through consideration of its siting, bulk, form, scale, character, colour, texture and material. Imitation should generally be avoided.

New work should be consistent with Articles 3, 5, 8, 15, 21 and 22.1.

These may require changes to significant fabric but they should be minimised. In some cases, continuing a significant use, activity or practice may involve substantial new work.

For many places associations will be linked to aspects of use, including activities and practices.

Some associations and meanings may not be apparent and will require research.

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Article 25. Interpretation

The *cultural significance* of many *places* is not readily apparent, and should be explained by *interpretation*. Interpretation should enhance understanding and engagement, and be culturally appropriate.

Conservation Practice

Article 26. Applying the Burra Charter Process

- 26.1 Work on a *place* should be preceded by studies to understand the place which should include analysis of physical, documentary, oral and other evidence, drawing on appropriate knowledge, skills and disciplines.
- 26.2 Written statements of *cultural significance* and policy for the *place* should be prepared, justified and accompanied by supporting evidence. The statements of significance and policy should be incorporated into a management plan for the place.
- 26.3 Groups and individuals with associations with the place as well as those involved in its management should be provided with opportunities to contribute to and participate in identifying and understanding the *cultural significance* of the place. Where appropriate they should also have opportunities to participate in its *conservation* and management.
- 26.4 Statements of *cultural significance* and policy for the *place* should be periodically reviewed, and actions and their consequences monitored to ensure continuing appropriateness and effectiveness.

Article 27. Managing change

- 27.1 The impact of proposed changes, including incremental changes, on the *cultural significance* of a *place* should be assessed with reference to the statement of significance and the policy for managing the place. It may be necessary to modify proposed changes to better retain cultural significance.
- 27.2 Existing *fabric, use, associations* and *meanings* should be adequately recorded before and after any changes are made to the *place*.

Article 28. Disturbance of fabric

28.1 Disturbance of significant *fabric* for study, or to obtain evidence, should be minimised. Study of a *place* by any disturbance of the fabric, including archaeological excavation, should only be undertaken to provide data essential for decisions on the *conservation* of the place, or to obtain important evidence about to be lost or made inaccessible.

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Explanatory Notes

In some circumstances any form of interpretation may be culturally inappropriate.

The results of studies should be kept up to date, regularly reviewed and revised as necessary.

Policy should address all relevant issues, e.g. use, interpretation, management and change.

A management plan is a useful document for recording the Burra Charter Process, i.e. the steps in planning for and managing a place of cultural significance (Article 6.1 and flow chart). Such plans are often called conservation management plans and sometimes have other names.

The management plan may deal with other matters related to the management of the place.

Monitor actions taken in case there are also unintended consequences.

28.2 Investigation of a *place* which requires disturbance of the *fabric*, apart from that necessary to make decisions, may be appropriate provided that it is consistent with the policy for the place. Such investigation should be based on important research questions which have potential to substantially add to knowledge, which cannot be answered in other ways and which minimises disturbance of significant fabric.

Article 29. Responsibility

The organisations and individuals responsible for management and decisions should be named and specific responsibility taken for each decision.

Article 30. Direction, supervision and implementation

Competent direction and supervision should be maintained at all stages, and any changes should be implemented by people with appropriate knowledge and skills.

Article 31. Keeping a log

New evidence may come to light while implementing policy or a plan for a *place*. Other factors may arise and require new decisions. A log of new evidence and additional decisions should be kept.

Article 32. Records

- 32.1 The records associated with the *conservation* of a *place* should be placed in a permanent archive and made publicly available, subject to requirements of security and privacy, and where this is culturally appropriate.
- 32.2 Records about the history of a *place* should be protected and made publicly available, subject to requirements of security and privacy, and where this is culturally appropriate.

Article 33. Removed fabric

Significant *fabric* which has been removed from a *place* including contents, fixtures and objects, should be catalogued, and protected in accordance with its *cultural significance*.

Where possible and culturally appropriate, removed significant fabric including contents, fixtures and objects, should be kept at the place.

Article 34. Resources

Adequate resources should be provided for conservation.

Words in italics are defined in Article 1.

Explanatory Notes

New decisions should respect and have minimal impact on the cultural significance of the place.

The best conservation often involves the least work and can be inexpensive.

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The Burra Charter Process

Steps in planning for and managing a place of cultural significance

The Burra Charter should be read as a whole.

Key articles relevant to each step are shown in the boxes. Article 6 summarises the Burra Charter Process.



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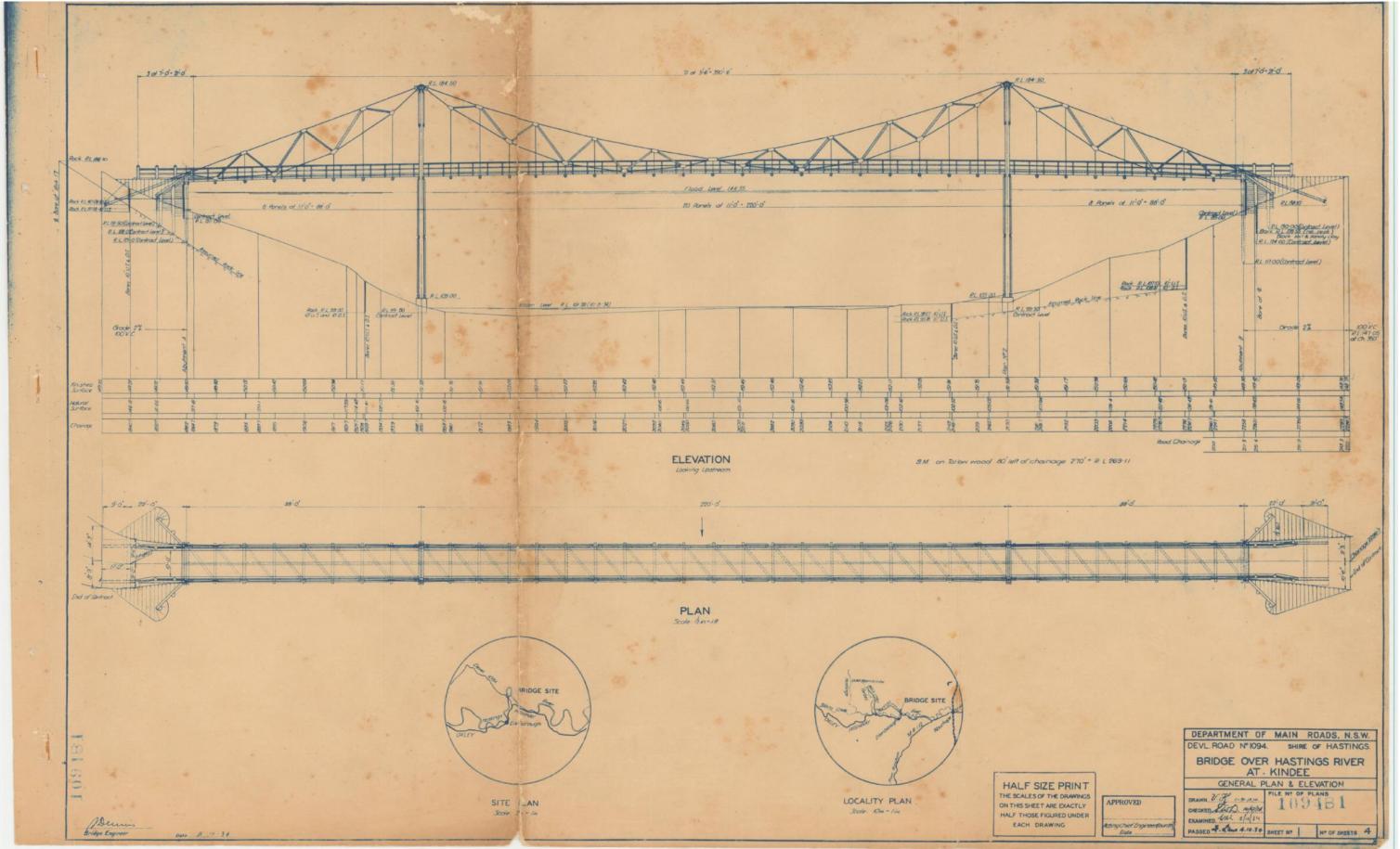
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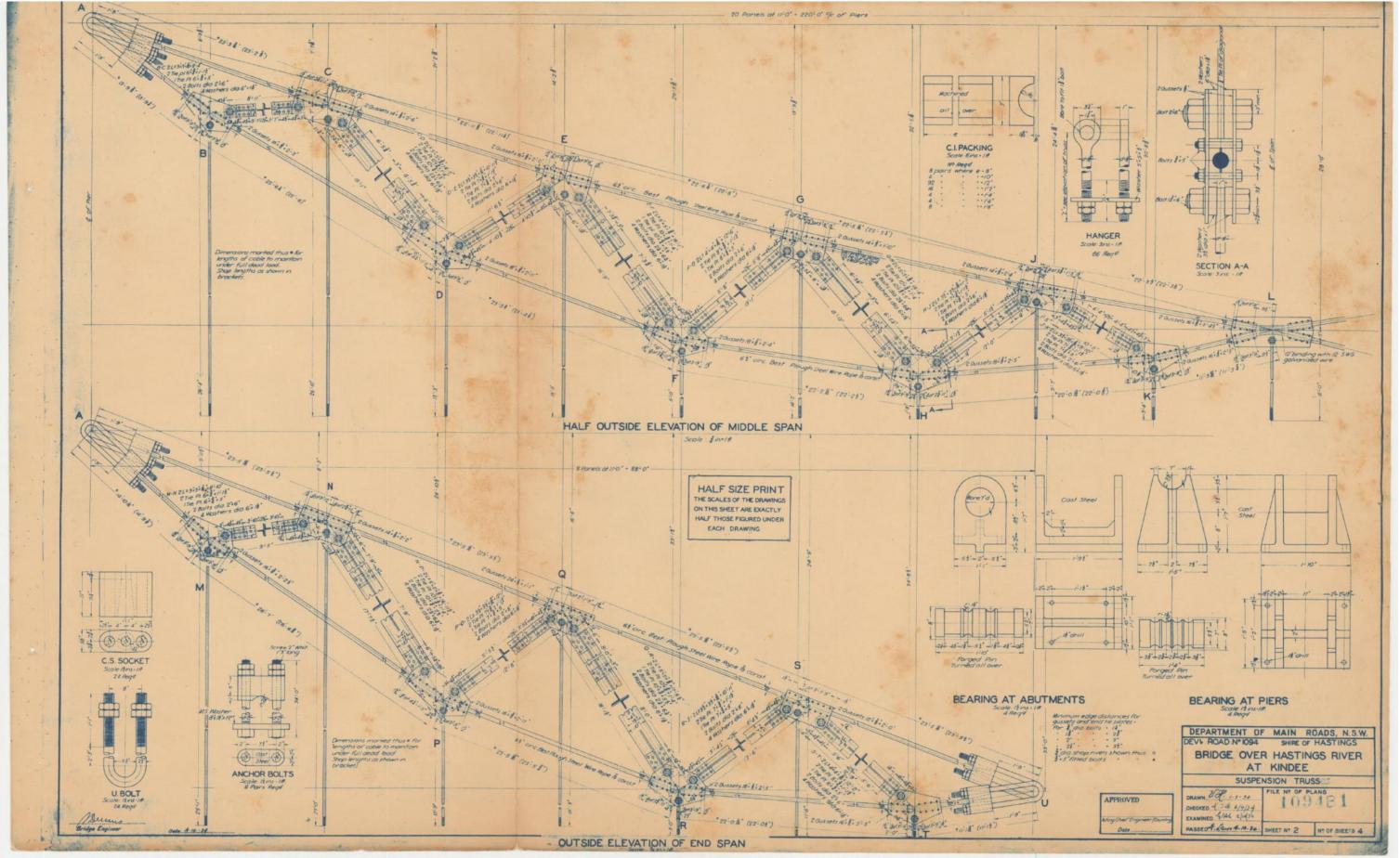
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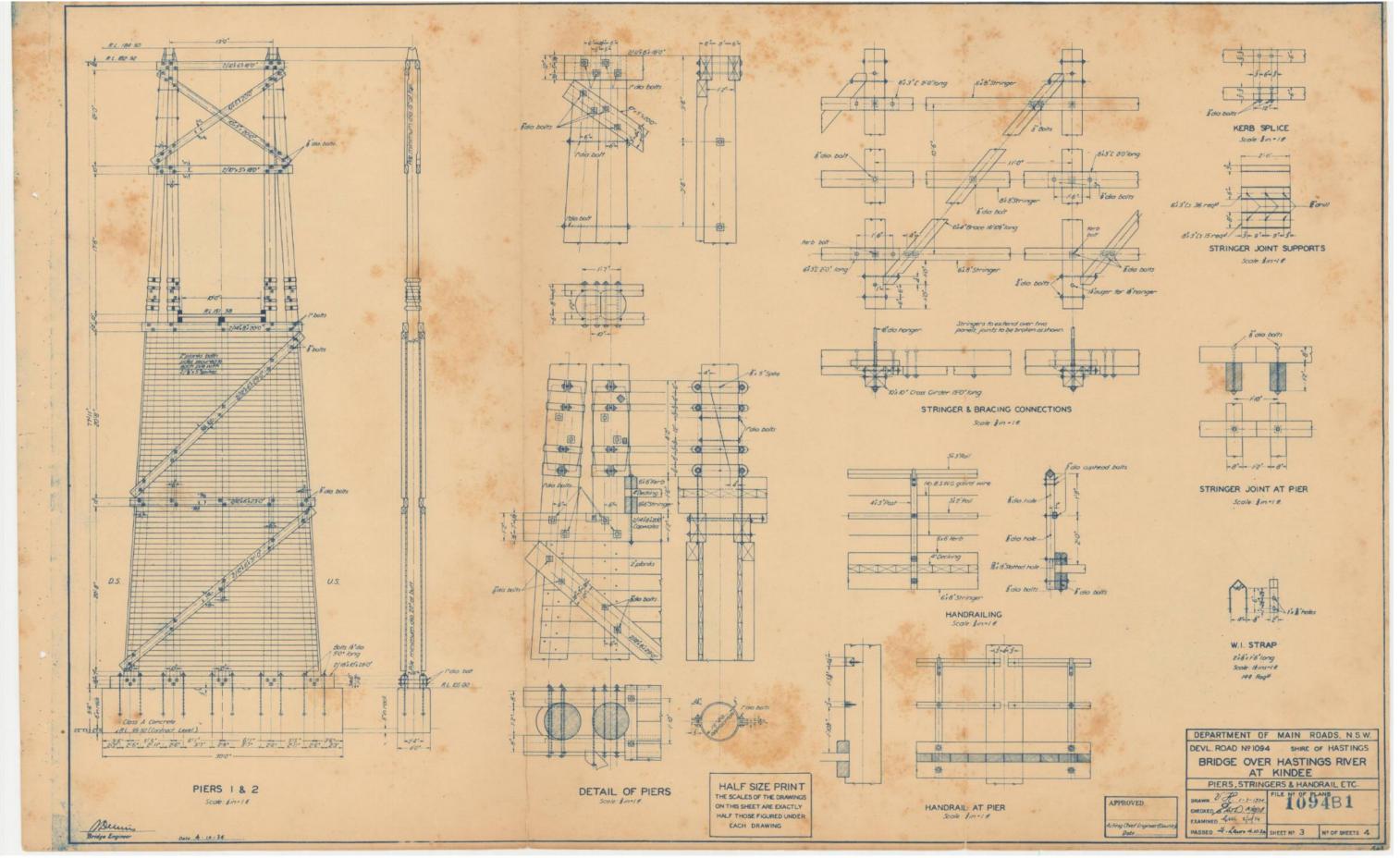
Appendix E – Record Drawings

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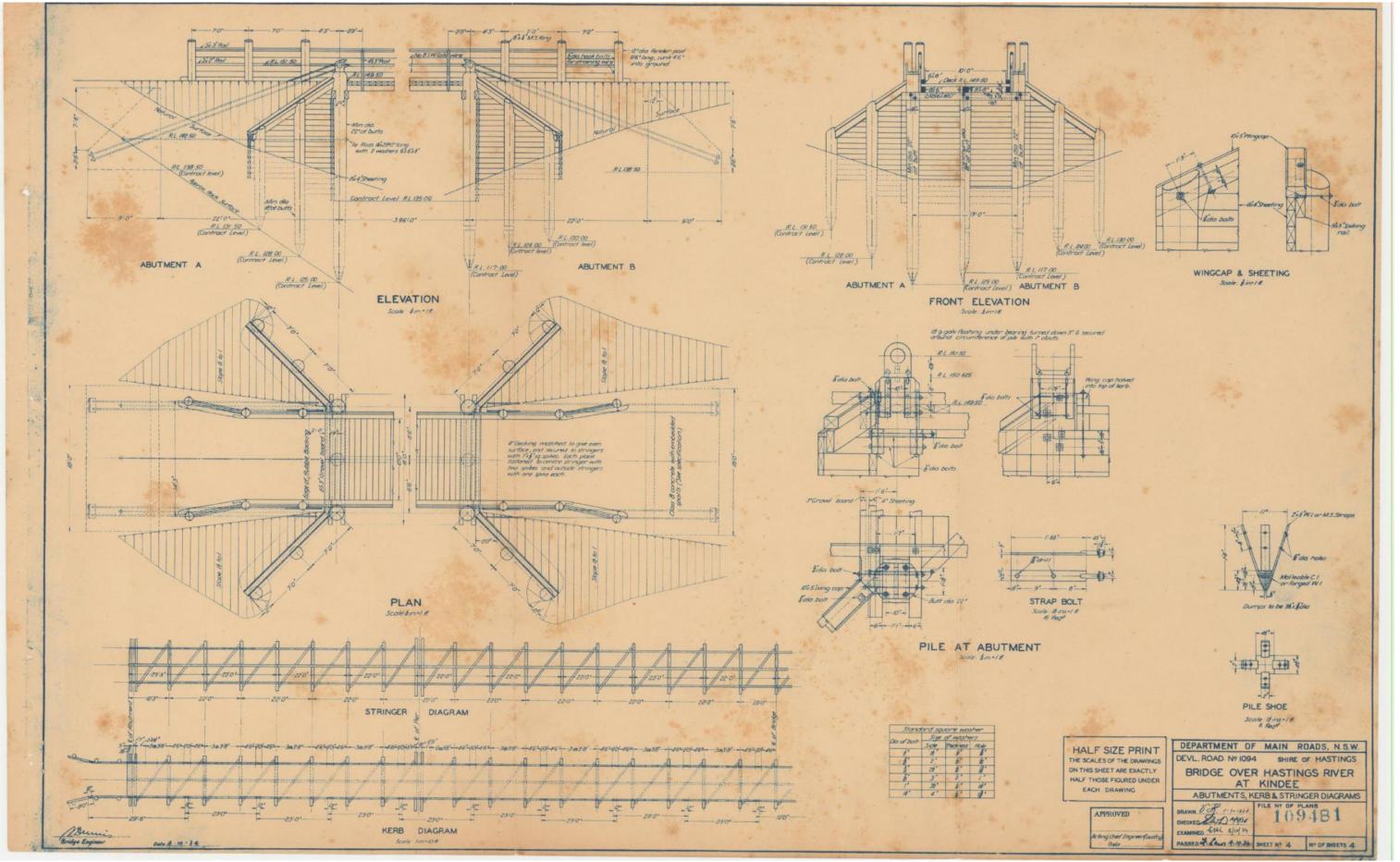




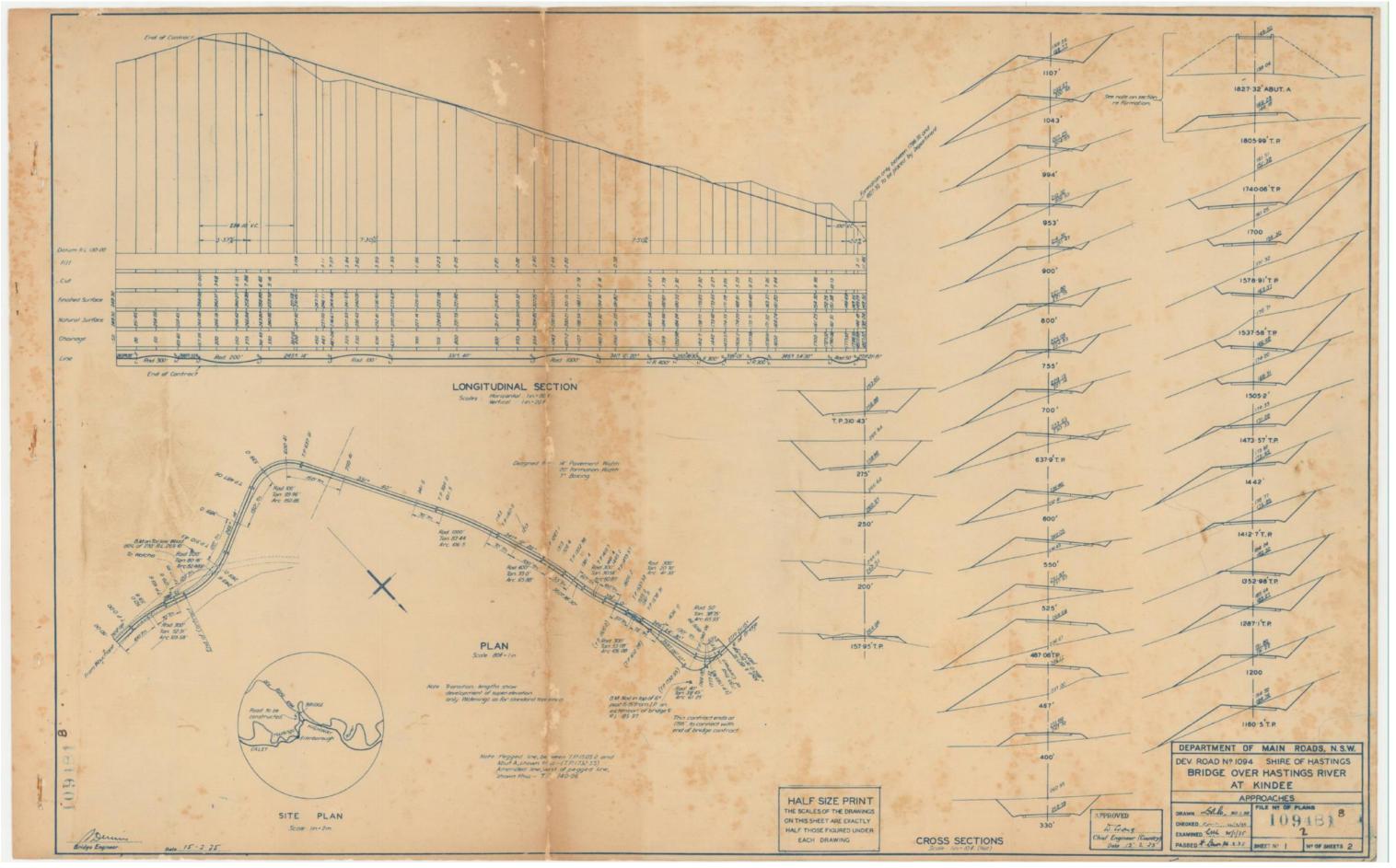
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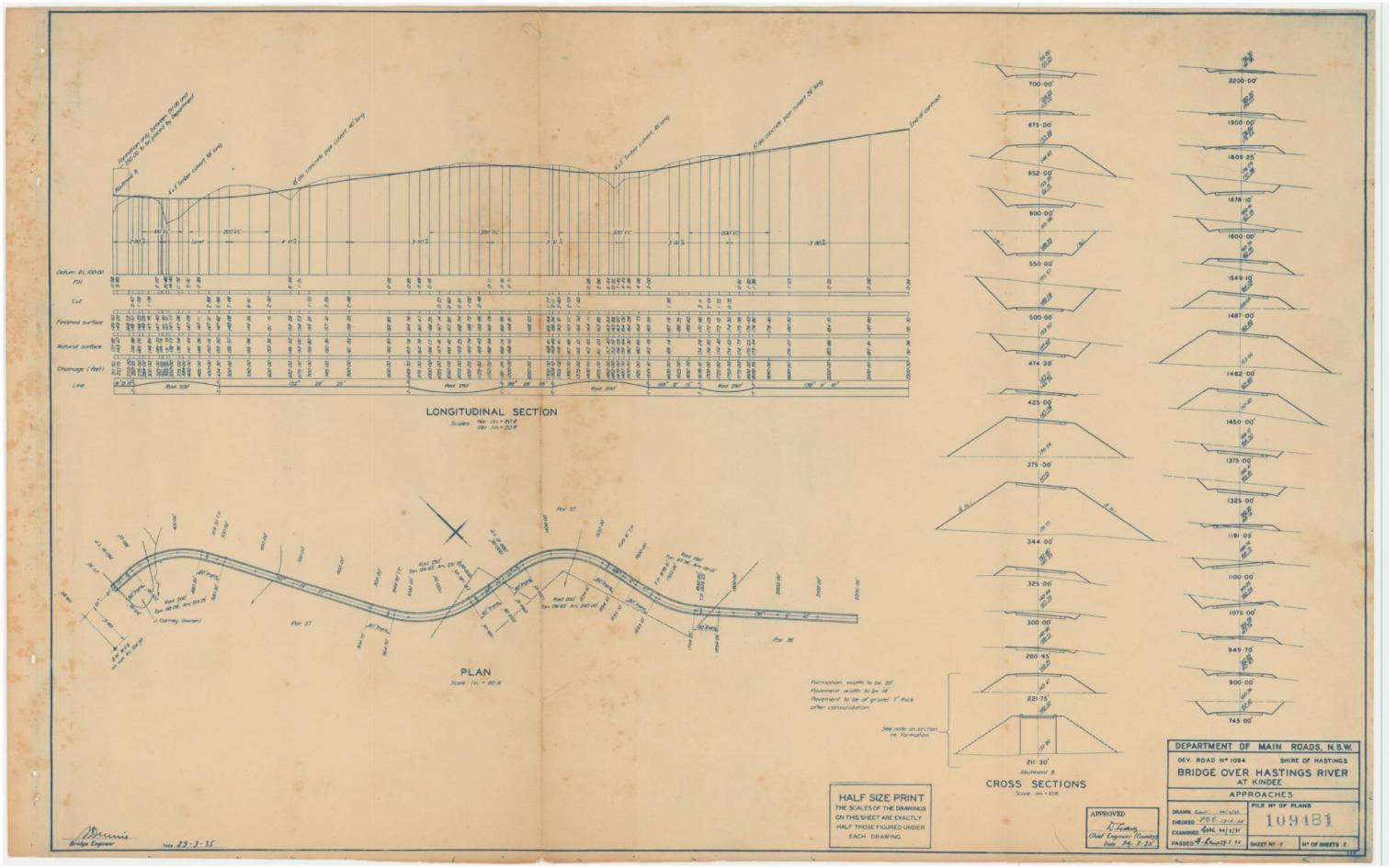
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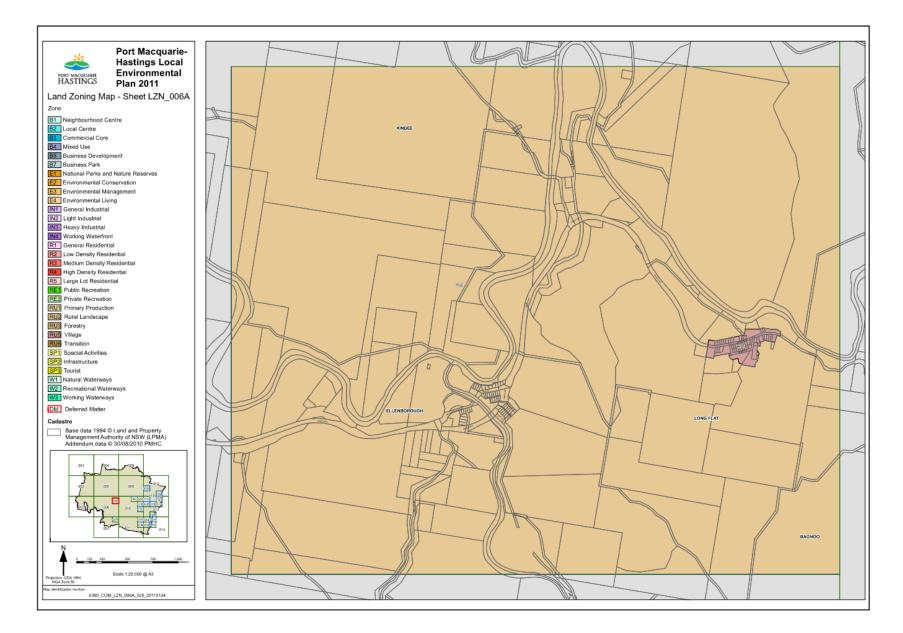
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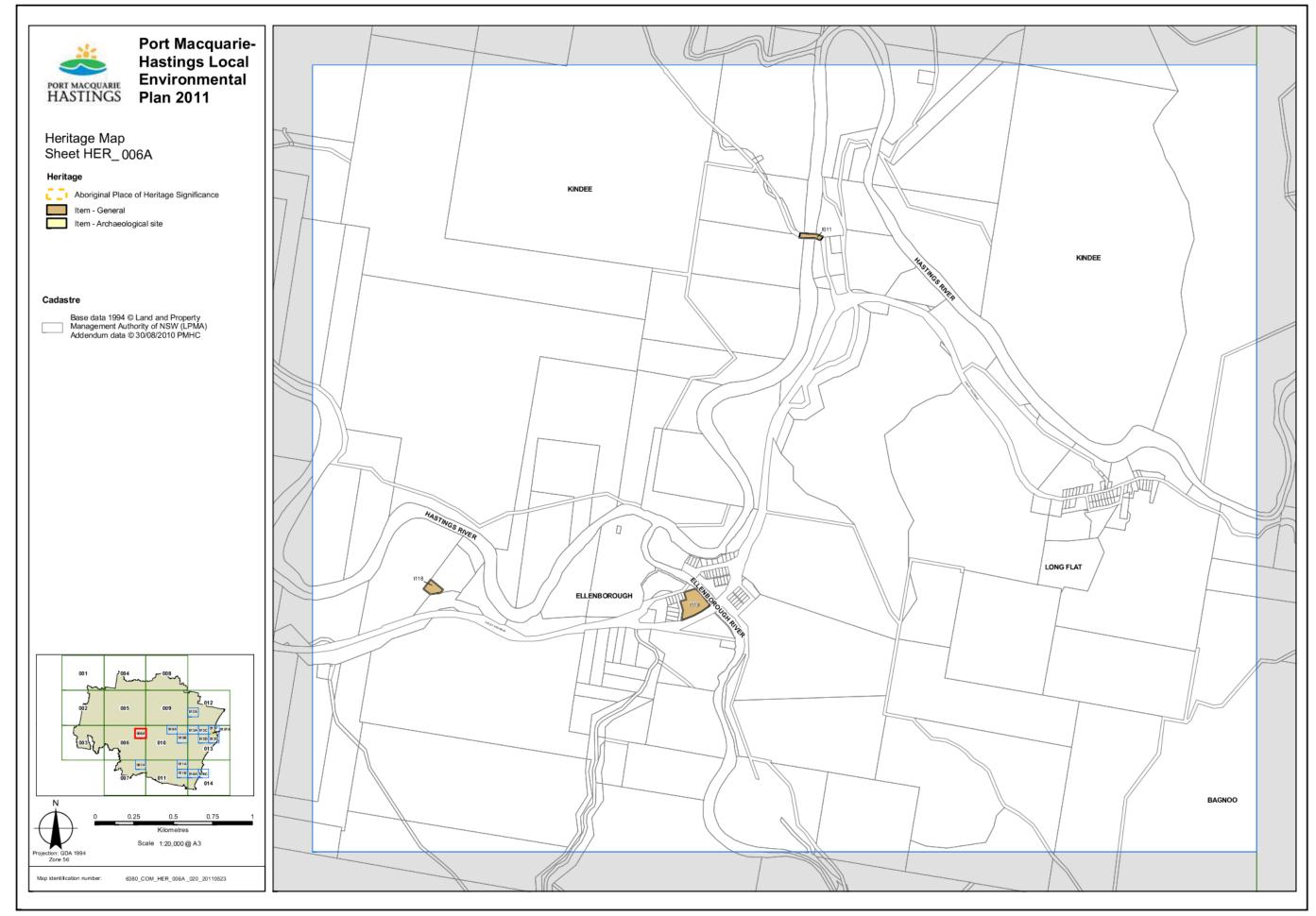
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Appendix F – Council Land Zoning and Heritage Maps



Item 12.01 Attachment 2



ORDINARY COUNCIL 16/06/2021

Appendix J - Aboriginal Heritage Information Management System Results



AHIMS Web Services (AWS)

Search Result

Purchase Order/Reference : 181989 Client Service ID : 413249

Date: 08 April 2019

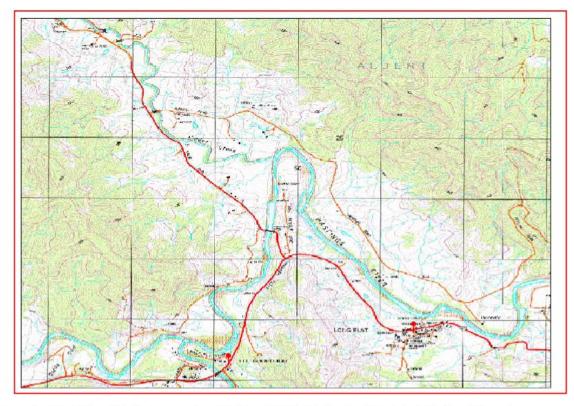
Angus Busuttil L3 48 Chandos Street st leonards New South Wales 2065 Attention: Angus Busuttil

Email: angus.busuttil@ttw.com.au

Dear Sir or Madam:

AHIMS Web Service search for the following area at Lat, Long From : -31.4391, 152.4394 - Lat, Long To : -31.3974, 152.5055 with a Buffer of 50 meters, conducted by Angus Busuttil on 08 April 2019.

The context area of your search is shown in the map below. Please note that the map does not accurately display the exact boundaries of the search as defined in the paragraph above. The map is to be used for general reference purposes only.



A search of the Office of the Environment and Heritage AHIMS Web Services (Aboriginal Heritage Information Management System) has shown that:

0 Aboriginal sites are recorded in or near the above location.
0 Aboriginal places have been declared in or near the above location.*

If your search shows Aboriginal sites or places what should you do?

- You must do an extensive search if AHIMS has shown that there are Aboriginal sites or places recorded in the search area.
- If you are checking AHIMS as a part of your due diligence, refer to the next steps of the Due Diligence Code of
 practice.
- You can get further information about Aboriginal places by looking at the gazettal notice that declared it.
 Aboriginal places gazetted after 2001 are available on the NSW Government Gazette

 (http://www.nsw.gov.au/gazette) website. Gazettal notices published prior to 2001 can be obtained from
 Office of Environment and Heritage's Aboriginal Heritage Information Unit upon request

Important information about your AHIMS search

- The information derived from the AHIMS search is only to be used for the purpose for which it was requested. It is not be made available to the public.
- AHIMS records information about Aboriginal sites that have been provided to Office of Environment and Heritage and Aboriginal places that have been declared by the Minister;
- Information recorded on AHIMS may vary in its accuracy and may not be up to date .Location details are
 recorded as grid references and it is important to note that there may be errors or omissions in these
 recordings,
- Some parts of New South Wales have not been investigated in detail and there may be fewer records of Aboriginal sites in those areas. These areas may contain Aboriginal sites which are not recorded on AHIMS.
- Aboriginal objects are protected under the National Parks and Wildlife Act 1974 even if they are not recorded as a site on AHIMS.
- This search can form part of your due diligence and remains valid for 12 months.

Port Macquarie-Hastings Council	21 / 06 / 2019
Kindee Bridge Conservation Management Plan	181989

Aboriginal Heritage Information Management System Results

Introduction:

The purpose of this assessment is to exercise due diligence and to assess whether carrying out maintenance work on the Kindee Bridge has potential to harm Aboriginal Objects. The purpose is also to determine whether the proposed maintenance works for the Kindee Bridge should apply for consent in the form of an Aboriginal Heritage Impact Permit (AHIP).

The procedure is a step by step process which forms a decision tree and identifies by Step 5 (if reached) if an AHIP needs to be carried out.⁸³ The context of this assessment is based on the repair and maintenance of fabric to the existing bridge. Assumptions made in this assessment assume that the bridge will remain, and no secondary bridge will be constructed. If further construction / additional bridges are planned to be constructed, a separate Aboriginal Heritage Information Management System (AHIMS) assessment will need to be carried out.

Step 1: Will the activity disturb the ground surface?

For the majority of maintenance and repair the ground surface will not be touched or disturbed. For repair of timber abutment piers, limited excavation / revealing of subsoil timber piers will be required. If the bridge is to be demolished, significant disturbance to the ground surface will arise. Areas that may require minor excavation are likely to be fill and would not disrupt the natural soil level.

Continue to Step 2a

Step 2a: Search the AHIMS database and use any other sources of information of which you are already aware

The search of the AHIMS database is included in the following pages of this appendix and indicated:

- No Aboriginal Sites are recorded in or near the above location
- No Aboriginal Places have been declared in or near the above location

It is acknowledged as part of this assessment that some parts of New South Wales have not been investigated in detail, and there may be fewer records of Aboriginal Sites in those areas. The regions of the Kindee Bridge may contain Aboriginal Sites which are note recorded on the AHIMS.

Continue to Step 2b

Step 2b: Activities in areas where landscape features indicate the presence of Aboriginal objects

Aboriginal objects are often associated with particular landscape features as a result of Aboriginal peoples' use of those features in their everyday lives and for traditional cultural activities. The Kindee Bridge is located within 200m of waters, which presents some chance of Aboriginal objects being in the area.

⁸³ Environment Climate Change and Water NSW, Due Diligence Code of Practice for the Protection of Aboriginal Objects in New South Wales, 2010

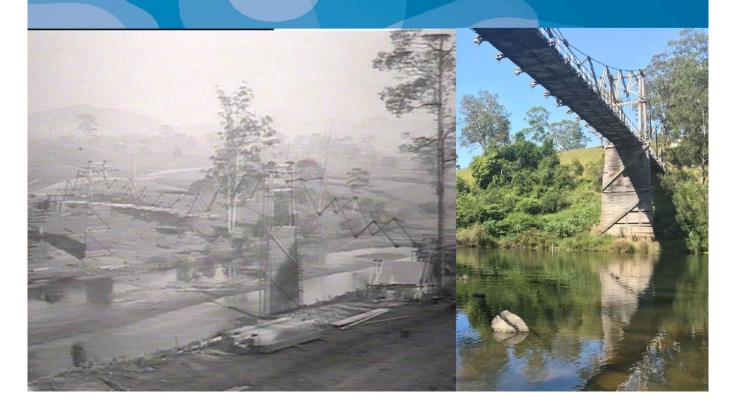
Port Macquarie-Hastings Council Kindee Bridge Conservation Management Plan 21 / 06 / 2019 181989

End Assessment

Conclusion

Having completed Sections 1 and 2 of the Due Diligence Code of Practice for the Protection of Aboriginal Objects in New South Wales, we have assessed that there is a low probability of objects occurring in the area of the proposed activity in relation to the expected minor works. This is based on the assumption that the only ground disturbance will be at abutment pier locations where limited but more extensive excavation has already been undertaken. Following this evaluation, the project for maintenance on the bridge can proceed without applying for an Aboriginal Heritage Impact Permit (AHIP). We advise however that this will not be the case for bridge demolition or constructing a new bridge. An AHIP will have to be carried out by an appropriately qualified consultant if these strategic options are selected.

Kindee Bridge Strategic Options Report



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Introduction

The existing suspension bridge over the Hastings River at Kindee Crossing (Kindee Bridge) was constructed and officially opened on 19 December 1936 and is approaching 100 years of age. The bridge remains open to traffic and maintains the original load limit of 10 ton prescribed in the original design. Its condition as a whole and its parts is fair to poor generally given its age and materials, and maintenance of key structural elements has been limited due to difficult access.

The Kindee Bridge Strategic Options Report has been prepared for the Kindee Bridge to:

- Investigate its history and heritage significance and prepare a Conservation Management Plan (ref: D2019/282712) for the structure.
- Investigate the current condition of all key elements on the bridge, and scope repairs and rehabilitation works, and prepare a Maintenance Planning Report (ref: D2021/063106).
- Develop strategic alternative route options for bypassing and/or replacement of the
 existing crossing when the bridge is deemed to have reached its end of life, and complete
 a multi criteria assessment to recommend the preferred option for further development;
 and
- Document on-going management strategies for the Kindee Crossing over the short, medium and long term.



Figure 1 - Kindee Bridge view from southwest

Background

The existing bridge was constructed in 1936 by the Department of Main Roads and care and responsibility for the structure was handed to Hastings Municipal Council in 1940. Continued miss-use and overloading along with limited maintenance over a 40-year period found the structure to be in state of disrepair in the early 1980's. In 1984, the National Trust of Australia classified the bridge, and included it in its Register under the following definition: *"Those places which are components of the natural or cultural environment of Australia, that have aesthetic,*"

historic, scientific, or social significance or other special value for future generations, as well as for the present community".

The Hastings Municipal Council at the time made representations to the Department of Main Roads (the Department) to take back ownership of the structure due to limited funds and skills to complete on-going essential maintenance. Formal advise received in 1987 stated the "Department is not prepared to assume responsibility from Council for the Kindee Bridge", but was able to offer "technical assistance in determining the extent of maintenance work required and advise on the most appropriate manner to complete the work". Council had also made numerous unsuccessful applications for State Government Grants on historic grounds. In 1987, Council allocated \$80,000 towards repairs and improvements to the Kindee Bridge, although noted it was unlikely that all necessary work could be completed with the funds available.

Investigation works were completed in 1987 by Council and the Department. The value of the identified scope of works was estimated to be in the order of \$300,000, including repairs to piles, construction of two (2) new abutments, repairs to top capwale, re-deck and repairs to cross girders and longitudinal stringers, abrasive blasting and repainting of steel work, other miscellaneous works and traffic control. These works were expected to provide a safe and maintenance free life for approximately 10 to 15 years. In 1989, \$300,000 was provided by the Department under the 1989/90 ACRD Programme as a Specific Work Category.

Works on the rehabilitation of Kindee Bridge commenced in 1989, and the estimated cost at completion was revised to be \$550,000 due to the time-consuming nature of the work. In 1991, rehabilitation work on Kindee Bridge was completed.

Strategic need

The Strategic Bridge Management Report (ref: D2016/225340) prepared by GHD in 2016 identified Kindee Bridge as a complex, high risk structure within the transport network of Port Macquarie-Hastings Council. The key risks identified included unknown suspension cable condition, suspension cable metalling socket connections, suspension cable anchorages, hanger rod design of upset end and threads, bridge posted load limit of 10 tonnes unchanged since original construction, and loading exceeding load limit by greater than 50% may cause cables to be put into compression.

High-level options presented in the Strategic Bridge Management Report included:

- Retain the bridge and apply for State Heritage Status and commensurate level of funding to overhaul and upgrade the bridge.
- Replace the bridge with a modern structure capable of carrying modern vehicles.

Key risks associated with the above options:

- The bridge is of high heritage significance and lodgement on the State Heritage Register will mean statutory approvals are required to undertake any work.
- Feasibility of upgrade to gain any additional load capacity is negligible.
- Cost and complexity of demolition and replacement due to the terrain and access limitations is likely to be a considerable undertaking.
- Maintenance of the existing structure is required until the preferred options can be identified, including condition assessment and load rating.
- Rehabilitation is unlikely to achieve increase is service level or meet current safety or engineering standards.

The National Road Safety Strategy 2011-2020, NSW Road Safety Strategy 2012-2021 and the Road Safety Plan 2021 have been implemented to improve road safety and to reduce road fatalities by 30 percent. A key priority area of these plans and strategies includes saving lives on

7

country roads. The existing road condition at Kindee Bridge includes steep grades, sharp approach road alignments, poor sight distances, and a single lane bridge requiring vehicles to give way prior to proceeding. Although no fatalities have been recorded at the bridge, there remains a high risk of serious incident at the site due to the poor approach road alignment and limited edge protection on the bridge deck, which is located over 15 m above river level.

NSW Long Term Transport Master Plan and the North Coast Regional Plan 2036 have goals towards thriving and interconnected economies, and to support an efficient and productive freight industry. The plans identify important farmland to support long-term agricultural production along the flood plains of the Hastings River and the Kindee Valley. The current bridge load limit and poor alternative routes limit access and any future development of the area for these important farmlands to the standards identified in 1936.

Key objectives

The key objectives of the management strategy have been identified as:

- Provide a safe and reliable crossing of the Hastings River now and into the future between the Oxley Highway and the Kindee area.
- Improve access for local and heavy vehicles in the Kindee area and encourage traffic efficiency and freight productivity.
- Minimise impacts on the community and the environment.
- Identify heritage and community issues and recognise these in the development of the options.
- To be a cost effective and affordable outcome.

Funding sources

2017-2021 Delivery and 2018-2019 Operational Plan

The Port Macquarie Hastings Council 2017-2021 Delivery Plan and 2018-2019 Operational Plan identified the following key objectives relevant to this Project:

- 4.4 Plan for integrated transport systems that help people get around and link our communities
 - 4.4.1 Plan, investigate, design and construct transport assets which address pedestrians, cyclists and vehicular needs to cater for the future growth of the region.
 - 4.4.1.22 CW Kindee Bridge structural repairs and Bridge Replacement Optioneering (multiyear project) – Adopted 2017-2018 -\$550,000
 - 4.4.1.36 CW Kindee Bridge Replacement Detailed Design Allocated Amount - \$300,000
 - 4.4.2 Develop and implement annual maintenance and renewal programs for transport assets.
 - 4.4.2.4 Undertake bridges and culverts maintenance and repair program including inspection, monitoring and bridge repair works.

At this stage, construction funding for an alternative crossing or full rehabilitation of the existing bridge has not been allocated.

Office of Environment and Heritage

As the bridge is not registered on the State Heritage Register, grant funding from Office of Environment and Heritage is not currently available. If the bridge is listed on the State Heritage Register applications for grant funding are assessed on the following basis:

 length of time on the State Heritage Register (with preference given to heritage items recently added to the Register)

- level of risk to the item, in particular from natural disaster (with preference given to those at higher risk)
- the sustainable long-term heritage benefits (with preference to those with longer term and more significant benefit)
- the degree of facilitation of heritage tourism, public benefit and enjoyment (with preference to those with a higher degree)
- the degree of innovation and use of best practice (with preference to the more innovative and better practice)
- cost effectiveness (with preference to the more cost effective) and
- any other.

The level of funding available for heritage items is generally in the order of \$10,000 per year, or \$150,000 for major projects. At this stage, this appears to be inadequate for the works required at the bridge.

Community and stakeholder consultation

A detailed community engagement plan will be implemented to ensure key stakeholders within the Kindee community and surrounding villages are informed, consulted and involved about the project.

Engagement objectives

- To inform the community about the current situation and expected remaining lifespan of the Kindee Bridge
- To inform the community about Council's scheduled maintenance program of the existing bridge
- Gather community input into the preferred option to help inform Council's decision making and ensure we are planning for the long-term future of our region
- Depending on community feedback and Council's recommendations, collaborate with the community to show case the heritage of the existing bridge.

Stage 1 - initial engagement

- Briefing to Executive 9 February 2021
- Briefing to Councillors 10 March 2021
- Invitation to briefing session- inform key stakeholders 16 March 2021
- Initial engagement 16 March to 8 April 2021 (extended to 20 May 2021)
 - Key stakeholder briefing sessions
 - o Dedicated Have Your Say page with supporting documents
 - Information session online
 - Drop-in session in person
- Analysis and engagement report
- Council meeting 16 June 2021
- Report back to the community 23 June 2021

Stakeholders identified as part of the Project Plan are as follows:

- Port Macquarie-Hastings Council Networks/Committees
 - Transport and Stormwater
 - Development and Environment
 - Water and Sewer
 - o Community Place
 - o Economic Development
 - Mayor and councillors
- Local residents and landowners
- Community Groups Long Flat / Kindee / Ellenborough

- Heritage Groups
- Utility Providers
- Department of Fisheries
- National Parks and Wildlife Services
- Roads and Maritime Services
- Rural Fire Service
- Transport Industry
- Forestry
- Other

Next steps

Once the initial phase of engagement and pending Council's recommendations, the next step would involve detailed design works of the preferred option(s). This would take place over a 9-to-12-month timeframe. Following the completion of the detailed design works, a briefing will be held for the Executive team and Councillors. The detailed design works would then be placed on public exhibition for a period of 28 days.

The Hastings River Valley

Locality

Kindee is located on the northern side of the Hastings River in the upper reaches of the Hastings River Valley within the Port Macquarie-Hastings Council Local Government Area approximately 30 kilometres west of Wauchope, and 300 km north of Sydney.

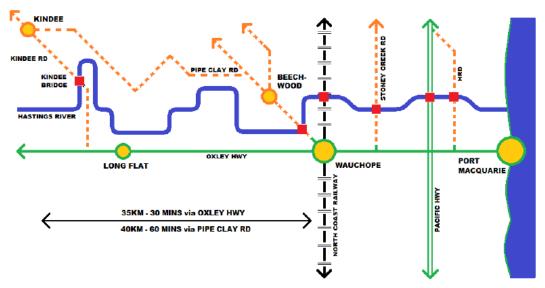


Figure 2 - Kindee Bridge Locality schematic

The Hastings River catchment is located on the Mid North coast of NSW and covers an area of approximately 4500 square kilometres. The catchment falls within an area where the New England Fold belt overthrusts the Sydney basin, resulting in a complex pattern of ridges and valleys (*NSW National Parks and Wildlife Service 2003*) that extends eastward to the coast.

Approximately 77% of the Hastings catchment is covered by native vegetation, much of this within DEC reserve (25%) or State Forest (20%) on the steeper upper catchment and the coast. Less than 23% of the catchment is covered by crop or pasture, and less than 1% is used for other purposes (*Beale et al. 2004*). Clearing is concentrated on the flatter, coastal plains. The major regional centres of Port Macquarie and Wauchope support a population of approximately 80,000.

The northern and western sections of the Hastings River Valley consist mainly of rugged to mountainous terrain, with the central area being hilly to steep and undulating. From about six kilometres above Wauchope, where the Hastings River becomes tidal, the hills give way to open and almost entirely flat land. Downstream from Wauchope the river widens and flows through a relatively broad floodplain to the South Pacific Ocean. Most of the coastal fringe of the valley is characterised by low lying and swampy terrain, backed at intervals by extensive alluvial flats within the tidal sections of the Hastings and Wilson Rivers.

The Hastings River Valley receives an average annual rainfall exceeding 1150 mm, except for an area on the coastal plain between Kempsey and Telegraph Point. The highest occurs on the Comboyne Plateau, receiving an annual average of 2000 mm, with Laurieton receiving 1550 mm, Port Macquarie 1400 mm, Telegraph Point 1300 mm, and Kindee 1200 mm. The rainfall in individual years varies markedly. The wettest period in the valley is experienced from December to April, continuing on the coastal fringe until June, during which time approximately 55% of the annual rainfall is received.

The major tributaries of the upper Hastings River include Morton's and Frazer's Creeks, and the Ellenborough, Forbes and Pappinbarra Rivers. These tributaries all drain in an easterly direction and discharge to the Hastings River above Bain Bridge. The Upper Hastings River and its tributaries are all incised within narrow floodplains.

History

1818 - Settlement of the Hastings

2018 marks 200 years since John Oxley and his team of 15 explorers traversed the mountains from the west and surveyed our region for early European settlement. For thousands of years before, the Birpai people, the traditional custodians of our land, inhabited our region.

Port Macquarie was named by John Oxley after the governor of NSW, Lachlan Macquarie in 1818. The Hastings River was named after the Governor General of India at around the same time. Although the area had been first noticed by Captain Cook on his voyage along the coast in 1770 and again later by Matthew Flinders in 1802, it was not explored in any detail until Oxley returned in 1819. Macquarie initiated Oxley's expedition, as he was interested in the site's potential as a penal settlement.

The penal settlement would be established in 1821 under Captain Francis Allman who landed at the "town green" at the top of what is now Clarence Street. Captain Allman immediately began directing the 60 convicts sent to establish the settlement, to clear the area of trees and begin farming in order to become self-sufficient.

1850 - Original Settlers

The Forbes River and Kindee Creek areas continued to be developed from the mid-1900th century, with settlers engaging predominantly in dairying and logging. Both areas were effectively isolated due to the difficult crossing of the Hastings River, which was forded at numerous open crossings including Double Crossing, Ellenborough and at Kindee Crossing. The hand transportation of cream cans up the steep banks of the river was always difficult and was often delayed or rendered impossible in times of flood, causing considerable loss.



Figure 3 - Early settlers at Kindee Crossing

One of the original settlers, Mr. John "Uncle Johnny" Coombes born in 1844 arrived in Newcastle, Australia on the sail ship "Victoria" in 1849 with his family at the age of five. His father and brother worked in the coal mines until an opportunity for a job of shepherding on Tiara Station near Walcha presented. In 1859, the family took on dairying in the Parish of Moorabark on the Forbes River after his father had bought Filly Flat. Major Innes had a big dairying establishment at Cowaral on the Forbes River prior to his arrival. He stated "When we came from the tablelands there was little settlement on the upper river, and the blacks, then in their wild state, were plentiful. There was a big camp right here where I now live".

"Uncle Johnny" also worked with the teamsters on the roads using bullocks and drays, and carting bales of wool or flour from Walcha on the tablelands to Port Macquarie. He grew wheat and corn and carried this to the old flourmill, which was situated on Windmill Hill in Port Macquarie. In later days, cedar, beech and mountain ash were taken to Wauchope for shipment to Sydney. He helped cut the track through to Bruin's Camp in 1871, the route that the Oxley Highway now follows.

1933 - Double Crossing Suspension Bridge

In 1928, the road along Kindee Creek from the Yarras-Wauchope Road (now the Oxley Highway) over to the Forbes River, and continuing northwards up the valley was proclaimed a developmental road, and a grant of £15,000 was made of Federal Aid funds. This included allowance for erection of a bridge over the Hastings at Kindee Crossing. By 1931, due to limited funds the future of the programme looked unpromising. Funds were redirected to a section of road along the Forbes River as a greater number of settlers reside in the Forbes Valley, which included a "cheap bridge" over the Hastings at Double Crossing.



View of the completed bridge, taken from the northern side, downstream.

Figure 4 - Double Crossing Bridge

Mr. E. E. Booth, Hastings Shire Engineer suggested a light suspension bridge for Double Crossing to carry vehicles weighing not more than 10 tons gross. This bridge with a central span of 176 ft. (53.6m) and two side spans of 66 ft. (20m) was designed by the Department of Main Roads and constructed under contract by Mr. J. J. McPhillips with cables and steelwork fabricated and delivered by Walsh Island Government Dockyard. Although the structure was of a somewhat novel type, only minor difficulties were experienced in erecting and adjusting the cables. The bridge was officially opened on 1st July 1933 by Hon. L. O. Martin M.L.A Minister of Justice in the presence of a representative gathering.

Mr. Newell, Commissioner for Main Roads attended the opening. "As can be seen from the appearance of the structure, it is not so robust as those constructed by the Department of Main Roads, the carrying capacity being only about three-fifths of the usual capacity. But being designed for any one load of 10 tons, or a load of 60 lbs. per square foot all over, it will with perfect safety carry any load which this district can place upon it, except perhaps, bullock teams hauling jinkers laden with timber."

"Continuing, Mr. Newell stressed the fact that it was an all-Australian bridge, every item being produced or made in Australia".

Mr. John "Uncle Johnny" Coombes was in attendance as the oldest resident of the district aged 89 years of age, and was invited to cut the ribbon. "It was the proudest day of his life, he said, and a day he had looked forward to. He was too happy to be able to say much, but had great pleasure in cutting the ribbon".

Mr. A. Blackie, Forbes River Progress Association. "We, as residents of the isolated Forbes River district, deeply appreciate and are proud of this magnificent structure, not only for its structural beauty and economic importance, but also because of the fact that it is the first of its particular type in Australia."

Cr. John Downes (President of Hastings Shire Council). "The completion of the structure represented a long battle over many years by the pioneers and residents of the district it will serve. The speaker drew a mental picture of the conditions prevailing when this locality was in its virgin state, and the pioneers made their entry to make homes for themselves – a picture of great disabilities, of creeks and rivers to be crossed, and of dense bush, practically cut off from the outer world, and with little hope of reaping rewards in increased values to their holdings as a result of their great efforts. This bridge would give great and deserving relief to those settlers and their descendants."

In 1943, maintenance and repair of the suspension bridge at Double Crossing was completed including re-cambering and general tightening, noting heavy loads had been using the bridge *"although difficult to catch the offenders"*. An alternative low-level crossing was identified to avoid timber trucks utilising the existing suspension bridge, and stated to be less than any alterations in the design of the suspension bridge to strengthen it. Unfortunately, on 24 March 1946, a major flood *"close to '29 levels"* washed the existing suspension bridge away. *"The Forbes River suspension bridge has gone, and some idea of the height and rush of water can be gauged from the fact."* A new low-level bridge at Double Crossing was proposed and commenced construction in 1950, although delayed indefinitely due to heavy rain and major scour exposing boulder sub strata in lieu of rock, as was expected.

1936 - Kindee Crossing Suspension Bridge

Works on the Kindee Road was progressing in February 1930 with the construction of nine concrete pipe culverts, utilising local river gravel, and construction of two wooden culverts. It was noted, "when the river is bridged, this road will be a great convenience and joy to the settlers. It will open up some fine dairying country, and give access to the timber. It is quite likely a sawmill will be working on this roadside."

The method of crossing the river at Kindee Crossing was captured in correspondence from June 1935. "The continued dry weather has made it possible for motor traffic to cross the river, providing the driver is not nervous, and is prepared to get his feet wet occasionally."

The road is purely a spur road of developmental character serving a number of dairy farms, etc., in a fertile side valley surrounded by impassable mountains and there is no possibility of through traffic developing. Hence, a lightweight suspension bridge was proposed by the Department

similar to the bridge constructed at Double Crossing. This bridge with a central span of 220 ft. (67.1 m) and two side spans of 88 ft. (26.8 m), was designed by V. Karmalsky and A. T. Britton, Engineering in the Bridge Section of the Department of Main Roads.



Figure 5 - Double Crossing Bridge - Official Opening

Correspondence from the time indicated piling works for the bridge were well in hand in April 1935, and by September 1935 the fox was on hand to swing the heavy material into position. On 30 April 1936, the road and bridge contractor, Mr. Roy Haydon died after an accident at the Kindee Bridge site. *"With a man named Hayworth, he was working in a cradle, suspended on a wire cable, from poles either side of the river. A pole broke without warning, and the cradle dropped about 80 feet. Mr. Haydon received frightful injuries, but his companion only slight injuries. Mr. Haydon's wife witnessed the accident from the riverbank." Subsequently, the Department took over construction of the bridge and in August 1936, all cables were in place and were anticipating opening the bridge to traffic by November of that year.*

1937 - Allan Bridge

A similar structure was designed and built over the Pages River at Segenhoe near Scone, NSW in 1937. The design was adopted following loss of a previous low-level bridge at this location. The Shire Engineer visited the site of the Double Crossing suspension bridge and the Kindee Crossing suspension bridge (under construction) in April 1936 and reported to council recommending the same bridge to be constructed over the Page River at Segenhoe. This bridge was officially opened in June 1937.

This bridge was washed away in the 1955 flood and rebuilt in the same form at a location further downstream. This bridge remains in use although load limited to 4 tonnes.

Anniversary's and celebrations

The Kindee Bridge was officially opened on 19th December 1936 and a plaque was erected on the site in 1988 to celebrate Australia's Bicentenary by the Department of Main Roads (DMR) and the National Roads and Motorists Association (NRMA).



Figure 6 - Kindee Bridge - Existing interpretation

The Kindee Hall Progress Association has arranged celebrations for the anniversary of the construction of the bridge in 1986 (50th), 2011 (75th) and in 2017 (80th). In 2017, descendants of Kindee's Hollis family gathered with generations of the region's original settlers to mark the bridge's 80th year. The Wingham and Hastings Marching Bagpipe Band played while vintage cars paraded over the historic structure. Kindee's oldest resident Dot McKinnon cut the cake with 80-year-old Lenny Kirkman, who was the first baby to be taken across the bridge when it was opened in 1936.

Heritage significance

Taylor Thomson Whitting Pty Ltd was engaged in 2019 to prepare a Conservation Management Plan (ref: D2019/282712) for the Kindee Bridge.

A desktop review of vehicular suspension bridges has identified that there may be less than 10 suspension bridges remaining in service in NSW, and Australia. The suspension bridges identified in order of construction date are as follows:

- 1880 Fitzroy Bridge, Rockhampton, Queensland Replaced with steel plate girders in 1950's
- 1890 Woolaston Bridge, Warnambul, Victoria Bypassed but remains as footbridge
- 1892 Long Gully Bridge, Northbridge, NSW Stiffened truss girder type similar to Hampden Reconstructed as concrete arch in 1939, sandstone towers remain.

- 1898 Hampden Bridge, Kangaroo Valley, NSW Stiffened girder type Still in use
- 1903 Maldon Bridge, Picton, NSW Abandoned but still in place Damaged by arson
- 1933 Double Crossing Bridge, Yarras, NSW Stiffened cables Washed away in 1946 and replaced with low level bridge
- 1936 Walter Taylor Bridge, Indooroopilly, Queensland Still is use
- 1936 Kindee Bridge, Kindee, NSW Still in use
- 1937 Allan Bridge, Segenhoe, NSW Stiffened cables original washed away in 1955, but re-built – Still in use
- 1970 Gresford Bridge, Gresford, NSW Un stiffened suspension bridge, 3 tonne limit Still in use

Specific findings and recommended from the heritage assessment and Conservation Management Plan are summarised in a later section within this report.

Kindee Crossing Suspension Bridge

Existing road network

Kindee Road crosses the Hastings River between Ellenborough and Long Flat in the Upper Hastings River catchment. The road provides access for approximately 41 properties to the Oxley Highway, although limited to 10 tonnes due to the capacity of the existing bridge.

The alternative route to Wauchope via Beechwood using Pipeclay Road is a mountainous and unsealed gravel road and joins Pappinbarra Road near Beechwood. Travel time to Wauchope is approximately 30 minutes longer utilising this route.

The Kindee Road was described in the original design as "purely a spur road of developmental character serving a number of dairy farms, etc., in a fertile side valley surrounded by impassable mountains and there is no possibility of through traffic developing".

There are no other alternative access roads from the area other than via steep tracks through State Forestry and private property.

Existing topography and land use

The northern and western sections of the Hastings River Valley consist mainly of rugged to mountainous terrain, with the central area being hilly to steep and undulating.

Land use within the Kindee locality includes rural residential properties, cropping (for hay production), and livestock grazing.

Existing bridge details

Original drawings of the bridge are attached to Appendix A. The design and construction of the Kindee Bridge was captured in a paper prepared by Karmalsky V. and Britton A.T. for the Transactions of The Institute Vol XVIII titled *"Three Span Light Suspension Bridge Over Hastings River at Kindee Crossing, NSW, 1937"*, and attached to Appendix B.

The Bridge over the Hastings River at Kindee comprises of three spans; 88 ft. (26.820 m), 220 ft. (67.055 m) and 88 ft. (26.820 m) respectively. The timber deck in all three spans is supported from the trussed cables with a panel length of 11 ft. (3.350 m) used throughout. The bridge has a through camber of 3 ft. (0.915 m) for appearance and the clearance height above the highest known flood is 6 ft. (1.830 m) at the centre of the river and 3 ft. (0.915 m) at the abutments.

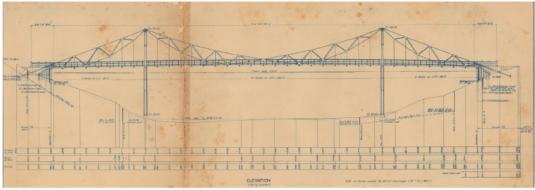


Figure 8 - Kindee Bridge - Original Drawings - Elevation

The centre span truss is an inverted three-hinge arch and it is calculated that it is statistically determinate. The side spans act as simply supported trusses to carry loads on the span itself

whilst also acting as a backstay to support the horizontal tension in the centre spans truss when loaded.

The cables are of best plough steel wire ropes in 7/19 construction and cable ends are secured in cast steel sockets by white metal. The metalling sockets are provided with mild steel U-bolts that pass around forged steel pins supported by cast steel bearings at each abutment and pier. The cables are protected against corrosion by asphaltic paint. Gussets are securely clamped to the cable by means of grooved cast iron packing pieces and fitted bolts with the diagonals and hangers secured to the gussets by pin joints. All diagonals are compression members of two angles riveted together to form a star section. Hangers are tension members of round mild steel bar.

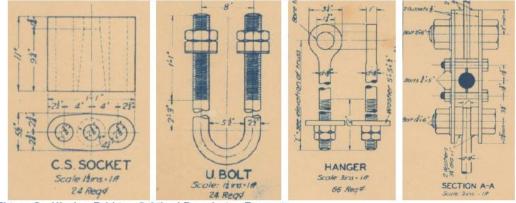


Figure 9 - Kindee Bridge - Original Drawings - Excerpts

The cable anchorage is provided by means of mass concrete dead-man blocks buried in the approach embankments. Anchor bolts are embedded in the blocks and are provided with saddles that rest on the abutment bearing pins. Where these bolts are below ground level, they are encased in steel piping filled with concrete to prevent corrosion.

The deck structure and piers are of hardwood timber, with the timber decking being plywood replacing the original transverse hardwood decking from the original design in the 1991 rehabilitation work.

Bridge management prioritisation

The Port Macquarie-Hastings Council Bridge Management System utilises a prioritisation system comparing Probability of Failure versus Consequence of Failure.

The Probability Factors include existing condition of the foundation, sub-structure, superstructure, deck and barriers considering existing load limits, deterioration factors and any current exceedances (Condition Rating 5). Kindee Bridge is currently ranked 2nd overall (at March 2021) based on probability factors, due to the poor condition of the substructure and key structural elements in the suspension system.

The Consequence Factors consider road hierarchy, traffic volumes, road surface type, length and economic importance of detour route, regional or local road, bus route or emergency vehicle requirements, and heritage listing and status. Kindee Bridge is currently ranked 44th overall (at March 2021) due to its the relatively low importance of the route within the Local Government Area and associated low traffic volumes.

The combined Risk Rating considering Probability Factors and Consequence Factors currently ranks Kindee Bridge at 2nd overall within the LGA, heavily weighted by the existing condition of the

structure. Based on the age of construction materials utilised (steel and timber), it is considered that a number of key elements have passed their original intended design life and the bridge is an ongoing risk and maintenance liability to Port Macquarie-Hastings Council.

Maintenance costs are anticipated to escalate partially due to minimal and sporadic maintenance to date, and partially due to the age of its elements (primary structural elements of timber and steel greater than 80 years of age from original construction), along with the increased potential risk of failure due to overload, flooding or bushfire. On-going impacts to the community will be experienced due to bridge closures due to maintenance or load limits resulting in an approximately 70 km detour of 1.5 hours from the Kindee area to the Oxley Highway at Long Flat, NSW.

Bridge operations and maintenance is allocated approximately \$1,000,000 per year to cover general maintenance and inspection, repairs, rehabilitation and renewal of the existing 133 public bridge assets across the Local Government Area. It is not feasible to provide adequate on-going maintenance of Kindee Bridge from this fund given its complexity. Port Macquarie-Hastings Council allocated additional funds within the Operational Plan of \$500,000 (2018/2019) and \$500,000 (2019/2020) to address critical structural repairs and maintenance requirements.

Key risks

The key risks associated with the on-going care and control of the existing Kindee Bridge have been identified as follows:

- Bridge load limit posted as per the original design of 10 ton (10.160 tonne), and susceptible to misuse and overloading;
- Any load exceeding the original design load of 10 ton by 50% or more, along with an
 impact load, may put the cables into compression and resulting collapse of the structure.
 Assessment may be required to confirm bridge behaviour, and investigate failure
 methods and implications based on specific element failure.
- Bridge susceptible to catastrophic failure under significant flood events greater than 1 in 100-year ARI, or flood events with significant debris or driftwood.
- Bridge susceptible to damage or catastrophic failure from bushfire impact due to timber and steel materials of construction.
- Dead man anchor block buried in approach roadway is key element for stability of structure. Any movement of anchor is critical, and inspection is not feasible. Condition and performance of this is reliant on original construction and stability of existing abutments.
- Verticality of piers essential to ensure vertical loads only to these elements.
- Suspension cables have been inspected, and one location noted as having external strands broken in 1992, but this has not been confirmed in subsequent inspections (Noble Engineering Services (Noble) Pty. Ltd. 2016 ref: D2016/225348 and D2016/685310).
- Suspension cable white metalling socket connections are not able to be inspected and can be a location where internal cable corrosion can initiate. Noble have commented on this in 2016 report but have not identified any current condition issues at these points.
- Nicking of wires can cause stress raisers and slippage due to high load twisting.
- Fatigue loading of the rope at anchorages (white metal socket) is a potential cause of failure.
- Suspension cable anchorages are primary structural elements. Inspected in 1992 and 2016 including non-destructive testing. No cracking detected. Corrosion section loss and protective sheathing damage identified in 2016.
- Hanger rods in original design have upset ends and rolled threads that can both induce fatigue failure. Hangers have been replaced with plate welded to rod. NDT of weld completed during Noble inspection resulting in replacement of two (2) hanger rods due to cracking or discontinuities identified in butt welds.

- Node clamp assembly has significant corrosion and clamp bolts lost throughout. Implications of slippage at these locations needs to be investigated and replacement prioritised.
- Bridge decking not per original design and has been replaced with softwood ply decking with bitumen sealing. Potential increased dead load compared to original design, and condition of ply deck deterioration due to water damage and current age (approx. 30 years).
- Bitumen sealing and debris build-up affecting node points and accelerating corrosion of node bolt connections and gusset plates.
- Asphaltic paint used to protect cables is likely to be carcinogenic.
- Red lead paint was used previously for steel protection coating, and implications to ongoing maintenance related WHSE implications.



Figure 10 - Kindee Bridge - Vehicle impact in 2018

Maintenance history

The Maintenance Planning Report (ref. D2021/063106) has been prepared to document the known history of inspection, monitoring, maintenance and repairs that have occurred at the existing bridge since original construction in 1936. There are significant gaps in information due to availability of records, so this report is not intended to be a complete and accurate record, but a live document to capture information to assist with planning and maintenance activities.

Limited evidence of maintenance or repairs of the existing bridge were found between 1940 and 1980, and inspection reports from the time show a significant level of degradation of all key elements. In 1990, with assistance from the Department of Main Roads, major rehabilitation works were completed including:

replacement of steel hanger rods

- replacement of all timber cross girders and stringers
- replacement of the existing timber deck with ply decking
- repairs and rehabilitation of abutments
- Timber pile epoxy resin filling and replacement of lower downstream pile column at both piers.

Since 1990, limited works have been completed on the bridge other than maintenance of the wearing surface (spray seal), and over-coat painting prior to birthday celebrations in 2011 and 2017.

In 2020, major rehabilitation works commenced following key findings from recent condition inspections. The works are anticipated to include: node block repairs and rehabilitation; handrail repairs and replacement; deck substructure inspection and repairs; ply decking condition inspection; timber pier wrapping and epoxy filling, and cable system anchorage repairs and protection.

Existing condition

The Maintenance Planning Report (ref: D2021/063106) has compiled all historic inspection data available at the time of this report, to record condition of all key elements, and to allow identification of trends in condition if possible. The inspection data included timber bore log records, level 2 and 3 inspection reports, suspension system non-destructive testing reports, and photographic records where available.

Further investigations have been completed into the condition of the existing structure during the preparation of this report including node block gusset plate corrosion survey, timber ply deck core samples and moisture content testing, Level 2 inspection of timber cross girders and longitudinal structures, and Level 2 inspection and stress wave testing of timber piers.

The key findings from current known condition are as follows:

- Foundations
 - Concrete dead man anchors blocks are of original construction and buried behind abutments. Un-inspect able and reliant on stability of abutment soil block to support bridge using passive soil pressure. Condition assumed to be fair, although condition of cast in anchor rods critical to performance of bridge.
 - Timber piles in abutments are likely to be approaching their design life as they are approaching 30 years of age. Limited recent timber boring records are not available for these elements. Visual inspection shows surface deterioration, splitting and rot on the back face of these elements generally. The concrete capping beam (installed over original piles in 1990) appears to be intact, although piles resting on this surface are rotting from their base.
 - Concrete foundations at piers are original construction of mass concrete founded on bedrock. Generally, in fair condition with minor cracking and abrasion. Steel HD bolts are in poor condition. Debris and vegetation build up generally should be cleared to ensure stability.
 - Steel tie-rods restraining abutment piles to the dead man anchor at abutment A are heavily corroded and condition within the abutment soil block is unknown.
 Overall Condition State: 3
- Substructure Piers and Abutments
 - Timber columns at piers are of original construction and are over 80 years of age. Historic boring records (2005) indicated some elements had severe internal piping and rot. The external surface is deteriorating with splitting evident at notch locations, and these elements are not inspect-able and likely to have high moisture content and more susceptible to rot or insect attack where confined within timber-sheathed sections.

- Wood Research and Development Pty Ltd completed non-destructive condition 0 inspection of the timber piers in 2019 utilising stress wave techniques (ref: D2019/256593). This report recommended glass fibre jacketing and epoxy filling of timber piles at deteriorated locations.
- Timber capwales, sill beams and bracing elements at piers are all in very poor 0 condition generally with splitting and insect attack evident, and shrinkage resulting in loss of support at connections to the timber piles.
- Timber sheathing remains in place below deck level although all timbers are 0 deteriorating, and up to 10% are either missing, loose, or severely damaged.
- Cast steel bearings at piers and abutments supporting the steel suspension 0 system are intact although surface corrosion is evident especially where in contact with anchor bolts. Hold down bolts, tang bolts and supporting timber elements are loose generally and in very poor condition.
- Timber gravel boarding at abutments appears to be in fair condition although a small number of elements have significant damage or rot.
- Timber capwale at abutment A in poor condition and deteriorating. 0
- Timber piles at piers are in poor condition generally. 0
- Overall condition Rating: 4 0
- Superstructure Suspension system
 - Steel cables have been visually inspected by Noble in 2016 and were found to be 0 straight generally and to be in good serviceable condition. Sections enclosed within node blocks are liable to have local corrosion, nicking, or damage of the wires due to moisture ingress and debris built up. These cables are not inspectable without removal of the node block plates.
 - Steel cable white metal sockets were inspected by Noble in 2016. No movement 0 of the core was detected indicating security of the sockets was good.
 - Abutment anchor bolts are in poor condition with pitting corrosion reducing 0 section by up to 10%, with protective sheathing lost generally. These were tested by Noble in 2016 and found to be continuous without internal defects.
 - Steel anchor U-bolts are in fair condition with surface corrosion evident. 0
 - Steel node blocks were in very poor condition generally. Steel node plates have 0 severe pitting corrosion, clamp bolts are severely corroded with multiple bolts with section loss exceeding 50%, and debris and bitumen built-up trapping moisture and accelerating corrosion of these elements. These elements are priority items in the 2020 rehabilitation works.
 - Steel hanger rods are in good to fair condition being replaced in 1990. Surface 0 protection is starting to deteriorate. Defects were found in two (2) hanger rods in 2016 testing by Noble. These defective elements were replaced in 2017. End plates supporting the cross girders are in poor condition with severe corrosion generally. These elements are priority items in the 2020 rehabilitation works.
 - 0 Steel bracing members have a moderate level of surface corrosion generally, and paint protection system is lost throughout and in poor condition. Overall Condition State: 4
 - 0
- Deck
 - Timber ply decking was installed in 1990 and is approaching 30 years of age. 0 Deterioration and pot holing of the wearing surface and subsequent maintenance of the wearing surface has resulted in continuous damage to the top surface of these elements reducing overall thickness and encouraging moisture to sit on the surface of the timber. Visual inspection of the underside of these elements indicates timber has increased moisture levels. Given the age and condition of these elements, they would be considered to be in poor condition.
 - PMHC completed timber coring inspection and moisture content testing of the ply 0 decking in 2020. The outcomes of this testing indicated a high level of moisture ingress into the ply decking.

- Timber stringers and bracing were replaced in 1990 due to deterioration from timber spiking. The existing stringers were inspected in 2020 and found to be in good to fair condition with minimal damage. Steel splice plates were supported with steel brackets where found to be excessively corroded, and remaining steel work was painted for protection.
- Timber cross girders were replaced in 1990. The existing cross girders are in fair condition although the ends of these members are starting to deteriorate and split at hanger connection locations.
- Overall Condition Rating: 4
- Kerb and Barriers
 - Timber kerb was replaced in 1990 and is generally in poor condition with paint protection lost throughout.
 - Timber handrails were of original construction and are in poor condition generally with paint protection lost throughout. Connection to kerb and stringers were renewed in 1990. These elements are priority items in the 2020 rehabilitation works.
 - Timber end posts were in poor condition generally with splitting and damage due to vehicular impacts evident. These elements are priority items in the 2020 rehabilitation works.
 - Steel w-beam approach barriers are in fair to poor condition with vehicular impact evident.
 - Overall Condition Rating: 3

Load capacity and design

The existing bridge is currently load limited to 10 tonnes based on the original design load not considering reduced capacity based on condition:

 "10-ton gross vehicle, seven tons on rear axle, three tons on front axle, 11 ft. wheel base with 25 deg impact for deck and stringers, 20 deg for cross girders and 15 deg for cables, or a distributed load of 60 lb per square foot with no impact" (Hasting River at Kindee Crossing, N.S.W, Transactions of the Institution, Vol XVIII, December 1937)

Based on this load limit, it is considered that overload could occur with legal general access vehicles, and there are limited methods of control or regulation. Structural analysis and load rating of the existing structure has not been completed to date.

Based on the original design methodology documented in the *Transactions of the Institute Volume XVIII (December 1937),* the design considerations and safety factors assumed to have been adopted in the design of the key structural elements are as follows:

- Cables
 - Modulus of Elasticity 12,000,000 lb per sq. in.
 - Load reversal (compression in cables) Design Vehicle with 50% margin
 Governing loads
 - Side Span top chord Full distributed load on main span
 - Side Span bottom chord Full distributed load on main span
 - Main span top chord Distributed load on rather more than half main span
 - Main span bottom chord Concentrated load at point concerned.
 - o Side span top chord Heaviest stressed cable in the structure
 - Ultimate strength using factor of safety of 3.5
 - U-bolts for sockets designed on core area of threaded section ble details
- Cable details

.

- Testing of actual cable with socket dimensions and construction methodology defined.
- Hangers

- Permissible stress of 18,000 lb per sq. in
- Pin connection designed for full capacity of the member.
- Diagonals
 - Designed as pin ended struts
 - Span to radius of gyration ratio = 120 (governed in all cases)
 - Permissible stress of 18,000 lb per sq. in
 - o Pin connections designed for one and one-half the calculated load
 - Permissible stress in bending of 27,000 lb per sq. in.
- Node blocks
 - Clamping friction on cable by tension bolts
- Anchor Bolts
 - Designed on core area
- Anchor Blocks
 - Factor of Safety against sliding of 1.5
 - Coefficient of friction on base 0.33
 - Ground reaction to fall in middle third of block (prevents overturning instability)
 - Permissible passive earth pressure = 21.4 kips per ft. width
 - Maximum slope limited to 45 degrees for concrete (constructability and to minimise tension developing)
 - Distance behind abutment sufficient to develop passive resistance with internal angle of friction >= 3.0
- Piers
 - Vertical reaction from main span, side span and vertical reaction from supporting tension, balancing H in the main span.
 - o Reaction is greatest with both the main and side span loaded.

Design commentary

The following commentary has been extracted from key sections of the article "Three Span Light Suspension Bridge over Hastings River at Kindee Crossing, NSW" in the Transactions of the Institute Volume XVIII (December 1937).

<u>Truss layout</u> – The economical ratio of centre span to side span was found to be about 2.5 to 2.7. The "rise" of the truss is governed by economy and appearance, finding the ratio of rise to span should be reduced slightly for longer spans, and increased slightly for heavier loading.

<u>Cables</u> – The cables are incapable of carrying any compression stress and it is necessary to arrange the truss layout so that no cable member can be thrown into compression by any possible combination of loads. In the present design, the only possible combination is live load and impact plus dead load. A single load slightly greater that the load producing reversal may cause total failure of the structure. For this reason, a margin of 50% against collapse is provided i.e., the cables should not be thrown into compression until the live load and impact are increased 50% or, as a more convenient method of calculation, the combination of full live load and impact with two-thirds of the dead load should not produce compression (i.e. reduced dead load as equivalent affect).

<u>Cables</u> – The cables have usually to be specially made and it is advisable to keep the same size cable throughout; if, therefore, on determining the stresses in the cables, it be found that one cable is particularly heavily stressed on such a layout, it will be economical to vary the lay-out to reduce this stress and permit the adoption of a smaller cable throughout. In the present design, it was found that the stress in the upper side cable was much greater than all other if all trusses were made of minimum depth. To reduce this stress the upper cable was raised as much as possible, i.e. To a straight line between centres of bearings.

<u>Cable details (White metal sockets)</u> – The only parts in which trouble is likely to arise in construction are the cable attachments. These details are not usually employed in this country and careful supervision and direction is necessary. The Department of Main Roads has found that the form and dimensions of the socket given is the minimum required to ensure proper socketing of ropes for the heavy direct stress imposed in suspension bridge work, smaller sockets are used generally for lift and haulage work where, owing to being over sheaves, etc.., a much smaller direct tension is carried by the rope. The parallel portion of the socket is advisable to ensure that the rope is concentric and co-axial with the socket.

For the same reason it is necessary to carry out the socketing operation with greater care than is usually given to ordinary lift and haulage ropes. The rope should be seized for the length of the parallel portion of socket and the seized rope should be a close fit in this portion. For the coned part, the individual wires should be carefully and thoroughly separated and spread uniformly to fill the cone, thoroughly cleaned and washed using petrol to remove all grease and dirt and the rope drawn tight into the socket. The socket should then be heated with blowlamps to allow the white metal to run, the parts in contact with white metal sprinkled with resin to serve as a flux and a suitable hard white metal with a low melting point melted and poured in to fill the coned portion.

The proportions and the method of testing were adopted after tests of sockets on the actual rope used had been carried out at the Engineering School of the University of Sydney.

<u>Tie rods</u> – The tension in the tie rods and the force on the anchor blocks (since no horizontal force is taken by the abutments or the pier) is the force necessary to resist the horizontal reaction of the main span.

<u>Anchor bolts</u> – The anchor bolts have also to resist the horizontal tension from loads on the main span. Since the side span acts as a simply supported truss it produces no horizontal force at the abutment support. The anchor bolts are again threaded bars designed on core area. They are protected against corrosion below ground by enclosing in large diameter galvanised pipes, filled with concrete, which extend into the concrete anchor blocks in which the lower ends of the bars are embedded.

<u>Hangers</u> – All hangers are round mild steel bars with threaded ends and are design on a permissible stress of 18,000 lb per square in on the core area of the threaded portion.

<u>Bearing Pins</u> – The bearing pins are ordinary mild steel pins grooved for bolts etc. and are designed for bending.

<u>Diagonals</u> – Diagonals are designed as pin ended struts with maximum I/r ratio = 120 and permissible stress = 18,000 lb per sq. in (80 l/r). In the present design, the former condition always governed, and stresses were negligible. A star section of two angles was found to be most economical. The prevent local buckling, the minimum thickness of any leg was made one-twelfth of its width as required by design specifications for steel bridges.

<u>Node blocks</u> – The stress increments are transferred to the cable by means of friction, special clamps being used to grip the cable without causing deformation. Care should be exercised in the manufacture of the clamps at intermediate panel points to ensure that sufficient clearance is left between the two halves of the clamp to prevent the halves coming into contact before the rope is properly gripped, and in their erection to ensure that the tightening bolts are fully drawn up to develop the necessary friction.

<u>Piers</u> – The horizontal reaction at the pier is zero and the piers must be designed and constructed so as to take no horizontal force.

<u>Abutments</u> – The tie rods beyond the abutments are placed in the line of the upper chord and the abutment also is relatively flexible and takes no horizontal force. Therefore, vertical reaction from trusses on abutment is the vertical reaction for a truss of span 1.

<u>Deck, piers and abutments</u> – The deck piers and abutments are of ordinary timber construction designed for light load and require no description.

<u>Anchor blocks</u> – The anchor blocks are designed to resist the horizontal forces from cable tension by the passive earth pressure developed on the front face and friction on the base. As is usual in such structures a factor of safety of 1.5 against sliding is provided by increasing the tension in the anchor bolts by 50% for calculation purposes. It is also necessary that the block should be placed so far back from the abutment that the friction between the earth in front of the block is sufficient to develop the passive resistance. For this calculation, the earth is assumed to slide as a horizontal plan at the level of the base of the block, assuming the earth moves is a width in plan equal to the width of the block at the face of the block. Thence, spreading out on each side at an angle of 45 degrees with the direction of movement until the face of the embankment is reached. The ratio of horizontal resistance required to weight of earth moved should not exceed the coefficient of internal friction of earth fill (i.e., ≥ 3).

<u>Lateral performance</u> – Wind loading considered in original design and due to small exposed area to wind, and the constant heavy tension in the cables it was found lateral bracing of the suspension system was not required.

<u>Flooding</u> – To prevent drift lodging in the spaces between the piles, the piers were sheathed with timber to flood level.

<u>Temperature</u> – No expansion joints are provided in the timber deck. The truss system for the centre span consists of an inverted three-hinge arch, and is, therefore, able to be expand and contract freely with temperature, while the side spans are equivalent to simple trusses. The effect of temperature changes in the truss system will be to produce horizontal movements of the pier tops and vertical movements at the centre of the main span and the structure has ample flexibility for these movements.

<u>Structural performance</u> – The centre span truss is an inverted three hinge arch and is, therefore, statically determinate. The stresses in the centre span truss are unaffected by loads on the side spans. The side span truss carrier out two functions; first it acts as a simply supported truss for loads on the span itself, and secondly, it acts as a back stay to support the horizontal tension in the centre span truss due to loads on the centre span. The piers are too flexible to take any appreciable horizontal forces. It is not stressed by loads on the other side span.

Heritage status

Burra Charter

The Burra Charter defines the basic principles and procedures to be followed in the conservation of Australian heritage places. In 1979, the Australia ICOMOS Charter for the Conservation of Places of Cultural Significance was adopted at a meeting of Australia ICOMOS (International Council on Monuments and Sites) at the historic mining town of Burra, South Australia. It was given the short title of The Burra Charter.

The Charter accepted the philosophy and concepts of the ICOMOS Venice Charter but wrote them in a form that would be practical and useful in Australia. The Charter was revised in 1999 and has since been adopted by the Australian Heritage Council (December 2004), the Heritage Council of New South Wales (December 2004), the Queensland Heritage Council (January 2005) and the Heritage Council of Victoria (July 2010). It is also recommended by the Heritage Council of Western Australia and the Tasmanian Heritage Council.

Under the Burra Charter, people involved in the conservation of heritage places should:

- understand the place and its cultural significance, including its meaning to people, before making decisions about its future
- · involve the communities associated with the place
- care for its cultural significance and other significant attributes, taking account of all aspects of significance
- care for the place's setting
- provide an appropriate use
- provide security for the place
- use available expertise
- make records of the place and changes to it, and the reasons for these decisions
- interpret and present the place in a way appropriate to its significance.

Heritage listing

The existing bridge is listed in the Port Macquarie Hastings Council Local Environment Plan 2011 Schedule 5 Environmental heritage:

- Item No: I011
- Suburb: Kindee-Long Flat
- Item name: Kindee Road suspension bridge (part)
- Address: Kindee Road, over Hastings River, between Kindee and Long Flat
- Significance: Local

It is also listed in the *North Coast Regional Environment Plan 1988* in Schedule 2 Heritage items of State and regional environmental significance (repealed in 2008) as follows:

 Ellenborough – Kindee suspension bridge, Kindee Road off Oxley Highway (Gazetted in 1994).

State Heritage Inventory

The following is an extract from the State Heritage Inventory Public Presentation Report 1990, regarding the physical appearance of the existing bridge:

"Kindee Bridge is a three-span suspension bridge on the road to Kindee over the Hastings River at Kindee Crossing. Because the Hastings River at the site flows in a narrow deep gully with steep banks, and because flood levels of 13m (43ft) have been recorded, and no through traffic was contemplated, a high-level light single lane bridge was considered most economical.

The bridge is of trussed cable type and has three spans of 27m, 67m and 27m respectively. The deck is supported throughout by hangers attached to trussed cables. The cables are plough steel ropes and are anchored to concrete blocks buried in the approach embankment. Gussets are clamped to the cables and diagonals and hangers are secured to the gussets by pin joints. All diagonal are compression members of two angles riveted together to form a star section. Hangers are tension members of rod mild steel.

The deck is of timber consisting of transverse planking and longitudinal timber stringers carried on timber cross girders supported by the hangars. Timber wind bracing is provided in the plane of the deck. Piers 24 m high consist of round timber piles forming a braced trestle on concrete foundations. To prevent debris lodging in the spaces between the piles, the piers are sheathed with timber to flood level.

The bridge still carries its design load limit of 10 tons and is in a good state of preservation".

The following is an extract from the State Heritage Inventory Public Presentation Report 1990, regarding the historical notes of the existing bridge (National Trust):

"The bridge was a novel design by V. Karmalsky and A.T Britton of the NSW Department of Main Roads. Steel work was supplied by Morison and Bearby of Newcastle and timber by R. McMillan of Long Flat via Wauchope. The contractor for the construction, R.B Haydon, was killed in an accident at the site in April 1936 and the work was subsequently completed by the DMR. The bridge was officially opened on 19th December 1936 and the total cost including approaches was approx. \$10,000."

The following is an extract from the State Heritage Inventory Public Presentation Report 1990, regarding the statement of significance of the existing bridge:

"An extremely rare and unusual form of bridge construction, being one of a small number of suspension bridges in the State (Northbridge, Hampden Bridge at Kangaroo Valley). Providing a spectacular and rare example of extensive use of timber in engineering.

Provides physical evidence for the study of bridge and road engineering. In excellent state of repairs."

Heritage management

Managing Significance

Many engineering works or objects are rare and represent the only surviving remnants of an earlier way of life. They are the physical evidence of history. Their retention is necessary for the education of present and future generations. For example, very few gas works or remnants of the coal gas industry survive today although they represent what was once the prime source of energy in the community.

Cost and impracticalities will normally prevent the preservation of these items in their original condition and format. Some alteration to the fabric will usually be necessary to allow function for modern usage. Such change is acceptable provided the significance of the work is maintained.

The Statement of Significance will have defined the elements that make the work significant. Conservation will build on this definition to provide guidance on the level of significant and the degree of interference that can be tolerated on these elements.

Retention of a heritage item without a plan to manage its future, and especially manage the funding for its future, is simply extending the time to its demolition. Such lack of action can even lead to all knowledge of the place being lost because adequate recording is not carried out.

A Conservation Management Plan (CMP) consists of three main parts: investigation, assessment and management. Investigation and assessment will have been completed if the conservation process has been followed. The Conservation Management Plan, as its name implies, provides the framework for management – alteration, adaptation, and prevention from deterioration or, in a worst case, conservation by media only in the case of demolition.

A CMP is intended to provide guidance to those who will work on or conserve the value of the heritage item in the future. The preparation of a CMP requires a certain discipline to ensure that that all facets of future involvement are delineated and controlled, and that no elements of significance can be accidentally destroyed. It must develop a policy for the future.

It should include recommendations for:

- · Physical conservation action and care necessary for the retention of significance
- Uses that are both feasible and compatible with the above
- Maintenance
- · Public access and interpretation (where appropriate)
- · Security, and the control measures to reduce risk of loss or damage
- Controls on future development and change
- · The mechanism for the adoption of the policy and for subsequent review
- · Archiving of historical documents related to the item
- · Sources of funding into the future
- · Retention of significance and memory if the item is totally destroyed or lost

A Statement of Heritage Impact (SOHI) will normally be required to accompany any approval by statutory bodies. It should be succinct and should not repeat the detail in the Conservation Management Plan. Pertinent documents, such as physical condition reports, can be attached. Evidence must be included as to why alternative solutions are not viable.

Statutory requirements

The Environmental Planning and Assessment Act 1979 (EP&A Act) is the prevailing planning legislation that applies to all development and environmental assessment within NSW.

SEPP (Infrastructure) 2007 applies as the works relate to road and road infrastructure facilities (Division 17). Depending on the nature of the work (i.e. emergency works, routine maintenance, minor repairs, major repairs, remediation, structural strengthening, reconstruction, or demolition and replacement), the works may be considered:

- Exempt development (cl97)
 - In accordance with Schedule 1 and;
 - If it is likely to affect a State or local heritage item or a heritage conservation area, must involve no more that minimal impact on the heritage significance of the item or area (cl 20 2e)
 - Must not involve the demolition of a building or work that is, or is part of, a State or local heritage item (cl 20 2e1)
 - Emergency works to protect a road or road infrastructure facility, the environment or the public, but only if they involve no greater disturbance to soil or vegetation than necessary (c97 1e)
- Complying development (cl97A) Not applicable
- Development permitted without consent (cl94) Construction works (whether or not in a heritage conservation area), emergency or routine maintenance works, alterations or additions to an existing road, or environmental management works (cl94 2a)
- Development permitted with consent (cl96) Not applicable

All works on the Kindee Bridge other than emergency works, comprise "development for the purpose of a road or road infrastructure facilities" and would be "carried out by or on behalf of a public authority", and would be considered to be development permitted without consent through provisions of SEPP Infrastructure Clause 94 2 (c). The proposal would be assessed under Part 5 of the EP&A Act with Port Macquarie Hastings Council the determining authority.

The Conservation Management Plan for the Kindee Bridge details maintenance activities, methodologies and outcomes and the associated level of heritage impacts that may allow work to progress, streamlining the approval process, provided the works involve minimal impact on the heritage significance of the item.

Written notice of the works including the Statement of Heritage Impacts is required to be forwarded to council, and any response received within 21 days of the notice be considered.

Conservation Management Plan

Port Macquarie-Hastings Council engaged Taylor Thomson Whitting (NSW) Pty Ltd to complete a heritage assessment and prepare a *Conservation Management Plan (ref: D2019/282712)* for the Kindee Suspension Bridge in 2019.

The heritage assessment found the bridge to have exceptional significance due to its; rarity, being only one of two remaining bridges of this type in the world; the materials of construction, utilising local hardwood timber and composite steel construction; the design, being unique to this region of the world showing creativity and ingenuity; and its economic and social importance to the local community providing access over the Hastings River to the Kindee Valley. Elements of the bridge were also identified as having high or exceptional significance including the cable suspension system and steel diagonals, the timber hand railing, the timber piers, and the abutments.

The Conservation Management Plan included the investigation phase reviewing locality, site context, history of the site, design, physical evidence and condition of the structure. The assessment phase included comparative analysis of similar structures and assessment of the significance of the structure as a whole and of its parts in accordance with the Burra Charter. This included preparation of the Statement of Significance for the Kindee Bridge. The management phase identified constraints and opportunities, conservation policies, and a schedule of conservation works related to all key elements of the structure.

Statement of significance

The following statement of significance was prepared for the *Kindee Bridge Conservation Management Plan (ref: D2019/282712).*

"The Kindee Bridge is an extremely rare example of a timber trussed cable bridge, which at the time of construction in 1936 provided a critical link between communities within the Kindee Region.

Kindee Bridge is the only known remaining original timber trussed cable bridges in the world, other than Allan Bridge at Segenhoe, which was reconstructed downstream from its original location in 1955. Only three timber trussed cable bridges are known to have been constructed, with only one other (reconstructed) bridge now remaining, at Segenhoe NSW.

The use of steel angles as truss members within the cable suspension structural system is unique to Kindee Bridge and the Allan Bridge at Segenhoe NSW. The locally sourced timber piers are of high significance because they identify as a key characteristic of the regions selfsufficiency. The design of the bridge reflects an economic approach to allowing passage for farmers and their produce to pass from Kindee to Wauchope in the 1930's, ensuring its high economic value to the community. The Kindee Bridge is the highest and longest known timber

trussed cable bridge in existence. The bridge was a novel design when built, however similar structures are understood to have been built in France.

Historical bridges are a vital part of Australian heritage, as they tell the story of colonial movement and progression. The Kindee Bridge demonstrates the high durability of local timber and, in tangible terms, what was possible at the time in terms of the length of span possible using timber and light steel composite structures."

Strategic Options - Constraints

Study area

The study area has been defined as approximately 10 km upstream and downstream of the existing bridge site.

Relevant legislation

State Environmental Planning Policies

State Environment Planning Policy (Infrastructure) 2007 State Environment Planning Policy No. 44 (Koala Habitat Protection)

Local Environment Plans Port Macquarie-Hastings Local Environmental Plan 2011

NSW legislation

Environmental Planning and Assessment Act 1979 Local Government Act 1993 Roads Act 1993 Biodiversity Conservation Act 2016 (BC Act) National Parks and Wildlife Act 1974 (NPW Act) Biosecurity Act 2015 Fisheries Management Act 1994 (FM Act) Water Management Act 2000 Heritage Act 1977 Crown Lands Management Act 2016 Protection of the Environment Operations Act 1997

Commonwealth legislation

Environment Protection and Biodiversity Conservation Act 1999 Native Title Act 1993

Traffic and access

Original design intent

"The road is purely a spur road of developmental character serving a number of dairy farms, etc., in a fertile side valley surrounded by impassable mountains and there is no possibility of through traffic developing."

Road network

The Kindee Bridge provides access to the northern side of the Hastings River from the Oxley Highway between Ellenborough and Long Flat in the Upper Hastings River Valley. Other bridges crossing the Hastings River include Double Crossing Bridge at Yarras on the Forbes River Road, and Mt Seaview Road Bridge near Mt Seaview located 25 km and 40 km upstream of Kindee respectively. Bain Bridge that crosses the Hastings River between Wauchope and Beechwood at Camerons Falls is located near Beechwood 48 km downstream of Kindee.

The roads in the vicinity of the existing crossing include:

 the Oxley Highway, which is an east-west state road linking Port Macquarie to the northern tablelands near Walcha.

- Kindee Road, which is a local sealed road linking the Oxley Highway to the Kindee Valley
 crossing the Hastings River at Kindee Bridge
- Pipeclay Road that is a local unsealed collector road linking Kindee Road to Beechwood Road west of Wauchope and runs approximately parallel and on the northern side of the Hastings River.
- Rocky Falls Road is a local unsealed 4WD track providing private property access to Kindee Road near the western abutment of the Kindee Bridge.
- Boat Harbour Lane is a local unsealed access road providing private property access to the Oxley Highway, approximately 2 km west of Long Flat.
- Churchills Road is a local unsealed access road providing private property access to the Oxley Highway, approximately 5 km east of Long Flat.
- Boytrang Road is a local unsealed access road providing private property access to Pipeclay Road approximately 10 km east of Kindee Road.

Traffic surveys

Traffic surveys were completed in 2016 and 2017 in the local government area, and included one station on the eastern approach to the existing bridge on Kindee Road, and one station located at the extent of the sealed section of Pipeclay Road approximately 4 km west of the intersection with Pappinbarra Road at Beechwood.

Results from these traffic surveys are included in the following table.

Table 1 - Traffic Counts

Location	AADT	%HV	AM Peak	PM Peak
Kindee Road	160	9.2%	7.00am - 9.00am	4.00pm - 6.00pm
Pipeclay Road	96	13.0%	7.00am - 9.00am	4.00pm - 6.00pm

It is worth noting that only one (1) long heavy vehicle (6-axle semi-trailer) was recorded using Pipeclay Road during the monitoring period, with the majority being two, three or four axle rigid heavy vehicles making up the remainder of the heavy vehicle loads. All heavy vehicles using Kindee Road during the monitoring period were two or three axle rigid heavy vehicles, and one occurrence of a long 3-axle articulated truck.

It is anticipated that Forestry NSW may intermittently use Pipeclay Road to transport timber out of Mount Boss State Forest, although more likely to utilise Hasting Valley Way / Bellangry Road / Bottlebrush Road to the east, or Forbes River Road to the west.

Road safety

Limited crash data exists on the local road network in the vicinity of Kindee Bridge, with most crashes recorded located on the Oxley Highway.

It is noted that in September 2018, a vehicle crashed on the deck of Kindee Bridge on the eastern side span damaging a structural brace in the suspension system. This was repaired and re-opened to traffic on 28 September 2018. It is likely that non-serious incidents or near misses occur at the bridge on a relatively regular basis due to the steep grades, tight road geometry, narrow bridge deck, and single lane nature of the crossing. This is evident based on the damage to the existing steel barriers and impacts at end posts to the bridge.

Public transport – School buses

Busways operate a school bus service No. 56 mornings and afternoons during school terms between 1500 Pipeclay Road located approximately 15 km east of Kindee, towards Wauchope utilising Pipeclay Road. This service does not directly service the Kindee area.

The Kindee Bus Service runs a small school bus to take local children from the Kindee area over the Kindee Bridge, and transfers to a Busways service on the Oxley Highway. The bus utilised is a Toyota Hiace with a licence to a carry maximum of 13 passengers. Busways bus service No. 54 travels between Forbes River Road (Yarras Mill) and Wauchope on the Oxley Highway including transfer to and from the Kindee Bus Service at Kindee Road.

Emergency services

NSW Police and ambulance services may need to access the Kindee area in emergencies and may be restricted in taking some vehicles over the Kindee Bridge due to load limits on the existing bridge resulting in delays attending to these emergencies.

NSW Rural Fire Service provide fire and emergency services to approximately 95 percent of NSW and responsibilities are set out under the Rural Fires Act 1997. Fires in the Kindee Area would be attended by units out of the Port Macquarie-Hastings Mid Coast office located in Wauchope, NSW. Access would be via Pipeclay Road only due to the load limit on the existing Kindee Bridge.

Roads liable to flooding

The Hastings River Flood Plan has identified flood levels resulting in road closures relative to the Kindee Bridge Gauge. This indicates that the Oxley Highway is closed to through traffic during flooding over and above 8.7 m on the Kindee Bridge Gauge. An alternative route remains open over the Kindee Bridge via Pipeclay Road for light traffic only, although it would not be recommended to advertise this route during flooding of this level due to the sensitivity of the structure under combined flood and traffic loads.

Table 2 - Hastings River Flood Plan - Road access flood levels

Road	Location	Alternative Access	Road	Flood Level
Oxley	Long Flat	No		8.71m on Kindee
Highway				Bridge gauge
Kindee	Kindee	Impacts access east of	Yes – Pipeclay Road	14 metres on the
Road	Bridge	Kindee		Kindee Bridge gauge

Flooding

Background and History

The Hastings River drains a 3700 km² catchment that extends west from Port Macquarie on the Mid North Coast of New South Wales. The river rises in the Great Dividing Range near Mount Werrikimbe, about 90 kilometres to the west of the coast. It is about 120 kilometres long and discharges to the Pacific Ocean at Port Macquarie, which is located about 300 kilometres north of Sydney.

"The Hastings River, NSW South Wales, at the site flows in a narrow deep gully with steep high banks; the grade of the stream is steep and the velocity high. Floods are fairly common, deep and fast running and carry large quantities of big drift timber. The greatest rise recorded is about 43 feet, while small rises of a few feet are very frequent." (Three Span Light Suspension Bridge over Hastings River at Kindee Crossing, NSW, Transactions of the Institute, Vol XVIII, p463)

The river has a long history of flooding. Since European settlement of the area in the early 19th Century, major floods have occurred in 1864, 1894, 1929, 1950, 1954, 1963, 1968 and 1978, 1990, 1995, 2001, 2013 and 2021. Numerous other less severe floods have also occurred over this period.

Because of the rugged nature and steepness of the catchment area, floods in the Hastings River are characterised by very rapid rises. Floods occur typically between February and April, which suggest that flooding in the region is influenced by southerly dominated rainfalls system that

occur in late summer and early autumn. Of these, the March 1978 and the January 1968 floods are the most severe to have occurred in the last 70 years.

The Hastings River catchment has a critical storm duration of 72 hours for events of the magnitude of the 100-year recurrence flood. That is, a design storm with a 72-hour duration will generate the highest peak flood discharge at Port Macquarie.

Ellenborough gauging station is located about 50 kilometres upstream of Wauchope at the Kindee Bridge. Approximately 43% of the Hastings River catchment drains to this gauge. The peak 100-year recurrence flood discharge at this gauge is estimated to be 3910 m3/s, compared to a peak 100-year recurrence flood discharge at Bain Bridge of 6850 m3/s.

Table 3 - Ellenborough Gauging Station - Historical Flood Discha
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	HISTORICAL FLOOD PEAK FLOOD DISCHARGE AT ELLENBOROUGH GAUGE (m ³ /s)	ESTIMATED RETURN PERIOD (years)
February 1954	1855	8
April 1963	1727	6
January 1968	4853	89
March 1974	2146	12
March 1978	3446	33
May 1980	1776	12
October 1982	1609	7
July 1985	1579	5
February 1990	2230	15
March 1995	2621	21

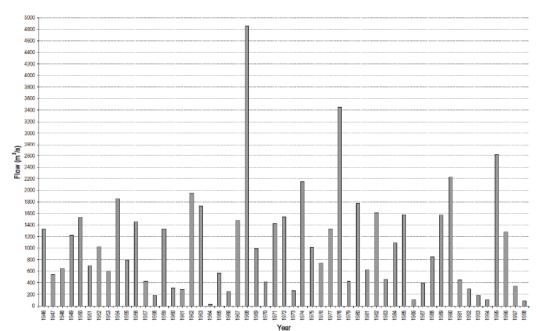


Figure 11 - Ellenborough Gauging Station - Historical Flood Discharge

The January 1968 flood was the highest in the upper river area since 1945, reaching a height of 12.6 metres on the Ellenborough gauge (Kindee), and a 9.1 metres AHD reading at Wauchope Railway Bridge due to the high rainfall recorded in the upper catchments (in excess of 450mm in 48 hours). In the lower river area thousands of hectares of rich dairying land were inundated, homes received extensive damage as well as many crops and fencing being destroyed. The

Dennis Bridge gauge recorded 3.1 metres AHD and Settlement Point gauge 2.15 metres AHD. The lack of storm surge and low tidal conditions at the river mouth allowed the floodwaters to escape freely, thereby preventing higher water levels that would have caused further damage.

The 1963 flood was different in character with lower rainfall being experienced in the upper valley, as indicated by gauge readings at Ellenborough (Kindee) of 7.6 metres and Wauchope Railway Bridge of 7.8 metres AHD. Lower river conditions however were worse, shown by the Dennis Bridge gauge being 3.6 metres AHD and Settlement Point gauge at 2.4 metres AHD, both being the highest recorded since 1945. The main cause of this flood was the coincidence of heavy local runoff due to intense coastal rain, storm surge and winds from cyclone remnants as well as high tides.

The Hastings River Local Flood Plan has defined the predicted flood impact and Flood Classification and associated recurrence intervals at the various flood gauges.

	Flo	od Classifica	tion	PEAK LEVEL* (<i>metres</i>) for each AEP			
	Min	Mod	Maj	5%	1%	PMF	
Bain Bridge	-	-	-	11.1	13.2	19.7	
Kindee	3	5.5	7.6	-	12.57	-	
Wauchope	2.5	4.3	5.5	6.9	8.7	12.4	
Settlement Point	1.2	1.5	1.75	2.3	2.7	6.2	
Rocks Ferry Bridge (us)	-	-	-	6.1	7.4	10.8	
Sandy Point	-	-	-	5.8	7.1	11.0	
Rawdon Island Bridge	-	-	-	3.8	4.8	8.0	
Dennis Bridge	-	-	-	3.0	3.8	7.0	

Table 3 - Hastings River - Flood Classification

Refer Figure 12 for graph showing relationship of flood levels, flood discharge and average recurrence interval.

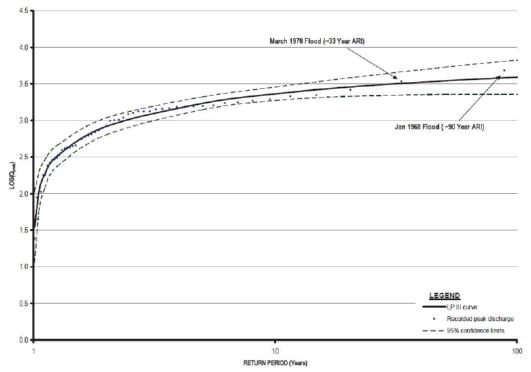


Figure 12 - Hastings River - LOG (Qpeak) versus Average Return Interval

Kindee Bridge flooding

The original design of the Kindee Bridge provided for a design flood level of 144.55 feet (local datum), which was equivalent to 3 feet clearance below deck level at abutments and 6 feet clearance below deck level in the centre of the main span. The design flood level was equivalent to 43 feet rise (13.1m) from normal river levels and stated to be similar in level to the 1929 flood. This flood level adopted in the original design appears to be consistent with the current estimated 100-year recurrence flood level in the Hastings River Flood Study.

Aerial drone survey has been completed to define the existing condition and geometry of the bridge as well as geo-referencing the structure to AHD. The summary of key levels relative to AHD are provided below at the existing bridge location.

Reference Level	ORIGINAL DATUM (')	AHD (m)	Q _{peak} (m³∕s)	Ref:
Abutment A Deck Level	149.5	60.37	-	Drone survey
Centre of Main Span Deck Level	152.5	61.19	-	Drone survey
Abutment B Deck Level	149.5	60.25	-	Drone survey
Normal Water Level – Original Design Levels (1934)	101.38	45.61	-	Calculated
Zero Gauge (Kindee)	-	44.955	-	Water NSW
Minor Flood (3.0m)	-	47.655	185	Water NSW
Moderate Flood (5.5m)	-	50.455	1100	Water NSW
Major Flood (7.6m)	-	52.555	1850	Water NSW
Original Design Flood (43 ft rise)	144.55	58.77	-	Calculated

Table 4 - Kindee Bridge - Reference levels

20-year ARI (~10m)	-	~55.0	2700 - 2900	Estimated
50-year ARI (~11m)	-	~56.0	3800 - 4100	Estimated
100-year ARI (12.57m)	-	57.53	3900 - 5800	Hastings Flood Report

The existing bridge was rehabilitated in the 1990's including replacement of the existing 100 mm depth transverse timber deck planking with 125 mm depth softwood ply deck, with a spray seal water-proofing membrane. Therefore, existing deck levels are estimated to be approximately 35 to 50 mm higher than original as-built levels. The superstructure depth from current wearing surface to the underside of the cross girders (or the lowest structural element) is approximately 650 mm (600 mm from original design).

The resultant freeboard to the 100-year ARI flood event is approximately 3.01 m at the centre of the main span of the bridge, and 2.07 m at each abutment.

Geology and soils

Kindee Bridge is located in the Upper Hastings River valley on the Hastings River between Ellenborough and Long Flat.

Reference to the 1:250,000 Hastings Geological Map shows the site is located on the northsouth geological boundary underlain by formations from the Devonian period in Birdwood Beds (Db) comprising laminated sandstone, mudstone and shale to the west, and the Carboniferous periods in Byabarra Beds (Cb) comprising lithic sandstone, siltstone, tuff, shale and limestone to the east.

Reference to the 1:100,000 Port Macquarie Coastal Quaternary Geology Sheet indicates the Hastings River between Ellenborough and east of Long Flat comprises an incised narrow river flood plain with deposits varying in width along the length. Flatter sections of river near Long Flat comprise wider deposits of Holocene levee and in-channel deposits that can consist of fluvial sand, gravel and silty clays. The steeper sections of the river near Kindee Bridge and immediately to the east of Long Flat comprise Pleistocene terrace that can consist of silty clay, fluvial sands and gravels. Due to the narrow nature of the incised river, flood plain style deposits are narrower where river grades are steeper indicating potentially better geotechnical foundation founding conditions.

From site inspections, there is exposed rock at the base of the river below the Kindee Bridge on which the existing pier foundations have been placed, indicating the river has reached the lower rock strata with minimal deposits. Downstream of the Kindee Bridge, there is a bank of built up gravel and cobbles although rocky outcrops are visible in the river at this location indicating the depth is not excessive, and a wide stretch of river flat gravels at the bend.

The river flats narrow again on approach to Long Flat indicating potentially shallow founding conditions to bedrock. Long Flat area could potentially comprise deep gravel deposits due to the flat nature of the river at this location. Immediately to the east of Long Flat, the river is significantly steeper again, and exposed rock is visible in the riverbanks at this location, with limited extent of gravel deposits.

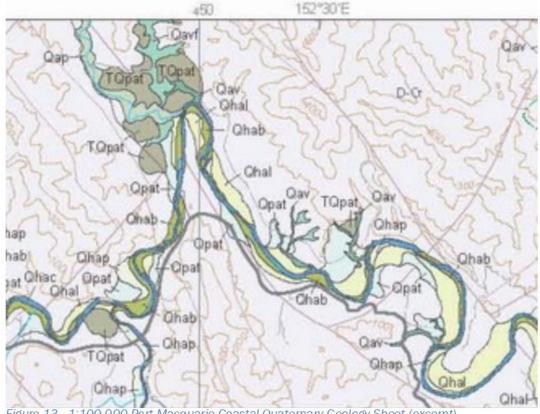


Figure 13 - 1:100,000 Port Macquarie Coastal Quaternary Geology Sheet (excerpt)

Survey

Aerial drone survey was completed at the existing bridge location in September 2018 to capture existing condition records and combine this with a point cloud survey linked to MGA and AHD datum. Alternative crossing zones were also flown and captured to identify issues and constraints associated with these alternative crossings.

Future ground survey will be required once a preferred option has been established to allow conceptual and detailed design to progress.

Socio-economic profile

Main towns and villages

Kindee is in the Port Macquarie-Hastings LGA of which Port Macquarie and Wauchope are the major commercial and urban centres. Other smaller communities in the vicinity of Kindee include Long Flat and Ellenborough. The Oxley Highway is the key state road linking these communities to Wauchope and Port Macquarie as well as to the Northern Tablelands near Walcha.

Characteristics

Results of the 2016 Census found there were 81.202 people in the Port Macquarie - Hastings LGA, of which approximately 0.5 per cent of the population was located in the Kindee / Long Flat and Ellenborough areas. Aboriginal and Torres Strait Islander people make up 4.0 per cent of the Port Macquarie - Hastings LGA.

Following are the key characteristics of the study area.

Table 5 - 2016 Census data

Characteristic	Kindee	Long Flat	Ellenborough	LGA
Population	94	124	194	81,202
Number of Dwellings (Private)	41	61	94	30,668
Average people per household	2.5	2.0	2.6	2.3
Median Age	44	46	45	47.7
Average Weekly Household Income	\$1,375	\$849	\$1,062	\$677
Unemployment rate	-	-	-	6.8%
Aboriginal and Torres Strait Islander Peoples	-	-	-	4.0%

The main employing industry in the Port Macquarie-Hastings LGA is Health care and social assistance.

Potential socio-economic impacts associated with the ongoing management of the Kindee Bridge include:

- Non-aboriginal heritage
- Noise and vibration
- Air quality
- Traffic and access
- · Landscape character and visual amenity
- Land use
- Property acquisition
- Access and amenity
- Economy and employment
- Safety

Land use and property

The following land zoning and objectives detailed in the *Port Macquarie-Hastings Local Environmental Plan 2011* relevant to the Kindee area and surrounds include:

RU1 Primary Production objectives include:

- To encourage sustainable primary industry production by maintaining and enhancing the natural resource base.
- To encourage diversity in primary industry enterprises and systems appropriate for the area.
- To minimise the fragmentation and alienation of resource lands.
- To minimise conflict between land uses within this zone and land uses within adjoining zones.

RU2 Rural Landscape objectives include:

- To encourage sustainable primary industry production by maintaining and enhancing the natural resource base.
- To maintain the rural landscape character of the land.
- To provide for a range of compatible land uses, including extensive agriculture.
- To provide for rural tourism that does not compromise the primary industry capabilities of the land and is based on the rural attributes of the land.
- To secure a future for agriculture in the area by minimising loss of potential agricultural productivity and fragmentation of rural land.

RU5 - Village objectives include:

- To provide for a range of land uses, services and facilities that are associated with a rural village.
- To permit development that is appropriate in scale and type with the characteristics of a rural village.
- To minimise conflict between land uses within the zone and land uses within adjoining zones

The village of Long Flat supports a range of land uses typical of most rural villages of a similar size. Beside residential dwellings, the town supports a public primary school, hotel, community hall, recreation ground, tennis court and general store. The surrounding area is mostly an agricultural region with dairy farming and beef cattle breeding the main industries.

Public utilities identified that would need to be considered in the development of any alternative option include electricity, telecommunications, and water and sewer assets. There have been no gas utilities identified as part of this study.

The alternative routes proposed for the crossing of the Hastings River within this report may affect private property, Crown Land or other land in the form of partial or complete acquisition depending on the option. Acquisition of private land is negotiated under the *Land Acquisition (Just Terms Compensation)* Act 1991 and in accordance with NSW Government Land Acquisition Reform 2016.

Crown land may be impacted by any alternative crossing proposals in the form of the Crown Waterway, Crown Roads and Crown Public Reserves.

Environment

Biodiversity

Aquatic habitat surrounding and within the Hastings River in the vicinity of the alternative options under consideration is classified as Class 1 key fish habitat according to the *NSW Fisheries Habitat Classification Scheme* (Fairfull and Witheridge, 2003). The river is a permanently flowing system with a defined channel; it contains a number of riffle/pool sequences, large woody debris, cobble substrate and a good amount of riparian overhang. The Hastings River at this location is a stream order 8.

Hollow bearing trees have been mapped near the proposed alternative options specifically located on Kindee Road on the eastern approach to the existing bridge, and on Boat Harbour Lane and Pipeclay Road.

The banks of the Ellenborough River at the confluence with the Hastings River are managed public bushland associated with the Ellenborough Public Reserve.

Endangered Ecological Communities may exist along the banks of the Hastings River and its tributaries in the study area including:

- Water Gum Gallery Dry Rainforest
- River Oak Gallery Forest
- Sydney Blue Gum Tallowwood +/- Brush Box White Mahogany Moist Forest
- Riparian Scrub Forest
- Cabbage Gum Coastal Valley Grassy Woodland / Forest

Non-Aboriginal heritage

Heritage items listed in the Port Macquarie-Hastings Local Environment Plan include:

Item 11 – Kindee Road suspension bridge (Part) – Kindee – Long Flat

Item 118 – Ellenborough Cemetery

• Item 119 – Police Station building and associated mature trees (Ellenborough) The heritage aspects of the Kindee Road suspension bridge have been covered separately within this report.

Aboriginal heritage

The Hastings River is the boundary of the Birpai and Bunyah Aboriginal Land Councils with the Birpai Land Council located north of the Hastings River, and the Bunyah Aboriginal Land Council located south of the Hastings River.

A search of the Aboriginal Heritage Information Management System (AHIMS) was conducted on 29 October 2018 (items or places) recorded in or near the area and did not identify any declared Aboriginal places in or near the area.

Constructability

The construction of any alternative crossings of the Hastings River including road and bridge construction would require use of existing tracks and construction of new access tracks, establishment of laydown areas, delivery of oversize elements, crane pads, and river access. Due to the difficult terrain, construction methodologies and access arrangements are key considerations for any future options.

Construction would be required to be staged to minimise environmental, traffic impacts and impacts on private property. Flooding during construction is anticipated to be a high risk and impacts of flooding on construction methodologies would need to be considered for any preferred option.

Strategic options - Design and assessment criteria

Standards and references

The standards relevant to the strategic options assessment, concept and detail design development include, but are not limited to:

- AS5100 Bridge Design (Full Suite)
- Other relevant Australian Standards
- Austroads Guide to Road Design

Design life

The new bridge would be designed for a 100-year design life in accordance with the requirements of AS5100.

Design criteria

The conceptual road works component of the project associated with the strategic route options development has been designed in accordance with Austroads - Guide to Road Design. The engineering design parameters for the road works component are summarised in below.

The road across the replacement bridge (and the bridge approach roads) would meet the design criteria for a 60 kilometre per hour speed limit, although it is anticipated that vertical grades, and horizontal alignment may not achieve this requirement at all locations due to steep and difficult terrain, requiring advisory signage.

Design Criteria - Road	Value
Design Speed	60 km/h
Curve radius	150 metres desirable 100 metres permissible 75 metres minimum <75 metres to be signed
Crest "K"	
Sag "K"	
Grade	Maximum 10.0 %
SSD Reaction time	1.5 sec
Traffic lane width	3.5 metres
Road shoulder width	1 metre
Verge width	0.5 metres
Cross fall/superelevation Maximum	3.0 %
Earthworks	1 in 4 batters desirable 1 in 2 batters maximum

Table 5 - Road Design Criteria

The alternative route options and the associated bridge structures will be designed in accordance with the Australian Standard for Bridge Design (AS 5100). The design will meet the design criteria for Bridge Classification Type II, earthquake design category BEDC-1, and the additional engineering criteria presented below.

One of the key criteria for assessing the alternative route options was the flood immunity level achieved at the river crossing. The existing bridge currently provides flood immunity for all flood events up to the 1 in 100-year flood level, although with a reduced load carrying capacity. The alternative routes have considered low level crossings that will be inundated on more a regular basis, mid-level crossings that may be inundated in major flood events, or high-level crossings designed to be above the 1 in 100-year flood level. The proposed replacement bridge would also be designed to withstand floods up to the 1 in 2000-year event without collapse or loss of structural integrity.

Table 6 - Bridge Design Criteria	
Design Criteria - Bridge	Value
Horizontal alignment	Straight
Vertical grade	Maximum 2.5 %
Cross fall	3 %
Overall bridge width	5.5 metres
Road carriageway width	4.5 metres
Number of lanes on bridge	1
Lane width	3.5 metres with 0.5 metres wide shoulders
Foot Path / Shared Path	Not required
Traffic loading	SM1600
Flood immunity – High Level	1 in 100-year flood (High level)
Flood immunity – Mid Level	Major Flood (Hastings Flood Plan)
Flood immunity – Low Level	No requirement
ULS Design Flood	1 in 2000-year flood
Earthquake design category	BEDC-1

It is noted that the options assessment has been completed based on a single lane bridge crossing to match the current arrangement and level of service, and the multi-criteria assessment has been completed on this basis.

Option identification – Existing Bridge

Description of management options for existing bridge

The following management options have been identified based on utilising the existing bridge and road as the primary crossing of the Hastings River for this locality.

Option 1 – Do nothing

The 'do nothing' option involves minimal maintenance, and subsequent load rating and closure as condition and safety warrants. Closure would lead to escalating deterioration, and ultimately will require demolition due to safety concerns. The period for this may be in the order of 2 to 5 years based on current condition of the key structural elements.

No flood free alternative crossing of the Hastings River exists in the local area, resulting in long detour via Pipeclay Road via Beechwood to Wauchope. Heritage aspects of the bridge would ultimately be lost, and appropriate interpretation should be considered.

Option 2 - Minor structural repairs

This option involves completing key structural repairs feasible within cost and budget, and considering safety of road users and workers. Limited work on timber piers, cabling system, braces or timber deck structure is possible under this option, due to cost of access and safety requirements. Key items include; remove damaged sheathing and protect anchor bolts at abutments; replace clamp bolts; replace hanger base plates, and timber inspection and replacement at abutments as required. Minor painting over-coating at damaged sections may be considered under this option.

Risk remains that deterioration will continue and failure of key structural elements (timber piers / node blocks, timber deck, cabling system) may occur. The period for this may be in the order of 5 to 10 years or sooner. Structural failure may result in closure and ultimately demolition of the structure due to safety concerns.

No flood free alternative crossing of the Hastings River exists in the local area, resulting in long detour via Pipeclay Road via Beechwood to Wauchope. Heritage aspects of the bridge would ultimately be lost, and appropriate interpretation should be considered.

Option 3 - Full remediation of existing bridge

This option involves rehabilitation, reconstruction and protection of the existing structure such that it is capable of taking original design loads. If completed in full, it would be expected that this would provide in the order of 20 to 30 years additional life to the structure. Based on access, temporary works, environmental constraints and safety requirements this is expected to be a significant undertaking.

Works anticipated would include:

- Abutment anchor bolts reconditioning and protection including sacrificial cathodic protection (SCP);
- Timber inspection and renewal at abutments as required
- · Reconditioning and protection of steel bearings at abutments and piers;
- · Reconditioning and protection of all node points, hangers and diagonals;
- Re-protection of cables at node points and installation of SCP, and confirmation of white metal socket condition and capacity;
- Reconstruction or strengthening of timber piers with new materials to reinstate pier condition including monitoring and investigation of existing concrete foundations;
- Full grit blast and paint protection of all steel elements including removal of existing systems (red lead potentially);

- Replacement of timber ply decking including water proofing wearing course and drainage provisions
- · Inspection and replacement of timber stringers and cross girders as required.

Monitoring would include:

- · Strain gauges to confirm dead man anchor block stability
- Load points to confirm actual loads (overload) and behaviour of bridge
- CCTV to identify overloads and vehicle type

The existing bridge would be reinstated to 10 tonne load limit per the original design. Based on the limiting structural elements (cable system, dead man anchor and timber piers) strengthening to cater for higher vehicle loads is not possible. On-going maintenance would be required to ensure condition of all elements (especially timber elements).

Access would remain limited to heavy vehicles (less than 10 tonne), and restrictions on the crossing would remain in place including 20 km/hr speed limit, give way provision, and single vehicle on the bridge at any one time. Heritage aspects of the bridge would be maintained following the recommendations of the Conservation Management Plan.

Option 4 - Strengthen existing bridge

This option would include strengthening and supplementing the existing structure to cater for higher vehicle loads. Based on the limitations of the existing structure (existing suspension cabling system, dead man anchor block, geometry and timber piles), it is considered that this option is not feasible within the constraints of existing structure, while maintaining the heritage aspects of the structure.

This option was not considered further.

Option 5 - Replace with new bridge on current alignment

This option involves the demolition of the existing bridge and replacement with a new high-level single lane bridge on the same alignment, designed to current load standards. Bridge options available may include suspension, cable stay, arch, balanced cantilever or other long span options. Shorter span options would not be considered appropriate given the height and velocity of flooding at this location. Due to the constraints at the site, construction in this location would be a significant undertaking.

This option would provide an upgrade in service level to the local community, due to the increase vehicle load capacity. Access would be closed during the construction period, resulting in long detour via Pipeclay Road via Beechwood to Wauchope. The effective useful life for the new bridge is estimated to be 100 years. Heritage aspects of the bridge would be lost, and appropriate interpretation would be required if this were to progress.

Option 6 - Close existing bridge and upgrade Pipeclay Road

This option involves the closure, removal and demolition of the existing bridge, and upgrade of Pipeclay Road as the primary access for the Kindee Valley.

Pipeclay Road is approximately 30 km long and comprises single lane gravel windy road with one section through very mountainous terrain, and upgrade is considered to be a significant undertaking. This option significantly reduces the level of service provided to the local community and would unlikely to be acceptable. The heritage aspects of the existing bridge would be lost, as it would be demolished due to on-going safety considerations and excessive maintenance costs to council.

Option A-F - Provide a new bridge on an alternative alignment

These options involve the construction of a new bridge and road approaches over the Hastings River in the general locality of the existing bridge crossing (nominally 10 km up or downstream from the site of the current bridge). The options considered are described and assessed in further detail within this report.

Planning would commence for the replacement of the existing structure. Following completion of the replacement structure, the existing bridge may be required to be closed to vehicular traffic due to on-going safety considerations and excessive maintenance costs to council.

This option would provide an upgrade in service level to the local community due to increase vehicle load capacity. Heritage aspects of the bridge may be retained in the short to medium term, but appropriate consultation would be able to progress to formalise appropriate heritage interpretation to the site.

Preliminary Options comparison

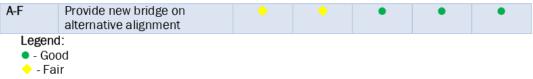
In assessing the alternative options available, criteria have been established that identifies the key issues and quantitative and qualitative constraints associated with each criterion. These criteria include:

- <u>Environment</u> Impact on existing aquatic and terrestrial flora, fauna and habitat.
- <u>Design and construction</u> Constraints related to geotechnical conditions, flooding and hydraulic impacts and constructability issues related to access and construction methods.
- <u>Heritage</u> Impact on existing heritage listed item and status.
- <u>Community</u> Impacts on road users including local traffic, school buses and industry, road closures or detours, changes in traffic volumes, and property impacts and acquisition requirements.
- <u>Operation and Maintenance</u> Impacts or improvements to maintenance operations including length of road, size of bridge and improved access for other operations (bin trucks, jet patcher, emergency services etc...)

The multi-criteria assessment of each option was completed utilising a traffic light system to provide a comparative assessment of each option.

Option	Description	Environment	Design and Construction	Heritage	Operation and Maintenance	Community
1	Do nothing		•	•	•	
2	Minor structural repairs		•	•	•	
3	Full remediation of existing bridge	•	•	•		
4	Strengthen existing bridge		•	•	•	•
5	Replace with new bridge on current alignment			•	•	
6	Close existing bridge and Upgrade Pipeclay Road	+	+			

Table 7 - Existing bridge options assessment



- Poor

From this multi-criteria assessment, **Option 3 – Full remediation of the existing bridge** is the preferred option relating to the existing bridge as assessed against that above criteria, although this may provide for only 20 to 30 years additional life for the structure. Following this period, it is likely that an alternative crossing of the Hastings River would be required, so is unlikely to be the preferred options from a Whole-of-Life cost perspective.

It is also noted that this option provides similar results the alternative crossing options **Options A**-F. The heritage aspects of the existing bridge may be ultimately lost in these options. It is assumed that the bridge would be closed to vehicular traffic and adaptive re-use options considered following construction of the new crossing. There remains a high likelihood that the bridge is closed and demolished in the medium term due to the potential for misuse, dilapidation or safety concerns. Costs of an alternative route are also expected to be higher in the initial phase, although ongoing maintenance costs would be significantly reduced relating to the new bridge crossing.

Notwithstanding any of the above commentary, the risk remains that significant overload, or damage from flooding may occur at the existing bridge, resulting in collapse or failure of the existing bridge, hence it is recommended to identify and establish the preferred alternative crossing option to be progressed in parallel with the on-going management of the existing heritage bridge structure.

The various alternative crossing options are further developed and assessed below.

Option identification – Alternative route options

The following options have been identified for alternative locations for crossing the Hastings River in the Ellenborough, Kindee and Long Flat localities. These may be considered separately or in conjunction with the various options available at the existing bridge.

Refer to Figure 14 the schematic sketch of locations and details of the alternative options.

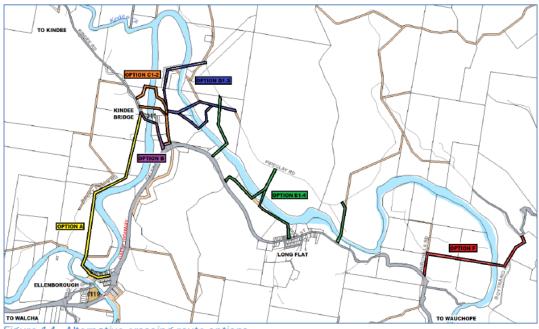


Figure 14 - Alternative crossing route options

Option A – Ellenborough Reserve via Rocky Falls Road

This option involves constructing a low-level bridge at the northwestern corner of the Ellenborough Reserve on the western extent of the Ellenborough township. The southern approach would utilise the existing Main Street and upgrading this road through the existing reserve to connect between the Oxley Highway and the new bridge. The western approaches would require significant upgrade of Rocky Falls Road between the proposed bridge and the existing western abutment of the Kindee Bridge on Kindee Road, including a section of new road between Rocky Falls Road and the proposed bridge.

The new bridge would be flood effected and potentially closed during large flood events due to the low-level nature of the crossing.

Option B – New bridge immediately upstream of existing bridge

This option involves constructing a new bridge immediately upstream (within say 10 m to 20 m) of the existing bridge. The bridge abutments will align with the existing bridge abutments and the bridge deck will be at a similar level to the existing bridge, potentially higher to ensure soffit levels do no impact on flood events. The road approaches will utilise the existing Kindee Road approaches on the eastern approach to connect to the Oxley Highway. The road approaches will utilise the existing Kindee Road approaches on the western approach, except at abutment B where approximately 20m of new road embankment would need to be constructed.

This option minimises road construction aspects although construction will be relatively constrained due to the difficult access at the site. The new bridge level is likely to be high enough to be above significant flood levels.

Option C1 - Boat Harbour Lane to Kindee Road - High Level

This option involves constructing a new high-level bridge approximately 200 m downstream from the existing bridge. The bridge would be located near the historic river crossing used prior to construction of the existing bridge. The eastern approach from the Oxley Highway would utilise the existing Kindee Road intersection, upgrade of Boat Harbour Lane, and reconstruction of the eastern approach utilising an existing cutting or travelling stock route. The western approach would connect to Kindee Road utilising a new road through private property.

The new bridge level is likely to be high enough to be above significant flood levels.

Option C2 - Boat Harbour Lane to Kindee Road - Low Level

This option involves constructing a new low-level bridge approximately 300 m downstream from the existing bridge. The bridge would be located near the historic river crossing used prior to construction of the existing bridge. The eastern approach from the Oxley Highway would utilise the existing Kindee Road intersection, upgrade of Boat Harbour Lane, and reconstruction of the eastern approach utilising an existing cutting or travelling stock route. The western approach would connect to Kindee Road utilising a new road through private property.

The new bridge would be flood effected and potentially closed during large flood events.

Option D1 - Boat Harbour Lane to Pipeclay Road Alignment 1

This option involves construction of a new mid-level bridge approximately 2.2 km downstream from the existing bridge on the eastern side of the river meander. The western approach (from the Oxley Highway) would utilise the existing Kindee Road intersection, upgrade of Boat Harbour Lane, and construction of a new length of road to the western abutment. The eastern approach would utilise Pipeclay Road and a new section of road through private property and existing paper road reserve. A length of Pipeclay Road back to the Kindee Road intersection may also need to be upgraded as part of the works.

The new bridge level may provide low-level flood free access, although is likely to be impacted by higher recurrence interval flooding.

Option D2 - Boat Harbour Lane to Pipeclay Road Alignment 2

This option involves construction of a high-level bridge approximately 2.8 km downstream from the existing bridge on the eastern side of the river meander. The western approach (from the Oxley Highway) would utilise the existing Kindee Road intersection, upgrade of Boat Harbour Lane, and construction of a new length of road to the western abutment. The eastern approach would utilise Pipeclay Road and a new section of road through private property and existing historic road reserve. A length of Pipeclay Road back to the Kindee Road intersection may also need to be upgraded as part of the works.

The new bridge level is likely to be high enough to be above major flood levels.

Option D3 - Boat Harbour Lane to Pipeclay Road Alignment 3

This option involves construction of a low-level bridge approximately 3.1 km downstream from the existing bridge on the eastern side of the river meander. The western approach (from the Oxley Highway) would utilise the existing Kindee Road intersection, upgrade of Boat Harbour Lane, and construction of a new length of road to the western abutment. The eastern approach would utilise Pipeclay Road and a new section of road through private property and existing historic road

reserve. A length of Pipeclay Road back to the Kindee Road intersection may also need to be upgraded as part of the works.

The new bridge would be flood effected and potentially closed during flood events.

Option E1 - Oxley Highway to Pipeclay Road Alignment 1 west of Long Flat

This option involves construction of a mid-level bridge approximately 3.5 km downstream from the existing bridge, crossing the Hastings River on the eastern side of the river meander and west of the township of Long Flat. The southern approach (from the Oxley Highway) involves construction of a new length of road through private property, and a new intersection on the Oxley Highway. The northern approach involves construction of a new length of road to connect with Pipeclay Road. A length of Pipeclay Road back to the Kindee Road intersection may also need to be upgraded as part of the works.

The new bridge level is likely to be high enough to be above significant flood levels, although may be impacted or over topped by major flood events (in excess of 1:50 year ARI).

Option E2 - Oxley Highway to Pipeclay Road Alignment 2 west of Long Flat

This option involves construction of a low-level bridge approximately 4.3 km downstream from the existing bridge, crossing the Hastings River immediately west of the township of Long Flat. The southern approach (from the Oxley Highway) involves construction of a new length of road through the existing river flats via an existing Crown Road, and a new intersection on the Oxley Highway. The northern approach involves construction of a new length of road to connect with Pipeclay Road. A length of Pipeclay Road back to the Kindee Road intersection may also need to be upgraded as part of the works.

The new bridge and southern approaches would be low level and flood effected, and potentially closed during flood events.

Option E3 - Oxley Highway to Pipeclay Road Alignment 3 via Long Flat

This option involves construction of a low-level bridge approximately 4.5 km downstream from the existing bridge, crossing the Hastings River immediately west of the township of Long Flat. The southern approach (from the Oxley Highway) involves construction of a new length of road from a new intersection on the Oxley Highway within the township of Long Flat and through the existing river flats via an existing Crown Road. The northern approach involves construction of a new length of road to connect with Pipeclay Road. A length of Pipeclay Road back to the Kindee Road intersection may also need to be upgraded as part of the works.

The new bridge and southern approaches would be low level and flood effected, and potentially closed during flood events.

Option E4 - Oxley Highway to Pipeclay Road Alignment 4 east of Long Flat

This option involves construction of a high-level bridge approximately 5.6 km downstream from the existing bridge, crossing the Hastings River immediately to the east of the township of Long Flat. The southern approach to the bridge and intersection on the Oxley Highway have a very short approach to the proposed bridge. The northern approach would require a section of new road utilising an existing Public Road (paper road) to connect with Pipeclay Road. A length of Pipeclay Road back to the Kindee Road intersection may also need to be upgraded as part of the works.

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The new bridge level is likely to be high enough to be above significant flood levels.

Option F - Churchill Road to Boy Trang Road

This option involves construction of a high-level bridge approximately 9.1 km downstream from the existing bridge, crossing the Hastings River approximately 2.6 km east of Long Flat. The southern approach (from the Oxley Highway) would utilise the existing Churchill Road intersection and road, and construction of a new length of road to the southern abutment. The northern approach would utilise the existing Boytrang Road (Private Road) and a short length of new road at the northern abutment to connect with Pipeclay Road. A significant length of Pipeclay Road back to the Kindee Road intersection, including the mountainous section may also need to be upgraded as part of the works.

The new bridge level is likely to be high enough to be above significant flood levels.

Flood Immunity

The alternative strategic route options and crossing locations considered in this report are a variety of low-level, mid-level and high-level bridge structures positioned according to existing topography and appropriate road grades. To estimate the expected flood immunity at the various crossing locations, a simplistic approach has been adopted utilising similar flood volumes and comparing these to the available waterway area at each crossing location to define flood levels and associated flood immunity. Areas downstream of Kindee Creek may have higher discharge rates as the volumes considered do not cater for any additional volume entering the river along the stretch under consideration. Further hydrological and hydraulic studies would be required to define these levels if an alternative option is to be progressed further.

Crossing	Location	Proposed Deck RL	Minor	Mod.	Major	5% AEP	2% AEP	1% AEP
Option A	2.8 km U/S	60.0	51.5	54.2	56.3	57.9	59.5	61.0
Option B	0.0 km D/S	60.5	47.4	50.5	52.6	54.5	56.2	57.9
Option C1	0.5 km D/S	62.0	47.4	50.5	52.6	54.5	55.8	57.7
Option C2	0.6 km D/S	52.5	47.1	50.0	51.9	53.8	55.2	56.7
Option D1	2.55 km D/S	50.5	44.8	46.8	48.7	50.0	51.4	52.4
Option D2	3.05 km D/S	56.0	43.2	45.1	47.0	48.2	49.7	50.7
Option D3	3.35 km D/S	48.0	42.8	44.7	46.7	47.9	49.4	50.4
Option E1	3.75 km D/S	52.5	43.2	46.3	48.5	50.5	52.1	54.1
Option E2	4.35 km D/S	46.0	44.2	48.5	51.0	53.0	54.5	56.5
Option E3	4.60 km D/S	46.0	44.7	49.2	51.5	53.4	55.0	57.0
Option E4	5.90 km D/S	52.5	39.7	42.2	44.3	45.7	46.7	48.2
Option F	8.50 km D/S	50.0	36.2	39.1	41.2	43.1	44.9	47.7

Table 8 - Alternative route options - Flood recurrence intervals

Legend:

White: Flood levels more than 2.5m below proposed bridge deck level. No superstructure impact during flood event.

Yellow: Flood levels less than 2.5m below proposed bridge deck level. Potential superstructure impact during flood event.

Orange: Flood levels less than 1m below proposed bridge deck level. Superstructure affected during flood event.

Red: Flood levels over proposed bridge deck level. Bridge overtopped during flood event.

Long List Options comparison

A preliminary comparison of the alternative road alignment and bridge crossing options has been completed comparing overall construction quantities and assumed level of service to allow identification of a short list of options for further development.

Option	A	В	C1	C2	D1	D2	D3	E1	E2	E3	E4	F
Total Length (exc. Pipeclay Rd)	3200	710	1200	1218	1835	1565	1810	1050	1070	1190	550	1785
Gravel Road Upgrade	2200	0	340	340	935	550	550	0	0	0	0	665
Sealed Road Upgrade	400	460	80	0	75	75	75	0	0	0	0	0
New Road	470	100	610	770	665	760	1100	930	950	1070	390	970
Oxley Highway Intersection	N	N	N	N	N	N	N	Y	Y	Y	Y	Ν
Pipeclay Road Upgrade	0	0	0	0	2900	3900	3900	3650	4800	4800	6000	10900
Bridge Length	130	150	170	108	160	180	85	140	120	120	150	150
Flood Immunity	Low	High	High	Low	Mid	High	Low	Mid	Low	Low	High	High
Short list	N	Y	Y	Y	N	N	N	Y	N	N	Y	N

Table 9 - Long list options comparison

Option A was discarded due to the low-level flood affected crossing, impacts on the existing Ellenborough Village and Reserve, and the long and winding nature of the existing Rocky Falls Road requiring significant upgrade.

Option D1, D2 and D3 were all discarded due to the overall length of new roads required through private property and the level of service provided by the bridge crossings with both D1 and D3 both being flood affected. Option D2 was discarded due to the overall length of bridge required being significantly longer than other options, in combination with the longer road approaches required.

Option E2 and E3 were discarded due to the low-level nature of both the proposed bridge crossing and the approach roads liable to damage or closure during flooding, along with the requirement for a new intersection on the Oxley Highway. Option E3 was also discarded due to the impact on the existing Long Flat Water Treatment facility.

Option F was discarded due to not meeting the key objectives of providing alternative access directly to the Kindee Valley, and the excessive length of Pipeclay Road required to be upgraded, including the narrow winding section through the mountainous area.

Based on the above comparison, Option B, C1, C2, E1 and E4 were identified as options to be further developed for additional comparison.

Short List Options Assessment

The short list options that are to be assessed are discussed further below, including:

- Option B New Bridge immediately upstream of existing bridge
- Option C1 Boat Harbour Lane to Kindee Road High Level
- Option C2 Boat Harbour Lane to Kindee Road Low Level
- Option E1 Oxley Highway to Pipeclay Road Alignment A west of Long Flat
- Option E4 Oxley Highway to Pipeclay Road Alignment C east of Long Flat

Strategic concept drawings are attached to Appendix C.

Option B - New Bridge immediately upstream of existing bridge

Eastern approach road would utilise the existing Kindee Road alignment between the Oxley Highway and the existing bridge. Works would involve batter trimming and stabilisation, shoulder widening, improved drainage and barrier protection. Minor upgrades and improvements to the existing Oxley Highway intersection would be required. Minor property acquisition may be required near the eastern abutment as part of this work.

The western approach would utilise the existing Kindee Road and new road embankment on the southern side of the existing bridge for the new bridge abutment within the existing road corridor. There may be tree clearing required under the footprint of the proposed bridge, and on both approaches related to road widening and batter stabilisation.



Figure 15 - Option B: New Bridge immediately upstream of existing bridge

The proposed bridge would be a single-lane bridge with an overall width of 5.5 m and an overall length of 150 m, with a height above normal water levels of 16.5 m. Due to the height and complexity of construction access, longer spans are preferred. For the purposes of comparison, a five (5) by 30 m span super T structure with four (4) in-river piers has been adopted, although longer span options may be found to be feasible. Geotechnical conditions are likely to be favourable given the shallow depth to relatively high strength rock identified at the site.

Access for pier construction and lifting of superstructure elements would require temporary access tracks and in-river rock platforms with appropriate low flow and fish passage provisions.

Kindee Road would need to be closed at various points during construction to allow road works and tie-in works but would be maintained open generally with appropriate construction staging. There is a relatively high risk of traffic impacts during construction at this location. There is no requirement to upgrade Pipeclay Road as part of this Project unless required for construction purposes.

Key advantages of this option are as follows:

- Utilise existing Kindee Road with minimal new roads or upgraded roads.
- Minimal impacts on the Oxley Highway intersection
- · Minimal impacts on private property or existing residents
- · Favourable geotechnical conditions for pier construction.
- High level flood immunity

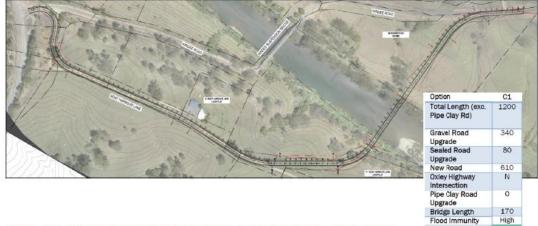
Key disadvantages of this option are as follows:

- Very difficult terrain for construction access
- Very long high-level bridge construction
- Difficult staging construction with existing traffic at Kindee Bridge
- Relatively large impacts on existing vegetation primarily within the existing Kindee Road corridor.
- Impacts of heritage aspects of the existing bridge due to the nature and scale of the proposed structure immediately adjacent to the existing heritage listed structure.

Option C1 - Boat Harbour Lane to Kindee Road - High Level

The eastern approach road would utilise the existing Oxley Highway - Kindee Road intersection and upgrade of Boat Harbour Lane through to the historic cutting. A new road to the eastern abutment would be constructed along an existing public road corridor with limited property impacts on private property. Some vegetation clearing would be required on the eastern approach to the bridge.

The western approach road requires a new length of road between the western abutment and Kindee Road through a single parcel of private property. Property acquisition and a new road corridor would need to be established. Limited clearing of vegetation is required as the land is already cleared for agricultural purposes.





The proposed Bridge would be a single-lane bridge with an overall width of 5.5 m and an overall length of 170 m, with a height above normal water levels of 16.5 m. Due to the height and complexity of construction access, longer spans are preferred. For the purposes of comparison, a five (5) by 34 m span super T structure with four (4) in-river piers has been adopted. Geotechnical conditions are likely to be favourable given the shallow depth to relatively high strength rock identified at the site, although there is significant depth of river gravel and cobbles at this location.

Access for pier construction and lifting of superstructure elements would require temporary access tracks and in-river rock platforms with appropriate low flow and fish passage provisions.

Minor impacts to the use of Kindee Road may be experienced due to upgrades to the existing Oxley Highway intersection. Users of Boat Harbour Lane may be impacted during construction due to upgrades, and operation due to increase traffic volumes along this section road. Limited impacts on existing road users of Kindee Road due to the offline nature of the construction.

Key advantages of this option are as follows:

- Utilises existing Kindee Road intersection and Boat Harbour Lane.
- · Minimal impacts on the Oxley Highway intersection
- · High level flood immunity
- Minor impact on the heritage status of the existing bridge due to the distance of separation.

Key disadvantages of this option are as follows:

- · Long length of road upgrade and new road
- Steep road approaches
- Significant earthworks and vegetation clearing required for eastern road tie-in.
- Increased traffic adjacent existing residents
- Significant property acquisition through private property.
- Potential deep gravel and cobble deposits.
- Very difficult terrain for construction access
- Very long high-level bridge construction

Option C2 - Boat Harbour Lane to Kindee Road - Low Level

The eastern approach road would utilise the existing Oxley Highway - Kindee Road intersection and upgrade of Boat Harbour Lane through to the historic cutting. A new road to the eastern abutment would be constructed along an existing public road corridor with significant earthworks required to achieve levels for the lower level bridge. Some impacts on private property are expected requiring partial property acquisition.

The western approach road requires a new length of road between the western abutment and Kindee Road through a single parcel of private property. Property acquisition and a new road corridor would need to be established.



Figure 17 - Option C2: Boat Harbour Lane to Kindee Road – Low Level

The proposed bridge would be a low-level single-lane bridge with an overall width of 5.5 m and an overall length of 108 m, with a height above normal water levels of 6.5 m. Although the structure is impacted by flood levels, the height of the bridge deck still warrants full barrier containment. The bridge would be designed to be over-topped in major flood events and the superstructure would be required to be designed with lateral and hold down restraint. For the purposes of comparison, a six (6) by 18 m space plank deck unit spans with five (5) in-river piers has been adopted. Geotechnical conditions are likely to be favourable given the shallow depth to relatively high strength rock identified at the site, although there is significant depth of river gravel and cobbles at this location.

Access for pier construction and lifting of superstructure elements would require temporary access tracks and in-river rock platforms with appropriate low flow and fish passage provisions. The existing gravel and cobble build-up may be utilised as part of this works.

Minor impacts to the use of Kindee Road may be experienced due to upgrades to the existing Oxley Highway intersection. Users of Boat Harbour Lane may be impacted during construction due to upgrades, and operation due to increase traffic volumes along this section road.

Key advantages of this option are as follows:

- Utilises existing Kindee Road intersection and Boat Harbour Lane.
- Minimal impacts on the Oxley Highway intersection
- Shorter and less expensive bridge construction.

Key disadvantages of this option are as follows:

- Low level flood immunity
- · Long length of road upgrade and new road
- · Steep road approaches
- · Significant earthworks and vegetation clearing required for eastern road tie-in.
- Increased traffic adjacent existing residents
- Significant property acquisition through private property.
- · Potential deep gravel and cobble deposits.
- Very difficult terrain for construction access

Option E1 - Oxley Highway to Pipeclay Road Alignment A west of Long Flat

The southern approach to the southern abutment would require a new road to be constructed through a single portion of private property. A new intersection would be required to be constructed with the Oxley Highway in an existing 100 km per hour speed zone. Partial acquisition of this property would be required. Minimal vegetation clearing except at the riverbanks would be requires as the land is cleared for agricultural purposes.

The northern approach requires a new road to be constructed through a single portion of property and would require property acquisition and a new road corridor to be established. Minimal vegetation clearing except at the riverbanks would be required as the land is cleared for agricultural purposes.

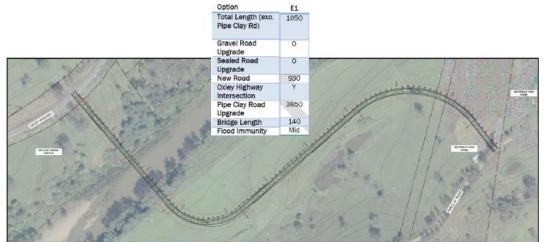


Figure 18 - Option E1: Oxley Highway to Pipeclay Road Alignment A west of Long Flat

The proposed bridge would be a single-lane bridge with an overall width of 5.5 m and an overall length of 140 m, with a height above normal water levels of 13.0 m. Based on the current design, the bridge superstructure may be impacted by high flood events and would need to be designed accordingly. Due to the height and complexity of construction access, longer spans are preferred. For the purposes of comparison, a five (5) by 28 m span super T structure with four (4) in-river piers has been adopted. Geotechnical conditions indicate relatively shallow bedrock, although deep alluvial gravels and cobbles may be present at the site.

Access for pier construction and lifting of superstructure elements would require temporary access tracks and in-river rock platforms with appropriate low flow and fish passage provisions.

Oxley Highway intersection construction may involve widening of the highway to allow for appropriate right-in turn lanes and acceleration lanes. There would be on-going impacts on the existing highway due to these works. The construction of the bridge and the new roads would have limited impact on the existing road network.

A length of Pipeclay Road of approximately 3.65 km between the new northern approach road and Kindee Road would need to be upgraded as part of this Project. A significant length of Pipeclay Road is not located within existing road reserves and any upgrade of this road would require these property issues to be resolved.

Key advantages of this option are as follows:

- Relatively direct link between Oxley Highway and Pipeclay Road.
- · Minimal impact on vegetation except at riverbanks.

- Better construction access directly adjacent Oxley Highway.
- Heritage impacts on existing bridge are minimised.

Key disadvantages of this option are as follows:

- Major new intersection required on the Oxley Highway
- Potential deep gravel and cobble deposits.
- Mid-level flood immunity impacted by higher flood events, including northern road approaches.
- All new roads required through private property requiring acquisition
- Very long high-level bridge construction
- Pipeclay Road requires upgrade, including formalisation of road corridor.

Option E4 – Oxley Highway to Pipeclay Road Alignment C east of Long Flat

The southern approach to the southern abutment would be very short due to the proximity to the existing Oxley Highway. The southern abutment and intersection upgrade may require property acquisition. A new intersection would be required to be constructed with the Oxley Highway in the eastern limits of the existing 50 km per hour speed zone. Some vegetation clearing may be required on the riverbanks.

The northern approach requires a new road to be constructed following an existing public road corridor (paper road). Partial property acquisition will be required on the adjoining property. Some clearing of vegetation would be required at the riverbanks, along with impacts to planted driveway trees on the adjoining property. The remainder of the new road would not require any vegetation clearing as the land is already cleared for agricultural purposes.

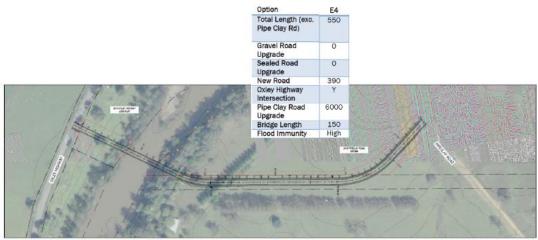


Figure 19 - Option E4: Oxley Highway to Pipeclay Road Alignment C east of Long Flat

The proposed Bridge would be a single-lane bridge with an overall width of 5.5 m and an overall length of 150 m, with a height above normal water levels of 16.0 m. Due to the height and complexity of construction access, longer spans are preferred. For the purposes of comparison, a five (5) by 30 m span super T structure with four (4) in-river piers has been adopted. Geotechnical conditions are likely to be favourable given the shallow depth to relatively high strength rock, as sighted exposed on the existing northern riverbanks.

Oxley Highway intersection construction may involve widening of the highway to allow for appropriate right-in turn lanes and acceleration lanes and appropriate sight distances for vehicles

utilising the new bridge. There would be on-going impacts on the existing highway due to these works. The construction of the bridge and the new roads would have limited impact on the existing road network. Potential queuing at the southern approach would need to be considered due to the single lane operation of the bridge, and the proximity to the Oxley Highway intersection.

A length of Pipeclay Road of approximately 6.0 km between the new northern approach road and Kindee Road would need to be upgraded as part of this Project.

Key advantages of this option are as follows:

- Utilises existing public road corridor between Oxley Highway and Pipeclay Road
- Minimal impacts on private property requiring property acquisition
- Favourable geotechnical conditions for pier construction.
- High level flood immunity
- Better construction access directly adjacent Oxley Highway.
- · Relatively minor impacts on existing vegetation.

Key disadvantages of this option are as follows:

- Major new intersection required on the Oxley Highway
- Very long length of Pipeclay Road requires upgrade, including formalisation of road corridor.
- Increased traffic adjacent existing residence.
- Very long high-level bridge construction.
- · Pipeclay Road requires upgrade, including formalisation of road corridor.

Strategic Cost Estimates

Basis of Estimate

The strategic cost estimates are preliminary estimates only, have been developed solely for the purpose of comparing and evaluating different options, and may not have been fully scoped. Actual prices, costs and other variables may be different to those used to prepare the cost estimate and may change. Unless as otherwise specified in this report, no detailed quotation have been obtained for tasks identified in any future construction project.

The strategic cost estimates have been prepared using information reasonably available and is based on assumptions and judgments including no allowance for client costs (including but not limited to costs associated with staffing, project management, supervision, contract management, tendering, approvals, associated works, etc.) to undertake the work and that all work is undertaken in the quickest and most efficient manner without delays for reviews, procurement, installation and shutdowns.

A contingency level of 50% has been adopted to reflect the preliminary nature of the estimate.

Strategic cost estimates

The CAPEX strategic cost estimate for construction of the short listed options is detailed below in Table 10. Assumptions related to scope and methodology have been included in within the report generally.

The strategic cost estimates are based on value of money in 2021 and have not allowed for escalation of 2.5% based on delay to construction. Estimates for escalation have been allowed for within the Whole of Life cost estimate below.

CAPEX Strategic Cost Estimate	(%)	Option B	Option C1	Option C2	Option E1	Option E4
Construction / Capital Costs						
Approaches		\$1,166,000	\$2,369,000	\$3,291,000	\$2,908,000	\$1,656,000
Bridge Structure		\$4,950,000	\$5,610,000	\$2,970,000	\$4,620,000	\$4,950,000
Pipe Clay Road		\$0	\$0	\$0	\$1,825,000	\$3,000,000
Oxley Highway Intersection		\$500,000	\$500,000	\$500,000	\$1,000,000	\$1,000,000
Sub Total		\$6,616,000	\$8,479,000	\$6,761,000	\$10,353,000	\$10,606,000
Design and Approvals	5%	\$330,800	\$423,950	\$338,050	\$517,650	\$530,300
Project Management	10%	\$661,600	\$847,900	\$676,100	\$1,035,300	\$1,060,600
Sub Total		\$992,400	\$1,271,850	\$1,014,150	\$1,552,950	\$1,590,900
Contigency (50%)	50%	\$3,804,200	\$4,875,425	\$3,887,575	\$5,952,975	\$6,098,450
TOTAL		\$11,420,000	\$14,630,000	\$11,670,000	\$17,860,000	\$18,300,000

Table 10 - CAPEX Strategic Cost Estimate

Whole of Life cost study

A whole-of-life cost study has been undertaken for the five (5) short listed options as well as Option 3 - Full Remediation of the existing bridge for comparison purposes.

The results are summarised below in Table 11 and include an annualised cost based on expected useful life. These costs include 50% contingency, 2.5% cost escalation of future costs and discounting as applicable.

The whole-of-life study shows that Option B is the preferred option based on annualised cost of \$232,400 over the expected life of the asset. It also shows that Option 3 - Full Remediation of the Bridge while having lower capital costs has an increased annualised cost of \$430,000 due to the reduced expected useful life, and significant costs would be expected at that time to implement replacement of the structure.

ltem:	Option 3 Full Remediation of Bridge	Option B New bridge immediately upstream of existing bridge	Option C1 Boat Harbour Lane to Kindee Road - High Level	Option C2 Boat Harbour Lane to Kindee Road - Low Level	Option E1 Oxley Highway to Pipe Clay Road Alignment A	Option E4 Oxley Highway to Pipe Clay Road East of Long Flat
1. Construction / Capital Costs	\$0	\$10,622,000	\$13,763,000	\$10,845,000	\$16,992,000	\$17,151,000
 Bridge Rehabilitation Costs 	\$4,823,000	\$2,425,000	\$2,425,000	\$2,425,000	\$2,425,000	\$2,425,000
 Ongoing Operations and Maintenance 	\$3,143,000	\$2,809,000	\$2,809,000	\$4,061,000	\$4,027,000	\$5,245,000
4. Disposal	\$1,247,000	\$743,000	\$743,000	\$743,000	\$743,000	\$743,000
Total Whole of Life Costs	\$9,213,000	\$16,599,000	\$19,740,000	\$18,074,000	\$24,187,000	\$25,564,000
Contingency (50%)	\$4,607,000	\$8,300,000	\$9,870,000	\$9,037,000	\$12,094,000	\$12,782,000
Total Whole of Life Costs + Contingency	\$13,820,000	\$24,899,000	\$29,610,000	\$27,111,000	\$36,281,000	\$38,346,000
Expected Useful Life (Years)	30	100	100	100	100	100
Annual Expenditure (\$/year)	\$460,700	\$249,000	\$296,100	\$271,200	\$362,900	\$383,500

Table 11 - Whole-of-Life Strategic Cost Estimate

Options Assessment

With consideration of the whole-of-life cost assessment, the multi criteria assessment (MCA) was completed for the short-listed options utilising pair wise comparison methodology for the selected assessment criteria.

This assessment identifies Option B as the preferred option due to the limited impact on property, and utilisation of the existing road network, although it identifies potential risks associated with construction at this location, and impact on existing road users during construction.

The assessment also recommends Option E4 be further progressed in parallel with Option B as part of the early concept design development to confirm feasibility and the preferred option moving forward. The advantages of this option related to the improved road network, good geotechnical condition and flood immunity levels, and proximity to Long Flat, although required property acquisition, construction of a new intersection on the Oxley Highway, and may also require the upgrade of Pipeclay Road through to Kindee Road intersection.

Refer to the multi-criteria assessment attached to Appendix D, and Table 12 below for a summary of the options.

Table 12 - Options Assessment - Recommendations					
Option	Rank	Review of Option			
B - New bridge immediately upstream of existing bridge	1	 Key advantages of this option are as follows: Utilise existing Kindee Road with minimal new roads or upgraded roads. Minimal impacts on the Oxley Highway intersection Minimal impacts on private property or existing residents Favourable geotechnical conditions for pier construction. High level flood immunity Key disadvantages of this option are as follows: Very difficult terrain for construction access Very long high-level bridge construction Difficult staging construction with existing traffic at Kindee Bridge Relatively large impacts on existing vegetation primarily within the existing Kindee Road corridor. Impacts of heritage aspects of the existing bridge due to the nature and scale of the proposed structure immediately adjacent to the existing heritage listed structure. 			
C1 - Boat Harbour Lane to Kindee Road - High Level	3	 Key advantages of this option are as follows: Utilises existing Kindee Road intersection and Boat Harbour Lane. Minimal impacts on the Oxley Highway intersection High level flood immunity 			

		 Minor impact on the heritage status of the existing bridge due to the distance of separation. Key disadvantages of this option are as follows: Long length of road upgrade and new road Steep road approaches Significant earthworks and vegetation clearing required for eastern road tie-in. Increased traffic adjacent existing residents Significant property acquisition through private property. Potential deep gravel and cobble deposits. Very difficult terrain for construction access Very long high-level bridge construction
C2 - Boat Harbour Lane to Kindee Road - Low Level	5	 Key advantages of this option are as follows: Utilises existing Kindee Road intersection and Boat Harbour Lane. Minimal impacts on the Oxley Highway intersection Shorter and less expensive bridge construction. Key disadvantages of this option are as follows: Low level flood immunity Long length of road upgrade and new road Steep road approaches Significant earthworks and vegetation clearing required for eastern road tie-in. Increased traffic adjacent existing residents Significant property acquisition through private property. Potential deep gravel and cobble deposits. Very difficult terrain for construction access
E1 - Oxley Highway to Pipeclay Road - Alignment A	4	 Key advantages of this option are as follows: Relatively direct link between Oxley Highway and Pipeclay Road. Minimal impact on vegetation except at riverbanks. Better construction access directly adjacent Oxley Highway. Heritage impacts on existing bridge are minimised. Key disadvantages of this option are as follows: Major new intersection required on the Oxley Highway Potential deep gravel and cobble deposits. Mid-level flood immunity impacted by higher flood events, including northern road approaches. All new roads required through private property requiring acquisition Very long high-level bridge construction Pipeclay Road requires upgrade, including formalisation of road corridor.

E4 - Oxley Highway	2	Key advantages of this option are as follows:
E4 - Oxley Highway to Pipeclay Road - East of Long Flat	2	 Utilises existing public road corridor between Oxley Highway and Pipeclay Road Minimal impacts on private property requiring property acquisition Favourable geotechnical conditions for pier construction. High level flood immunity Better construction access directly adjacent Oxley Highway. Relatively minor impacts on existing vegetation.
		 Key disadvantages of this option are as follows: Major new intersection required on the Oxley Highway Very long length of Pipeclay Road requires upgrade, including formalisation of road corridor. Increased traffic adjacent existing residence. Very long high-level bridge construction. Pipeclay Road requires upgrade, including formalisation of road corridor.

Conclusion and recommendations

Current situation

The bridge over the Hastings River at Kindee Crossing (Kindee Bridge) was constructed in 1936, and is a three (3) span trussed cable suspension bridge with an overall length of 120.7 m, and central span length of 67.1 m. The bridge consists of a stiffened cable suspension system supported from two (2) timber piers and dead man concrete anchor blocks at abutment locations. The single lane timber deck comprises timber cross girders, longitudinal stringers and a timber deck supported by mild steel hangar rods from the cable suspension system.

The existing bridge is a heritage-listed item in the *Port Macquarie-Hastings Council Local Environment Plan 2011.* The Heritage Assessment found the bridge to have exceptional significance due to its; rarity, being only one of two remaining bridges of this type in the world; the materials of construction, utilising local hardwood timber and composite steel construction; the design, being unique to this region of the world showing creativity and ingenuity; and its economic and social importance to the local community providing access over the Hastings River to the Kindee Valley. Elements of the bridge were also identified as having high or exceptional significance including the cable suspension system and steel diagonals, the timber hand railing, the timber piers, and the timber abutments.

The existing bridge has received minimal maintenance over its lifetime due to limited access and availability of funding. Major rehabilitation of the bridge was completed in 1991 with funding support from the Department of Main Roads and involved repair and replacement of a number of key elements, although the timber piers and cabling system remain of original construction. Detailed condition inspections of key structural elements have been completed in recent years, finding the bridge is now in overall poor condition, and requires major repairs and rehabilitation.

Due to the location and form of the existing structure, the bridge is considered a high-risk structure in the Port Macquarie-Hastings Council transport network and is susceptible to catastrophic failure due to vehicle overload, flood impacts or bushfire. The bridge is also load limited to 10 tonne, putting a further constraint on the road network for the local community, primary producers, emergency services, and council operations. The only alternative route to the Kindee Valley is via Pipeclay Road, which is an unsealed gravel road through windy and mountainous terrain.

Conservation Management Plan

Port Macquarie-Hastings Council engaged Taylor Thomson Whitting (NSW) Pty Ltd to complete a heritage assessment and prepare a *Conservation Management Plan (ref: D2019/282712)* for the Kindee Suspension Bridge in 2019.

The Conservation Management Plan included the investigation phase reviewing locality, site context, history of the site, design, physical evidence and condition of the structure. The assessment phase included comparative analysis of similar structures and assessment of the significance of the structure as a whole and of its parts in accordance with the Burra Charter. This included preparation of the statement of significance for the Kindee Bridge. The management phase identified constraints and opportunities, conservation policies, and a schedule of conservation works related to all key elements of the structure.

The Conservation Management Plan provides a key document for reference for on-going management of the bridge, along with an interpretative record of the structure.

Strategic options

Due to the age, form and high-risk nature of the existing bridge, an assessment of management options for the existing bridge was completed. The assessment of these options recommended

that full remediation of the existing bridge should be completed to ensure safe and reliable crossing of the Hastings River is maintained, in lieu of limited maintenance, or demolition and replacement. Due to the high-risk nature of the structure, there remains a risk that the bridge becomes unserviceable due to overload, structural failure, or damage from environmental events. It is therefore considered prudent to consider and assess alternative options of crossing the Hastings River at this location to ensure access to the Kindee Valley is maintained in the long term.

A desktop study of alternative options to cross the Hastings River was completed to understand the overall extent, cost and implications of these options. A high-level comparison of a long list of route options was completed to define the short-listed options. A multi-criteria assessment of the short list options was then completed to determine the preferred route options for further development.

The preferred route options recommended to be developed further with concept and detailed design were:

- Option B New bridge immediately upstream of existing bridge (Preferred)
- Option E4 Oxley Highway to Pipeclay Road Alignment B via Long Flat (Second Ranked)

Next steps

The above investigations, strategic conceptual designs, and assessments have been completed by Port Macquarie-Hastings Council with limited input from the community to date. As Kindee Bridge is an important public asset to the local community, and the broader regional community, consultation of the findings of this investigation is critical to ensure a robust outcome is selected in line with the expectations of the community.

It is recommended that engagement with the local community is completed including public submission, community briefings, review and incorporation of community submissions, and confirmation of the preferred route to be progressed towards concept and detailed design.

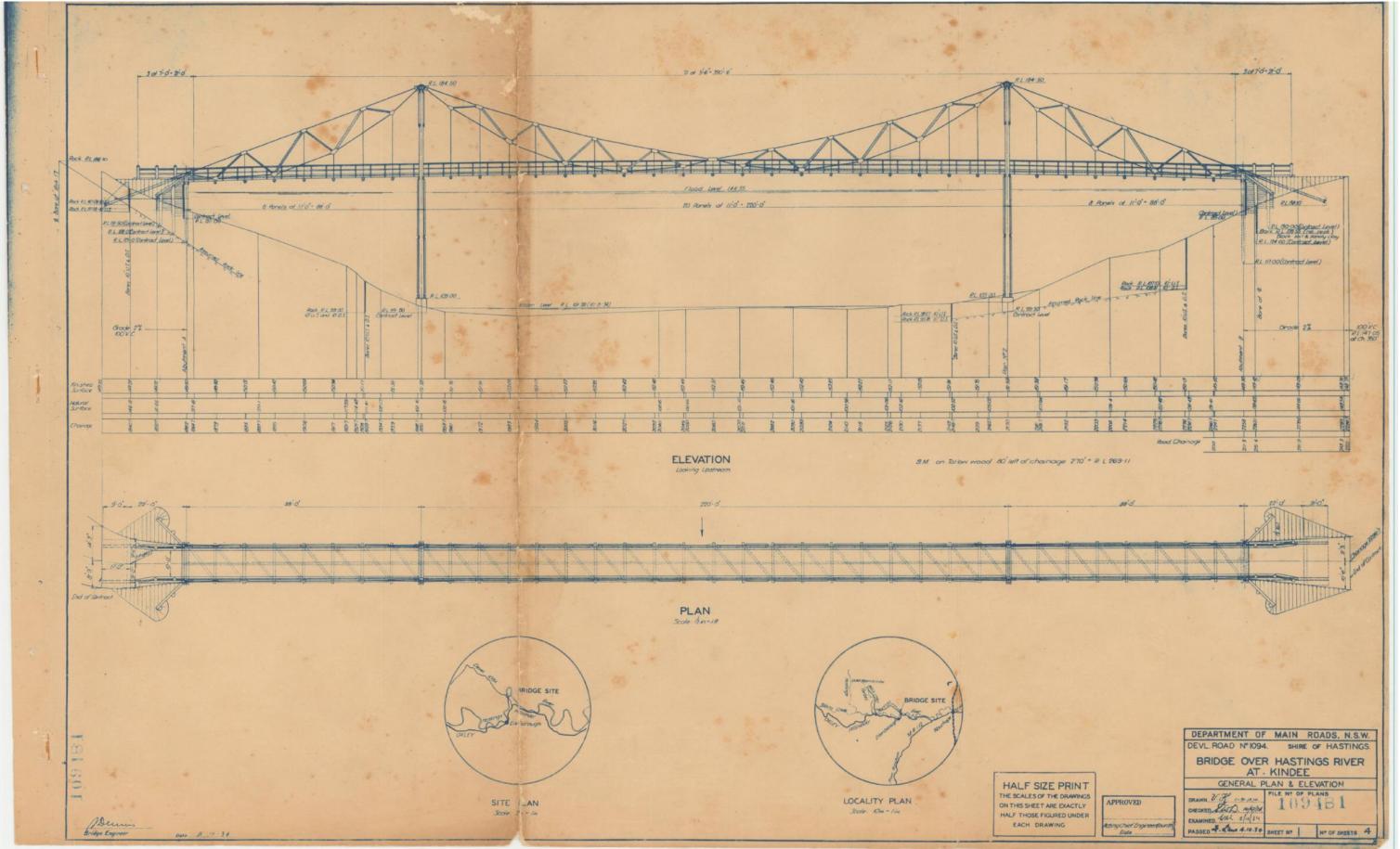
It is expected that council endorsement of the Kindee Bridge Management Strategy and Options Report including the Kindee Bridge Conservation Management Plan would be sought following the successful completion of community engagement process.

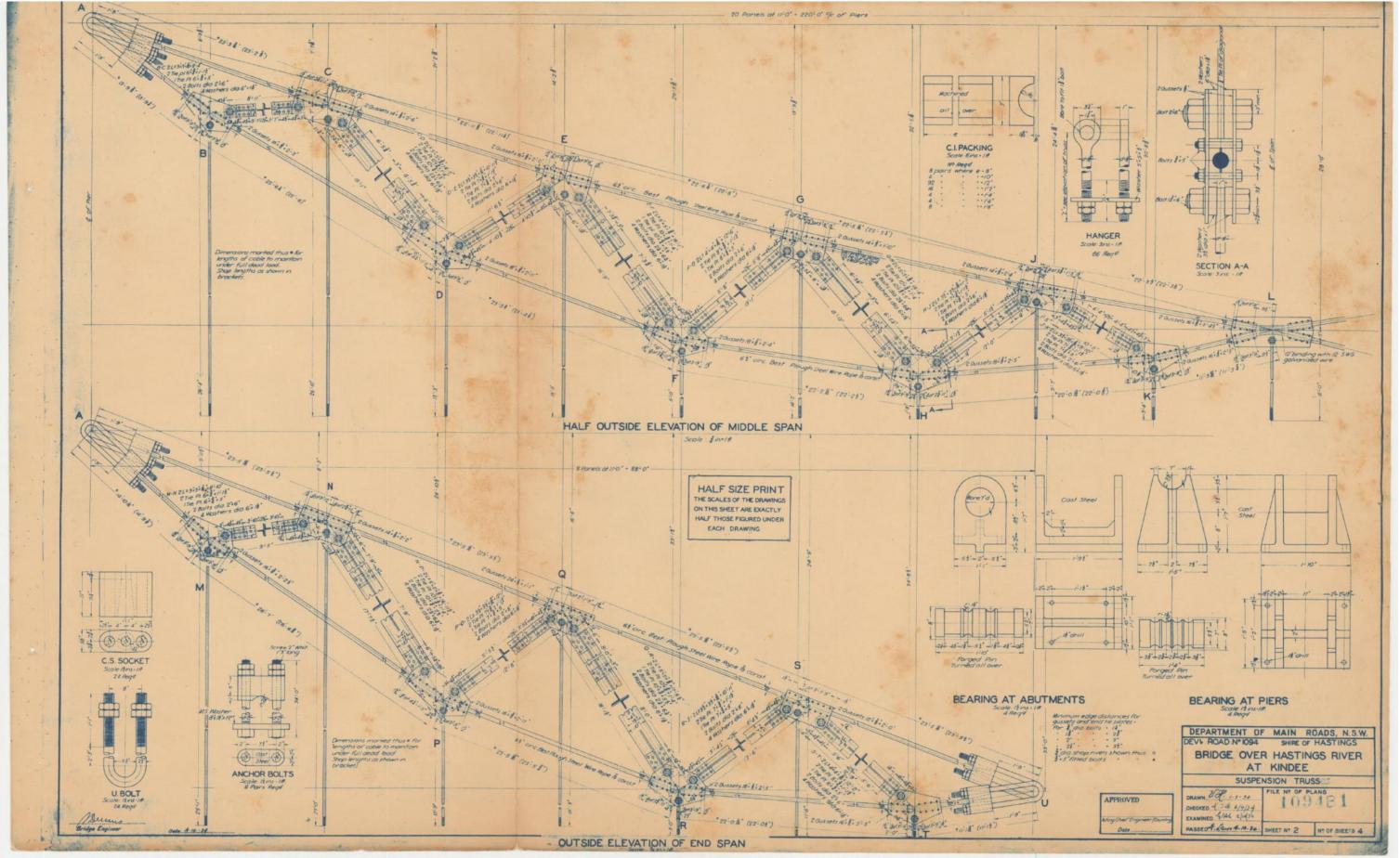
Appendix A – Original Drawings

Includes:

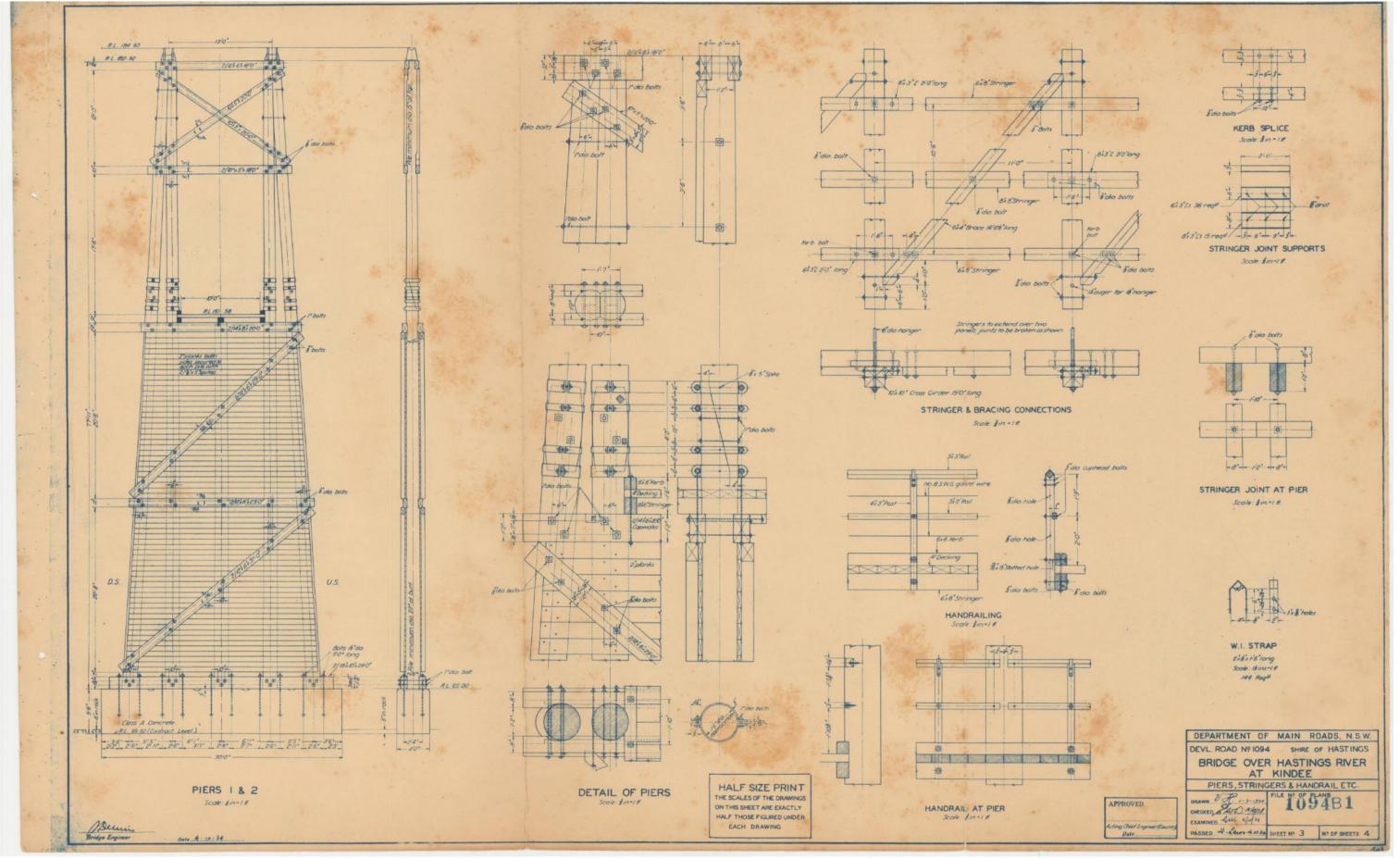
- Bridge over Hastings River at Kindee, DMR, 1934 (Sheet 1 to 4)
- Bridge over Hastings River at Kindee Approaches, DMR, 1935 (Sheet 1 to 2)
- Kindee Road Alignment 0 km to 1.24 km HSC, Unknown Date (Sheet 1)

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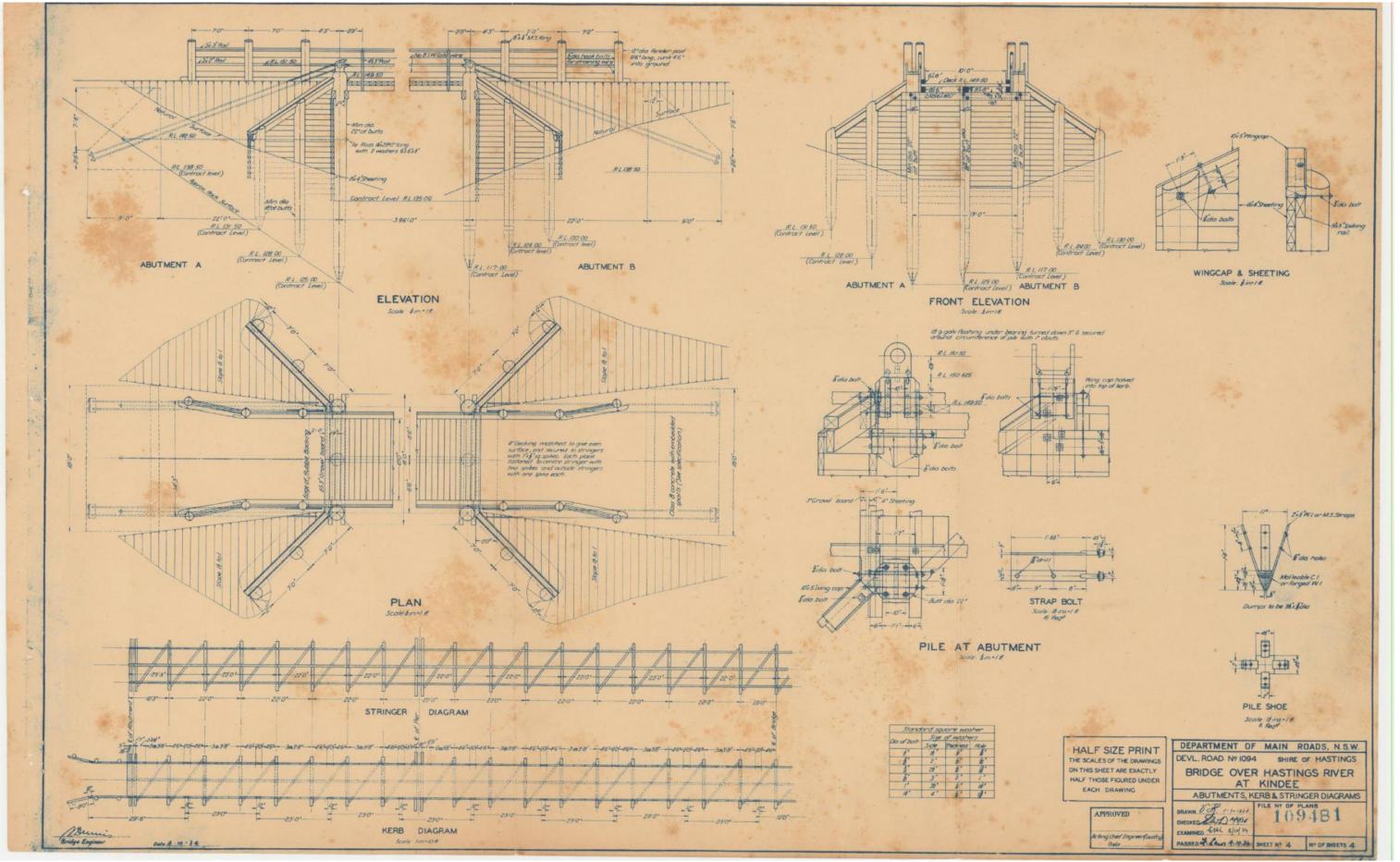




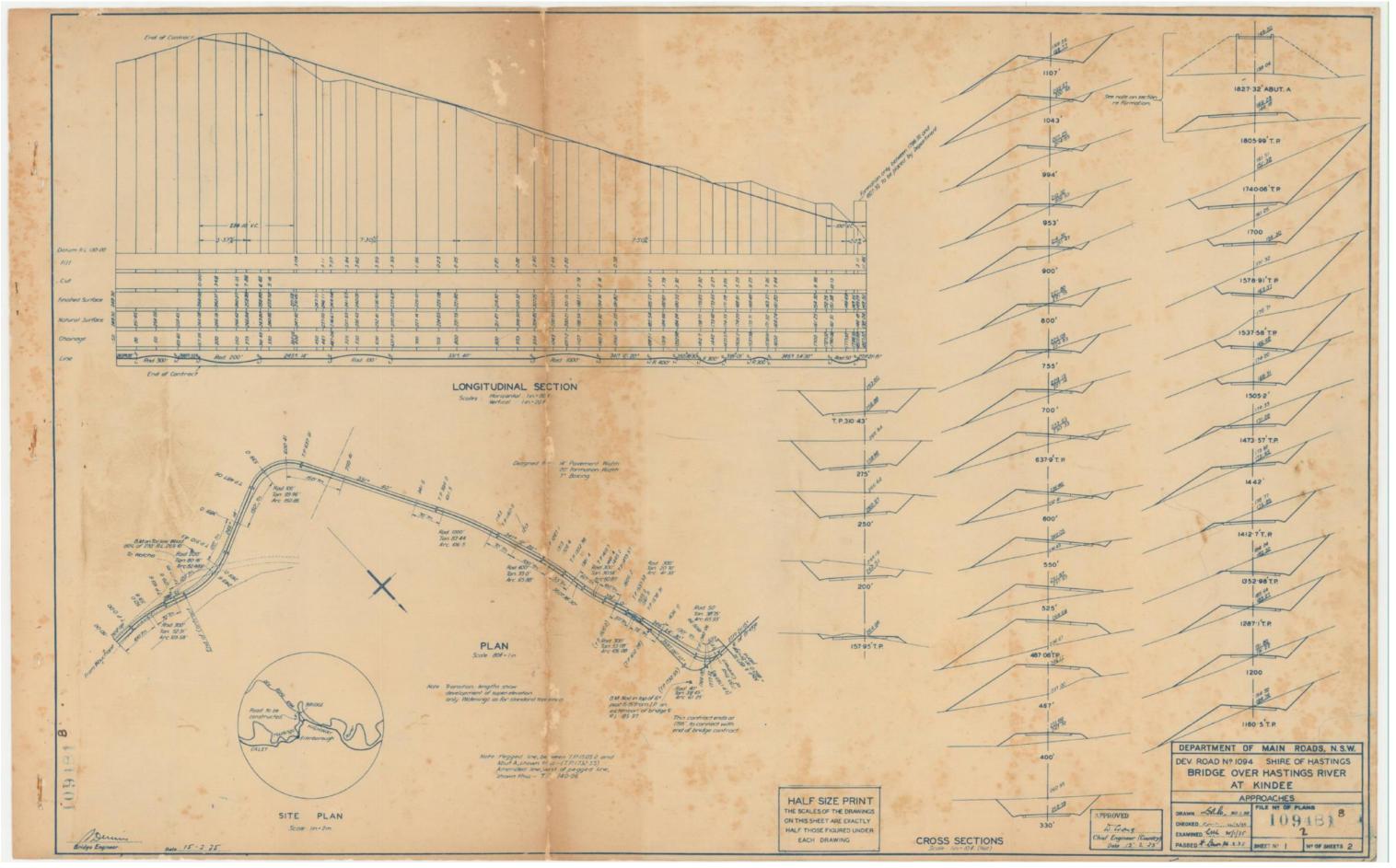
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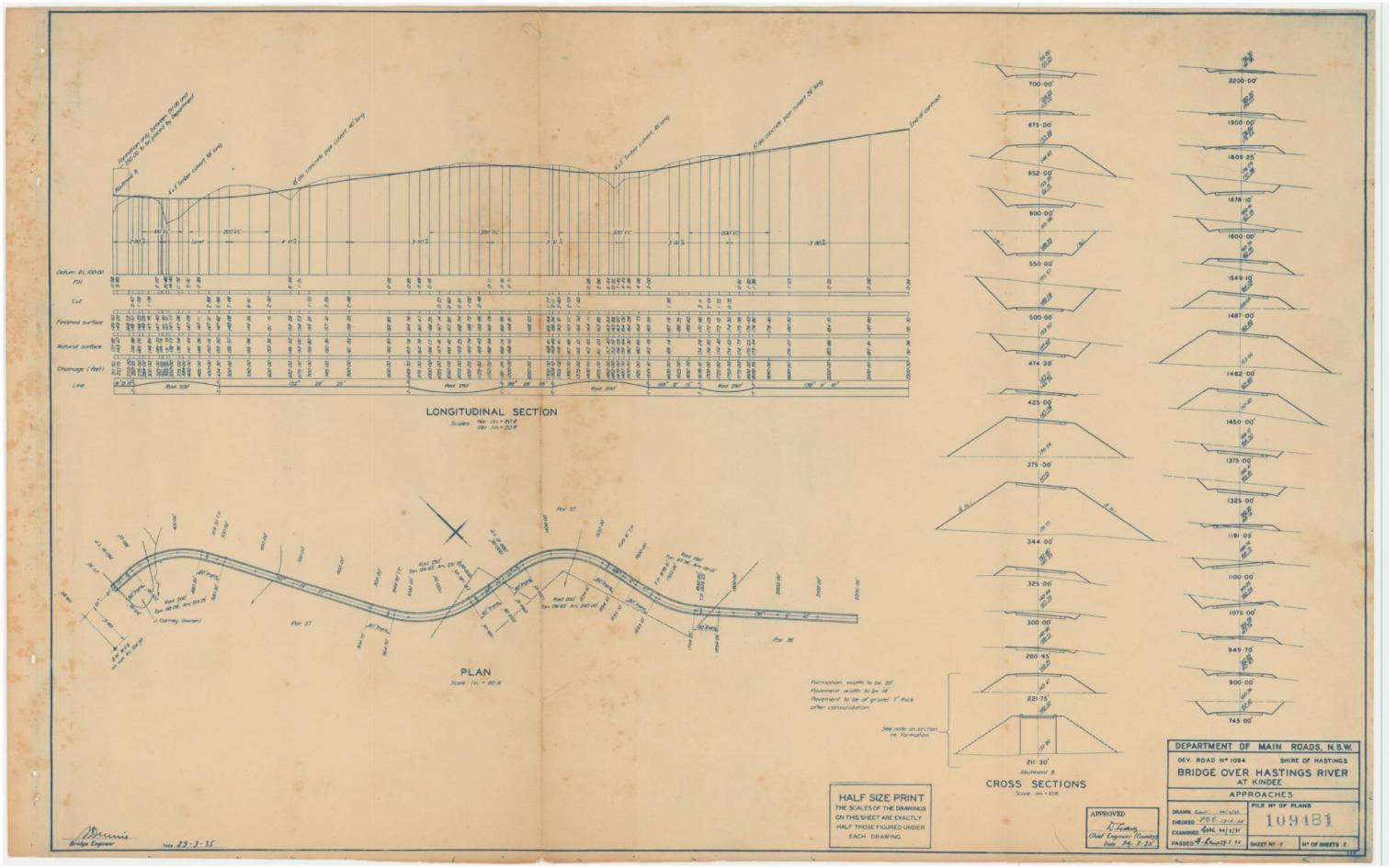
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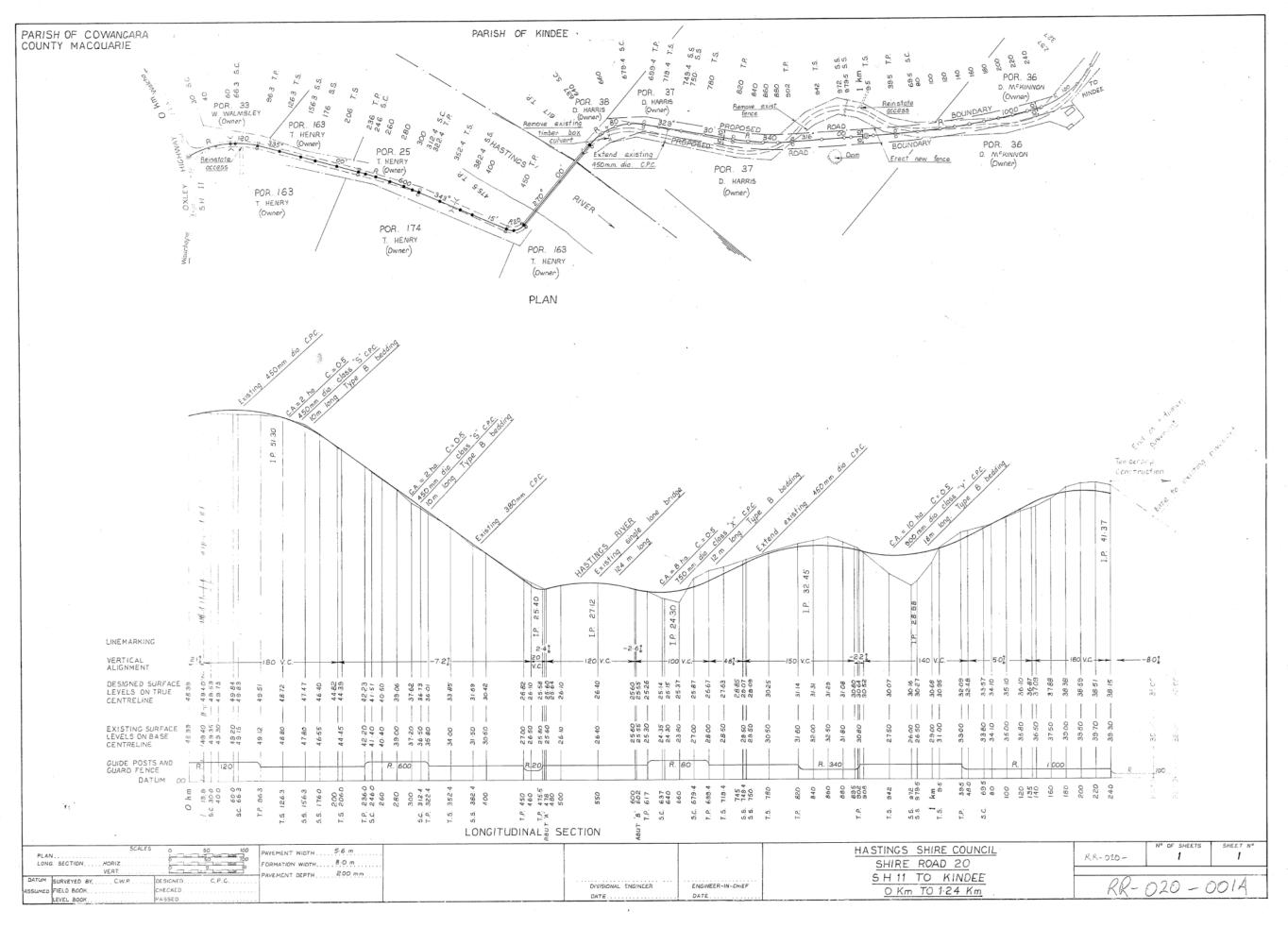


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Appendix B - Transactions of the Institute Vol. XVIII

Includes:

• Three Span Light Suspension Bridge over Hastings Rver at Kindee Crossing, N.S.W 9By Vladimir Karmalsky, B.C.E., and Alexander Theodore Britton, B.Sc., B.E (December 1937)

Dec., 1937

TRANSACTIONS OF THE INSTITUTION, VOL. XVIII.

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RECORDS

TRANSACTIONS OF THE INSTITUTION

Three Span Light Suspension Bridge over Hastings River at Kindee Crossing, N.S.W.

BY VLADIMIR KARMALSKY, B.C.E., AND ALEXANDER THEODORE BRITTON, B.Sc., B.E.

Associate Members.*

Summary.—This paper is a description of a three-span suspension bridge for light traffic recently constructed by the Department of Main Roads, New South Wales. The type of structure is considered by the authors to be very economical for the conditions at the site. The theory of the design and method of calculation are given.

SITE AND SELECTION OF TYPE.

The Hastingst River, New South Wales, at the site flows in a narrow deep gully with steep high banks; the grade of the stream is steep and the velocity high. Floods are fairly common, deep, and fast running and carry large quantities of big drift timber. The greatest rise recorded is about 43 ft., while small rises of a few feet are very frequent.

The road is purely a spur road of developmental character serving a number of dairy farms, etc., in a fertile side valley surrounded by impassable mountains and there is no possibility of through traffic developing. Traffic will always be light and the loading adopted for design was as follows :--

(a) A 10-ton (gross) vehicle, seven tons on rear axle, three tons on front axle, 11 ft. wheel base, with 25°_{0} impact for deck and stringers, 20°_{0} for cross girders and 15°_{0} for cables, etc.; or

(b) A distributed load of 60 lb. per sq. ft. with no impact.

Two proposals for bridging the river were investigated, high level and low level. The first proposal considered was the latter as the cost of the bridge itself would have been so much smaller. A low level bridge consists of a structure with its deck a few feet above low water level and will allow traffic to cross at low water or with a small rise in the river, but the bridge is submerged and impassable at time of flood. Such structures are designed to offer little resistance to flood flow and to allow drift timber to float harmlessly over in flood time. It was found, however, that the low level structure required long approaches in heavy sidling ground, diagonally down the steep river

"This paper, No. 612, which originated in the Sydney Division of The Institution, was presented before the Structural Engineering Branch of the Division on 37th August, 1927. The authors are officers in the Bridge Design Section of the Design Branch of the Department of Main Roads, New South Wales. banks, to connect the bridge with the roads on either side, and the extra cost of these approaches made the cost of the low level bridge scheme equal to the cost of the high level scheme adopted. A high level bridge (i.e., a structure above the reach of the highest anticipated flood) permits traffic to cross at all times and gives much better service; it is also less liable to damage in flood time.

To avoid trouble from drift timber, long spans were necessary and it was advisable to cross the main channel with a single span (Fig. 1); the combination of light loading, tall piers, and long spans obviously made a suspension bridge economical. Several designs were investigated and the trussed cable type as shown in Fig. 1 was adopted as the most suitable for the conditions. The trussed cable type has the great constructional advantage that the cable system is self-rigid and, if a suitable order of erection be used, the decking can be erected and supported from the cables without recourse to falsework. Stiffening girders at deck level are unnecessary. This type of structure has been used in France, and has proved satisfactory in service. The truss used for the stiffened cable system of this bridge is of a novel and very simple type; it is considered to be economical and preferable to the more complex designs previously employed and is very convenient for fabrication and erection.

DESCRIPTION.

The structure is of three spans, 88 ft., 220 ft., and 88 ft., respectively. The deck in all spans is supported from the trussed cables, and a panel length of 11 ft. is employed throughout. The structure has a through camber of 3 ft. for appearance and the clearance above highest

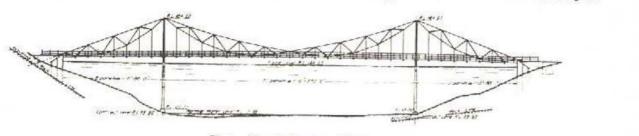


Fig. 1. General Elevation of Bridge

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wn flood is 6 ft. at the centre of the river and 3 ft. at the ments.

The cables are of best plough steel wire ropes 7 19 struction; the cable ends are secured in cast steel kets by white metal. The sockets are provided with d steel "U" bolts which pass around forged steel pins ported by cast steel bearings at each abutment and r. The cables are protected against corrosion by asultic paint. Gussers are securely clamped to the cable means of grooved cast iron packing pieces and fitted its, and diagonals and hangers are secured to the gussets pin joints. All diagonals are compression members of o angles riveted together to form a star section. Hangers : tension members of round mild steel bar.

Anchorage is provided by means of mass concrete ocks buried in the approach embankments. Anchor its are embedded in these blocks and are provided with ddles which rest on the abutment bearing pins. Where ese bolts are below ground level they are encased in sel piping filled with concrete to prevent corrosion.

To reduce weight and first cost, the deck is of timber misiting of transverse planking and longitudinal timber ringers carried on timber cross girders at the panel points. he hangers pass through holes drilled in the cross girders ad each is provided with a pair of lock nuts and a large asher to support the cross girder; a 12 in length of thread cut on each hanger so that the cross girders can be acurately adjusted to the correct height for the camber. 'imber wind bracing is provided in the plane of the deck or the full length of the bridge. As the cables are always ubjected to heavy tension, and the trusses only offer a mall exposed area to wind, bracing is not required for he suspension system.

Piers consist of braced timber trestles supported on concrete blocks. To prevent drift lodging in the spaces between the piles, the piers are sheathed with timber to lood level. Abutments are of the normal type for timber bridges, i.e., driven piles and sheathing.

In conformity with the New South Wales Department of Main Road's standard practice for timber bridges, no expansion joints are provided in the timber deck. The truss system for the centre span consists of an inverted three hinge arch, and is, therefore, able to expand and contract freely with temperature, while the side spans are equivalent to simple trusses. The effect of temperature changes in the truss system will be to produce horizontal movements of the pier tops and vertical movements at the centre of the main span and the structure has ample flexibility for these movements.

DESIGN OF TRUSSES.

(a) General :— The centre span truss is an inverted three hinge arch and is, therefore, statically determinate. The stresses in the centre span truss are unaffected by loads on the side spans and the funicular polygon for any system of loading will pass through the three hinges A, L, and A'.

The side span truss carries out two functions; first, it acts as a simply supported truss for loads on the span itself, and secondly, it acts as a back stay to support the horizontal tension in the centre span truss due to loads on the centre span. (The piers are too flexible to take any appreciable horizontal forces). It is not stressed by loads on the other side span.

It was found that, if the side spans were made equal to half the main span (as an attempt to make the lay-out of all trusses identical) the depth of truss and cable section required for the side span were uneconomically great for the centre span. The economical ratio of centre span to side span for bridges of this type and span was found to be about 2.5 to 2.7.

There are two stages in the design, first, the adoption of a suitable truss lay-out, and secondly, the determination of stresses and the sections of members. The first stage includes the selection of span lengths as discussed above and the "rise" of the main span. The "rise" (or vertical distance of L below AA') is governed by economy and appearance, increasing the "rise" reduces the stress in the main cables and, therefore, their cost, but increases the Trial designs showed cost of piers, hangers, and diagonals. that, like most economic problems of a similar nature, over quite a large range of rises the difference in cost was negligible, and to produce any appreciable difference the structure had to be made either unreasonably squat or unreasonably tall. The proportions selected are near the middle of this economic range and were chosen to give a satisfactory appearance and fully stress a large wire rope cable that could conveniently be spun.

It will be found generally that these proportions will be suitable for other designs, but that for maximum economy :---

(i.) The ratio of rise to span should be reduced slightly for longer spans and increased slightly for shorter spans.

(ii.) The ratio of rise to span should be increased alightly for heavier loading.

Having determined on a suitable rise ratio it is then necessary to determine the shape of the trusses. This is best and most simply carried out by the ordinary graphical method of funicular polygons as explained below.

The second stage of the design is the determination of the stresses in the members of the trusses and the selection of suitable members for these stresses. This is most conveniently treated by means of influence lines. The method of preparing these influence lines given below he recommended by the authors for this type of structure, being simple (and, therefore, free from liability to serious error), rapid and of sufficient accuracy for even the most important of structures. In ordinary cases the lengths required for the calculation of main ordinates can be scaled from the drawing with sufficient accuracy, the influence lines drawn to scale and the intermediate ordinates scaled. If desired, the same method can be used but all lengths and ordinates can be computed to any accuracy desired.

(b) Truss Lay-Out :- The cables are incapable of carrying any compression stress and it is necessary to arrange the truss lay-out so that no cable member can be thrown into compression by any possible combination of loads. In the present design the only possible combination is lowed and impact plus dead load. The condition is governing failure by collapse of a cable are very different from failure by overstress. In the latter case, no harm will result if a single vehicle rather heavier than the designed load crosses the structure provided the stresses do not approach the clastic limit and that the load is applied conce slightly greater than the load producing reversal may cause total failure of the structure. For this reason, a margin of safety of 50°, against collapse is provided, i.e., the cables should not be thrown into compression until the live load

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C., 1937.

LIGHT SUSPENSION BRIDGE Karmalsky & Britton.

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and impact are increased 50% or, as a more convenient method of calculation, the combination of full live load and impact with two-thirds of the dead load should not produce compression.

The necessary and sufficient condition for no compression in the cables is that no funicular polygon, drawn for any combination of live load and impact plus two-thirds dead load, should fall above any upper truss panel point or below any lower truss panel point. The first step in the design is, therefore, to draw the governing funicular polygons.

Figure z shows typical funicular polygons for both spans; the construction of these polygons is familiar to all engineers and requires no explanation. In the design

heavily stressed on such a lay-out, it will be economical to vary the lay-out to reduce this stress and permit the adoption of a smaller cable throughout. If at any point one cable be moved further from the dead load polygon, the effect will be to reduce the maximum stress in the cable moved and to increase the maximum stress in the other cable by a smaller amount.

In the present design, it was found that the stress in the upper side span cable was much greater than all others if all trusses were made of minimum depth. To reduce this stress the upper cable was raised as much as was possible, i.e., to a straight line between centres of bearings. Incidentally this simplifies the calculation as the horizontal tension from arch action in the main span only produces

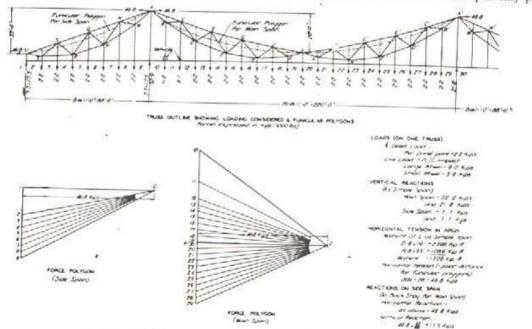


Fig. 2. - Truss Outline and Construction of Typical Funicular Polygons.

under discussion, the loading which gave the governing polygons was as follows :---

Side span; top chord :- Full distributed load on main span. Side span; bottom chord :- Full distributed load on side span. Main span; top chord : Distributed load on rather more than half main span; top chord :- Distributed load on rather more than Main span; bottom chord :- Concentrated load at point concerned.

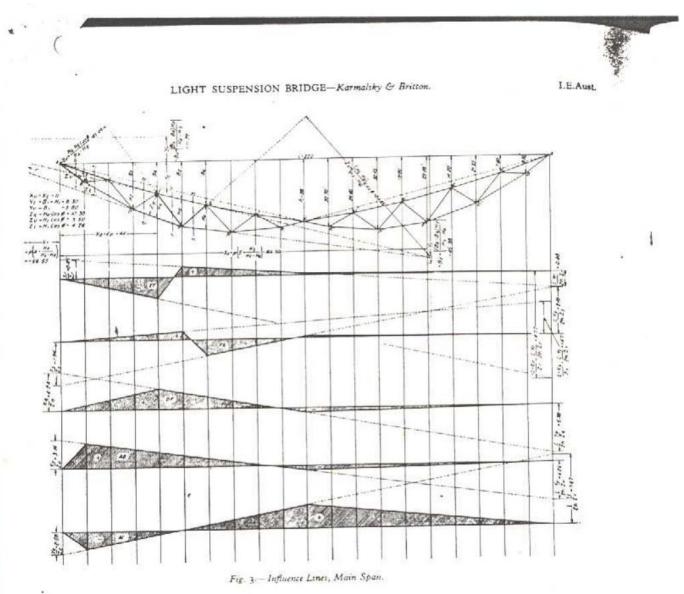
If the bridge is designed for a different loading, different critical conditions will occur; the actual worst loading can readily be determined from the influence line for stress in the member of the other chord at the point concerned.

It will usually be found that the most convenient lay-our for each cable is a series of chords of a parabola, and that the shallowest possible truss will be the most economical as the lengths and sections of the diagonals will be reduced. The cables, however, have usually to be specially made and it is advisable to keep the same size cable throughout ; if, therefore, on determining the stresses in the cables, it be found that one cable is particularly

tension in the upper side span cables and does not affect the other members of the side span.

(c) Influence Lines for Stresses in Members :- The main span is an inverted three hinge arch, and, therefore, the vertical reactions at A and A' are the same as for a simple beam of the same span while the horizontal reactions at A and A' are equal and are equal to the moment at the centre point L of the span divided by the "rise" or vertical distance between the hinge L and the line A-A'. Loads on the side spans produce no stresses in the main span.

Proceeding as in the ordinary method of sections (i.e., if the stress in any member is required, a section is taken intersecting the member required and in generaltwo other members ; the stress in the member may then be determined by considering the equilibrium of the portion to the left of the section ; taking moments about the point of intersection of the other two members-produced if necessary-the stress in the member times its perpendicular distance from the moment point must equal the sum of



the moments of the external forces acting on the left hand portion of the truss about the moment point) let :-

- tion of the truss about the moment point) let :-l = length of main span;
 h = rise of main span;
 l₁ = length of side span;
 h₁ = rise of side span;
 h₂ = length of panel;
 p length of panel;
 p length of panel;
 m length of panel;
 p length of panel;
 M moment points from A;
 H₁₀ H₂₀, H₃₀, etc. = depths of truss at same points;
 x = horizontal distance of a unit load applied to the bridge deck from A;
 W = vertical reaction at A (always upward);
 H = horizontal distance of moment point from A (positive if to right of A);
 Y = vertical distance of moment point from A (positive if below A);
- Z
- below A),
 perpendicular distance of moment point from member in which stress is desired;
 stress in member (positive if tensile).
 Note: -X, Y and Z will be identified for various mem-bers by subscripts. F

Considering first a typical chord member DF and drawing section qq the other two members cut are DE and CE. See Fig. 3 Therefore, E is the moment point with CE. See Fig. 3 Therefore, E is the moment point with co-ordinates X_{ϕ} and Y_{ϕ} determined from the co-ordinates

adopted for the chords and with Z_e as shown on the drawing which can be readily sealed or calculated. (Note - $Z_q = H_q \cos \varphi$, where φ is the angle the member *DE* makes with a horizontal line).

Assume the load of t lb. to be placed between the "cut-panel" DE and the centre hinge L. Then :—

VМ 2 H 2h

Taking moments about E:

F.

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Dec., 1937.

This is clearly a straight line which may readily be plotted by taking the values :-

$$F = \frac{X_q}{Z_q}, \text{ when } x = 0$$

$$F = -\frac{i}{2h}, \frac{Y_q}{Z_q}, \text{ when } x = i$$

Note : - These are not real points on the influence line but merely convenient points for plotting. If now the load a

panel:
F
$$Z_q = V.X_q - H.Y_q - i(X_q - k)$$

F $+ \frac{V.X_q - HY_q - i(X_q - k)}{Z_q}$
 $= \frac{X_q \left(1 - \frac{x}{l}\right) - Y_q \left(\frac{x}{2h}\right) - (X_q - k)}{Z_q}$

 $\frac{x}{Z_q} = x \left(\frac{1}{l} \cdot \frac{X_q}{Z_q} - \frac{1}{2h} \frac{Y_q}{Z_q} \right)$ This is clearly a straight line passing through the

values o when x = o (i.e., at A) and intersecting the previous line (Equation t) at $x = X_e$ (i.e., at the moment point). It can be shown by similar reasoning that if a load

is placed on the right hand portion of the span the influence line will be a straight line passing through the value o at A' and intersecting the first line at L.

Also (as in simple trusses of similar style) the fourth or closing line drawn across the "cut panel" must coincide with one of the first two lines.

The influence lines for chords can, therefore, be drawn by the following simple procedure :--

The section cutting the member, determine moment point and distances X, Y, and Z.

2. Calculate ordinates of first line, i.e., $\frac{X}{Z}$ and $-\frac{IY}{2\hbar Z}$ and lay them off at A and A' on opposite sides of the datum line. 3. Join these two plotted points and the portion between moment lines to A and A'.

It will be seen that no attention has been given to sign of stress in the above, but although the determination of sign by a rule involving clockwise and counterclockwise aspect of member and moment point is possible, the following simple rules are to be preferred as they avoid complicated thinking and avoid all possibility of mistakes in sign :-

For Chord Members :--

1. The first line should always cross the datum line somewhere between A and A', but the influence line may or may not cross the datum. 2. The larger area is the positive (or tension) area; the area on the other side of the datum is negative.

No difficulty will be found in identifying the moment points for intermediate chord members but for end chord members the moment point is :-

For member AC the point B. "AB the point B₁ in top chord, vertically above B. "JL the point K. "KL the point K, in top chord vertically above K.

It will be found that unless chords differ considerably from a parabola the highest stress in the top chord will occur in AC and in the bottom chord in AB. Influence lines for these members (sections 11 and uu and distances

 $X_i, Y_i, Z_i, X_i, Y_i, Z_i$, respectively) are shown as well as the influence line for a typical member DF.

Considering now a web member such as FG draw the section is and produce the other two cut members FH and EG to meet in the moment point S

The co-ordinates X, and Y, and the distance Z, can be scaled but (on account of the poor intersection, the chords being nearly parallel) they are better calculated by

$$\begin{split} X &= \beta \left(n - \frac{H_n}{H_n} - \frac{H_n}{H_n - 1} \right) \\ Y &= B_n - \frac{(B_{n-1-1} - B_n)}{H_n - H_{n-1}} \\ Z &= \left(-\frac{H_n}{H_n} - \frac{H_{n-1-1}}{H_n - 1} \right) \cos q \\ \text{(positive value always disregarding sign,} \end{split}$$

where q = angle between member in which stress is required and a horizontal line and the section is taken between the panel points n and (n - 1).

The previous calculation of the influence line for the chords will still apply; it will be seen, however, that since

the moment point will not be at one end of the cut panel, the closing line across this panel coincides with neither

Also it will be found that in many cases the moment point for the diagonal is so placed that it is not possible to draw satisfactorily the second line by this method; this difficulty can be overcome by calculating the ordinate at A' for the line produced; from Equation 2 this ordinate is (putting x = l):-

$$\frac{1}{Z} - i \left\{ \frac{1}{I} \cdot \frac{X}{Z} - \frac{1}{2h} \cdot \frac{Y}{Z} \right\} = \left\{ \frac{1 - X}{Z} - \frac{1}{2h} \cdot \frac{Y}{Z} \right\} \dots \dots (3)$$

The direction of allows

The direction of plotting can readily be seen from the fact that the second line should intersect the first at the moment point. The method of drawing the influence line for web members will, therefore, he as follows :-

1. Take section cutting the member, determine moment point and calculate distances X, Y, and Z.

2. Calculate ordinates of first line, i.e., $\frac{X}{Z}$ and $-\frac{IY}{2hZ}$ and lay

them out on opposite sides of datum line at A and A, respectively, (unless the moment point falls above and to the right of A or below and to the left of A when X and Y are of opposite sign and these ordinates should be plotted on the same side of the datum line.

3. Join these two plotted points and the portion between the cut panel and L will be the first part of the influence line.

4. Mark out the ordinate
$$\left(\frac{l-\chi}{2} - \frac{l}{2}, \frac{Y}{2}\right)$$
 as d

second line from this point to zero at A meeting first line at the moment point. This will be the portion of the influence line between A and the cut panel.

5. Draw the closing lines across the cut panel and line from L to 1

Note as before sign, etc., should be determined and checked by the simple rules :--

I. The first line should generally cross the datum line between A and A but not if the moment point lies above and right or below and left of A.

2. A load at the left hand end will produce a tension (generally) and a load at the right hand end a compression (generally) in a member sloping up to the right or if the stress in some unusual layout is the same sign for a load at both ends of the member a greater

ATTACHMENT

LIGHT SUSPENSION BRIDGE - Karmaliky & Britton.

ordinate at the right hand end means both are compression and a greater ordinate at the left hand means both are tension. (Vice versa for members sloping down to the right).

It may happen that the chords are parallel and do not intersect. In such case the same method may be used for calculating the first line replacing :-

$$\frac{X}{Z} \text{ by } \frac{\cos \alpha}{\sin \beta}$$

$$\frac{Y}{Z} \text{ by } \frac{\sin \alpha}{\sin \beta}$$

where a is the angle between the chords and the horizontal, and β is the angle between the chords and the web member.

And making the second line drawn (i.e., the line from A to the cut panel) parallel to the first line.

As previously stated the side span carries out two functions :à

(a) It acts as a tie to resist the horizontal reaction of the main span; and (b) It acts as a simple truss for loads on itself.

2.11

In the present design the upper chord of the side span is made straight. This will be found to be preferable to The a sagging chord in all except extremely long spans chord can be built considerably lower without risk to collapse (and thus reduce the truss depth) but this will increase the stress carried by this chord (which is even now the heaviest stressed cable in the structure) and will require either a heavier cable throughout or the adoption of two kinds of wire rope for the cables.

It is not desirable to raise the cable above the straight line as it makes the structure much less pleasing in appearance and reduces the stability of the side span trusses against overturning.

A straight upper chord in the side span greatly simplifies the calculation as the horizontal force from the main span produces only a tension in this upper chord and does not affect any other member in the span.

The tension in the upper chord due to loads on the main span is given by :-

$$F = H \sec \varphi$$

(where q = inclination of upper chord of side span to the horizontal)

or
$$F = \frac{M}{h} \sec \varphi$$

Now the influence line for M is two straight lines from A to A' intersecting at L with an ordinate at the centre equal to 1.4.

Therefore, the influence line for stress in the upper chord of the side span for loads on the main span is two similar straight lines with a centre ordinate,

$$= \frac{1}{4h} \cdot \sec q$$
$$= \frac{1}{4h!} \cdot \sqrt{h!^{r}} = 1$$

The other portion of the influence line and the influence lines for other members of the side span truss are the ordinary influence lines for a simple truss of span lin and the method of construction is shown in Fig 4.

If the lower chord be parabolic the greatest chord stresses will occur in AM and AN and no other chord members need be checked.

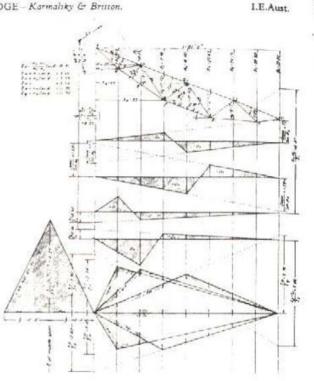


Fig. 4. Influence Lines, Side Span

(d) Pier and Abutment Reactions :- The total vertical reaction of the trusses on the pier is the sum of the following

(i.) Vertical reaction from main span (equal to reaction of simple span length ();

(u.) Vertical reaction from supporting tension balancing H $\frac{M}{h} \cdot \frac{h_1}{I_1}$ in main span (equal to H tan q -

where M = moment at L as simple beam span l; $\varphi = \text{inclination of top chord of side span to the horizontal};$ Ŧ and
 h, h_i, l_i as before)
 (iii.) Vertical reaction from loads on side span (equal to reaction)

of simple beam span l_1).

This reaction is greatest with both the main and side span loaded.

The horizontal reaction at the pier is zero and the piers must be designed and constructed so as to take no horizontal force

The tie rods beyond the abutment are placed in the line of the upper chord and the abutment also is relatively flexible, and takes no horizontal force. Therefore, vertical reaction from trusses on abutment is the vertical reaction for a truss of span l_1 .

The tension in the tie rods and the force on the anchor blocks (since no horizontal force is taken by the abutments or the pier) is the force necessary to resist the horizontal reaction of the main span,

1.C.

$$F \sim H \sec q$$

or as before,
$$F = \frac{M}{h} \sec q$$

$$= \frac{M}{hl_1} \sqrt{h_1^{*} + l_1^{*}}$$

3

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DETAILED DESIGN OF PARTS.

(a) Deck, Piers, Abutments :- The deck, piers and abutments are of ordinary timber construction designed for the light load and require no description. See Fig. 5.

(b) Hangers :- All hangers are round mild steel bars with threaded ends and are designed on a permissible stress of 18,000 lb. per sq. in. on the core area of the threaded portion. See Fig. 6.

(c) Cables :—The cables are designed on their ultimate strength using a factor of safety of $3\frac{1}{2}$ and the "U" bolts for sockets on core area of threaded portion.

(d) Diagonals :- The diagonals are designed as pin ended struts, i.e.,

Maximum / r ratio = 120 · Permissible stress = 18,000 - 80 / r

In the present design the former condition always governed and stresses were negligible. A star section of The stress increments are transferred to the cable by means of friction, special clamps being used to grip the cable without causing deformation. (f) Anchor Bolts :- The anchor bolts have also to

(J) Anchor Bolts :- The anchor bolts have also to resist the horizontal tension from loads on the main span. Since the side span acts as a simply supported truss it produces no horizontal force at the abutment support.

The anchor bolts are again threaded bars designed on core area. They are protected against corrosion below ground by enclosing in large diameter galvanised pipes, filled with concrete, which extend into the concrete anchor blocks in which the lower ends of the bars are embedded.

(g) Bearing Pins :- The bearing pins are ordinary mild steel pins grooved for bolts, etc., and are designed for bending.

(h) Anchor Blocks:—The anchor blocks are designed to resist the horizontal forces from cable tension by the passive earth pressure developed on the front face and friction on the base. As is usual in such structures a factor

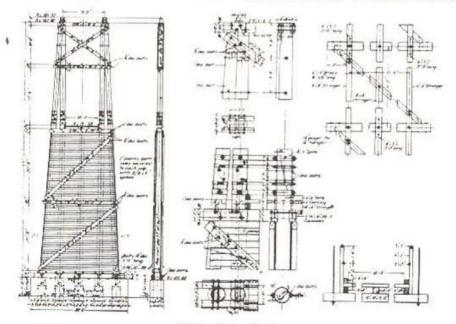


Fig. 5 .- Timber Details.

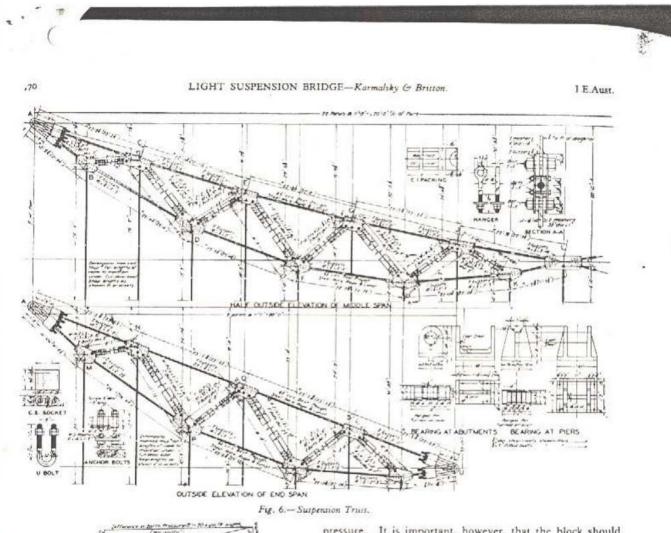
two angles was found to be most economical. To prevent local buckling, the minimum thickness of any leg was made one-twelfth of its width as required by design specifications for steel bridges.

(e) Connections: The connections of the diagonals in conformity with the Main Roads Department's standard specification for design ("Connections shall be designed either to develop the member or to carry one and onehalf times the calculated stress") were designed for one and one-half times the calculated load. Simple pin connections were used, the permissible stress for bending in short pins being 27,000 lb. per sq. in. To avoid a multiplicity of sizes only two sizes of pin were used.

The hanger connections are also simple pin joints designed on bending moment; here the member is fully stressed and the connection is only designed for the capacity of the member. of safety of 1.5 against sliding is provided by increasing the tension in anchor bolts by 50% for calculation purposes. The analysis is best carried out graphically as shown in Fig. 7.

The tension A in the anchor bolts (total for one abutment) to be designed for is 275 kips plus $50^{\circ}_{\circ\circ}$, equals 412 kips or 22.9 kips per ft. width of block. The weight W of the block acting downwards including overlying earth is 12.3 kips per ft. width. These two known forces are plotted first on the space diagram and force polygon which show the equilibrium of the anchor block plus overlying earth (inside dotted line boundaries), and the magnitude and position of their resultant is found in the usual way.

This resultant must be balanced by the total difference in horizontal earth pressure E and the reaction under the base, R. The first is clearly horizontal while the second acts at angle to the vertical determined by the coefficient



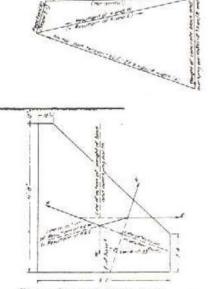


Fig. 7. - Design of Anchor Blocks.

of friction between the base and the ground (assumed at 0.33) hence the magnitudes of these forces are readily determined from the force polygon.

It is clearly not necessary to check the bearing pressure on the base as the effect of the cable pull is to reduce this pressure. It is important, however, that the block should be of sufficient weight to prevent R becoming negative.

The position of the line of action of the active earth pressure on the back face is $d \ge a$ above the base (where dis the depth of the base below surface) and the passive earth pressure on the front will also act at this height in the present design (the incomplete development of passive earth pressure in the small thickness of ground above front of block may be neglected). The lines of action of R and E can thus be determined on the space diagram.

For safety it is necessary that R should fall inside the middle third of the base and that the permissible value of passive pressure should not be exceeded. The first is readily checked from the space diagram and the second from the value of E determined from the force polygon :-

E = 20.0 kips per ft. width.

Active earth pressure on back face of block and overlying earth

100 \cdot 11¹ \times $\frac{1}{2}$ \times 0. 1.7 kips per ft. width. 0.283 1,000

Total passive pressure developed = 20.0 - 1.7= 217 kps per it width Permissible passive pressure (100 - 11' + $\frac{1}{2}$ '0.28

(100 · 11' · 10.283)/1,000.

= 21.4 kups per ft. width Theoretically, it would be economical to reduce the width of the anchor block considerably and to increase

slightly the height but in mass concrete it was not considered advisable to have a steeper slope than 45 degrees for the back face (to avoid developing tension stresses).

It is also necessary that the block should be placed so far back from the abutment that the friction between

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earth in front of the block is sufficient to develop the assive resistance. For this calculation the earth is assumed to slide on a horizontal plane at the level of the base of the block assuming the earth moved is a width in plan equal to the width of the block at the face of the block thence spreading out on each side at an artigle of 4s degrees with the direction of movement until the face of the embankment is reached and the ratio of horizontal resistance required to weight of earth moved should not exceed the coefficient of internal friction of earth fill (i.e., 2 3)

CABLE DETAILS.

The only parts in which trouble is likely to arise in construction are the cable attachments. These details are not usually employed in this country and careful supervision and direction is necessary.

The Department of Main Roads has found that the form and dimensions of the socket given in Fig. 6 is the minimum required to ensure proper socketing of ropes

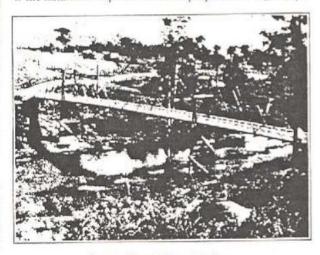


Fig. 8 General View of Bridge

for the heavy direct stress imposed in suspension bridge work, smaller sockets are used generally for lift and haulage work where, owing to bending over sheaves, etc., a much smaller direct tension is carried by the rope. The parallel portion of the socket is advisable to ensure that the rope is concentric and co-axial with the socket.

For the same reason it is necessary to carry out the socketing operation with greater care than is usually given to ordinary lift and haulage ropes. The rope should be seized for the length of the parallel portion of socket and the seized rope should be a close fit in this portion. For the coned part the individual wires should be carefully and thoroughly separated and spread uniformly to fill the cone, thoroughly cleaned and washed using petrol to remove all grease and dirt and the rope drawn tight into the socket. The socket should then be heated with blow lamps to allow the white metal to run, the parts in contact with white metal sprinkled with resin to serve as a flux and a suitable hard white metal with a low melting point inelted and poured in to fill the coned portion.

These proportions and the method of testing were adopted after tests of sockets on the actual rope used had

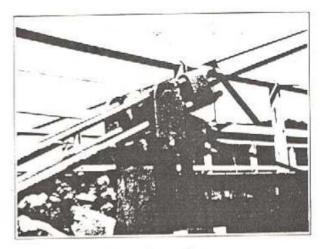


Fig. 9. Aburment Bear on.

been carried out at the Engineering School of the University of Sydney

Care should be exercised in the manufacture of the clamps at intermediate panel points to ensure that sufficient clearance is left between the two balves of the clamp to prevent the halves coming into contact before the rope is properly gripped, and in their erection to ensure that the tightening bolts are fully drawn up to develop the necessary friction.

Rope lengths should be measured in the shop under sufficient tension to remove initial spring in the rope. Allowance should also be made for the stretch of the rope under dead load, when erected ; the modulus of elasticity 'determined by the University tests mentioned above') for 7 19 rope is about 12,000,000 lb per sq. in.

ACKNOWLEDGMENTS

The authors desire to thank Mr. D. Craig, M.I.E.Aust., Deputy Commissioner for Main Roads. New South Wales, for permission to make use of official documents and to publish this paper.

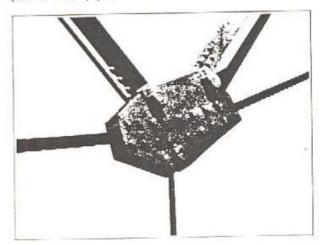
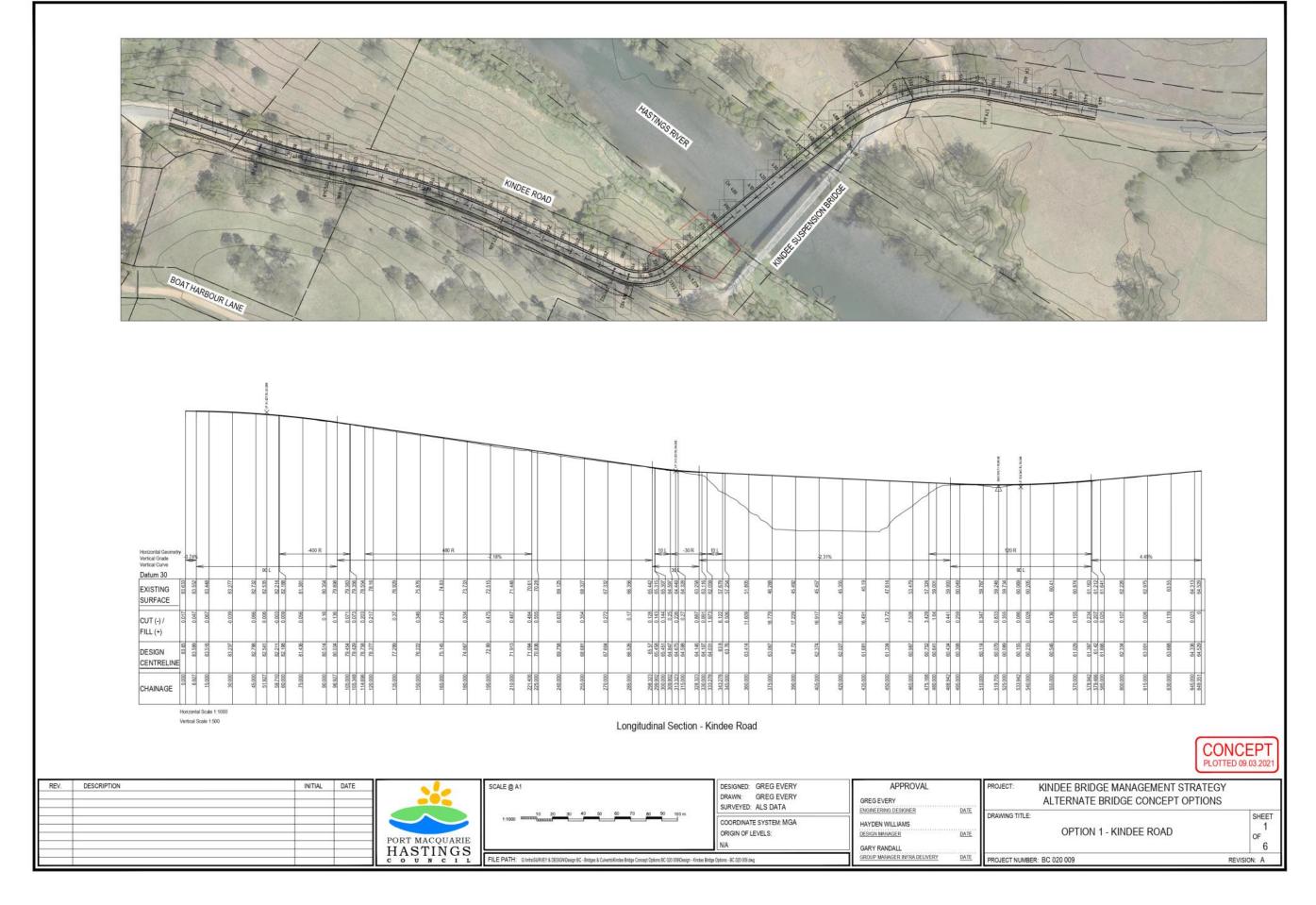


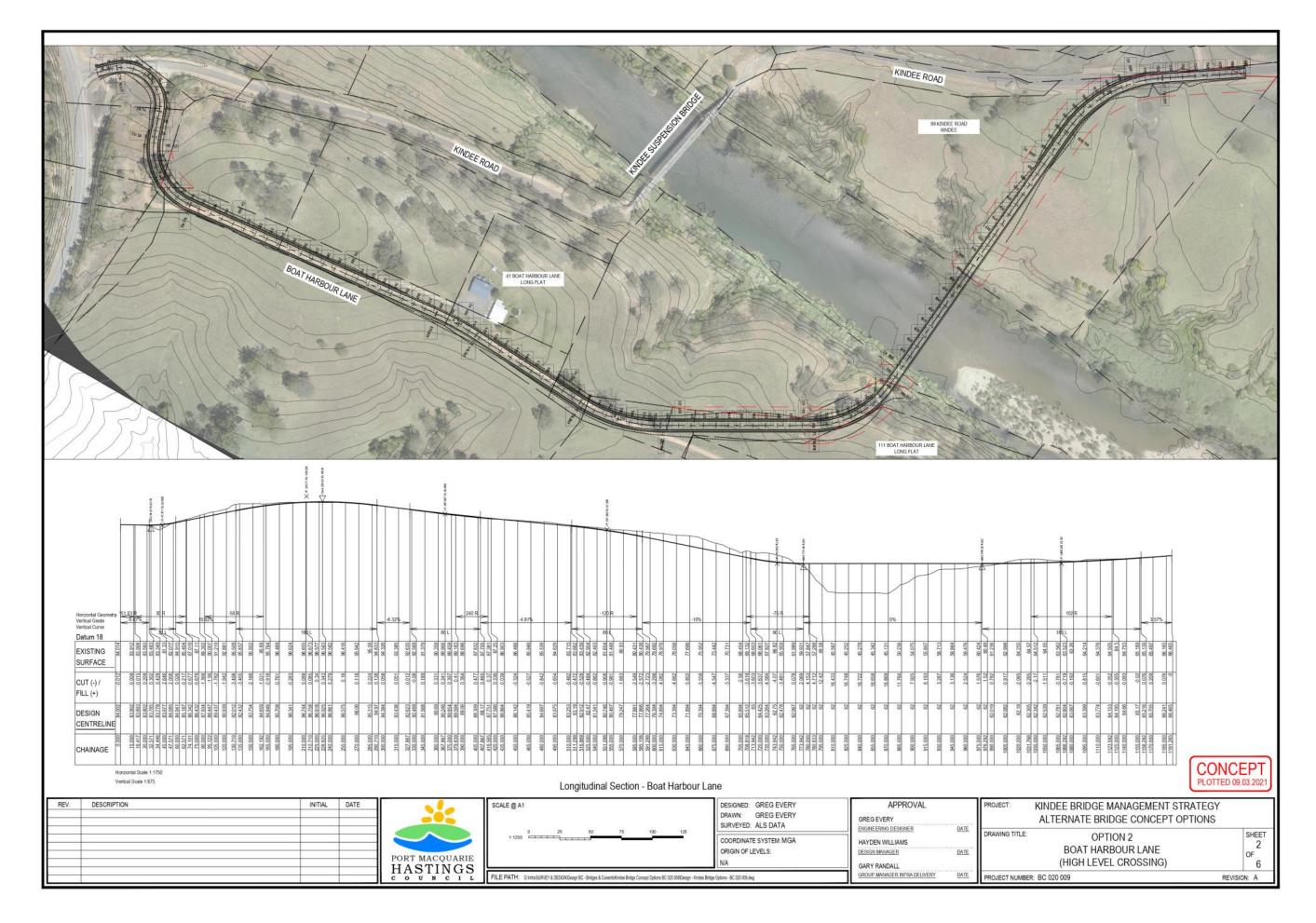
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Appendix C – Strategic Concept Drawings

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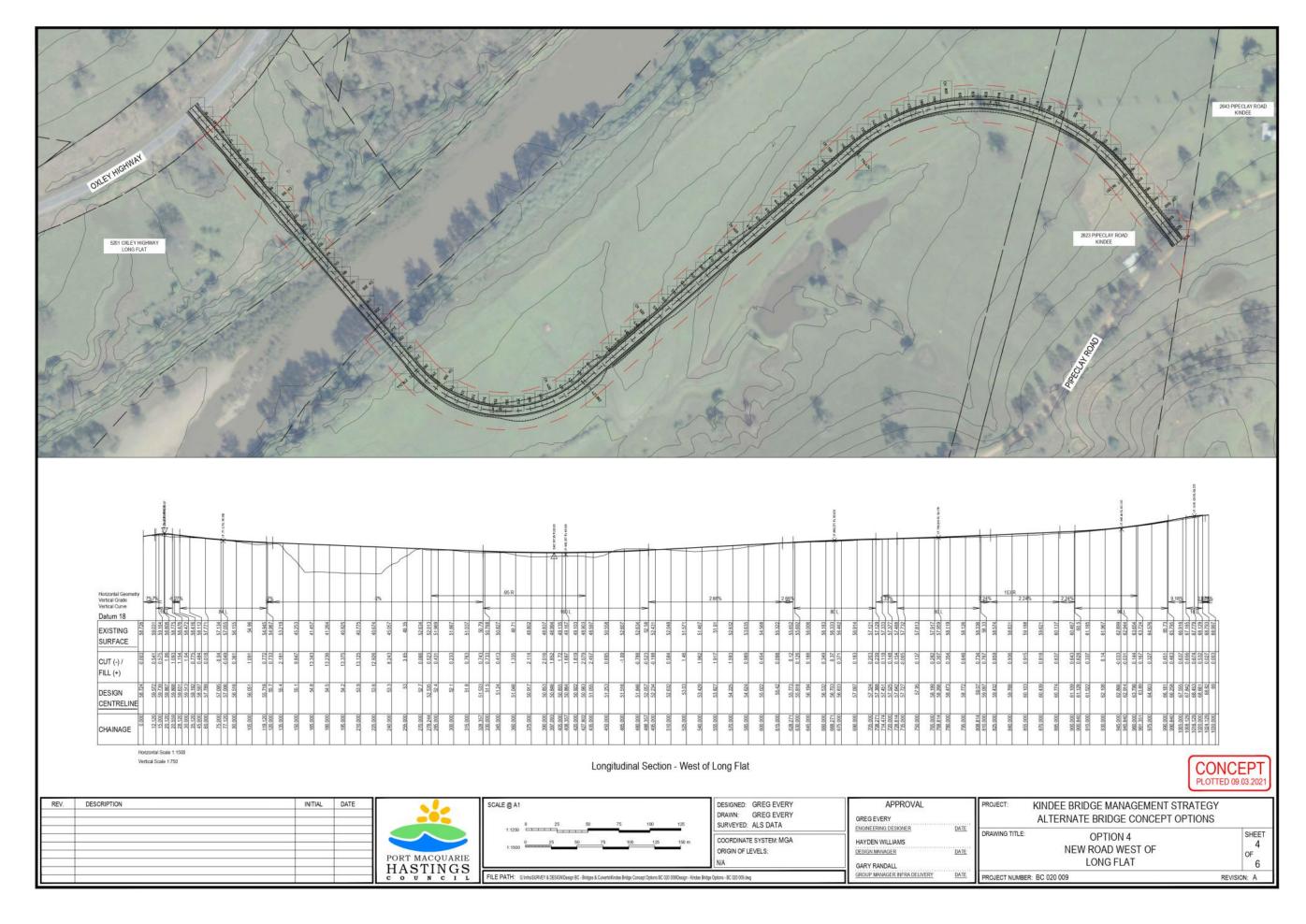
- Sheet 1: Option 1 Kindee Road (Option B per report), PMHC
- Sheet 2: Option 2 Boat Harbour Lane (High Level Crossing) (Option C1 per report), PMHC
- Sheet 3: Option 3 Boat Harbour Lane (Low Level Crossing) (Option C2 per report), PMHC
- Sheet 4: Option 4 New Road West of Long Flat (Option E1 per report), PMHC
- Sheet 5: Option 5 New Road East of Long Flat (Option E4 per report), PMHC
- Sheet 6 Waterway Areas, PMHC

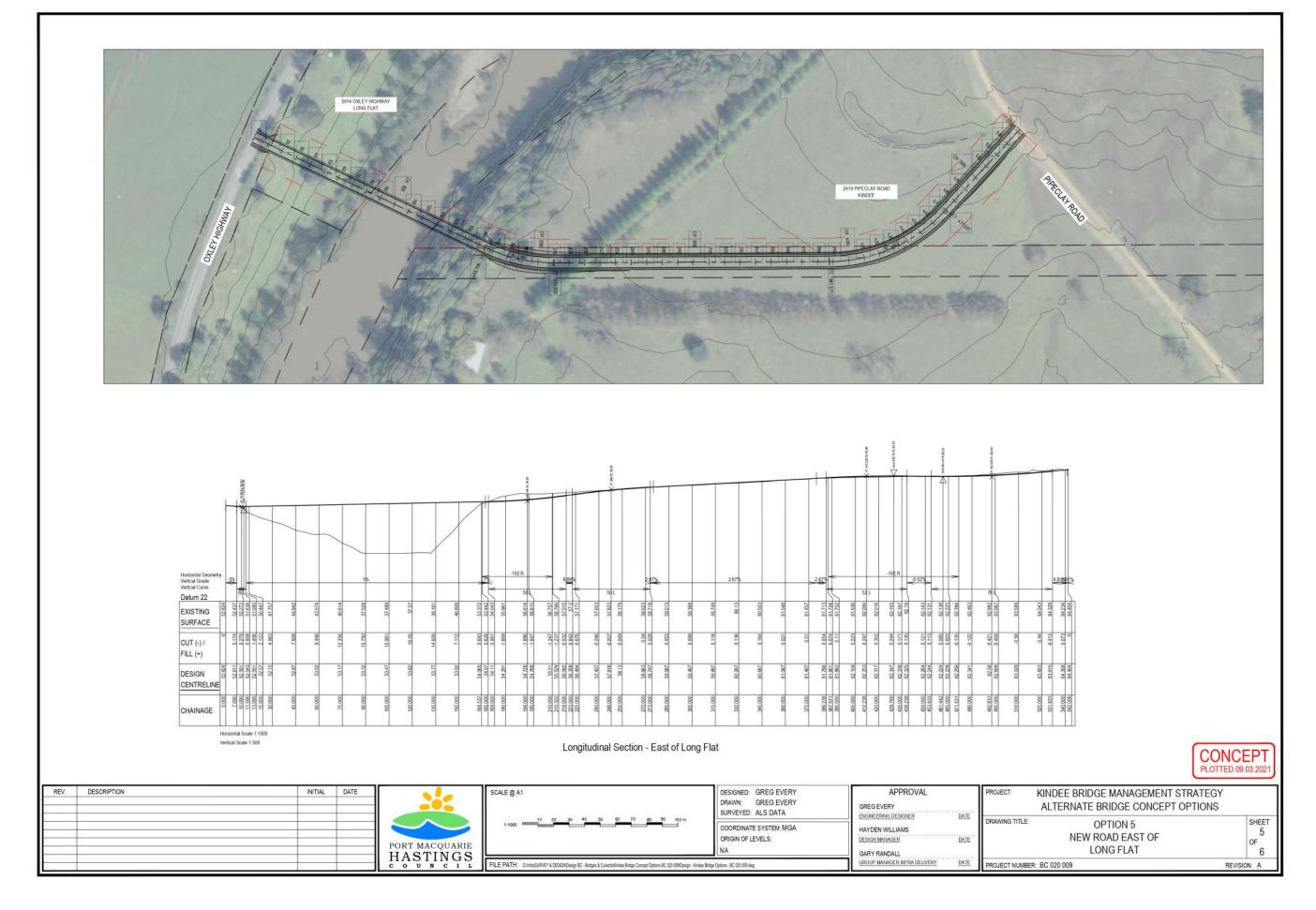


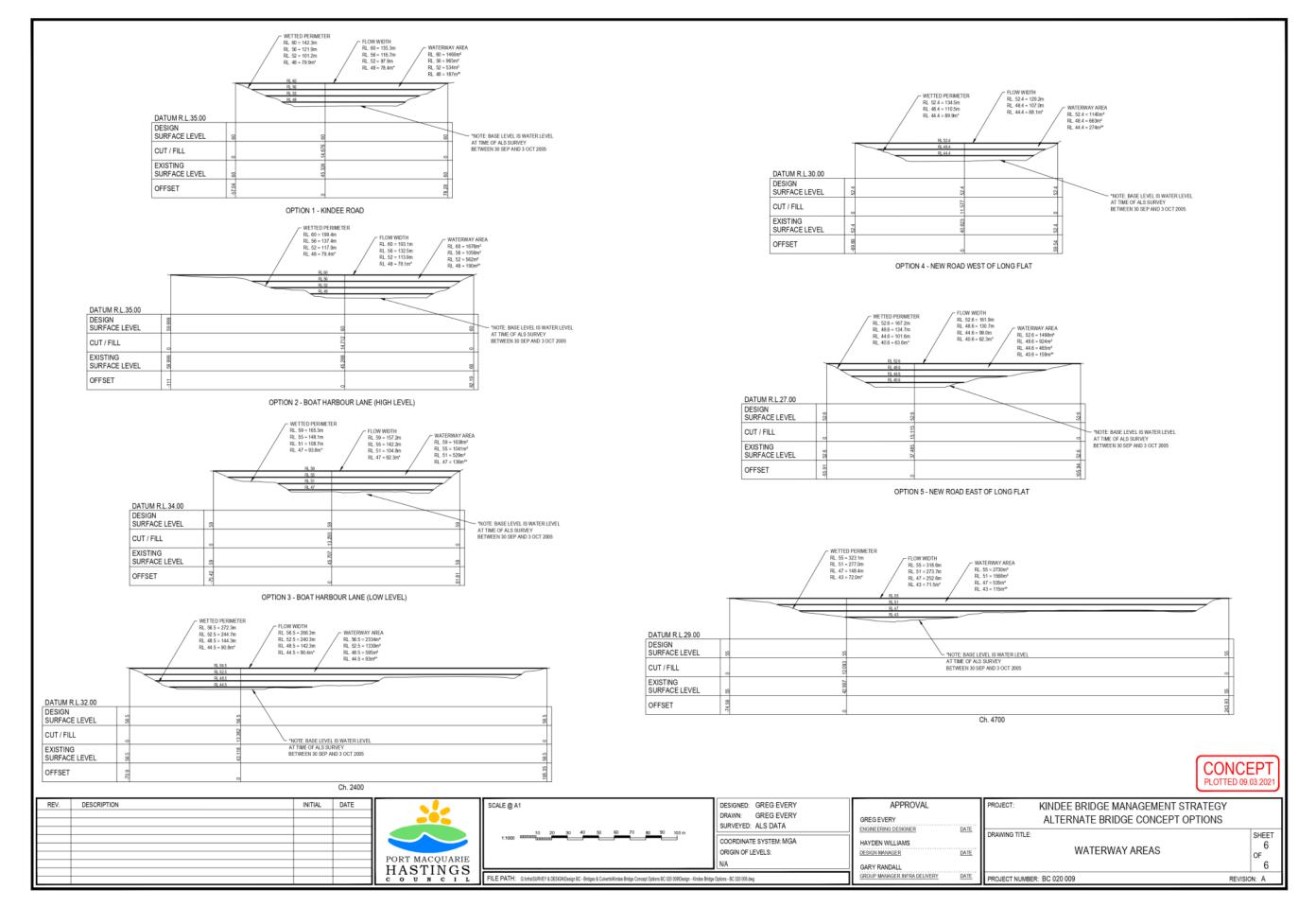


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Appendix D – Multi-criteria Assessment (MCA)

Includes:

Kindee Bridge Strategic Options Multi-Criteria Assessment, PMHC



Infrastructure Planning

Kindee Bridge Strategic Options Multi-Criteria Assessment

The following memo has been prepared to summarise and describe the methodology utilised for the multi-criteria assessment completed for the Kindee Bridge Strategic Options.

Twelve (12) high-level route options were identified for crossing the Hastings River between between Ellenborough and Long Flat. These options were short-listed to five (5) options for further consideration.

The short-listed options were as follows:

- 1. Option B New bridge adjacent existing bridge
- 2. Option C1 Boat Harbour Lane to Kindee Road High Level
- 3. Option C2 Boat Harbour Lane to Kindee Road Low Level
- 4. Option E1 Oxley Highway to Pipe Clay Road west of Long Flat
- 5. Option E4 Oxley Highway to Pipe Clay Road east of Long Flat

Refer to the Figures relating to the above options.

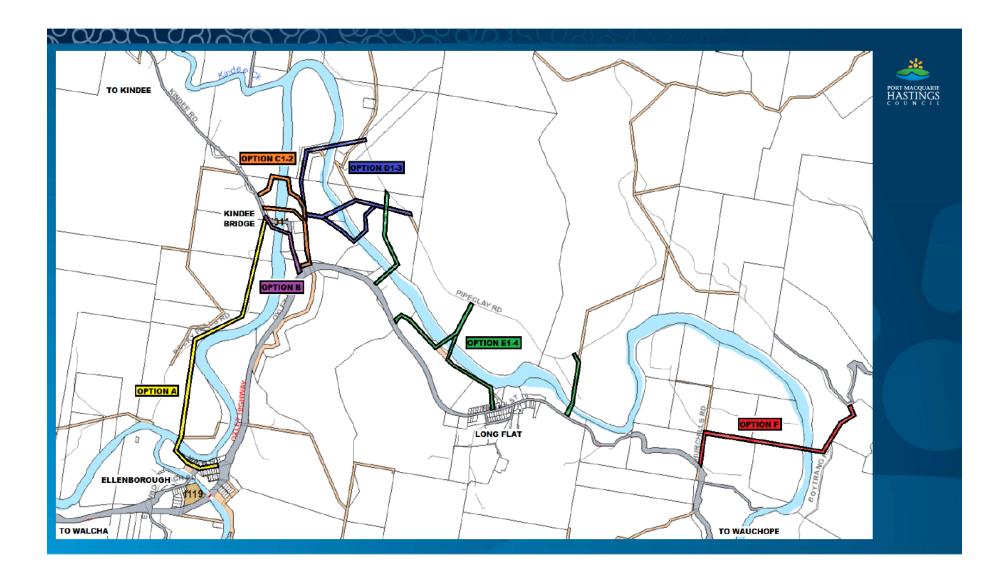


Strategic Options – Alternate Crossings

"Long- list" options:

- Twelve (12) locations between Ellenborough, Kindee and Long Flat
- Basis of existing road network, geology, flooding and topography
- Quantified to allow comparative assessment





RODUCE CONTRACT POT REPORT OLD STORE

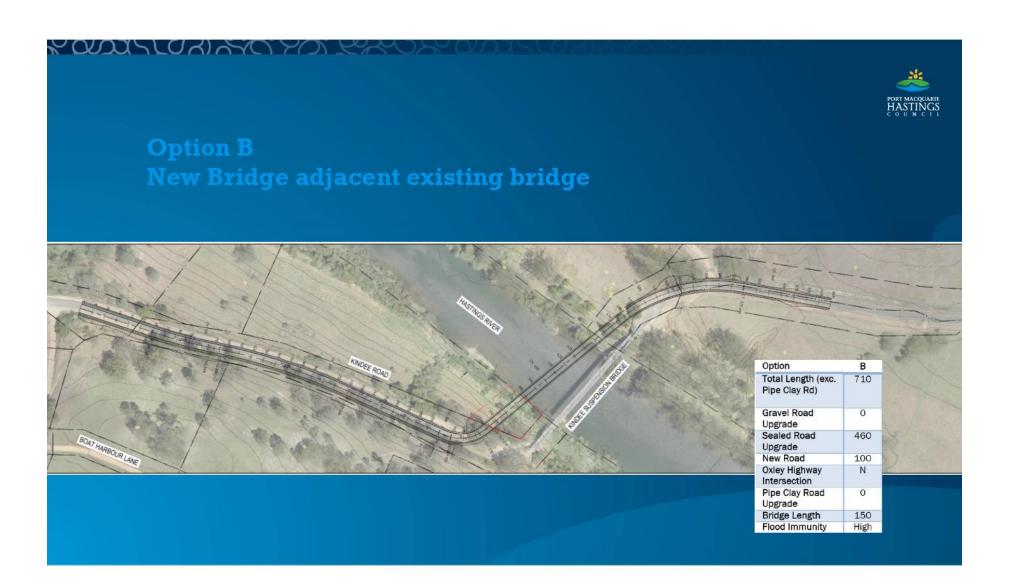
PORT MACQUARIE HASTINGS c o u n c i l

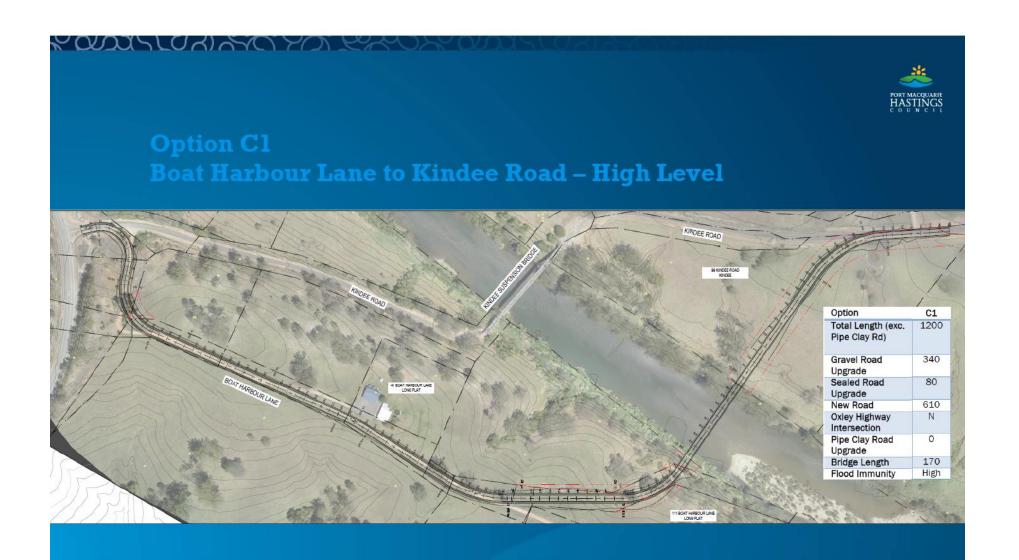
Strategic Options – Alternate Crossings

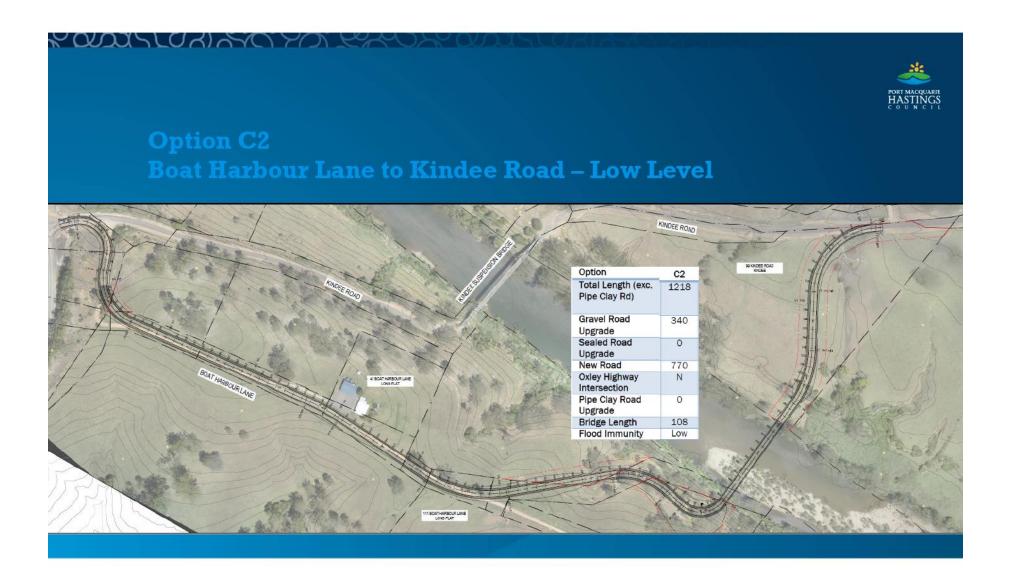
High level comparative assessment:

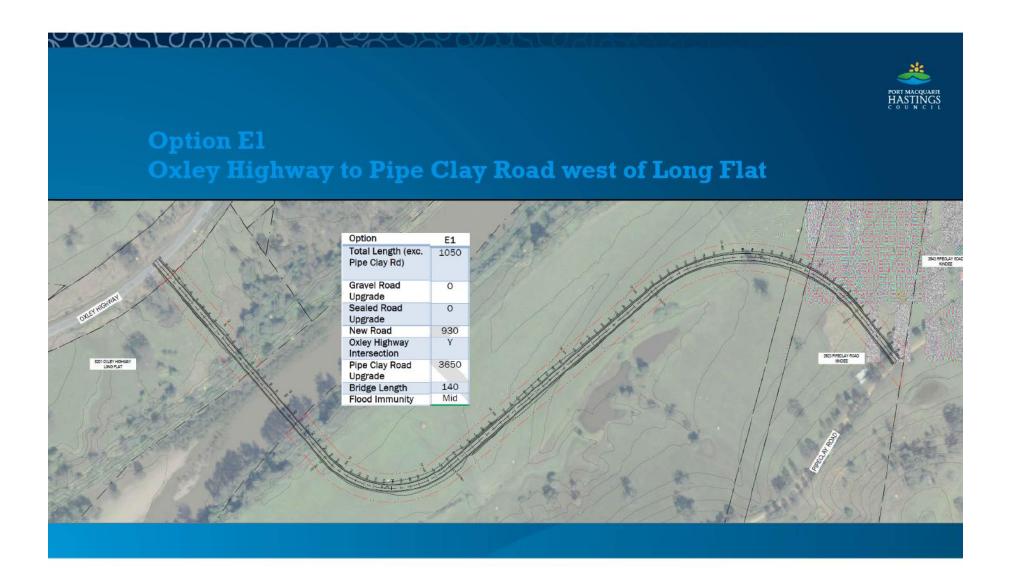
Long List High Level Comparison												
Option	Α	B	C1	C2	D1	D2	D3	E1	E2	E3	E4	F
Total Length (exc. Pipe Clay Rd)	3200	710	1200	1218	1835	1565	1810	1050	1070	1190	550	1785
Gravel Road Upgrade	2200	0	340	340	935	550	550	0	0	0	0	665
Sealed Road Upgrade	400	460	80	0	75	75	75	0	0	0	0	0
New Road	470	100	610	770	665	760	1100	930	950	1070	390	970
Oxley Highway Intersection	N	N	N	N	N	N	N	Y	Y	Y	Y	N
Pipe Clay Road Upgrade	0	0	0	0	2900	3900	3900	3650	4800	4800	6000	10900
Bridge Length	130	150	170	108	160	180	85	140	120	120	150	150
Flood Immunity	Low	High	High	Low	Mid	High	Low	Mid	Low	Low	High	High
Option Developed	N	Y	Y	Y	N	N	N	Ŷ	N	N	Ŷ	N











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Criteria

The criteria considered to allow comparison of the various options are listed below in the table. Descriptions of key aspects considered in each of these criteria are also listed.

Category and Criteria	
Environment	
Bio-diversity	 Overall construction footprint
	 Impact on established vegetation / EEC / habitat
	 Impact on aquatic environment
Noise and Vibration	 Construction footprint and access
	 Proximity to residences
	 Change in traffic volumes and make-up
Heritage	 Impact on existing heritage bridge
	 Opportunity for future interpretation
Community	
Road Users	 Road grades and alignment
	Travel distance
	Safety
Property	 Area of potential acquisition
	 Number of properties/owners impacted
	 Resultant un-usable portions
Heavy Vehicle and	 Load limits or restrictions
Emergency Access	 Access to area of need
	Emergency services
Engineering	
Design and Construction	 Impact on Oxley Highway
	 Construction methods for new bridge
	 Staging around existing traffic
Geotechnical	 Deep gravels versus shallow rock
	 Foundation requirements for bridge
	 Construction of road approaches
Flood Immunity	 Level of bridge deck for access during flood
	Flood levels and velocity
Cost and Operations	
Maintenance and Operations	 Low maintenance structure
	 Low maintenance approaches
	 Improved access for council operations
Capital Costs	 Total capital construction cost

Criteria – Pair-wise comparison

A pair-wise comparison was completed to apply appropriate weighting of the criteria relative to each other, in terms of relative importance to the Project. (ie 3 = significantly more importance, 2 = more importance, 1 = similar level of importance).

		Biodiversity	Noise and vibration	Heritage	Road Users	Property	Heavy vehicles and access	Design and Construction	Geotechnical Condition	Flood Immunity	Capital Cost	Maintenance and	Score	Weighting	Rank
		4	•	U	٥	ш	ш	σ	I	-	-	×			
	Environment												21	20.5%	
Α	Bio-diversity	Α	1	0	0	1	0	0	0	0	1	1	4	3.9%	10
в	Noise and vibration	1	В	0	0	0	0	0	0	0	0	1	2	2.0%	11
с	Heritage	2	3	с	1	1	1	1	1	1	2	2	15	14.6%	3
	Community												33	32.2%	
D	Road Users	2	3	1	D	1	2	1	2	1	1	2	16	15.6%	2
Е	Property	1	2	1	1	Е	2	0	0	1	1	2	11	10.7%	6
F	Heavy Vehicle and Access	2	2	1	0	0	F	0	0	0	0	1	6	5.9%	8
	Design and Construction												43	42.0%	
G	Design and Construction	2	2	1	1	2	3	G	2	2	1	2	18	17.6%	1
н	Geotech	2	2	1	0	2	2	0	н	2	1	1	13	12.7%	4
I	Flood Immunity	2	2	1	1	1	2	0	0	I	2	1	12	11.7%	5
	Cost and Operations												16	15.6%	
J	Capital Cost	1	2	0	1	1	2	1	1	0	J	2	11	10.7%	6
к	Maintenance and Operations	1	1	0	0	0	1	0	1	1	0	к	5	4.9%	9

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Discussion

The pair wise assessment identified Design and Construction (G – 17.6%), Road Users (D – 15.6%), and Heritage (C – 14.6%) as the top three most important criteria in relation to the options assessment. These criteria capture the location and methodology of construction, the impact on road users during construction and operation, and how the existing bridge heritage will be addressed.

The next most important criteria were Property (E), Geotech (H), Flood Immunity (I), and Capital Cost (J) all with similar weightings (10.7% to 12.7%). These are considered sub-criteria to the above key criteria and address construction, impact on private property, level of service and the overall investment required. Other criteria that were not weighted as high included Biodiversity (A), Noise and vibration (B), Heavy Vehicle and Access (F), and Maintenance and Operations (K) as these were not considered to be highly differential criteria in the options assessment.

Options Assessment

Each option was then scored against each criteria using the following scoring system:

- 5 = Excellent Meets all of the criteria requirements
- 4 = Good Meets most of the criteria requirements
- 3 = Fair Meets most of the criteria requirements, but may be lacking
- 2 = Poor Does not meet most of the criteria requirements
- 1 = Very Poor Does not meet any of the criteria requirements

				RA	w sco	RES			WEI	GHTED SCO	DRES	
		Weighting	Option B	Option C1	Option C2	Option E1	Option E4	Option B	Option C1	Option C2	Option E1	Option E4
Environ	ment											
А	Bio-diversity	3.9 %	4	3	3	3	3	15.6	11.7	11.7	11.7	11.7
в	Noise and vibration	2.0%	4	2	2	3	2	7.8	3.9	3.9	5.9	3.9
с	Heritage	14.6%	3	3	3	3	3	43.9	43.9	43.9	43.9	43.9
Commu	nity											
D	Road Users	15.6%	5	3	2	3	3	78.0	46.8	31.2	46.8	46.8
E	Property	10.7%	5	3	3	2	4	53.7	32.2	32.2	21.5	42.9
F	Heavy Vehicle and Access	5.9%	4	4	3	4	4	23.4	23.4	17.6	23.4	23.4
Design a	and Construction											
G	Design and Construction	17.6%	2	3	2	4	4	35.1	52.7	35.1	70.2	70.2
н	Geotech	12.7%	4	2	2	2	4	50.7	25.4	25.4	25.4	50.7
I.	Flood Immunity	11.7%	4	4	2	3	4	46.8	46.8	23.4	35.1	46.8
Cost and	d Operations											
J	Capital Cost	10.7%	4	2	3	1	3	42.9	21.5	32.2	10.7	32.2
к	Maintenance and Operations	4.9 %	4	3	3	3	3	19.5	14.6	14.6	14.6	14.6
	Score		43	32	28	31	37	417.6	322.9	271.2	309.3	387.3
	RANK							1	3	5	4	2

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Discussion – Environmental Impacts

In relation to the environmental criteria of Biodiversity (A), Option B scored higher than other options based on the use of Kindee Road and hence the reduced overall construction footprint, whereas other options required new road construction for considerable lengths. Widening of Kindee Road in Option B may be required on the eastern approach requiring clearing of some significant vegetation that may impact the scoring for this option.

All other options will have similar impact on terrestrial and aquatic habitats due to the nature of these environments being located adjacent to the river, and the construction methods to be utilised at these locations (ie access tracks, rock platforms, temporary structures etc..).

Noise and Vibration (B) considered impacts, both construction and operation, on existing residences near the existing or proposed route alignments. Option B again scored better than other options, based reuse of Kindee Road and the limited number of residences in proximity to the road and proposed bridge. Both Option C2 and E4 scored poorly due to the closer proximity to existing residences, and the likely increased traffic volumes at these locations compared to current conditions.

Heritage (C) was considered based on the existing heritage status of the Kindee Suspension Bridge. It is noted that all options were scored similarly on this criteria as it is assumed that if a new bridge crossing is constructed, the existing bridge would be decommissioned and ultimately removed from site. It is important to note that any decommissioning of the existing bridge would require de-listing and an appropriate interpretation strategy to be defined.

Discussion – Community Impacts

Road Users (D) were considered based on the impact to existing road users in terms of level of service, length of route and travel time, road alignment and grades, and overall safety. Impacts on road users during the construction period are captured within Criteria G (Design and Construction).

Option B was considered to meet the overall requirements by maintaining existing road users while removing any load restrictions at the existing bridge location. All other options were considered to be fair relative to this criteria, based on the additional length of travel required along Pipe Clay Road or Boat Harbour Lane. Option C2 scored poorly due to the very steep and winding approaches required on the eastern approach to the proposed bridge.

Property (E) was considered based on impact on private property and the potential requirement for property acquisition.

Option B scored well on this criteria as only minimal property acquisition would be required immediately adjacent to the existing bridge for construction of the new eastern bridge abutment and approaches.

Option E4 scored relatively well on this criteria as it utilised an existing Crown Road Easement for a significant portion of the proposed new road.

All other options required major property acquisition that may result in loss of amenity for the properties due to being split. A further complexity was identified for Option E1 and E4 where any upgrade of Pipe Clay Road may require property adjustment and acquisition for significant lengths due to it being currently located outside of any existing road corridor.

Heavy Vehicle and Access (F) considered the ability of heavy vehicles to utilise the proposed route and improve the level of service to the Kindee Valley. All options scored similarly on this criteria as this would be a key requirement of the design of any new bridge crossing. Option C2 scored worse than other option due to the potentially steep and winding nature of the eastern approach to the proposed bridge.

Discussion – Engineering

The Design and Construction (G) criteria considered the proposed bridge site, overall span lengths, terrain and access requirements, road approach upgrades and construction, and any staging considerations required to allow construction under traffic.

Option E1 and E4 scored well relative to this criteria as they were considered to be "green field" construction options and able to be completed with minimal impacts on existing road users. Both options required a new intersection to be constructed with the Oxley Highway that would required significant staging considerations.

Option C1 was considered to be fair relative to this criteria as it was also mostly "green field" construction although some impact on road users of Boat Harbour Lane, although quite minimal. Option C2 scored poorly due to the difficult construction of the eastern approach.

Option B also scored poorly, as staging of the construction around existing road users would be very difficult in this very tight road corridor with very steep terrain. It would be likely that Kindee Road would be closed for significant periods during the construction period which could be in the order of 12 to 18 months.

The Geotech (H) criteria considered the expected geotechnical conditions at the proposed bridge crossing location and the impact on the construction of a new bridge. Shallow and high strength rock foundations are preferred to prevent excessive foundations in poor ground conditions.

Both Option B and E4 scored well on this category due to the presence of exposed rock and the steeper nature of river banks at these locations. Option C1, C2 and E1 all scored poorly due to the expected deep gravels and cobbles likely to be encountered at these locations.

The flooding (I) criteria considered the potential impact on road users, and also the design of the bridge structure based on the level of flooding it would likely be exposed to during its life.

Option B, Option C1 and Option E4 were able to provide high level flood free bridge structures above the 100-year ARI flood level. This ensured unrestricted access, but also reduces the loads imposed on the structure during these flood events.

Option C2 and Option E1 were both exposed to lower-level flood impacts and scored poorly on this criteria.

Discussion - Capital Cost, Maintenance and Operations

The Capital Cost (J) criteria considered the expected construction costs associated with the entire Project, excluding Project Management, Design, Property Acquisition and Utilities. Detailed cost estimates have not been completed at this point in time so the scoring for this criteria was based on comparison of the following:

- Overall Length of Option (New and Upgraded Road)
- Overall Bridge Length and flood immunity level
- Bridge Foundation Conditions
- Oxley Highway Intersection
- Length of Pipe Clay Road Upgrade required.

Option B scored well on this criteria due to the minimal addition road construction required. Staging and construction access for this option may increase costs significantly following further investigations.

Option E4 scored fair on this criteria due to the bridge construction, good expected foundations, and staging methods, although significant additional costs would be required for the Oxley Highway Intersection, and the upgrade of Pipe Clay Road.

Option C2 scored fair on this criteria due to the reduced length of bridge and the low level nature of the structure minimising the costs of bridge construction, although offset by the significant length of new roads, upgraded roads, and difficult construction on the eastern approach.

Options C1 and E1 all scored poorly on this criteria due to the overall length of new road required, length of bridge, and expected poor foundation conditions. Option E1 also would require significant costs associated with a new intersection on the Oxley Highway in a high-speed environment, and the upgrade of Pipe Clay Road.

The Maintenance and Operations (K) criteria considered the impact on existing council operations (sealed and unsealed roads maintenance, garbage collection etc..) and other services including emergency services (RFS, Fire and Rescue etc..) and utility providers (Essential Energy). It is expected that all options would result in an improved level of service due to the removal of the current load restriction.

Option B scored well on this criteria due to the minimal additional length of road, and the construction of a low maintenance concrete bridge structure.

All other options score fair on this criteria due to the additional length of roads requiring on-going maintenance, and the additional traffic volumes on currently low volume unsealed gravel roads.

Preferred Option and Sensitivity Analysis

The preferred option following multi criteria assessment was Option B with a weighted score of 417.6 followed closely by Option E4 with a weighted score of 387.3. This indicates that Option B is preferred over Option E4 by a margin of 7.8%. Option B is preferred of the the other options (C1, C2 and E1) by a clear margin of between 29% and 54%.

While there is a clear preference, there is potential that a different result may be identified if the weightings of the criteria are modified.

The last phase of the multi-criteria analysis is sensitivity testing to check the robustness of the findings. Sensitivity testing was undertaken using the stepwise testing method. This method changes the weighting for each criteria individually, while leaving the ratio of weightings the same for the remainder of the criteria. The weighting is increased or decreased accordingly until the preferred option changes.

The majority of criteria do not impact or change the result of the multi criteria assessment following sensitivity testing. Two (2) criteria were identified that may impact the outcome of the multi-criteria assessment, being Road Users (D), and Design and Construction (G).

If the importance of Road Users (D) criteria to the overall Project was reduced significantly to less the 0.5% weighting, Option E4 is found to be preferred over Option B. This is not considered realistic as Road Users (D) are a key criteria for this Project.

The weighting of Design and Construction (G) criteria is also relatively high at 17.6% indicating the importance of this criteria to the overall Project. If the weighting of this criteria is increased further to 27%, or a marginal increase of 10%, Option E4 is found to be the preferred option over Option B. While this appears feasible, this in turn reduces the importance of the other criteria, and would only be marginally preferred over Option B.

This indicates that Option B is still the preferred option, but the construction methodology and staging has a clear impact on whether it is the preferred option, and should be investigated early in the concept design to ensure a feasible or acceptable methodology can be determined given the potential impact on existing road users during the construction period. Sensitivity Analysis – Road Users (D) Criterion

					RA	w sco	RES			WEIG	HTED SC	ORES			SENS		ORES	
		Weighting	Sensitivity Weighting	Option B	Option C1	Option C2	Option E1	Option E4	Option B	Option C1	Option C2	Option E1	Option E4	Option B	Option C1	Option C2	Option E1	Option E4
En	vironment																	
Α	Bio-diversity	3.9 %	4.1%	4	3	3	3	3	15.6	11.7	11.7	11.7	11.7	16.4	12.3	12.3	12.3	12.3
в	Noise and vibration	2.0%	2.1%	4	2	2	3	2	7.8	3.9	3.9	5.9	3.9	8.2	4.1	4.1	6.2	4.1
с	Heritage	14.6%	15.4%	3	3	3	3	3	43.9	43.9	43.9	43.9	43.9	46.2	46.2	46.2	46.2	46.2
Co	mmunity																	
D	Road Users	15.6%	0.4%	5	3	2	3	3	78.0	46.8	31.2	46.8	46.8	2.0	1.2	0.8	1.2	1.2
Е	Property	10.7%	11.3%	5	3	3	2	4	53.7	32.2	32.2	21.5	42.9	56.5	33.9	33.9	22.6	45.2
F	Heavy Vehicle and Access	5.9%	6.2%	4	4	3	4	4	23.4	23.4	17.6	23.4	23.4	24.6	24.6	18.5	24.6	24.6
De	sign and Construction								 									
G	Design and Construction	17.6%	18.5%	2	3	2	4	4	35.1	52.7	35.1	70.2	70.2	37.0	55.4	37.0	73.9	73.9
н	Geotech	12.7%	13.3%	4	2	2	2	4	50.7	25.4	25.4	25.4	50.7	53.4	26.7	26.7	26.7	53.4
1	Flood Immunity	11.7%	12.3%	4	4	2	3	4	46.8	46.8	23.4	35.1	46.8	49.3	49.3	24.6	37.0	49.3
Cos	st and Operations								 									
J	Capital Cost	10.7%	11.3%	4	2	3	1	3	42.9	21.5	32.2	10.7	32.2	45.2	22.6	33.9	11.3	33.9
К	Maintenance and Operations	4. 9 %	5.1%	4	3	3	3	3	19.5	14.6	14.6	14.6	14.6	20.5	15.4	15.4	15.4	15.4
	Score			43	32	28	31	37	417.6	322.9	271.2	309.3	387.3	359.3	291.8	253.4	277.4	359.6
	RANK								1	3	5	4	2	2	3	5	4	1

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Sensitivity Analysis – Design and Construction (G) Criterion

					RA	N SCO	RES			WEIG	HTED SC	ORES			SENSI	τινιτγ so	ORES	
		Weighting	Sensitivity Weighting	Option B	Option C1	Option C2	Option E1	Option E4	Option B	Option C1	Option C2	Option E1	Option E4	Option B	Option C1	Option C2	Option E1	Option E4
Environ	ment																	
А	Bio-diversity	3.9%	3.1%	4	3	3	3	3	15.6	11.7	11.7	11.7	11.7	12.3	9.2	9.2	9.2	9.2
в	Noise and vibration	2.0%	1.5%	4	2	2	3	2	7.8	3.9	3.9	5.9	3.9	6.1	3.1	3.1	4.6	3.1
с	Heritage	14.6%	11.5%	3	3	3	3	3	43.9	43.9	43.9	43.9	43.9	34.6	34.6	34.6	34.6	34.6
Commu	nity								 									
D	Road Users	15.6%	12.3%	5	3	2	3	3	78.0	46.8	31.2	46.8	46.8	61.5	36.9	24.6	36.9	36.9
E	Property	10.7%	8.5%	5	3	3	2	4	53.7	32.2	32.2	21.5	42.9	42.3	25.4	25.4	16.9	33.8
F	Heavy Vehicle and Access	5.9%	4.6%	4	4	3	4	4	23.4	23.4	17.6	23.4	23.4	18.4	18.4	13.8	18.4	18.4
Design a	and Construction								 									
G	Design and Construction	17.6%	27.0%	2	3	2	4	4	35.1	52.7	35.1	70.2	70.2	54.0	81.0	54.0	108.0	108.0
н	Geotech	12.7%	10.0%	4	2	2	2	4	50.7	25.4	25.4	25.4	50.7	40.0	20.0	20.0	20.0	40.0
1	Flood Immunity	11.7%	9.2%	4	4	2	3	4	46.8	46.8	23.4	35.1	46.8	36.9	36.9	18.4	27.7	36.9
Cost and	d Operations																	
ı	Capital Cost	10.7%	8.5%	4	2	3	1	3	42.9	21.5	32.2	10.7	32.2	33.8	16.9	25.4	8.5	25.4
к	Maintenance and Operations	4.9%	3.8%	4	3	3	3	3	19.5	14.6	14.6	14.6	14.6	15.4	11.5	11.5	11.5	11.5
	Score			43	32	28	31	37	417.6	322.9	271.2	309.3	387.3	355.2	293.9	240.0	296.3	357.7
	RANK								1	3	5	4	2	2	4	5	3	1

Item 12.01 Attachment 3 KINDEE BRIDGE STRATEGIC OPTIONS REPORT

Appendix E - National Library of Australia Records

Includes:

- The Port Macquarie News, Saturday, March 1, 1930 (Kindee From a Correspondent)
- The Port Macquarie News, Saturday, July 15, 1933 (Double Crossing Bridge)
- The Port Macquarie News, Saturday, June 23, 1934 ("Uncle" Johnny Coombes)
- The Port Macquarie News, Saturday, April 13, 1935 (Kindee Contributed)
- The Port Macquarie News, Saturday, June 22, 1935 (Kindee Contributed)
- The Port Macquarie News, Saturday, September 7, 1935 (Kindee Contributed)
- The Port Macquarie News, Saturday, May 9, 1936 (Killed in Accident At Kindee Bridge Site)
- The Port Macquarie News, Saturday, August 29, 1936 (Kindee Bridge)
- The Port Macquarie News, Saturday, September 26, 1936 (Kindee Contributed)
- The Port Macquarie News, Saturday, March 29, 1946 (The Flood Close to '29 Levels)



	mail after a great deal of difficulty. At the time the child suffered no ill effects, but a few days later she be- cause ill and a doctor was called.	The drogher Oxiey is on the slip undergoing repairs, Mr. Reckless be- ing in charge of the work. Both the mills are at work. The health of the locality is O.K. AUCTION SALES. To-day (Saturday) Mesars. H. G. Biriagton and W. Landrigan, succion- eres in conjuction, will sell wright's Creek Battle, at Bulmer's corney. Wauchops, at 4 p.m. Mesars. H. G. Birington and A. & R. Crothers, in conjunction, will sell at Port Macquarte, oppontent the Royal Holei, the same day, at 230 p.m., "King's Point." And at Hamilton, at 5 p.m., Mr. A Hudson will dispose of pisso, fur- niture, de., on behalf of Mr. Bowyre	Dry times or wet, Taree and Wing- ham water supply, backed by indus- try, is making thngs of beauty out of desert gardens, and the end is not yet. Taree and Wingham are preparing	ing ropes for that craft and half-a- dozen others which were in difficulty	de the thanks of its members for the kindness and hopitality extended. On Thurwida yevening the Frenby- ten that with the congregation in pitter worship. The McGraton, Rev. Rev. Wannan, presided, and con- ducted the service, ansieted by Pera- t. J. B. Thorn and J. W. McGratis. Rev. B. T. Right presched the sermon, taking as his test Philippians, chap- ter 3, verse 14 : "I press towards the mark for the price of a high saling of God in Christ Jenna." D. I. McDonald's Visit. MR. D. 2. McDONALD, P.B.O.A. (Honours), P.B.M.C. (London), sigh- testing specialist, of Webster, Jack & McDonald (Syndey). Ltd., Opticians, of Martin Flace and Fitt-street, will be in percent attendence at the	four large and five smaller. These were made on the spot, the gravel be- ing brought from the river, a distance of about three and a half miles. The finish of these physe compares equally with city work. Two wooden culvests were also completed. The greater part of the elseing was done, and much of the side cuttings and fillings- in. Most of the fencing material was also inid. Apparently the men who have this job in hand understand the Tusiness -cold work, with a method. When the river is bridged, this food will be a great convenience and joy of the settlers. It will open up scome fine dearying country, and give access to the timber. It is quite	citement! Terrific thrilis! Every- thing that means entertainment. One of the most thrilling jungle stories ever produced, with a wonderful cast. James Peirce, noted athlete. Edma Murphy, and the world's biggest man, Liu Tù-Ghing, the Chinese giant, who is at present in Sydnay for the show. "The Battle of the Serse. Rappily married, years together, and a shallenge. A battle cry to the primitive institution to detend their own. Be "Battle of the Sexas" wages, world without and Wooman signate vommit, male viewas female: man against man. A tra- mendous formed to the programma. Beams Love in, "Sally of the Beand- las". And they in you have frageness.	
tional	Library of Austra	alia			htt	tp://nla.gov.au/n	la.news-page11	452121
							Δ	Item 12.01 Attachment 3

<page-header><text><text><text><text><text><text><text><text><text><text><text><text> THE PORT MACQUARIE NEWS, SATURDAY, JULY 15, 1933. DOUBLE CROSSING BRIDGE AUTO Beware FLU SERVICE OF THE MAKE YOUR OWN & HITE FINE AND TAR from OFFICIALLY OPENED. Day & Night Saturday, July 1, 1933, became a We ester for your needs at all hours. OILS, PETROLS, TYRES, TUBES, etc., stocked. GARROD'S ed-letter day in the annals of the Upper Hastings, when the suspen-sion bridge over the Hastings River, sion bridge over the Hastings fuver, at what is known as the Double Crossing, was afficially opened. The official ceremony was act down for 11.30 a.m., and punctual-ly at that hour proceedings com-merced EXTRACT WEST END GARAGE A. M. KEENA, (Late Harden & Johnson Ltd.), PORT MACQUARIE. 'Phone 87. nenced. However several hundred inter-HERBERT H. BLAIR, ested persons, from all parts of the district, besides official visitors and district, besides official visitors and those from adjacent rivers, were on the scene before this hour, to make their own personal inspection of a structure—the first of its kind in Australia—which is destined to make history, and to visualise the blessing and boon such a bridge must prove-to the Forbes River reci-dents, and to these actilers in simi-larly placed circumstances who will subsequently get relief from a like structure. HOUSE, LAND AND ESTATE AGENT, MdNHERNEY'S BUILDING (Opposite Post Office) PORT MACQUARIE. If you are in search of a Good Farm, Town Blocks and Furnished During the speeches, reference was made to the Sydney Horbour Bridge and also a comparison, and it was truly in order to make such a connection, for this suspension bridge is, we honestly believe, of more vital importance to those con-cerned than is the bigger and grander structure. as made to the Sydney Harbour CONSULT ME. R. NEILSON, THE PORT MACQUARIE HAIRDRESSER. But the Double Crossing Suspen-Ladies' Private Room sion Bridge is no mean structureof the clear flowing waters en-Cleanliness, Service, Satisfaction

JAMES GAMACK,

Apartment

Auctioneer & Valuator, ROLLANDS PLAINS and KEMPSEY.

BOATS FOR HIRE AT ANY HOUR

R. W. RECKLESS' BOATSHED.

Foot of Short Street, Port Macquarie.

Good Handy Boats at Modernte Rate by the Hour, Day or Week. Further Particulars from THE PROPRIETORS.

NURSE REID (Certificated). "Mount Adrah " Private

Hospital. and attend them in their owo Homes.

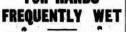
DREW, BROWN & DREW, (Sole Proprietor). sion & General Merch 197 SUSSEX STREET,

SYDNEY. WILL receive CONSIGNMENTS of ALL KINDS OF FIRM & DAIRY PODUCE, and give them every attention, GUARANTEEING HICH-EST MARKET RATES, with pr despatch of returns.

rders entrusted to us shall personal supervision.

A Trial Shipment Solicited. Reference: Bank of New South Wales, Sussex Street.

FOR HANDS



ing to the woman who does her outswork than the redness, rough and cornerse caused by constantly it for hands is water. Sometime writeling to in-own housework than the re-nees and sorennes caused by constituty having her kands in water. Sometimes it may only is an amburrasement, bet more often it is a painful acportence, especially in odd wather. This can all be avaided by keeping a bottle of Mirpil handy in the hitchen and a few times dally-especially after washing up, and always after handsry werk-rabbing a small quantity well into both

6

hances an already arresting pic-ture. A vision of what the struc-ture must mean to these settlers --both male and female-when the waters come down in spate is beau-

Just before 11.30 a.m., when over 500 people had assembled, the blue ribbon, was placed across the ba-flagged bridge, and the official eremony commenced.

ty indeed.

ceremony commenced. Mr. A. Biackle, on behalf of the Forbes Frogress Association. s.id it gave him great pleasure to be present on the amplelous occa-sion, the opening of the Double Crossing Bridge. It is first of all my pleasant duty to extend, on be-half of the Forbes River Progress Association, a most cordial welcome to our many and distinguished visi-tors. We derm it an honour to to our many and distinguished visi-tors. We deem it an honour to have the Hon. L. O. Martin, M.L.A., Minster of Justice, and Mr. New-ell, Commissioner for Main Reads, in our midst on this historic day. which lifts us out of the depths of retardation by what I would ven-ture to term at least 20 years. We, as residents of the isolated Forbes as residents of the isolated Porocs River district, deeply appreciate and are proud of this magnificent structure, not only for its structural beauty and economic importance, but also because of the fact that it the former would be first completed and connected to the Oxley High-way in the vicinity of Yarras by a bridge over the Hastings River at Deuble Crossing. The work on this section by deviation had eliminated four open crossings. betauty and ecolonic fract that it is the first of its particular type in Australia. Moreover, the whole Shire should experience a sense of pride to realise that this design originated in the fertile brain of our Shire Engineer, Mr. Booth, to whom it would always remain as a monument of distinguished achievement of his engineering ability, and we feel Mr. Booth has made a valuable contribution, not only to his Shire, but to the whole of Australia. I would like to pay a tribute to Mr. Martin, Mr. Newell, and also Mr. Bruzner, who was un-

section by deviation had eliminated four open crossings. The sum of 113.763/19/4 having been expended on road works and the final inspection of the last sec-tion made on 4th July, 1930, it was then decided to use the unaliotted balance of the f15,000 grant, which amounted to f1233, together with an additional f2000 saved on other Federal Aid Works, to provide funds for the construction of a bridge at Double Crossing. Plans for a suspension bridge were prepared by the Main Roads Department. The bridge has a to-tal length of 511 feet overall, and consists of a contral span of 176 feet and two side spans each 66 feet long. The width between kerbs is 10 feet. In the construc-tion the following materials have

nd also Mr. Bruxner, who was un ortunately not able to attend, for he untiring interest they have dis played in our long agitation for this bridge, and the valuable assistance which they have rendered. By closing my remarks, I wish to extion the following materials have been used .-- Cables, 5 tons; other steel, 20 tons; concrete, 80 cuble yards; round timber, 700 lineal feet; tend the sentiments of our warm appreciation to our genial and ob-liging contractor, Mr. McPhillips, whom we feel sure has handled the surn timber 30,000 super feet. The work was divided into two con-tracts, that for the supply and de-livery of steelwork being with Waich Island Dockyard, in the sum of 1,210/3/6, and for the erection undertaking in a most competent and efficient manner, and I feel we owe him an apology for holding this opening function before he has had an opportunity of thoroughly com-pleting his contract.

of the steel work and the construc-tion of the bridge wife J. J. Mc-Phillips, in the sum of 11,552/10/11. As may be seen from the appear-ance of the structure, it is not so robust as those constructed by the Department of Main Roads, the carrying capacity seing only about three-fifths of the usual capacity. Bat being designed for any one load of 10 tons, or a load of 60hs, per equare foot all over, it will with parfect after y carry any load which this district can place upon it, ex-cept, perhaps, bullock teams haul-ing jinkers laden with timber. You will observe that the ap-proaches, which, by the way, were constructed by your own Shire En-chapters have the perof the steelwork and the constru-

be disheartened, as he felt certain better days were coming; before very much longer, he said, the posi-tion will be much more satisfactory. and they could look forward to bet-

tion will be much more satisfiatory, and they could look forward to bet-iter prices. The bridge they were opening that day was a great bridge, which would bring to many some of the ordinary advantages of etrilised life. The speaker said it was just over 100 years since the first white man (Explorer John Ox-ley) had traversed from the table-lands to the sea in that vicinity, and in 1842, the New England road from Port Macquarie to Walcha, was officially opened. (Mr. Martin then read the "Gazette" notice, is-sued over the signature of the Po-lice Magistrate at Port Macquarie.) That road had been constructed by settlers, and what had been done then was largely done here and y now. The Government needed the i peoples' help, and, if all took part,



Juncheon, at which 60 persons at that if a man, of whatever party, down, with a choice of positry in was speaking on a subject which he great abandance, cakes, fruit, drc, and the necessary toasting bever-and the necessary toasting bever-upon, he was heard with attention. Others, who gave no heed to the Justice done to this fine spread, question under discussion, but talk-Mr. A Blackie, Chairman, propose de al cof stuff that would infiame "The King," which was accom-ted minds of the people, and who available the subject the spread of the people and who available the subject the spread of the spread the subject which was accompany to the spread of the spread the sprea

The bridge they were shared by the sea in that vicinity, and the sea in that vicinity. The sea is a second the sea in that vicinity, and the sea is the sea in that vicinity. The sea is a second the sea is the sea in that vicinity, the sea is the sea in that vicinity. The sea is a second the sea is the s

By simply adding Sugar and water as directed on the label, you have a family supply of this well known Cough Mixture. A small bottle of Garrod's Extract makes one and a quarter pints of White Pine and a guarter pints of White Pine and Sure for 10 of Post Fore for Garrod Eabert 144 Handisturing Chemins, Enkinerith, 97489. OPTICIAN'S VISIT.

MR. D. I. MCDONALD, F.B.O.A. (Honours), F.S.M.C. (London), sight-testing specialist, of Websier, Jack & McDonaki (Sydney), Loid, Opticiens, will be in attendance and may be Personally Consulted at the Hastings Hotel, Wauchope, on Tuesday, 25th July. Hours 9 a.m., to 5 p.m.



The North Coast S. N. Co.

STEAMER PROGRAMME. S. S. KINCHELA. ONE TRIP A WEEK LEAVING SYDNEY

TUESDAY EVENING. AND PORT MACQUARIE

SATURDAY MORNING.

G. SMITH. Local Agent.



There is not the slightest necessity to wait one day to give the children the Piano you have promised them, and that they need so badly.

Palings will place a glorious Victor Piano in your ho on payment of a deposit of

Mr. Blackie then called upon Cr. John Downes (President of Hast-ings Shire Council), who said he felt great pride at being present, amid such a representative gathering, at the official opening of the Double Crossing Bridge. The com-Double Grossing Bridge. The com-pletion of the structure represented a long battle over many years by the pioneers and residents of the district it will serve. The meaker drew a mental picture of the con-ditions prevailing when this lo-cality was in its virgin state, and the pioneers made their entry to make homes for themselves-a pic-ture of great disabilities, of creeks and rivers to be crossed, and of

ALINI	ope of reaping rewards in increas- venting this class of traffic from	taste in the mouth, sudden dizziness og resing, furred tongue, bewels loose one des od constituted to part All	and thus much sound advice and wisdom was frequently lost to the Government by the application of the "sag." Governments now hast- ened to delegate much of their re- grateful, said Mr. Martin, and	- Ang POST OFFICE, HISBARD	•
	ult of their great efforts. This It will be necessary in the near ridge would give great, and de- future to erect notice boards limit-	pany the bilious condition, and while so afflicted the patient's life is servely	missions, many of which were un- Mr. A. Blackle proposed, "The	SOLICITORS, TAREE.	
A 100 March	beir descendants	Pills relieve this trouble speedily and	pecessary and a waste of money. Double Crossing Bridge," which, he Certain Boards, such as the Main said, he regarded as a very great	Visit Wanchope Every Friday.	*
prepared lotion for Rough, Chapped and Chafed Skin, Red Hands, Chil-	y, continued Mr. Downes, it was Especially should droves of cattle	blood, and once more the human	Roads Board, were necessary, and carried on a good work, and he was represented a long and difficult pleased to see present a capable light; and a reward. Settlement on	Office :School of Arts Building. HOULS : 9 A.M. to 5 P.M.	
for powder, and after Shaving. On I	roducers, for on their labours de-	They are a safe, safe, and reliable	and practical man in Mr. Newell, the Forbes dated back 74 years, and	A	.′.€
The W. H. Comsteck Co. Ltd., Makers of Dr. Morse's Indian Root Pills, 23	to him, and to all thinking per- be spread out and not crowded onto	complaints of common il taken once or	morning to the spending of a big condants of the pioneers had fought sum of money on roads at the head the same battle, the same primitive		1
Copon receipt of name of this paper	ercourse, and from their food sup- tion had been the driving of the	healthy condition. They are purely vegetable, and their action is not ac-	Rivers: he (the manker) knew the down to the sons and daughters by	MARTIN & LAMPORT,	
postage a sample tube will be sent by	lies on numerous occasions. piles in the pier on the south bank hings have changed, and the These piles have not been driven as	companied by any nauerous or griping contaction, but is mild and gentle, 0	feit the money would be wisely (Continued on another Page)	129 PITT ST., SYDNEY,	
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•		THE PORT MACQUARIE NEWS, SATURDAY	, JULY 15, 1933.	
	hoped the bridge there would serve the district, and, while he would not	CATEL CATEL CATEL	Coloured Blankets in Silver and Dark Grey, all sizes, at Cost. A Spleadd Line Bush Rug at 6/11, new only	SALE! SALE!
(Continued from another page).	make any rash promises, he would say their policy was to help the peo-	— — — — — — — — — — — — — — — — — — —	Hundreds of other Blankets, one and two of different kinds, not sufficient to make a Sale Line of, but just Samples, to be Sold	DURING JULY W. CUTTRISS
hose ploneers. During the past access to	ple on the land, to put new people on the land, and to see that they were kept there and were able to	3	at Cost Prices.	Will Hold a DRAPERY SALE, Cash Only.
naley Highway, they had called to the Transport body, through the	get their produce out. Mr. E. E. Booth (Shire Engineer)	HALLETT'S	BOOTS AND SHOES.	MILLINERY. All Women's Feit Hats, three
when this was not forthcoming,	said he was greatly pleased to at- tend the opening of the bridge, and		LADIES', MEN'S MAIDS' AND BOYS'. All Styles, Shapes and Colours. Prices Equal if not Lower	prices only, 2/6, 3/11, 7/6 each. Children's Hats, 1/-, 2/6, 4/6.
nd constructed several railes of	that he had always felt a great in- terest in the Porbes River district. He had found the Main Roads	Grant Manager Sala	than City Boot Shops. These are only quite a few. I could fill a whole page with prices and styles.	
of self-help they had done work to the assessed value of £500. Pre-	Board ever ready to listen to new ideas. He could assure Mr. Newell		Maide' One Bar Patent Shoes 0/11 D/3 Maide' One Bar Patent Shoes 0/11 7/3	SHOES. Women's Shoes, the best makes, Whybrow's McMurtrie's, Bed-
niles to the Double Crossing was	that by voluntary effort the people had done big stretches of road, and	and the second	Girls' Patent College Shoes 9/11 7/3 Mnids' Patent College Shoes 10/11 8/6	goods'. Etc., 56 Pairs, all to go at 4/6 pair.
Crossing to Wauchope. Mr. Newell,	these had been a very valuable contribution to the Shire. The speaker also had very great pleas-	AND SHOES.	Ladies' Shoes, in Tan, Weits, Patent or The Wonderful Value at from 17/11 and 18/11, All One Price, 11/6. Best Value Ever Offered.	A Big Range of Slippers, Nevis
hat the Board would help those who helped themselves, and he now	ure in congratulating "Uncle John- ny" on the materialisation of his		500 Pairs Ladkes' Slippers, Swede Soles and Pom Pom, worth 1/6, Sale 9d. A Wonderful Line of Slippers, 2/11, Reduced to 2/8. Best Value Slippers ever offered	Women's and Children's Felt Slip-
appealed to the Commissioner to a nake available a grant sufficient o link up the portions already con-	Mr. Harris proposed "The Visi-	-FOR THREE WEEKS-	Men's Shoes-Wonderful Values.	Women's Sand Shoes, 3/3 pair, Men's Sand Shoes 3/11 pair.
tructed by voluntary labour, and hus make the whole an effective	tors," extending a hearty welcome to all on behalf of the Forbes River and district people, and expressing		MOMURTRIES WELL-KNOWN BRAND.	DRESS GOODS, Murray's Super 36-in, Coloured
work and outlet. The construction and opening to traffic of the Double and State of the Double and Sta	the great pleasure it was to have		75 Pairs only McMurtrie's, worth easily 21/-, for 12/6 Pair only, Blackman & Rose Men's Shoes, were 25/-, now only 17/6 pair.	Flannel, 3/11 to 2/11; 29-in. Col- oured Flannel, from 2/9 to 2/3.
Crossing Bridge was a grand ac-	day. They had had their dreams of roads and bridges; the bridge		A, Big Range to Select from. Working Boots, Men's Tan or Black 10/6 7.11	Coloured Finnnels, Three Specials, Flame, Lemon and Green, 2/9 to 1/6 yard, as per pattern.
If the people for whom it was built of t would be "just a beautiful cloge" if help was not given on 1	were very thankful to the Main	NO PATTERNS NOR GOODS ON APPROVAL.	Ditto Ditto	Fancy Tweeds, Friscas, Flecked and Waffle Flannels, Dress Lengths, no
	The bridge would be a stepping-	Note These Prices ! No Better Value in Australia.	Hundreds of Pairs in Black or Tan, Water-tight, sewn or sprigged soles, Marvellous Values. You cannot do better, or even	two alike, all reduced. Velveteens, beautiful shades for
was rather surprised to find he present to find he present and the present and	people; the next stepping-stone would be an all-weather road lead-	Crope de Chene, 36-in., all shades	as well, as these are freight paid from the Factory.	evening wear, only 3/6 yard. All Silk Satin, 5 yards lengths,
t was not exactly in his line, and, it is he had spoken about the bridge.	ing to the bridge. This toast was responded to by	Croppe de Chene, 36-in, all chades 3/11 2/6 Ditto Ditto 5/11 4/6 Silk Moracain, 36-in. 9/6 7/6	NOTE ! NOTE !	only 12/6 piece. Crepe de Chene, 2/6 yard. Erown-Fancy Cotton Tweed 1/3 yd.
te hardly knew where he was (Mr.) dartin suggested that they wanted) dim to speak about roads). "Well," []	Martin's party : and by Cr. W. H.	Zil-Inch Dress Tweeds (Fancy Check) 1/11 1/6 36-Inch Fancy Tweeds 2/11 1/11	THIS SALE is for THREE WEEKS ONLY, as I cannot	A limited quantity of Tobralcos, Navy, Figured, and White ground,
aid Mr. Newell, 'the bridge is a here," and a fair amount of the	on the completion of their bridge	36-Inch Flock Tweeds 3/11 3/3 27-Inch Velveteen (plain and floral) 4/6 3/3	possibly Sell at the Prices Quoted any longer, as I am Selling Every Line at Cost (except a few proprietary lines). So Call Early and	also dark shades, and small patterns for children, 1/11 yard.
oad work had been done. The peaker agreed that he had said ;	As a Councillor he had been in touch with those residents and	36-Inch All Wool Dress Flannels 3/11 2/11 38-Inch Fancy Breas Flannels 6/11 3/11 36-Inch Hopsne Flannels 4/8 3/6	Select Your Requirements. Every item guaranteed as advertised, but you cannot expect to have every line after the Sale has been	yard ; 30-in. Do., 1/- yard.
he Board would help those who t elped themselves and continued (an an interesting address which be-	though they had differed in opin-	30-Inch Cream Sports Sulling 5/6 4/3 31-Inch All Wool Creamaline 4/6 3/11	in progress some time.	Cream Fuji Silk 1/- yard; Kanebo Silk, 1/9 yard. Other lines all re- duced.
skened sincerity in the effort to do 1	ons, they had always threshed mat- ters out on a manly footing. He as- sured Mr. Newell that there were	36-Inch Cream All Wool Crepe 5/11 5/3 S6-Inch Cotton Cashmere, Gream, 1/8 1/3	:0:	Women's and Children's Frocks, Coats, Jumpers, Cardigans and Blaz-
undamental principle of the Main related a solution of the said, was to assist	no more grateful people than they, and, though Council and the Main	All Baby Wear Drastically, Reduced. A Big Assortment, Flaunci Shirtings, All Wool, Mid and Dark Grey 2/6 1/11	HALLETT'S HALLETT'S	ers. Big reductions in all lines. O.S. Women's Wool and Silk Vests
hires and municipalities in the paintenance and construction of a	Roads Board were held up for fin-	Norm Finnelette 1/- 9 Striped Finnelette 1/- 9 Striped Finnelette 1/- 9		from 1/6 each. HOSIERY.
ood roads, and developmental e bads. Motor registration went to be Board, and half of the money s	Shire, and he hoped they would	Striped Finnelette 1/9 1/3 44-Inch Table Darmsk 1/0 10	Family Emporium.	Full size Ladies' Black Lisle Hose (American), only 1/- pair. Women's Silk Hose from 1/3 pair.
unberland was spent in that area.	In proposing the toast of "The Ladies," Mr. R. N. Shaw (Shire)	Cream Flanneleite	Horton Street, Port Macquarie.	Silk and Woot Hose from 1/3 pair. Children's Sox, 6d and 9d pair.
be other half, and that contributed of good of the second	Clerk) paid glowing tribute to the imple and delicious spread provid-	36-Inch Twill Flannelette, pink edge. 1/6 1/3 27-Inch All Shades Ripple Flannelette 1/6 1/2	WHERE THE BARGAINS ARE ALWAYS GENUINE. BIGGEST REDUCTIONS EVER OFFERED.	Boys' Golf Hose, 1/6 and 1/9 pair. Cretonne, 6d yard. Curtain Net,
s the suspension bridge. Develop- c	ed that day—he did not think their could be a bird with a feather on left in the district. And, in serious	38-Inch All Shades Winceyette 1/3 1/- 1/3 1/- 1/3 1/- Special Value Dress Lengths, no two allice, all to go at Cost Prices, 1/3 1/-		101d and 1/9 yard. White Terry Towelling, 24-inch,
ble by Parliament. In 1931 the vederal Aid Scheme ceased, and in	rein, Mr. Shaw said that the open-	Ladies' Silk and Wool Vests, alcevelese	AGENCIES.	1/3 yard. Face and Bath Towels, White and Coloured, a big stock.
oney already voted, but not spec- ied, was available for develop-	trating would be a very great boon and blessin; to the womenfolk, in	Ladies Shetland Randeau Bloomers	-	54-Inch White Damask, 1/3 yard. All Linen Osnaberg, 1/4 to 1/- yd.
ransport (Col. Bruxner), the fi	dying them access to surrounding riends and to the townships. One ady he was speaking to recently	Silk and Weol Hose, Special at	A valuable feature of the Commonwealth Savings Bank service is the establishment of Agencies in practically all Post Offices	Flannelettes in Lemon and Salmon, only 6d yard.
buntry, and not the city, and he has a start they would soon be able a	had told him she had not been cross the river for two years. This	Lustre brand	in Australia, as well as in many business houses.	29-Inch Striped Twill Flanneletts, 1/- yard. 36-Inch Floral, Plain and Striped
they had had further money a	would now be a thing of the past, and he congratulated the Forbes	Beau Monde fully-fashioned Hose 9/11 7/11 Special Line Line Hose 2/9 2/3	It will therefore be readily seen that the Bank's service is extended wherever there is population. In every city, town and	Winceyette, 1/13. Cream Flannelette, 6d, 9d, 1/-, 1/3
ed. Mr. Newell said that he had the well-being of country settlers at	beople, both ladies and men, on he completion of the structure. Owing to the lateness of the hour,	Milanese Lingerie,—All Garments at Cost Prices or Less. Ladies' All Wool Coats	extended wherever there is population. In every city, town and district Savings Bank facilities are valuable to men, women and children, for the Savings account may be used by all.	Mileman Wess Michts Duteman
art, but, nevertheless, he person- set by thought there was too great on a	ome further toasts were eliminated and the luncheon concluded.	Ladies' Velou: Conts	Wherever you may live, wherever you may travel within	Best Liverpool Blankets, White
	Sports, the results of which are iven elsewhere, were then con-	Middy Coats, Cardigans, too many prices to quote, all at Cost Prices. A Few Ludies' Costumes-Must Be Sold. Flannet Skirts, all shades 12/8 10/6	the Commonwealth, you can utilise the Savings account for your profit and convenience.	Dark Grey and Natural Wool Flan- nels, 1/9 to 1/6, 2/6 to 2/3, 3/3 to 3/-
the tendency had been to spend of	ested, whilst a deputation waited in Mr. Newell, details of which are	Twood Skirts, are satured and the second sec		Men's Under Flannels, Natural and Grey, 4/6 to 3/9, 5/6 to 4/11.
me councils had undertaken ex-	iven in another column. A huncheon and a publican's	Fancy Wool Beret 3/8 2/11 Traced Work, All Pure Linen, in All Designs, in every	Commonwealth Savings Bank of Hustralia	Men's Tweed Trousers, 3/9, 6/5 10/6 pair. Men's Trousers, thorn proof, elas-
th greatly reduced assets. This th	both provided for the comforts of the inner man, and both did brisk tade, whilst many carloads had a	required articles, and about 500 articles to select from. These are to Be Sold at Absolute Cost.	(GUARANTEED BY THE COMMONWEALTH GOVERNMENT).	tic tops, 15/11 to 14/6, Boys' Navy Blazers, all Wool Flan-
a councils. The Main Roads p	icnic lunch, water for tea being houghtfully provided.	Caloured Border Curtain Net		nel, Boys' Tweed Hats and Caps. All Wool reduced 1d in price per
ead horses, with 5 per cent. in-	To express appreciation of the ef- orts of each and all of the Forbes	Paton's Wild Flower, Busy Bee, 4 ply,	INSURE T. ROSENBAUM,	Skein. Wendy Wool only 10d skein.
nest." The speaker reiterated he di build personally like to see mil- gu	istrict residents to entertain their uests and visitors would take too	6 Packets Best Blue-lined Envelopes for 1/- 54-Inch Piain Grey Sheeting, heavy,	WITH Builder, Contractor & Joiner,	Balls Wool all reduced. Coats' Cotton, 6d reel. Bluchers and Cossacks, a reduction
penditure, but he did not think ic	nuch space. The work of the lad- is was commented upon in the	70-Inch Twill Grey Sheeting 2/11 2/3 80-Inch Twill Grey Sheeting 3/11 2/6 51-Inch While Twill Sheeting, good value, 1/11 1/12	THE BRITISH HOBTON & CLARENCE STREETS, PORT MACQUARIE.	of 1/9 pair will be mede.
end too much money. Much bet- th	peeches, and we tender further hanks for kindly deed and action all in a well deserved word of	54-Inch White Twill Sheeting, good value, 1/11 1/42 70-Inch White Twill Sheeting 2/11 2/3 80-Inch White Twill Sheeting 3/11 2/9	AMERICA INSURANCE Sashes, Doors, and all Kinds et	Samples of 1/6 Flanzels.
oney we have and can afford, and prot spend the savings of our N.	raise for the Hon. Secretary, Mr. Hailoren, who was capability and	Large Size White Towels (English)	COMPANY, Glass Cut to any Size, and Glazing Done	W. CUTTRISS, florton-st., Port Macquarie.
andfathers and live on the earn- co gs of our grandchildren." The hi	ourtesy personified. On him, and is worthy band, fell the responsi-	Large Size Coloured Towels (English) 2/11 2/3 Large Size Coloured Towels (English) 3/5 2/5	Established in Canada 1833. Gavanised Iron. Cuttering, Ridge- capping, and all Builders' Material	Street and an and a second second
, and would not carry an interest ity	ility, and all are to be most heart- y congratulated on the able man- er in which the day's programme	36-Inch Grey Calico 1/3 /10k 78-Inch Heavy Duck 2/11 2/21 2/3 62. Inch Meavy Duck 2/11 1/3 1/3	Assets Exceed £1,000.600.	PÉNSIONER'S HOARD.
a it was a boast that there would	as put through. Thus concluded a unique day in	63-Inr-h field Ticking 2/11 1/11 27-Inch (Neck Linen Ounsberg) 1/31 1/6 Osnaberg, Cotton and Linen, 1/4 1/-	FIRE, MARINE & ACCIDENT	On January 12th, John Williams, 65 years, a one-legged pensioner, who
Macadam roads from end to end th New South Wales : but this iden in	the annals of the Upper Hastings, which we were proud to be in-	Giass (Coth and Line), 1/3 /10 36-Inch Lineme, all colours, 1/3 /13	INSURANCE	lived alone in an old garage at the rear of premises at May's Hill, Par-
	ted to participate.	35-Inch White Calloo	on Most Advantageous Teries	ramatta, was found lying III and in a state of collapse. He was taken to the Lidcombe State Hospital, but died
ads Act, which subsidised coun- if for f ; this grew to 30/-, then	AT CATHIE CREEK.	In fact Hundreds of other Lines, All Reduced to Cost, but Too Numerous to Mention.		two days later. The old man was thought to have died penniless until
£2 for £1. Then councils were RE	ESERVE FOR PUBLIC RECRE- ATION.	Rommanis Galore, Half Cost and Less. First to come gets the Pick.	For Rates and Particulara, Apply to R. G. DAVIDSON,	just recently, when nearly £200 was discovered, cunningly hidden in the
	Mr. L. O. Martin, M.L.A., Minister		Agent at Port Macquarie.	garage. Constable Shortland went to the building to examine the dead manin
ed to help councils in the con-	Justice, has received the following iter from the Under Secretary for	Clothing, Mercery, Boots, Shoes, Slippers, Etc. A Wide Range of All Description for less than you can	LARGE WHITE PICS. Remove the Film	building to examine the dead man's belongings, and came upon a hoard which amounted to £192/2/114. It
y, as with bad bridges the ex-	ands: "I have to inform you that by Ga-	purchase in the City. To inspect them is to buy. There are Won- derful Bargains in All Sections.	FROM GOVERNMENT HOG FARM. You merely think your teeth are dull, " off	was in notes and coin, and was hid- den in aumorous places. Some of the
ng beyond the finances of coun-	tte notification of 19th May, 1933, area of 50 acres on the northern reshores of Cathie Creek at its out-	Men's Strong Working Trousers	PEDIGREE STOCK OF EFFHER SEX AVAILABLE. Combats serious tooth and guns disorders. too.	notes were sewn into the hems of trousers and coats, while the coins
roughfares had been built up to	to the South Pacific Ocean was served from sale for Public Rec-	Men's Extra Heavy Working Trousers 6/11 5/11 Men's Extra Heavy Working Trousers 12/6 8/11 Men's Extra Heavy Working Trousers 12/6 8/11 Men's Extra Heavy Working Trousers 4/6 2/11	TERMS GIVEN. Old-type dentifrices failed to remove that Apply. flins successfully. That's why brushing has	were wrapped in birdseed bags and sewn in the corners of sugar bags.
partment. The idea was : "You feel over had reads ; but the 63."	ation as Reserve No. 55,945 (No. 946 from lease generally).	Boys' and Youths' Shirts	MANAGER, special film removing dentifrice, Pepsodent,	The rest was hidden in old socks, paper, match boxes, and tobacco tins. The money consisted of notes, sov-
set over bad roads; but the	"The locality in question is stated be a popular one for fishing and	Men's All Wool Black Cashmere Sox	L(T). CITY, P.O. CANBERRA. COST 20. Tech whiten ; sparkle like jeweis, Batteria that thrive is film are com- batter : toth deray. your truthles and	ereigns, half-sovereigns, one crown, half-crowns, florins, shillings, six-
icy was endorsed at the recent	mping, and the facilities offering	Men's All Woot Pullement	jeweis. Becteria that theire in film are come	
to can be a increased at the recent to can res. Conference, and its value the recisied at more, or better than	ere are largely availed of for those	Men's All Woot Fullovers	bated ; tooth decay, guns troubles and	half-pennies. It is thought the do-
to the state and the received of the state o		Ditto Ditto 16/6 11/6 Ditto Ditto 21/- 16/6 21/- 26/6 Ditto Ditto 22/- 20/- 20/-	Promodent now at any Chemist. 2019	half-pennies. It is thought the de- ceased had been hearding his savings for many years, and in small am-
to U. Was endowed at the recent to can res'. Conference, and its value the precisived at more, or better than the solid Crossing Bridge, Mr. Nevell able Crossing Bridge, Mr. Nevell have the Switcher Mr. Service Con-	re are largely availed of for those rposes by residents of and visitors Port Macquarie, a number of whom	Ditto Ditto	Messrs. McDonell & Moffitt,	hair-pennies. It is thought the de- ceased had been hearding his savings

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ORDINARY COUNCIL 16/06/2021

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Rind.		THE PORT MACQUARIE NEWS, SATURDAY,	IUNE 23, 1934.	· · ·	
ANNUAL EASTER	without which help the Club would be placed in an awkward position, Mr.	WINTER	"UNCLE" JOHNNY	the first brake he had fitted to his dray).	to anything almost, that I was call- ed up to do. I have built bullock
	A. L. Keens for the use of his supper room for the settling up. Mr. R. W.	WINIER	COOMBES.	"In my early days with a team	day would take my hat off to none
	Reckless for the use of his boatshed for housing the 18-footers, Mr. Ken	MARTED	The subject of this notice, now in his 90th year, was born in Dorset-	for wool and six shillings a cwt for back freight. The trip from Port	in the making of bullock tackling. I have known hard times as well as
A meeting of the Port Macountic	Waiton for valuable services render- ed, the ladies of Hibbard, Mrs. F.	WINTER	shire, England, on 28th May, 1844, and came to Australia with his par-	Macquarie to Walcha would take a fortnight if the weather was fair.	seed times, but, of course, life has not been all work and no play. In
ast the President Mr. A. A. Cum-	Condon, Mrs. C. Kilmurray, Mrs. Crozier, Mrs. Syron, and Misses	MINITED	ents and other members of the fam- ily in 1849. He can well recall in-	but of course in those days there	a long life I have seen many amus- ing things, such as when Wallan-
also in attendance, Dr. N. E. McLaren,	Blackman and Vera Staples for as- sistance with competitions, the ladies	WINTER	cldents on the voyage out on the sailing ship "Victoria," and par-	streams, and I mind being cump- ed by Yeldham's (site of the	garrie, the dog, with a tin tled to his tail, met Mary Dowd in the mid-
H. Marshall, R. Swanson, R. Dick, A.	who ran a ball at Port Macquarle in aid of the funds and the voluntary		ticularly remembers landing at Newcastle, then a small and dirty	present Yarras Post Office) for	die of the log bridge. (Here the old gentleman chuckled at the thought
ford N. T. Melaran	workers for their help in preparing the ground. Much of the success was		little coal-mining town.	other occasion, after swimming the bulleris through the Elienborough	of the good lady's immersion in the river). However, I sincerely hope
from Messrs. A. C. Elliot and A. E.	due to these people, who had spared no effort to help in every way.	A Special Perchase of all Your Woollen Require-	"Uncle Johnny," as he is known for and wide, is in possession of all	Hiver, two tearns being haoked on to the load, and all available men	none of my younger generation will know such hardships as I have
A letter was received from L. A.	The President seconded the vote of thanks, and added the names of Dr.	ments, made early, thus saving a big rise that has taken place in the Woollen Goeds.	would do a fair day's work with the	in the vicinity being pressed into	thrown. But I have lived through it all and, maybe, am none the
	N. F. McLaren and Mr. A. G. Munro	Ladies' Cardigans, Pullovere, in endless varieties.	best of them: In fact, the "younger generation" over that when the cut-	held up for three weeks near Nor- ric's (Ellenboreugh Post Office).	worse for it, and take pride in a wide circle of friends and acquaint-
o Sydney.	for their splendid efforts with the advortising scheme through papers	Dress Flannels, all shades, double width, direct from Mill.	that on the Forbes River side of the Pouble Crossing Bridge was being	The load on this occasion was	"Shire Gazette."
or noner with report of regatia.	on the tablelands and further west. The Club was very grateful for their	Flannelettes, Wincoyettes, in stripes or plain colors	but in by voluntary labour the old sentleman hold his own with the	got a good weiting. Six bales of	
F. West, thanking the Club for its support of "The Open Boat."	prised at the great number of people	-special values.	young brigade, swinging the shovel and mattools carly and lates	days, but as time went on, and the roads improved, we kept on increas-	NOTES FROM HAWNETE,
The correspondence was received. The Secretary then read the state-	attracted here as a result. He also supported M7. Blair's remarks in re-	A nice double width fancy Flannelette for Ladies' or Children's Pyjamas.	Histover, let the veteran tell his tale in his own way, although many	ing the number until I brought 16	Gold prices have improved 80 per cent! Why? No one is guite sure.
ment of receipts and disbursements	gard to all others who had helped the Regatta Club. The McInherney	A big stock of Beldwirs and Palone two, three or	of "the yains, aye, and super varies, forsauly," will lose much in	Par, Macquarie. This was the larg-	We trade on paper credits. Few sov- reigns pass in local shops now. No
Receipts.	Family had always been a great help to the Club in every direction, and	four ply Wool, in all wanted shades, at City Prices.	cold print; to be fully appreciated they need to be told as only "Uncle	New England road. I also brought	one makes any fuss. Misers hag some greedily. Platinum and osmoridium
Donations 126 14 6	were to be specially thanked for the very generous use of their grounds.	Knitting Needles ; pattern books of latest ideas just landed.	Johnny" can tell them in his own		are dearer metals, yet gold is at the top of our quite old day dreams and
tale of booths 46 0 0	They believed that this aid was one of the things in their life. All others	A good range of Waellen Dress Goods-what you	inimitable way. "On arrival in Newcast 2 (the	cred wool, one from Waterloo and nine from Tiara. I left Tin Station	ambitions.
Competitions 28 7 0	mentioned had willingly beloed in the interests of the Club and the town.	need for the cold weather. All New Goods at Reasonable	lote Mrs. George Hollis, my sister, was born that same night) where	1681."	When our Garden Paisce was built in 1879 an oblong shaft, 30 feet high,
Side shows		Prices.	we get off at night, our traps were loaded on to a cart, and we were	"Besides carrying wool and shep- berding. I have also shorn sheep	gilt over, represented £600,000,000 worth of gold won in Australia since
	Inherney family the regatta could not		taken to a bearding house. On the way the eart capsized, and Ted,	(for which we got 15/- a 100 and a	1850 and it only took 5/- worth of gold to do the gilding. See how
and the second	be held, and they were deserving of all the praise that could be bestowed,	MEN'S, YOUTHS' AND BOYS' WINTER WEAR.	then a little codger, was lost for a while in the sand. Father and my	have also shwn wheat, reaped it.	even if only make believe. And when
is d	and more than the Club could give them. However, the Club realized the	A Solourild Rouge of Sweaters in Pullovers, Button	brother, Harry, took work in the cool mines, but when a chance of-	it with a fiall, and ground it into flour. I also carried wheat to the	that great palace was destroyed by fire in 1882, the 5/- worth of gold
Printing and advertising 30 14 11	great value their help had always been, and he agreed with the Presi-	or Zipp Fastmers. These are worth buying-prices most reasonable for the newest goods.	fered they took a job of shepherd-	ald flour mill which was situated on Windmill Hill, Port Macquarie. We	mixed with the dust and ashes below. No one could collect it.
Catekeepers & attendants 14 0 0	dent and Secretary in connection with the other help rendered the Club.	Flannel Undershirts in mid, dark or natural greys	then the property of William and Richard Denn. We left Newcastle	grow wheat at Filly Flat when we first came to live on the Forbes	Turn now to Nature's stores of alluvial and reef gold in Australia!
Hire of launches & cars 12 3 6 Intertainment of visitors &	The motion was carried.	-Value Unsurpassed.	on a bullock dray and six bul- locks-Bounce and Monkey, Black-	River, but later turned to corn Towing. I well remember making	Will our fields re-open? Can new ones be found? Prospecting has gone
settling up 31 7 0 Loud speakers	INTEREST RATES.	Warm Flannelette Pyjamas, all splendid wearers, and extra special value.	bird and Jolly. Tippler and Roger, in charge of Geo. Jones. It took	- plough with an old share, every	on for 80 years with varied results. Still be sure many an old digger.
toad & ground expenditure 6 4 0 stamps, telegrams, etc. 3 10 8	AUSTRALIA AND THE SOVIET.		six weeks to reach our destination.	ther part-handles, mouldboard, landside and all being of wood, as I	many a young trier, many a woman like the woman in Christ's parables,
Store accounts 3 3 6 Woodchop blocks and re-		All Wool Indiana Sox, fancy marles. Hundreds of designs.	wage was-in those days they used to shepherd and watch-but a re-	of pork" better to hill the corn than	will be abroad before daylight on the
gistration 1 15 0	The Prime Minister (Mr. Lyons) commenting upon the Labour party's	All Wool Undershirts and Trousers, solt, warm	tion of 10, 10, 2, and 4 was supplied -10 ibs, each of flour and beet, 205.	roundly abused by my father for	old fields of Baliarat, Beechworth, Sandhurst, Turon, Bendigo, Hilf End,
Railway freight (numbers	plea for nationalisation of banking as one of the most effective methods	and cosy.	of sugar, as black as the devil's hat,	"We also engaged in cedar getting	Wyalong, Tumbarooro, Araluen Braid- wood, Lambing Flat, Mount Morgan,
Municipal rates	of reducing interest rates, suys : "The only country where there is	: o` :	and a j lb. of ica. The equipment supplied to a shepherd consisted of	ing it mostly to Walcha. For this	Gympie, Cloncurry, Palmer, Coolgar- die, Southern Cross, etc., after heavy
	complete nationalization of banking is Soviet Russia.	BLANKETS, RUGS, EIDER AND WADDED OUILTS-	a three-legged pot and frying pan, with a long handle"-the old gen-	though my brother Ted and I took	rain, with electric torches keenly scanning gutters, finding scaly,
	"The Soviet has just issued a loan at 7 per cent., while the Common-	SLANKETS, ROGS, EIDER AND WADDED COLLIS-	tleman here proudly produced a handle which belonged to the fry-	Tamworth, for which we got	shotty gold, as others did with lan- terns long ago. Did not the news-
	wealth of Australia, with a careful-	And the second sec	ing pen with which his people were supplied - "and a quart pot. Our	£1/10/0 per hundred. "Much of the cedar on the Forbes	papers tell us of that 15-ounce nug- get found a few weeks ago at Gui-
H. J. Blair, Secretary; H. T. McLaren,	ing a central bank and privately own- ed trading banks, is floating a loan at		people got a double ration because we had two flocks. However, father	River was felled by old Florence McCarthy, and left where it was	gong. Be sure of this fact :
Munm Auditors.	31 per cent, the lowest in the his-	Sall - USA	got bushed first night on watch and was cut all night. He shortly after-	felled. This same man was the shap who was responsible for the	breast, Man never is, but always to be blest!
Mr. Marshall moved the adoption	"These contrasts in methods and re- buits speak for themselves."	Extra_Warmth	wards developed rheumatism and from then on until he died on	naming of the hill, "Wine for Din- ner." Taking a load of wine from	What about local chances too? Years ago blacks came to a local
redit to all concerned to show a pro-	the second se		March 6th, 1870, he was more of less crippled up. Later on our fam-	Port Macquarie, where it was made	
avourable weather conditions. Mr. Swanson seconded the motion.	ROLLANDS FLAINS.		ity shepherded for Todd and Fen- wicks on Yarrowitch, and Peter and	tities, to New England he upset the	of something like lead in a deep gorge back of Bunya Mountain. They hit
Dr. McLaren supported the adop- ion, and said the result was a credit	(Contributed).		Sandy McNab, on Benditi, who then also owned Tiara Station.	mates had wine for dinner, and ever	it with a tomahawk, but only made a shallow cut. None of them could
o the President and Secretary and	The weather has been beautifully fine for the past week, with some		"We afterwards took on dairying at Moorabark, and used to pack the	name.	say what it might be. The little
aired with fine weather the credit	sharp frosts, but seasonable, and we must expect winter to set in in carn-		butter and cheese to the stations	beech and mountain ash were taken	scheme might have worked, but King "Cold Patter" wanted supplies of
and a line summariad the	est. The month of May was very mild and warm, more like spring.	but me	where we used to sall it. Major Innes had a big dairying establish-	to Wauchope for shipment to Syd- ncy. I remember sending a con-	flour, sugar, tea, bacca, and matches before setting out. The Bunya big
in indication of the result of having	The day's sports in aid of the Hast- ings District Hospital, held at the		ment at Cowarral on the Forbes River, and old Sandy Cameron and	signment of 1170 feet of mountain ash to a firm in Sydney, and, after	Hanging Rock was once a gold field
ear. Not only were the 18-footers	recreation ground. School of Arts, on the 4th inst., was a success. The sport	EXTRA COST !	Mat. Kirkman used to pack the but- ter and cheese to New England and	a considerable time, got a string of clamps to the value of one shilling	near the Manning and Hunter head- waters. John Jones, a coastal boot-
tractive, but also the speed motor	was very clean, and consisted of hunt- ers, high jumps, fag race, pie melon	It's because of their deep, lasting fleeciness that	Fort Macquarie, (Harry Hogan, with Jack Gillis as flunkey boy,	and ninepence back in return for our sweat.	
vents would always draw a big at-	and cigarotte race, foot races, throw-	Challenge Blankets are so warm and comfortable. The	used to pack rum round the sta- tions on New England and about.)	"Speaking of the old Walcha road. Old Jas. Noakes, his son Bob and	good bay hackney, and made 10 pairs of heavy boots, stowed them in bags
hat if all helped the regatta could	step and jump. The financial result, it is said, will be over £20. The ball	nap doesn't come out in use or disappear in washing.	However, very little of this remain- id when we came to the Forbes	my brothers Jim and Ted, cut the	well adjusted, armed himself with a double-barrelled pistol in a bolster.
The President said there was fory	at night was very well patronized. The recreation ground here could	And they have ample width and length for cosy tuck- ing-in. Ask to see a Challenge Blanket you can	Eiver, which was in July, 1859, fath- er having bought Filly Fist. When	1371. I was with them, but was laid up in camp for three weeks, and	and started south-west at 4 a.m. one summer morning. By noon he had
	be made a beautiful sport's ground	tell the difference by the touch.	we came from the tablelands there was little settlement on the upper	existed on a bit of boiled rice and a bottle of wine, which one of the	travelled 50 miles, and lunched. Again, at 5 p.m. John took a short
crease in the donations, as the re-	with very little expense. Mr. Laws has taken over Mr.	CHALLENCE	river, and the blacks, then in their wild state, were plentiful. There	station owners kindly sent me. Previous to that teams used to go	sleep under a shady tree, while his
the wheel Port Macquarie could	O'Leary's cream run on the north bank of the river.	CHALLENGE Odourless, free from	was a big camp right here where I now live. All the dress they wore	through by the Nobs, but it was a bad bit of road. (Mr. Hibbard re-	charger fed close by. Alas, John was too weary to notice an active
big things with the regatta. Had	Mr. C. Kent, it is reported, is going on to Mr. E. Farley's farm, vacated	DIANIKETC tood by every rotailer.	was a possum wool belt in front and behind, and they always car-	had bit of road. (Mr. Hibbard re- marks that he had occasion to fol- low the old road through the Nobs	swaggie drop his small bundle, and mount the horse, pistol in the holster
kings could easily have been £200.	by Mr. Albert Workman. Much sympathy is expressed for Mr.	BLANKETS filing and fully guaranteed by every retailer.	ried fire when on the move. The gins carried the picaninnies and the	a couple of years ago., and marvels	still, and canter away over the hill. No help for it, boots, horse, and gold-
f122/7/5. Had that little bit of (3. Baldwin and family in the loss of his wife. Also to Mr. and Mrs. Clou-	PCI .	puppies in a net hung from the head. Many of their customs were	how bullocks and laden drays were ever piloted through that country).	field and pistol were stolen forever, and Jones walked home two days
Club might easily have slipped it	ten in the death of their infant son.	. All bought before the rise in Wool. Blankets are	very queer, as were also some of their foods, roots of cungevois first	"The Oxley Highway practically follows cur route by way of Bruin's	later a sadder and wiser man. Some gold will be won in Copeland,
mes for the committee and the t	cook piace at Rollands Plains Cemei-	easily worth 8/- to 15/- more per pair than I purchased	beaten to a pulp, ants' eggs found	Camp, so probably I've had as much to do with Oxley Highway as most	Pignum Barney, Nowendoc, Coolongo- look, the Colls, and Terrill, but not
tab should get back to the position it	ev officiating at the grave, and Mr.	for, so there is a big saving in this department alone.	in a white gum, locusts after being singed over the fire were eaten like	men. (Mr. Coombes for years ad- vocated the claims of the Walcha	enough to satisfy the market which will still stand at something over
held a few years ago. The regatta i	of the funeral arrangements. The		shelled peas. The lower part of the bull-rush was also eaten, and many	Road as being the best direct route from the tablelands to the coast.	ers will make to big banks bearing
ractically the only thing spart from I	nother of this child was formerly Miss Cathy Fox, daughter of the late	LADIES' GENTS' AND CHILDRENS' FOOTWEAR-	a good feed of them I have had. Squirrels too, after being blown up	and the writer can vouch that it was his advocacy that was primar-	ounces, penny-weights, and grammes,
emarkably well, and put a wealth of 1	Mr. James Fox. The bridge across the river at	Over a thousand pairs to select from.	with wind, were reasted and eaten. "In the making of their spears	lly responsible for the ihen Minister for Local Government, Hon. J. J.	to be disposed of. Some years ago Jules Renault, a
ersonally, wished to thank the mem-	ing it. The bridge should be a great	A beautiful shoe in tie, lace, or court. Black, tan or white for 12/6. Absolutely the best value over offered.	and other weapons, the aboriginals used vines and a wax made from	Fitzgerald, having a sum of money f6000. I believe, was the amount	ratired money-londer near Paris, in France, offered the National Treasury
ort accorded him this year. It was h	selp to those farmers on the north		the grass-tree. Their spears were very light with a wooden point	-set aside for expenditure on this	£2,500,000 in gold on loan at 4 per cent. While waiting a reply Jules
stounding to find they had a credit	ide of the river, and much praise is	Men's Black or Tan Space, all latest from 12/6 pr	Agil when a woonen bound	road. This was really the begin-	CONTRACT OF A DECEMBER OF A
stounding to find they had a credit a alance, which no one expected under d he conditions experienced. On behalf a the Secretary and himself he	the to those persons who voluntarily asisted in providing the timber and	Men's Black or Tan Shoes, all latest, from 12/6 pr. What with leather values risen, these are exceptional prices.	made as hard as iron by some pro-	ning of the Oxley Highway as we	descended into his secret vault under the house, to tally his treasure. The heavy iron door fell down over him,

of the Becretary and himself his insumerable visitors, and all should support it in the town's interest. The motion was carried. It was resolved to pay the Secretary his strip to Sydney in the interest of the registra. The Secretary moved a very hearty and comprehensive vote of thank to the following: Tho Meinherney Fam By for the free use of the ground and other values and for the free use of the ground and other values and for the free use of the ground and other values and for the free use of the ground and other values and for the free use of the ground and other values and for the free use of the ground and other values and for the free use of the ground and other values and for an estimate in con-tary and staff for ansistance in con-and envy said picking them up again.

A HUGE STOCK.—CALL EARLY. AND GET THE BEST SELECTION EVER OFFERED IN PORT MACQUARIE.
In constraints and point of the old tamisters, among them be the observation of the old tamisters and the operation of the old tamisters and the operation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the observation of the old tamisters among them be the o

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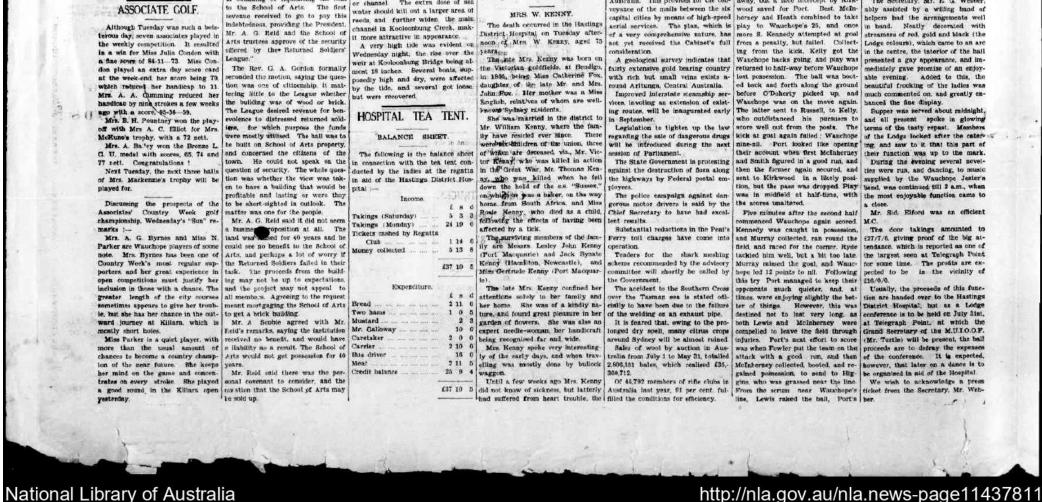
ORDINARY COUNCIL 16/06/2021



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quarle 7 p.m. Canon L. Gray. Parish of Upper Hastings. June 23. – Wauchnee 7.20 an (H.C.), 7 p.m., Ellenborough 11 an (H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.	 presided at a meeting of the School of Arts committee on Monday night, when the following business was 	Mr. J. Johns asked Mr. Orr would be advance the money. Mr. Orr said the only thing that would prevent him doing so was that he could prevent him doing so was that		cause of her passing. She had been an immate of the Hospital for four	FOOTBALL.	back line got going, but a dropped pass spelt finish to the movement
June 23Rollands Plains 11 an (H.C.), Telegraph Point 2.30 p.m Port Macquarie 7 p.m. June 30Upper Rollands Plains 1 am., Gum Scrub 2.30 p.m., Port Mac quarie 7 p.m. Canon L. Gray. Parish of Upper Hastings. June 23Wouchnee 7.30 a.m (H.C.), 7 p.m., Ellenborough 11 an (H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th.L. CATHOLIO CHURCH.	 presided at a meeting of the School of Arts committee on Monday night, when the following business was 	Mr. Orr said the only thing that would prevent him doing so was that				THE THE VERY AND A THE VERY AND A
(H.C.). Telegraph Point 2.30 p.m. Port Macquarie 7 p.m. June 30Upper Roliands Plains 1 a.m., Gum Serub 2.50 p.m., Port Mac quarie 7 p.m. Canon L. Gray. Parish of Upper Hastings. June 23Wauchnpe 7.20 a.m. (H.C.), P.m., Ellenborough 11 a.m. (H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.	 presided at a meeting of the School of Arts committee on Monday night, when the following business was 		APPL IN THE REAL PROPERTY AND INTERPOPERTY AND INT	daya.	WAUCHOPE TROUNCES PORT.	which looked like culminating in a
June 30.—Upper Rollands Plains 1 am., Gum Scrub 2:30 p.m., Fort Mac quarte 7 p.m. Canon L. Gray. Parish of Upper Hastings. June 23.—Wäuchnee 7.30 a.m. (H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.	when the following business was	we could get belier interest for his		The funeral took place on Wednes- day afternoon, and was largely at-		try. Kirkwood was next to break away, but with no one in support
am. Gum Scrub 230 p.m. Port Mac quarie 7 p.m. Canon L. Gray. Parish of Upper Hastings. June 23.—Waukinge 7.30 a.m. (H.C.), 7 kindee 3 p.m. (H.C.), Kindee 3 p.m. Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.		money.	District Hospital on Thursday morn-	tended. The service at St. Agnes'		was well tackled by Marchment
Canon L. Gray. Parish of Upper Hastings. June 23.—Wauchope 7.30 a.m (H.C.), 7 pra., Ellenborough 11 an (H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.		Mr. B. H. Pountney said that hav- ing agreed to allow the League to	Plains, aged 19 years.	was conducted by the Rev. Father O'-	and the cup team played similar teams from Wauchope on Saturday	missed a further pot at goal, but
Parish of Upper Hastings. Juns 23.—Wauchope 7.30 a.m (H.C.), 7 p.m., Ellenborough 11 a.m (H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.	A letter was received from the Fort Macquarie sub-branch of the	build a hall on the School of Arts	The lad was the eldest child of Mr.	Mahony, Mr. H. T. Wilkins had	last, on the latter's ground. In	play remained near Wauchope's line
(H.C.), 7 p.m., Ellenborough 11 a.m. (H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.	R. S. & S.LLA., signed by the Pro-	ests of the town, to have a brick	Plains, and had been in ill health		each game Wauchope recorded easy wins; the scores in the first game	way, and then Wauchope's forwards
(H.C.), Kindee 2 p.m. Rev. A. Edward Warr, Th L. CATHOLIC CHURCH.	Ro Returned Soldiers' Hall.	structure than a wooden one. Other informal discussion followed.	for some years. When about eight years of age Milton contracted scarlet		being 27 to nine, and the cup game, 28 to nil.	
CATHOLIC CHURCH.	At the meeting of the Fort Mac- quarie branch of the R.S. & S.I.L.A.,	and Mr. Orr, in reply, said it re-	fever, the rawages of which impaired	Drew, Brown & Drew report :	The attendance was fair, whiist	the blind side, sent to Annand, who
	held last week, it was decided to	minded him of the words of an old story, "Where there is no vision the			the conditions for football were good.	scored. A. Smith's kick went wide
June 23.—Port Macquarie 7.30 n.m. Waucheye 10 a.m. (Mass), Port Mat	submit to your committee the under-	people perish." If the people in the	The purezed and family will have	per bushel for both yellow and white.	game, and Mr. K. Bain the big fix-	and Gailagher, then Fowier, Ryde,
quarie 7.50 p.m. (Benediction).	prection of the proposed Memorial	past had done nothing where would to-day's generation be. They had a		To ensure market holding we advise steady abipments.	The competition more had not be	and Higgins took play to Wauchope end. Wauchope's three-quarters got
June 30Wauchope 7.30 a.m., Per Macquarie 10 a.m. (Mass), Port Mac	t Hall. The lowest tender received for the	choice in this matter. Permission	the Church of England Cometery to-	second mathematical	in progress long when Wauchope	going, and eased to half-way, and a
quarie 7.30 p.m. (Benediction).	hall , built of weatherboards , neither	had been granted to build a weather- board hall ; did they prefer that to	officiating.	KINDFE.	started to crowd the visitors line, and soundly predicted a good score. Port	little later Murray collected, ran up well, and sent to Marchment, who
Father O'Mahony.	lined nor celled, was £865. One builder, who went through the plans.	a brick hall. The costs of upkeep		(Contributed).	held them at hay for quite a while.	went over in the corner. Ed. Mor-
June 23 (Home Mission Sunday)	ote, estimated the extra cost of	of the two buildings was out of all proportion. There were various ways		Congratulations to Mr. and Mrs. J. Hollis on the arrival of a daughter.	however, and at times looked equally as good a team. Bad tackling by	nil. Wauchope were combining bet-
Port Macquarie 11 a.m., Telegrap Point 2 p.m., Wauchope 7 p.m., Rev	not submit a tender. The lowest	of liquidating the debt, and the R.S. League had already raised a lot of	Mrs. Celia Maud Hyde, wife of Mr.	Mr. N. Kirkman met with a nasty	Port's team was largely responsible	ter, and were now giving better foot-
M. Walker. Wauchope 11 a.m.	tenderer estimated the extra cost at	money. If they only thought of the	54 years.	accident a few days ago through be- ing thrown from a horse. He was	for their huge defeat, as has been the case in most of their games this	Lewis had both retired from the
Blackman's Point 2.30 p.m., Port Mac quarie 7 p.m., Captain J. E. Jenk	opinion of the architect.	present nothing would be done in the town. It was also questionable	The late Mrs. Hyde was bom at	rendered uncenseious for a time, but		game. Wauchopp next scored when
ins (22pd Battalian).	To erect the hall in brick and sup-	at what period the hall would revert	late Mr. and Mrs. S. A. Edwards. She	The new school tennis court is al-	up good combination, which over-	being tackied. He lost the ball, and
June 24 (Monday) Port Mac quarie ; Tuesday Wauchope ; Wed	fore cost about (1250.	to the School of Arts, and were they going to allow a wooden hall to be	was married at Port Macquarie to	most completed. With a little prac-	whelmned Port. At half-time, Wauc- hope only had the reasonable lead of	Morcom collected, and went on to
nesday, Beechwood, 7 30 p.m., Captai		built, in pereference to a brick one,	en up at Blackman's Point. There		10 to seven, Radley scoring the try	Wauchope, 23-nil. Shortly after-
June 30 Port Macquarie 11 a.m.	vernment Building Committee 1600	because they would not raise £250. The object of the hall was to raise	were five children, viz. Mesars Har- old, George, and Missen Elsic, Vera	The continued dry weather has	for Port, whilst Kirkwood landed two	wards, Annand and Marchment were
Wauchope 2 p.m., Beechwood 7 p.m.	(on the guarantee of 12 persons for fife each), leaving f250 to make up.	revenue to help distressed returned	and Doris.	made it possible for motor traffic to cross the river, providing the driver	hope resled off a succession of tries	score, and once more Murray goaled.
p.m., Mr. C. G. Orr. Wauchope	We are anxious to erect the hall as	soldiers. Mr. Reid feared the driving factors	Mrs. Hyde, who found pleasure in her home and with her family, had	is not nervous, and is prepared to	to take their total to 27, whilst Port	Wauchope 28, Port nil, which were the full-time scores. Just before the
p.m., O.K. Band. Bey, M. Walltor.	carly as possible, as building costs are rising, and appear likely to con-	in the League may not always be	been ailing for some time past.	"young fellow" still insists on keep-	Similar to the early game, the cup	final whistle, Annand, of Wauchope,
PRESEVTERIAN CHURCH	Linue to do so.	here and there may then not be funds to meet the expense.	The sorrowing husband and family will have the sympathy of a wide	ing bad hours.	team fixture provided most uninter- esting football, as except for a while	the result of a nasty bump.
OF AUSTRALIA. June 23. Port Macquarie 11 a.m.	As the hall is to be erected on School of Arts land, and will revert	Mr. Orr said they were to raise	circle of relatives and friends.	of the question now as the ground is	in the second half. Port were unable	The teams were :
Beechwood 2 p.m., Wauchope 7 p.m.	to that institution at the expiration	£250, whilst the League had £1000 to meet. If the League failed, a clause	The remains were inid to rest on Saturday morning in the Methodist	almost too hard to plough. Rain is	to make a race of it. McInherney was the only Port man that was play-	
June 30.—Telegraph Point 10.3 Lm., Huntingdon 2.50 p.m., Por	greatly to the advantage of the	in the agreement would allow the	portion of Port Macquarie Cemetery.		ing to form, and the little assistance	O'Doherty, Annand, Morcom (2).
Macquarie 7 p.m.	School of Arts if it is built of brick.	School of Aris to take the half over.	the officiating minister being the Rev. M. Walker, there being a large at-	BREVITIES.	he got greatly dimmed his efforts. On the other hand, Wauchope were	Port MacquarieKennedy (2).
Rev. G. A. Gordon. FREE FRESSYTERIAN CHURCH	Under the circumstances the League decided to ask the School of Arts	S votes to 4.	tendance of mourners and friends. The	A munition factory at Reinsdorf.	a definitely improved team since their visit to Port, and have established	Smith, Kirkwood, Gallagher, Heath.
OF AUSTRALIA. June 23.—Watchope 11 a.m. Up	members whether they would be will-	Mr. A. G. Reid, were appointed to	Wilkins.	in central Germany, blew up while	themselves as a formidable foe. Al-	
ser Plains 7 p.m.	Savings Rank the extra £250 re-	act on behalf of the School of Arts		working at full pressure. There was a heavy death-roll, and hundreds of		This Saturday Port Macquarie goes
June 30.—Forbes River 11 a.m. Wauchope 7 p.m.		to meet the R.S. Lengue in finalising this matter.	MISS J. GALLACHER. The sudden passing away carly on	workers are suffering from injuries.	Port could make little use of it. Sev-	to Taree in an endeavour to lift the
Rev. M. C. Ramsay.	The advantages of a brick build-	The President was authorised to	Wednesday moraling of little Miss	A fire followed the first explosion, causing other explasions, which lev-	eral times Best was seen sliding the ball along the ground to McInherney	Manning Cup. It will be the fiirst cup game in the Group this season.
THE	ing need hardly be pointed out, the saving in insurance is 10/6 per £100	necessary.	Joyce Gallagher, aged nine years, was a terrible blow to her parents, Mr.	elled the entire works. The fire, which	as a pass, or when the latter did	and should provide an interesting
Port Macquarie Actus.	per year, the repairs practically nil,	The Secretary with automotors is	and Mrs. J. S. Gallagher, of Port	had the appearance of a volcano, burned for many hours, and prevented	manage to get it, one of the centres, Kirkwood or Gallagher, would knock	game. Port's team will be :
	and the heavy costs of painting walls	templated applying for a loan over	Macquarie, and a shock to residents. It is also a reminder of the very	the rescue of the injured. Many	it on or drop the pass. With Wauc-	wood (2), McInherney, Best, Fowler,
SATURDAY, JUNE 22, 1935	be permanent.	the building, and inquiring the usual preliminary procedure.	uncertainty of life.	neighbouring houses were demolished, and windows were shattered miles	hope seizing every opportunity that these mistakes allowed the score	Ryde, Higgins, Dick and Lewis. 'Bus seats are available at 5/
LOCAL AND GENERAL.	The League has rented the Ro- tunda Green for another carnival at	It was resolved to forward a letter	Hale and hearty on Tuesday mora- ing. Joyce went to school. Whilat	away from the scene.	can be easily understood. Not that	
TALE OF COTTAGE & LAND	Christmas, and His Excellency, the	of thanks to the Municipal Council	passing through the town that morn-	The Victorian Transport Board re- fused applications for licenses to op-	of the above, but quite a few of these	TENNIS.
Mrs. A. M. Neville has disposed of	hall, and the proceeds of the carnival	the second s	ing, when a strong wind was blow- ing, the child brushed her hair from	erate 16 motor vehicles between Mel-	mistakes were opportunities missed	Last Saturday's games resulted :-
er cottage and land at the junction Munster and New Streets, to Mrs	and the opening (which will include	VERY HIGH TIDE.	her face, and then bumped into a	decision was criticised in the Victor-		Port I defeated Wauchope Wag- tails, five sets to four.
C Munster and New Streets, to Mrs Lindsay.	Regal Ball at night) will we are	Tuesday evening's tide was a very	barriende on the footpath of the main street, where building opera-	ian Assembly, a member saying that it was detrimental to the country.	and against Port's weak tackling gam	Port II defeated Wauchope War- riors, five sets to four.
WEIGHTS AND MEASURES.	confident, fully repay the 1250.	high one, backed up with a heavy	tions are in progress, fell on the con- crete footpath, bumping the back of	The Commonwealth Government	the impression that anything might happen when either had the ball.	In the second grade Hibbard de-
On Wednesday, Mr. Cumming. In	tenders in price, we state of serv-	and the effort man entdent at the	her head. Joyce went to school, but	has taken action to suppress interfer- ence with the erception of wireless	Shortly after the kick-off. Murray attempted a penalty goal, but failed,	fented Port five sets to two. Teams for Saturday are :
pector of weights and measures cov- ring the area from Port Macquarie	much obliged if a meeting of your	at the entrance has considerably dim-	complained of not feeling well, and was told to go home. On arrival	programmes in Canberra. The State	and soon afterwards this same player	Port I v Warriors, at Wauchope-
a the Queensland border, had a busy	possible to deal with the matter.	inished on the southern extremity, and the root of a tree which has rest-	home her mother told her to rest.	Governments will probably be asked to co-operate with the Commonwealth	was seen in a solid run, covering a lot of ground before being tackled.	D. Nicoll, H. Wariters, W. Gordon, A. Munro, Mrs. R. Rosenbaum and
ay testing scales and measures at he local Court House. Objections	Municipal Council, forwarding do-	ed on the eastern side of the spit was,	Later the child became worse and passed away next morning.	in effectively dealing with the nuis-	He played the ball, Earle Moreom	Miss T. Cains.
vers raised by some to a charge be-	Nr Orr menated arrested with	on Wednesday morning out in the middle of the river, having been wash-	Mr. T. Maloney, Coroner, held an	H. J. Bate, M.L.A., of Mountain	secured to run & few yards before	Port II v Wombats at Port Mac- quarieO. Walker, A. Lonsdale, R.
or made to certify their scales were prrect, which seems a bit rough.	the request of the Returned Soldiers'	ed right over the spit.	inquiry on Wednesday morning and, after taking evidence found that	View, Tilbs, gained first place in the	Moreom missed at the goal ; Waue-	Rosenbaum, T. Turner, Misses B.
	League, the committee having al- ready consented to the erection of	Adjacent to the concrete wall run- ning from the Council's wharf a	Joyce Gallagher died from hemorr-	coastal dairy farm championship con- test. Second place was won by R.	hope, 3-nil. Kirkwood and Heath were first noticeable for Port. taking	Cains and A. Forth. Port's 2nd grade v Telegraph Point
BEEAKWATERS DAMAGED. The very high tides and rough sea	the hall. There would be a remark-	wash-out occurred at the foot of the	hage of the brain, caused by an in- jury accidentally received through	Watson Hewett, of Ellerdaie, Rous ;	the play to Wauchope's 25, where S.	11. at Telegraph PointH. Nicoll,
id a fair amount of damage to the	able difference between a weather- board and brick structure, the latter	main street owing to the continual surge, and several feet of the bank	striking a batten and knocking her	and third by H. Badgery and Sons, of Ciramana, South Grafton.		Eric Marshall, Elwyn Marshall, F. White, Miss V. Glen and Miss N.
e port on Tuesday night, and also	being far superior. Eventually the	has gone. Preparations were in	to the levipath in Horton Street, Port Macquarie.	The Queensland open golf chum-	Murray and Smith brought play back	
a Wednesday. The tide on Wednes- ay evening was particularly high	hall would revert to the School of Arts. An immense saving would re-	hand early to repair the damage and prevent further erssion, as this will	Mr. and Mrs. Gallagher and fam-		to half-way, where Kelly secured and short-punted ; Kennedy waited for	M.U.I.O.O.F. LODGE BALL.
at, fortunately, the sea had mod-	sult in upkeep repair, painting and	quickly occur should the heavy seas	in will have the full sympathy of all in this very sad bereavement.	21 years old. His aggregate for the	the bounce, which went the wrong	
ated to some extent. At the North	insurance with a brick structure as	and high tides continue. The tide rise enabled salt water to	The funeral moved from St. 'Agnes'	72 holes was 294, and he won by a stroke from J. Ferrier (Manly), the		The Telegraph Point branch of the M.UIO.O.F. Lodge staged its annual
	they must think of the future. He	The tide rise enabled salt water to run over the Kooloonbung Bridge weir	Church on Thursday morning. and	title-holder.	score in the corner. Murray's kick	ball in the School of Arts at that
hore, the track was washed badly r several chains, and the rails car-	monthed mention	A set of the set of th	WHS STEROOMS OF BOT MOTORS MILLION	The second s		
hore, the track was washed badly r heveral chains, and the mils car- ed away. The old wall suffered to	1250 from the Government Savings	for some time. It did so a few months	was attended by her school mates. The Rev. Father O'Mahony read the		failed ; Wauchope, six-nil. Play re-	centre on Wednesday night last, and, as in past years, the function was
hore, the track was washed badiy r heveral chains, and the mils car- ed away. The old wall suffered to considerable extent from the curve	1250 from the Government Savings Bank, subject to no expense either	for some time. It did so a few months ago; the sait water killing a lot of the reeds and leaving a much wid-	The Rev. Father O'Mahony read the burial service, Mr. H. T. Wilkins coa-	pared a scheme for speeding up the long distance heavy mail traffic in	failed : Wauchope, six-nil. Play re- mained in midfield for quite a while, and then Kelly and Marchment broke	centre on Wednesday night last, and, as in past years, the function was a huge success.
hore, the track was washed badiy r heveral chains, and the rule car- ed away. The old wall suffered to considerable extent from the curve wmwnrds.	1250 from the Government Savings Bank, subject to no expense either in obtaining or liquidating the loan to the School of Arts. The first	for some time. It did so a few months ago; the sait water killing a lot of the reeds and leaving a much wid- or channel. The extra dose of sea	The Rev. Father O'Mahony read the burial service, Mr. H. T. Wilkins con- in-ting the funeral.	pared a scheme for speeding up the long distance heavy mail traffic in Australia. This provides for the con-	failed : Wauchope, six-nil. Play re- mained in midfield for quite a while, and then Kelly and Marchment broke	centre on Wednesday night last, and, as in past years, the function was
here, the track was washed hadly or everal chains, and the mile car- led away. The old wall suffered to considerable extent from the curve ownwards. ASSOCIATE GOLF. Although Tuesday was such a bols-	1250 from the Government Savings Bank, subject to no expense either in obtaining or liquidating the loan	for some time. It did so a few months ago; the sait water killing a lot of the reeds and leaving a much wid-	The Rev. Father O'Mahony read the burial service, Mr. H. T. Wilkins coa-	pared a scheme for speeding up the long distance heavy mail traffic in Australia. This provides for the con- veyance of the mails between the six capital cities by means of high-speed	failed : Wauchope, six-nil. Play re- mained in midfield for quite a while, and then Kelly and Marchment broke away, but a nice intercept by Kirk- wood saved for Port. Best. McIn-	centre on Wodnesday night last, and, as in past years, the function was a large success. The Socretary, Mr. E. G. Webber, ably assisted by a willing band of helpers had the arrangements well

1 11



Item 12.01

Attachment 3

ORDINARY COUNCIL 16/06/2021



tion to the general public are the test. "On the much-discussed question of the marks and possible demerits of provided for in sub-section (b) and provided

there is abundant evidence, not of the vaguer nort, but based upon sound experiment and observation. that milk is an exceptionally valuable food during the whole period of the prowth. It would seem indeed that, apart from merits which would be ex-pected from its known constitutents; its high-quality proteins, its richness in lime and phosphorus, and its (less abundant) vitamins, milk must pos-reas some subtle qualities—prhaps

Noniand and Japan, and lately in a smaller scale in many other centres. The admitted beneficial effect of a The admitted benchicial effect of a milk ration upon Japanese children is vory interesting, as so little is nor-mally consumed in that country. Not a few writers, face to face with the mainutrition of certain classes of the population in China and Japan, have attributed it in part at least to the absence of an adequate supply of mult." milk."

METHODS OF DISTRIBUTION. "It is well to know that our educa-tional authorities have fully recognis-ed the value of giving extra milk to school children. Four years ago the practice was extended, and recently, as you know, owing to Government action, still more is reaching the schoola. It is to be hoped that amid the great attention now being given to milk production, milk prices and milk purity, that inquiries should ex-tend to the question as to whether he existing methode of its distributend to the question as to whether the existing methods of its distribution to the general public are the best

tures Protection Act provides for traveiling statements to be used in the following cases:-(a) In the case of large stock being moved not more preservibed districts; (b) in the case of sheep or large stock being removed from one run to another of the same owner by the most direct route where such runs are by such route not more than 12 miles apart; (c) in the case of sheep or large stock (c) in the case of sheep or large stock being moved by the most direct route to a contiguous holding.

)-R(A STAINLESS RUBBING AND INHALING OINTMENT

Rub it in briskly (as a pain reliev-ing embrocation) for Rheuma-tism, Lumbage, Mascular Sorre-ness, Stiff Necks, Swolken Joints, Tired and Aching Feet. healing and soothing olnument) for Piles, Cats and Sores, Burns and Scalds, Sprsins and Bruises, Insect Bites and Stiags, Sus-burn and Chilblains.

AND VERY VALUABLE:-

COLOS FLU -CI BRONCHITIS =7 **B**" Jaonnung SEARCHES THEM OUT nigen Dresd Breachilds and other Cheel completes which being the gloom of 111-hers (th - the people and colds which Farmer

turesque the banks of the Medway as from Penahursi's narrow course it' travels in ever broadening atream, down lock, 'neath bridge, thro' the heart of the County Town, into the commercialised atmosphere of Ro-chester, Chatham and Sheerness. With their golden gorse and wind-ing path, how charming the undulat-ing Commors of favoured Tunbridge Wells, with hard by the quaint Pan-lies, where in days of yore Beau Nash led fambion's promeande. Little, surely, is more arrestive than our countryside. Take Worth-am Hill. What a panoramic scene of rurau beauty is open to view as the ye sweeps across the valley towards

eye sweeps across the valley towards Maidstone. With its well-cultivated

PACIFIC HIGHWAY

(Contributed)

It won't be long now before the Kindee Bridge begins to take shape, as the fox is on hand to swing the heavy material into position. The continued dry weather has been

The continued dry weather has been favourable for many bushfree, and it has caused farmers considerable trouble to check same. One of our eligible bachelors has decided to take unto himself a wife, so it won't be long before wedding bells will be ringing.



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Item 12.01 Attachment 3

		THE BOOT MACOUADIE	NEW CATIODAY MA	V 0 1024		
			and the second s			 Comparison Council In 201200 (Second Second Sec Second Second Sec
RELIGIOUS NOTICES.	COUNCIL MATTERS.	FOOTBALL.	MUNICIPAL COUNCIL.	lengths on the west side. 7. Delapidsted gutter crossings,	tractor's line. If the Contractor pros-	rates." It was left to this Council,
CHURCH OF ENGLAND. May 10Port Macquaric 8 a.m.		The first cup match of the season will be played at West Port Park		Horton Street : It is recommended that crossings be removed on the east	off the current as requested, and got	payers with overdue rates.
H.C., Women's Guild Communion), sollands Plains 11 a.m. (H.C.), Maria	Monday last Ald. Mowie, in a so-	on this Saturday, when Taree's cup	stating in reply to Council's letter of	ence and William Streets, and con-	ed into prosecuting the offenders.	Ald. Hudson said Council should
tiver 2.15 p.m., Port Macquarie 7.30	called reply to my letter, adopted his usual method of side-stepping the	team will be over to engage the lo- cal team in a cup match. Both	at your earliest convenience why per-	crete causeways substituted. 8. Tenders for blacksmithing : P.	Measrs. Hatsatouris and Sons. Ald. Maloney did not agree with	carry on as it began and not go into committee, but come straight out
May 12 (Tuesday) Pembrooke 11	points in issue. At the Council meeting on 18th of	Port and Taree defeated their oppon- ents last Saturday, and a ding-dong	mission granted to us for the erection of lamps and Neon sign has been	Foster's tender having been with-	Ald. Orr. In the first place the	in the open.
m. (and pichic lunch after service). Felegraph Point 7.30 p.m. (Confirma-	April, Aid. Mowle said that to his	Lussie is assured, and fans will get a		that the tender of Messrs. Latham	of the Council's hands, and all of	was square and above board. He
ion). May 13 (Wednesday).—Rollands	knowledge I had done no legal work for the Council for over three years.	In view of the convincing display	in any way refer to the erection of	The report was adopted.	moon-shine. They should get their	most was the one who owed most
Plains 11 a.m. (picnic lunch after ervice), Port Macquarie 7.30 p.m.	instances where I acted for the Coun-	when they defeated Camden Haven by	fers to the supply of current for	Questions. Ald. McCabe : Wag a wreath sent	solicitor's opinion. What was sauce	rates. If all paid up their rates the
Confirmation).	ci in the last year. Aid. Mowie does	28 points to 10, the cup team to meet	street lighting and private consump-	to the funeral of the late Mr. T. H.	der.	payers, and there appeared to be no
The Bishop. May 17.—Port Macquarie 9.30 a.m.	taken to admit that his statement		appears to have the right to supply	will the Mayor state why the prac-	Hatsatouris and Sons to cut off their	Ald. Hibbard resented Ald. Ma-
Sung Eucharist), and 7.30 p.m., elegraph Point 11.30 a.m. (H.C.).	was untrue. In his remarks at the Council	replaced by Jim Kirkwood Kirk-	provided he has carried out the terms	past was not followed in this case?	The Council was entitled to answer	stances were against some people
Gum Scrub 2.30 p.m. Canon L. Gray.	meeting Ald. Mowie stated I had not been Mayor since September. 1933,	wood being a centre, this alteration will mean "Dooley" Ringland moving	will therefore be plensed if you will	The Mayor : The Town Clerk in- formed me it was not the usual prac-	their letter. The amendment was put, and lost,	paying up their rates promptly. Ald. Maloney : That aspect of the
	and Ald. Hudson in a comment	into the five-eigth position, a position	inform us whether as ratepayers our	tice.	Ald. Orr, McCabe and Hibbard voting	matter can be well left to the Town
Parish of Upper Hastings. May 10Wauchope 7.30 a.m. (H.	same. If they are correct then I	urday, scoring two tries. The new	regard Referred to Orders of the	not flown at half mast from the Coun-	- then put, and carried.	The motion was lost.
L), 7.30 p.m., Rawdon Island 11 m. (H.C.), Bagnoo 2 p.m. (Opening	from that date until my resignation.	H. Pullen, played like men possessed,	Same, stating we are in receipt of	Anane say, as and always been bobe	Hudson, Council decided to refer the	Ald. McCabe : I'm doing pretty well.
(Hall).	and they owe a duty to the rate-	whilst the older members of the team.	Council's further communication, and	before?	letters from Messrs, E. Hatsatouris	Ald. McCabe then moved-"That
The Bishop. May 11.—Bellangry 11 a.m., (H.C.	hereby challenge both the persons	played their usual solid game. A	whether the Contractor is carrying	Town Hall between 9 and 9.30 a.m.	. The Mayor said they hoped to see	and many second a second
ad Picnic Luncheon). Byabarra 7.30	de not, then it is obvious they know	"Pud." Steele, and this player cer-	provisions of the agreement, as re-	en Anzac Day, with Ald. Hudson. The Town Clerk not being there, Ald.	the whole matter cleared up in a friendly way.	
The Bishop.	they were "talking through their hats."		quired by clause 31 Referred to		It was resolved to allow the electric	matter could well be left for consid-
May 15Kindee 7.30 p.m. Rev. A. Edward Warr, ThL	Incidentally, it is amusing to hear	gage on the goals, landing seven out	G. Quast, re Council's request to	wait and take delivery of a floral	until next meeting.	ing with the whole general scheme
CATHOLIC CHURCH	holding office as Mayor at the same	try.	sulting engineer the right of entry	was no pen and ink available and	been drawn to a report of the meet-	in connection with the green.
May 10Port Macquarie 7.30 a.m.	meeting that a letter was read from	Scorers for Port were T. Murphy	to inspect and report on the electric-	these were secured from the hotel.	ing of the Hasting Shire Council, at	Ald. McCabe : This motion ought
Byabarra 8 a.m., Wauchope 10 a.m., Beechwood 10 a.m. (Mass), Wauchope	pointing out that his own election	(1) try, and 7 goals.	replying he asked the meaning of the	march had begun, and we were help-	carried : Cr. Downes moved-"That	yards of white ridge gravel be sup-
and Port Macquarie 7.30 p.m. (Bene- Betion).	cupied the chair, although Ald. Orr	Wauchoe on Saturday, is reputed	i memorandum of agreement, etc." It	office. He saw a letter in the paper	application to the Department seek-	plied by Mr. Foster, at 2/1 per per yard, to various streets as required."
May 17Rawdon Island 8 a.m., Beechwood 8.30 a.m., Port Macquarie	pointed out he had no right to do so.	to have a very formidable side, but	appeared as if the Council seemed to	regarding this matter, but did not in-	ing a revision of the agreement ap-	Seconded by Ald. Orr. who said
10 a.m., Wauchope 10 a.m. (Mass).	was valid) was present.	the local brigade.	as being in force, or as being non-	pecially those written under a nom-	the employment of Port Macquarie	Ald. Hudson moved as an amend-
Wauchope and Port Macquarie 7.30 p.m. (Benediction).	Mowle referred to the support ac-	that the Port cup team consists of	ed for the moment. If the agree-	soldiers present would understand the	ports in the "News," some of the	ment—"That tenders be called for this gravel." They would get gravel
May 21 (Thursday) Port Mac- quarie 7 s.m., Wauchope 7 s.m.		local players only.	ment was not in force, then the Coun-	position.	Shire Councillors seem to be very	for nothing soon.
Beechwood 9 a.m., Rawdon Island 9	election only one vote out of three	chosen to represent Port : G. Fow-	any other clause. In that case he	letter ?	most of the statements appear to	Ald. Maloney seconded the amend-
am. Father O'Mahony.	if the promised alteration to the	Pullen, R. Ryde, A. Ringland, M.	quest. It would not appear to be	Ald. McCabe moved a motion to		ment, saying there should not be close preserves for anyone.
METHODIST CHURCH.	franchise is made, and Ald. Mowle acquires the right to contest the next	Hicks, S. Worboys (captain), A. Ken- nady, K. Fowler, J. Steele, D. Lewis,	any possible advantage or benefit to grant the request and then be told	that effect, which was seconded by Aid. Thompson.		The Mayor supported the amend-
May 10 Port Macquarie 11 a.m.	election, then we shall see what con-	T. Murphy	the Council did not consider the	Ald. Maloney moved an amendment	quire from the ranks of their own un-	could increase the tender quantity.
Beechwood 2 p.m., Wauchope 7 p.m., Mr. McDonald (of Pyrmont Mission);	Yours, &c.,	All players are requested to be on the ground no later than 3 p.m.	definitely decides whether the agree-	did so it would be a vote of censure	Municipal Council's relief scheme is	The amendment was carried. Ald. McCabe moved
Port Macquarie 7 p.m., Mr. C. G. Orr: Wauchope 11 a.m., Mr. B. Secombe.	A. C. ELLIOT.	The following players will repre- sent Taree :- G. Montgomery, W.	ment is still in force, and valid and binding, both on himself and the	cn the Mayor, whose explanation	being carried out by men who reside in the Municipality, and who are car-	Council define the powers and duties
Rev. M. Walker.	"CURLY TOP."	Platt, L Quinlivan, J. Whitelaw, W. Kane, W. Phillips, P. Rowe, A. Gag-	Council, I shall be pleased to reply	for no irreverence was meant.	rying out work of a permanent na-	Works Darks & Records Building
PRESBYTERIAN CHURCH		gin, K. Northam, D. Irvine, M. Smith,	year ending 31/12/35 was enclosed	known that a very fine wreath was	of it. The road on which the work	Lighting, and Health." The members of committees were wandering round
OF AUSTRALIA. May 10.—Huntingdon 11 a.m.,	At the Empire Theatre.	H. Single, and N. Moore.	Referred to Orders of the Day. Town Clerk's Report,		i. at present being carried out was	 Bike lost sheep. Seconded by Ald. Orr.
am., Wauchope 2.30 p.m., Port Mac- guarie 7.30 p.m.	Shirley Temple, the world's darling,		I have to report as follows :	respect was groundless.	the progress made has been very fav-	The period party of the targetted to cuth
May 17 Elienborough 11 a.m.,	in "Curiy Top," her best picture,	competition football smongst the	fees : Since last meeting the follow-	that I write a letter to the "News?"	who have real interest in the Munici-	 matter at the time the committees were elected, and the Town Clerk of-
	Theatre on Tuesday, Wednesday, and	are in the offing to play with a 9-	General, current £33/8/2, arrears	On the motion of the Mayor and Ald. Thompson, it was resolved to	Council should not allow the Shire's	fered to define the duties of the com-
-	Thursday, with a special matince at 2.30 p.m. on Wednesday. Shirley, the	stone weight limit, under 18 years of age. It is hoped Port Macquarie	1 £4/9/6 : street lighting, current	write a letter to the Returned Sold-	resolution to pass unchallenged, so I	Ald. McCabe went out to Mill Street,
OF AUSTRALIA.	incomparable, performing as she	will be able to field a similar team.	rates 4/- ; sanitary fees £2/8/11. leav-	Ald Maloney dissenting.	tion having been drawn to a resolu-	go there to look at the work in pro-
tops 11 a.m., The Elders.	things for which you have loved her.	are carefully handled, the movement	ing in the respective accounts :	In regard to Mr. Quast's first let-	tion of the Hastings Shire Council, at its meeting on 20th April, asking	gress. But he created dissention "
May 17Wauchope 11 a.m., Kemp-	and many more besides. Full of new	should greatly stimulate the game,	General, current 1688/6/10, arrears	ter it was resolved, on the motion of	the Minister for a reversion of the	against members of this Council. It
Rev. M. C. Ramsay.	tions.	ball. Teach them young to play foot-	£160/4/11, arrears £52/3/9; interest	the Council instruct its selicitor to	ment by the Shire Council of relief	refrained from spreading reports in
	Paramount's funniest picture, "Mc-		on rates £21/11/0 ; sanitary fees £112/13/8.		wirkers residing in the Municipality	this matter. He had spoken to Mr.
(Masonic Hall) Sabbath School 9.45	Fadden's Flats," a Scotch-Irish com-	POST OFFICE PRESEN-		and conditions of the agreement as	out that the system approved of by	Clerk, about the trouble, and said he
Macquarie (Masonic Hall) Sabbath	grand, human, homely souls, who pre-	TATION.	I beg to report having impounded		doubt, he was entirely in sympathy	the men were doing road work and
L.M.	quets from anyone else ! Join them	The entire staff of the local Post	a nume to stand merition to the hills	The mover assumed the Contractor	with, has been working in a most satisfactory manner, and that work	did not intend to allow any alderman
Pastor F. A. Allum.	of one fight and the start of another.	afternoon last to show appreciation	drain at the north end of Hay Street, as it is blocked, also the drain close	had broken all clauses. and read		objected some time ago to men hay-
THE	It's a riot of laughs.	of their association with Miss Grace	to it.	Ald. Orr heid that the motion would	ance with the schedule of works ap-	and yamed with the men when he
	screened this Saturday and Monday.	vice as head telephoniste.	pulling the palings off the fence and	Dealing with the second letter, Ald.	econded by Ald. Orr. and carried.	did not have any business to do with them, and nothing to do in the inter-
	The world's greatest singer. Lawrence	Mr. L. F. Madsen, Postmaster,	using them for firewood. I would	Maloney said they could sit back and	Ald. Hudson mentioned the Mayor's	esta of the Council. He moved as an
	pestuous drama rocks your heart, its	work and the pleasant manner she	around to prevent this.	be referred to Council's solicitor."	use of Council's seal, which had not	draw up the duties under ordinance
	Hear him sing "Pagliaci," "The Tor-	that the time had come when Grace	report is the one between Bridge	Beconded by Ald. Hudson. Ald. Orr moved an amendment-	yet been dealt with. The Mayor said he had the matter	1. clause 32, and submit same to the Council."
VITAL STATISTICS. For the guarter ending 31st March.	eador Song," "Carmen," "The Glory Road," "The Barber of Seville," and	was compelled, through an age-limit, to cease to be one of them. Conclud-	Street and Glebe Bridge.	'That Messrs. E. Hatsatouris and Sons be notified they had not com-	before him, and was referring it to	Seconded by Ald. Hudson.
he following statistics were record-		ing, he wished her every success.	Finance Report.	plied with Council's request, and if	The Mayor moved—"That this	Aid. McCabe : That is a wilful, un-
leaths, 19 ; marriages, 14.	Hotel Murder," a mystery thriller.	vey spoke in a similar strain, adding	day were : General £87/18/1. Light-	urday, Council would hold them liable	price of making a valuation and re-	since you got into the Council you
EMPIRE DAY.	SUPREME COURT	their regret at Grace's departure, and also wishing her future success.	ing 1154/5/2, Sanitary 147/14/8 (debit), Trust 1334/13/1, Main Road	for any costs incurred." They had	port on the electric light undertaking,	lie.
Empire Day, 24th May, falls on	SOTREME COORT.	Mr. Madsen, on behalf of the staff,	Trust £110Adopted.	rent. If Council did not intend to	neer, be asked to submit his fee." He	Cabe to withdraw those remarks.
Sunday this year, but will be observ- ed by the schools on Friday, 22nd	APPEAL CASE.	then presented Grace with a delight- ful dressing table clock.	The Finance Committee recom-	enforce the resolution why did they pass it ? Was the Council only pre-	day. This appeared to be the solu-	Ald. McCabe : I carried you on my back for three years, and now you can
May. At a meeting of the P. and C. Association on Tuesday it was de-	In the Supreme Court, Sydney, in	Miss Wrigiey suitably responded. expressing thanks for the presenta-	mended that the Council Instruct the Town Clerk that ratepayers in ar-	tending, or did it intend to stand by		drop off.
cided to mark the day in the usual	Chambers, before Mr. Acting Justice	tion, and saying she was sorry to be	rears be notified that a satisfactory	The Mayor said it was not a par-	ing without any trouble, if possible.	me to drop off. But Ald. McCabe
	ed and dealt with :	been most enjoyable.	Town Clerk willin seven days, falling	ed certain things regarding the	ried	readily forgets the afternoon he ap-
	Appeal by Ernest Aubrey Mowle, of Port Macquarie, from the decision			street lights.	In regard to Sunday football, it	Ald. Hudson was present. Ald. MC.
Bawtell & Consoli advise that in	of the police magistrate at Port Mac- quarie, who dismissed an information	KILLED IN ACCIDENT.	Works Report.		until next meeting, on the motion of	Cabe used the words, "United we
	laid by Mowie against Frederick John	AT KINDEE BRIDGE SITE		was being kept, but that was		Ald Maloney supported the motion,
and errera ante ne a abactar concea.			red to it for attention :		connecturation to ensure hefers the	
ion of "no deposit—no entrance fee"	Reid, Town Clerk of Port Macquarie, for refusing to allow him to inspect			and the second s		
ion of "no deposit—no entrance fee" for three months to local residents mly. See the advertisement in our	for refusing to allow him to inspect the minute-book of the Council. At	A well-known read and bridge con- tractor, Mr. Roy Haydon, died after	1. Shell Company's product, "Tero- las" : The Committee will interview	out our position. Ald. Orr : I moved the amendment,	Council on its invitation. Motions on Notice.	listened to all the title tattle Council would not get anywhere. The joker
ion of "no deposit—no entrance fee" for three months to local residents may. See the advertisement in our idvertising columns.	for refusing to allow him to inspect the minute-book of the Council. At it the time Mowle asked to be allowed to see the book he was an alderman	A well-known read and bridge con- tractor, Mr. Roy Haydon, died after an accident at the Kindee Bridge site on Thursday of last week, aged 40	 Shell Company's product, "Tero- ins": The Committee will interview the Company's District Representa- tive in represent to this matter on Wed- 	out our position. Aid. Orr : I moved the amendment,	Council on its invitation. Motions on Netice, Aid. McCabe moved—"That this	listened to all the tittle tattle Council would not get anywhere. The joker

S.P. BEFTING CASES. What will be remembered as the Port Macquarie betting cases, in and J. Brien were concerned, will be interesting to the server of the content of of the con

tenders in committee." His reason for doing so was because there was not a council in New South Wales that did not go into committee to Ald Maloney : We can j but a

A C. Elliot. A. Curita, and L. La- tham have been subposed to give evidence in this matter on Monday. AUTUMN BALL. The autumn weikkit ahrdulp fymw The cool, bracing weather is favour- able for the opening ball of the sea- ning. The secretaries and committee have been organising energetically for the Church of England autumn ball, and apticipate a big attend- bance. Pairous are saured of a very	the information. From this decision Mowie appealed by way of a stated case. His Honour upheid the appeal, and intimated that the decision of the magistrate was erroneous in point of law. His Honor remitted the case to the magistrate with that opialon, and ordered the rappoal. Mr. P. D. Shortland and Mr. E. P. Kinselia (instructed by Mesara. Bar- ry, Norria and Wildes, Sydney agents for Mesara. Sheridan and Sheridan, of Mempary) appeared for the appe- lant; and Mr. F. W. Kitto (instruc-	Ingested injures, but his companion only slight injures. Mr. Haydon's wife witnessed the accident from the river bank. Mr. John Bain, working in a cradie nearby witnessed the occurrence, and had to spend several hours in sus- pense until rigging was adjusted to allow him to descend to land . The remains were taken to Camp- belltown for interment, where his mother resided. A sorrowing wife and two children are bereaved in very sad circumstances. The happening will be the subject of an inquiry by the Coregeer, Mr.	 School Street—levelling : An in- spection has been made of this street and it is recommended that the maintenance man be instructed to at- tand to same. Condonia No. 2 subdivision : It is recommended that the plan of sub- division be approved and sealed. Long Britge—hand-railing : It is recommended that the balance of the fittings for hand-railing. It. 18 For hand-railing. It. 18 Scott and the sealed in the balance of the strings for hand-railing. It. 18 Scott and the sealed in the balance of the strings for hand-railing. It. 18 Scott and the sealed in the sealer of the sealer hand for the sealer of the sealer of the second sealer is string. 	sions, and said only Council's solici- tor could tell them whether the agreement was being kept. Ald. Orr : Council is not adopting the right attitude in this matter. The Mayor : Messre. Hatsatouris end Sons might get damages from the Council. Ald. Orr : Council can order the removal of erections as affected at any time. Also, Council did not give them permulsion to use their own current. Ald. McCabe seconded the amend- ment. The motion referred to was passed by six votes to three, to ask Messre. Hatsatouris and Sons to	this need was the tender for black- smithing at last meeting. When dealing with such matters is commit- tes they could be more fully and freely discussed. Seconded by Aid. Hibbard. Aid. Hudson mid he washed those tenders discussed in committee, but the meeting thought otherwise. Aid. World did not think the Council should resort to privacy. Aid. Monowy fold like Aid. Wor-	Ald. Orr was with Ald. Maloney, and if any alderman had anything to say let him asy it at the meeting, as was stated. The motion and the a- mendment appeared similar to him. The amendment was carried. The Mayor referred to a recent let- for in the Fress regarding his own veracity, in reference to council's solicitor. Mr. Elliot had stated how he had acted for the Council in legally held office since October, 1933. The result office since October, 1933. The the judges gave their opinion, should	
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Item 12.01 Attachment 3

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	1	HE PORT MACOUARIE	NEWS, SATURDAY, AUG	GUST 29, 1936.		
RELIGIOUS NOTICES.	MUNICIPAL COUNCIL.		stone was to be put in these drains. He saw the timber on the ground. He	mount spent on tool sharpening for	SURF CLUB BALL.	"MR DEEDS COES TO
CHURCH OF ENGLAND.	The regular meeting of the coun- cii was held on Monday, the Mayor,	this council may decide to get it at some other site. He favoured the	saw men in the council yard, to which he naturally took objection.		A SUCCESSFUL FUNCTION.	TOWN."
August 30Upper Rollands Plains 1 a.m., Pembrooke 2.30 p.m., Port	Ald. E. A. Mowle, presiding over a	motion, but thought the source of	Later he saw Dr. Murphy, a member	The Mayor : I refuse to answer.	The secretaries and organisers of	In directing "It Happened One
lacquarie 7 p.m.	fuil attendance of aldermen. The Mayor, before proceeding with	supply should be first considered. Aid. Maloney was quite in accord	of the green committee of the Golf Club, and spoke about this drainage	Ald. Thompson said he wished to make an explanation in regard to the	the Surf Club ball must have been	Night," Frank Capra made himself responsible for one of the gayest and
September 6.—Port Macquaric 9.30 m. (Children), 11 a.m. (H.C.), 7	the business of the meeting, replied at	with the Town Clerk, and thought	business. He told him that if the	work in West Port, but the Mayor	very gratified with the excellent pat- ronage at their fixture in Keena's	cleverest productions of 1935. Aiming
m., The Hatch 2 p.m., Hamilton	length to a letter appearing in the "News" above the name of Alderman	the Government's offer of help quite good. He saw the need of a water	application was put before the council it would receive sympathetic treat-	said his answer and the questions could not be discussed. He was-fol-	Hall on Tuesday evening. The novel-	at the same effect of mingled romance and levity in "Mr. Deeds Goes"to
0 p.m. Canon L. Gray	W. E. Thompson. In conclusion, he	scheme, and was satisfied that Port	ment. Ald. Thompson supported the mo-	lowing a precedent set in last council.	ty was the presentation of members of the Club, representing well-known	Town," he hits the target plumb in
Parish of Upper Hastings.	refused permission to the writer to reply, and said the incident was fin-	Macquarie could carry the cost. Its provision would mean a reduction in	tion, and did not think council would	Questions. Aid. McCabe : Who ordered Mr.	movie stars, as a burlesque on the usual presenting of debutantes. The	the centre. From many points of view, this film would be difficult to
August 27Meeting of Parochial uncil.	ished.	insurance rates. Ald McCabe said he had advocated	require this old timber. They had seen how the old cricket ground had	Zais to go to the golf links on Wed- nesday to take timber which had	hall was well filled, also the gallery.	surpass. In the first place, no one else could possibly have been found
August 30. Wauchope 7.30 a.m.	Correspondence. Local Government Department, re-	a water scheme since coming to the	been drained and improved, and it	been there over six months, and was	the door takings amounting to £37. The hall had been neatly arranged	who would have represented Mr.
I.C.). 7 p.m., Bellangry 11 a.m. I.C.). Bagnee 2 p.m.	garding the residential area within the Municipality, and asking had all	town several years ago. This was absolutely essential in any town. At	was up to council to help with this work on their own park. At the same	to be used to cover drains, and why was this done without reference to the	with black and gold streamers, the	Deeds with as much sympathy, pene- tration and finesse as Gary Cooper.
September 1Ellenborough 7.30 m. (Memorial Service late Matthew	residents been individually notified of	present there was a big water short-	time he would like to see some timber	council?	Club's colours and radiated from the central light, covered with a gold	The part might have been written
in (Menorial Service face afarthew	he proposal.—Received. In reply to the Mayor, the Town	age in town, and also on the camping area. He heard the old butter fac-	made available to provide seats. The Mayor said the motion provided	The Mayor asked for notice in writ- ing of the question.	bow), depicting surfers in varying	especially for this actor, so brilliantly does he play it. Every detail is extra-
September 2.—Huntingdon 7.30. September 3.—Byabarra 7.30.	Clerk, Mr. Reid, said that the usual	tory site mentioned as a supply, but	for an inspection, when it would be	Ald, Thompson : Referring to a	poses. Neatly designed sea gulls floated from the ceiling and adorned	ordinarily vivid and extraordinarily
Rev. A. Edward Warr, Th.L	formalities had been complied with. The Department to be asked if it	this was out of the question and in- adequate.	ascertained the amount of timber re- quired for the work.	letter on which the Mayor Lade a statement, I ask you, Mr. Mayor, as	the gallery, with other decorations	real. Which brings us to the second of the film's excellencies-but we
CATHOLIC CHURCH.	were necessary to individually notify	Ald. Hudson and Worrad feit the	Eoldiers' Poppy Day Appeal, seek-	you accuse me of a falsehood, will	representative of the Club and its	must say no more. Come to the Ox-
August 30.—Port Macquarie and Vauchope 7.30 a.m. (Mass), 7.30 p.m.	residents of the intention. Same, notifying approval of unem-	question was one for due thought and	ing the right for the local sub-branch of the R.S. & S.I.L.A. to sell poppies		(1000 munic, in a well valued bro-	ley Theatre this Saturday and Mon- day and see it for yourself.
Benediction), Rawdon Island and	ployed relief works programme for an	The motion was then carried.	n 6th November Received, and re-	The Mayor : I ask you to give no-	gramme, was dispensed by the Gloom- chasers' Orchestra, and the happy	
eechwood 10 n.m. (Mass). Father O'Mahony.	expenditure of £250 on the camping reserve, and £20 for tree guards, etc.	Stewart and Cadegan, auditors, for- warding report on half-yearly audit	quest granted. P. E. McInhemey, in reference to	tice of the question for next meeting. Tenders.	throng had a most enjoyable even-	NEW DAIRY BULLS.
	-Received.	of the council's books, and recom-	the timber recently shipped at Set-	Two tenders were received for the	ing's entertainment, free novelty dances adding sest to the proceedings.	
METHODIST CHUECH. August 30. — Port Macquarie 11	The Mayor pointed cut that this work had been approved before the	mending the writing off of an amount of £15, and that an effort be made to	tlement Farm, and complaining of the damage done to the river bank, which	supply of 120 cubic yards of gravel, at 2/6 and 2/81 per yard, that of P.	Shortly after 9 o'clock, Mrs. T. R.	Last week Mr. W. H. Bransdon placed three new Illawarra bulls on
m., Wauchope 2 p.m., Beechwood 7 m., Rev. M. Walker ; Port. Mac-	scheme was suspended. It had been	collect outstanding sanitary fees	he asked be protected Received.	Foster, at the first figure being ac-	Rosenbaum, President of the Girls'	his Rollands Plains properties. These
uarie 7 p.m., Mr. C. A. Muscio.	mentioned to the Hon. L. O. Martin	Received.	Ald. Hibbard said two loads had been taken from this locality, and	cepted. Finance Report.	Auxiliary, met the Matron of Honor, Mr. G. Quast, at the door, resplend-	were purchased from the well-known breeder of that strain, Mr. S. R.
Weck-Nights Hibbard, Monday.	turing his recent visit to Port Mac- puarie.	The Mayor said the report was a good one, and they should give credit	about six feet of the river bank had		ent in all the finery imaginable, facial	Chittock, Lemon Grove Stud, Jerrara,
Ist August, at 7.30 p.m. Rev. M. Walker.	The letter to he on the table.	where it was due.	been broken away. He moved-	that day were : General 452/19/0	adornment complete, and the latest marcelled head-dress. "Her" entry	New South Wales. Mr. Bransdon has improved his herd by the introduction
	Same, regarding works that could be carried out on a subsidized basis.	On the motion of Ald. Hudson and McCabe, it was resolved to wipe the	"That a request be made for the pro- tection of the shore at the spot to		simply brought down the house in	of high-class blood, and hopes to add
OF AUSTRALIA.	Received, and to lie on the table.	amount of £15 off the books, Ald.	prevent further damage."	Trust £100. Adopted.	roars of laughter. Shortly afterwards the two met Mr. A. C. Elliot, to whom	further good qualities with the pur- chase of these three animals. The
August 30 Huntingdon 11 a.m.,	Same, in regard to works, na out- tined in circular No. 908 of March	Malency and Thompson voting in the negative.	Seconded by Ald. Worrad, and car- ried.	Orders of the Day, Ald. Maloney moved"That tend-	the "ladice" were to be presented.	chase of these three animals. The pedigrees are as appended :
/auchope 2.30 p.m. Port Macquarie p.m.	16th lastReceived.	Same, forwarding certificate for the	John Hicks, complaining about the	ers be called for 1463 gallons of tar	Again the audience admired the cut and finish of "her" dress and adorn-	Lemon Grove Overseer, by Dan of
September 6 Upper Rollands		writing off of amounts totalling £24 2s 6d, as unrecoverable.—Received.	state of the watertable near the old butter factory site, causing flooding		ments, and beautiful complexion and	Greyleigh, from Luchy II of Lemon Grove. Dan of Greyleigh was by
lains 10.30 a.m., Wauchope 2.30 m., Port Macquarie 7 p.m.	of 1 perch, for wharf and landing.	and to be written off.	on his property, and asking for at-	this year."	hair. Amusement ran high as both passed through the hall, arranging	Foch of Greyleigh, from Dandy IIII
Rev. G. A. Gordon.	near the western extremity of Clar- once Street, at a rental of one pepper-	Cam and Sons in regard to the in- adequate shipping service to Port	tention to sameReceived and refer- red to the maintenance man.	Seconded by Ald. Thompson, and carried.	themselves with dignity and acknow-	of Greyleigh, by Fussy's Pride, from Gentle of Greyleigh, by Rufus of
REE PRESBYTERIAN CHURCH	corn, and enclosing agreement for	Macquarie, and offering £52 a year	Potter Bros., seeking permission to	An offer of 125 for the stone-crush-	ledging the applause of the audience. After being comfortably seated, each	Greyleigh, from Dandy I. Luchy II
OF AUSTRALIA. August 30.—Forbes River 11 a.m.,	completion Received and to be signed and sealed.	for the lease of council's wharf on the same conditions as obtaining	enclose part of footpath in front of Commercial Hotel during concreting		found it desirable to touch up their	of Lemon Grove was by Butterman of Lemon Grove, from Luchy of Lemon
auchope 7 p.m.	Registrar-General, notifying appli-	with the North Coast Co. They hoped	work Received, and permission was	Ald. Thompson moved-"That the	complexions. Escorted to the stage, and supported by their wives, the	Grove, by Plum of Hithom, from But-
Rev. M. C. Ramsay.	cation lodged by Mr. A. Mitchell and Mrs. O'Neill, under R. P. Act, in re-	to provide a satisfactory shipping ser- vice for the town, and referred to the	granted. G. W. Everingham, seeking ap-	council give permission for the erec- tion of a light in Hay Street, above	stage was set for the presentation.	tercup II, by Primrose Lad, from Punch.
SEVENTH DAY ADVENTIST.	spect of land fronting Hill, Mill and	large quantity of poles taken away	proval to alteration of shape of roof	the churches, free of cost to the	Freceded by three pages, Masters M. Lynch, L. Murphy, L. Brown, and J.	Lemon Grove Reliance, by Dan of
August 29 (Saturday).—Wauchope Masonic Hall) Sabbath School 9.45		for shipment elsewhere, and the clos- ing down of the saw-mill, Received.		council." 'This light was badly need- ed in that portion of the town, and	Latham, and three flower girls, Joan	Greyleigh, from Ethel 8th of Lemon
m., Church Service 11 a.m., Port	Hon. L. O. Martin, stating council's	The Mayor said this matter had	Building Applications.	where it would cost council nothing	Fagon, Dolores Dwyer, and Shirley Latham, the "debutantes," entered the	
acquarie (Masonic Hall) Sabhath chool 10 a.m., Church Service 11	request for an extra of £113/18/3 in	been discussed at the special moeting	The following building applications were approved	they should allow it to be erected. It would not affect the council taking	hall for the parade. Immediately the	Pride, from Gentle of Greyleigh, by
.m.	connection with the Long Bridge con- struction, was being placed before the				audience rocked with laughter, as,	
Pastor F. A. Allum.	proper authoritiesReceived.	subject to departmental approval.	E. W. Reckless, boatshed at foot		with their partners, Mesure, Jack Stewart (Edna May Oliver), Les	by Hero of Lemon Grove, from Ethel
THE	J. H. Butters, in reply to council's inquiry, and stating he would report	Rudder and Grout, architects for the Commercial Hotel, stating they	of Short Street. T. R. Rosenbaum, extension of gar-	Ald. McCabe seconded the motion. maying the light was very necessary.	Stewart (Janet Gaynor), Ray Feeney	of Lemon Grove, by Jellico of Hill-
dort Macquarie Aews.	on a water reticulation scheme for a	had received a letter from Mesars. S.	age for Mr. Beasley.	As it would not cost this council any-	(Kathleen Hepburn), Lance Anderson (Gracie Fields), Cecil Cumming	
ATURDAY, AUG. 29, 1936.	fee of 100 guineas, or, if the source of supply were to be investigated, 150	W. and R. C. Potter, stating the coun- cil would pay £25 towards the cost of	Town Clerk's Report. I have to report as follows :	thing he really did not think it was necessary to get permission to erect	(Joan Crawford), Ian Wilson (Zasu	
	guineas. If desired the fee could be	concreting the footpath and paving	1. Collection of rates and sanitary	a light, which would serve a useful	Pitts), Arthur Keens (Mae West), Harry Bailey (Jean Harlow), "Mick"	Archer, from Strawberry II of Lemon Grove, by Kitchener of Burradale,
OCAL AND GENERAL.	on the standard scale rates, Receiv-	in front of the premises, the total price being £75, which was accepted.	fees : Since last meeting the follow- ing amounts have been received :	purpose. However, the council should grant the permission sought.	Dick (Ginger Rogers), Cliff O'Don-	
REGATTA MEETING.	The Mayor asked Mr. Reid, Town	Received.	General rates, current £1/16/8, ar-	The Mayor said that a motion had	neil (Shirley Temple), Lloyd McNeil (Greta Garbo), came into view. The	Guss of Greyleigh, from Fussey III of Hillview, by Foch of Greyland,
The balance of the report of the		On the motion of Aid. Maloney and Worrad, £25 was voted as a contribu-	rears £2 ; lighting rates, current 11/8, arrears nil ; interest arrears,		change of raiment and wigs com-	from Mermaid II of Greyleigh, Straw-
estis Club meeting has been held er until pext issue.	was mised by the receipt of circular	tion towards this work.	nil : sanitary fees (113/12/3 (includ-	lights, and this should stand at the	pletely deceived the onlookers, who eagerly questioned the identity of a	berry II was by Dan of Greyleigh, from Strawberry of Lemon Grove, by
· · · · · · · · · · · · · · · · · · ·	902 in the first place. The Govern- ment, in handing the unemployed re-	The Mayor pointed out that usually this work was done on a 50/50 basis.	ing discount), leaving the following balances outstanding in the respec-	present period. Ald. Thompson said the light would	few. All acted their part nobly and	Foch of Greyleigh, from Dandy IIII
SEA SHELLS. I wan told last week : Wear a sea-	tief question, was making available	Central Blawarra Shire, seeking co-	tive accounts :General rates, cur-	not cost this council anything, and	well, to the delight of all present. The presentation then followed in ap-	of Greyleigh, by Cunumduram of Hill- view, from Cherry.
ell somewhere-brooch, belt buckle,	to councils money for water supplies and electricity undertakings. The	operation in approaching the proper authorities on the question of re-af-	rent £367/2/7. arrears £117/15/0; lighting rates, current £95/11/7, ar-	that was a point to be considered. The Mayor said the motion was	propriate fashion, the "debs." then	
clipped on bag. Miss I. Edwards, fooloonbung," Port Macquarie, can	Government granted to councils a	forestationReceived.	rears £40/7/2; interest arrears £17-	very vague, and no price was stated,	participating in a dance with their partners.	GOLF.
et your requirements.	certain portion towards the cost of such works mentioned, which form-	King George V Memorial in refer- ence to the proposed memorial, and	3/3; sanitary fees £232/13/8. 2. Annual Leave.—On the 11th	or who was doing the work, etc. And	Master Gordon McMillan's extras,	
CULVERT BREAKS IN.	erly had not been done. Previous the	forwarding subscription lists,-Re-	proximo, I will have completed	at this spot for 10 years, and to wait	accompanied by his younger brother on the violin, were well received, the	
Whilst driving along the Black-	councils had to find the money for the preliminary work, but this was not	ceived, and to lie in the council's of- fice and be open for subscriptions.	iwelve months service with the coun- cil, and application is made herewith		boys being applauded for their play-	A. Mitchell and Mrs. B.
a's Point Road on Tuesday morn-	the case now, it becoming part of any	A. H. Jacobs, Headmaster of the	for annual leave, to be taken as from	with the option the council had to	ing. The Ladies' Auxiliary were respon-	H. Pountney
ain, Mr. W. Bailey met with an acci-	grant for a scheme approved. The employment of Sir John Eutlers	Public School, stating he was much worried to find that only 62 out of an	19th October next. It is recommend- ed that a relieving clerk be appoint-	take over the undertaking. Ald. Orr said a decision was made	sible for the supper, to which full	J. Kennedy and Miss L.
at to his new car. Turning a bend the road early in the morning, with	would not cost the council anything	enrolment of 273 pupils were able to	ed to act during my absence.	py council regarding the erection of	justice was done. The big attend- ance caused a slight uncasiness, but	Wrigley
d visibility through smoke from the	and enable it to secure a preliminary investigation on which, it could pro-	swim. It meant that 200 children	Adopted, and holidays granted as asked, and the report confirmed.	further lights, for which it would pay,	all demands were met.	R. V. Dulhunty \$6-17-69
sh fires, the car went through a raing culvert. Mr. Balley rushed	ceed in the future if a water scheme	who entered the surf would do so in danger. Simple methods of teaching	Nuisance Inspector's Report.	but there was to be no charge for this light, and he felt like changing	The ball was kept merrily moving	
way to get assistance, whilst the	were decided upon.	the children to swim in a fortnight	From a complaint received from	his mind. He would not mind if a	until 2 a.m., all voting it a most enjoyable function.	A. G. Reid and Miss P.
o passengers endeavoured to keep a fire down with dirt from the road-	11.1	would be imparted by the teachers if suitable facilities were available. He	Mr. Ruprecht, I inspected the creek running through Mr. Doak's place.	free light was crected near his place. He was agreeable to the motion if		McInherney
de. Luckily, very little damage	The Town Clerk : No. If council	urged the provision of such, if only of	The run-in from Tacking Point way	all costs for it were defrayed till the	SHIPPING.	C. Uptin and Mrs. Munro 95-21-74 W. Drinkwater and Miss
is done, though the two front tyres	obtained a grant the amount would be paid from it, and the re-payment	a temporary natureReceived. The Mayor said he had been speak-	is full of weeds, but in Doak's place it has been cleaned of all rubbish.	expiration of this council. Ald. Worrad said he had support-	The Idante has the distinction of	Harriott 103-26
re slightly scorched. Needless to y, the passengers missed the train.	of the loan portion would be paid over	ing to Mr. Jacobs about this matter,	and is running clear water.	ed the former motion passed in re-	being the first steamer to cross into the harbour in the northern channel,	
	a period of years. Aid. Orr ; But this cost would have	and mentioned a pool near the camp- ing reserve, and boat harbour had	About 15 ducks go into it, and it is also full of water fowls, but I can-		adjacent to the North Shore training	Wednesday's draw-for-partner mixed
FOOTBALL.	to be paid !	been mentioned as a possible site.	not see that there is any harm done	Ald. Maloney said he stood by the	wall. She has already taken two loads of timber to Newcastle, for	foursomen :
EMPSEY V PORT MACQUARIE	The Town Clerk : But the council would have the necessary data avail-	The matter was referred to the Works Committee, on the motion of	as it is always runningAdopted. Questions on Notice.	former motion passed by council re-	loads of timber to NewCastle, for transhipment to larger boats, loading	
·	able to go shead with the work at	the Mayor and Ald. Maloney, the lat-	The Mayor said there were several		from the bank below the old New-	A. Bailey and Mrs. Mc-
On Saturday, at 3 p.m., Port Mac- uarie cup team will defend the K.B.	any time. If council decided to go ahead with a water scheme it could	ter saying it was lamentable that	questions on notice referring to work	Ald, Hudson said he would vote	town Mill. The Northern Rivers' Steamship	Kee
ager Cup in a game with Kempsey	then be submitted to the department	Port Macquarie had no place in which to teach children to swim.	of the former council, of which Ald. Orr was the only former member of		Company began its service to Port	Elliot
up team. 🖉	for approval. To-day, help to the	Golf Club, asking for the use of old	the Works Committee. Whilst the	Ald. Hibbard favoured the motion,	Macquarie at the week-end the Tun- curry crossing in on Saturday, and	
When Port travelled to Kempsey bey were victorious by 11 points to	extent of up to 50% was given by the Government in some cases. The	timber in council's yard to utilise in drainage on the links, and old sleep-	Hon. L. O. Martin was in Port Mac- quarie Mr. A. C. Elliot had taken Mr.	as council was getting something for nothing.	departing with timber loading on	A. Mitchell and Mrs. B.
Kempsey are bringing a more solid	loan portion of the cost of a scheme	ers for flooring the bottom of the	Hawkins over to see the work refer-	The motion was put and lost, Ald.	Monday. This bost will make regu- lar trips, and all information may be	H. Pountsey
embination over to avenge this de-	would be at a fixed rate of interest of 34% . The offer was open until	main drain through the areaRe-	red to in the questions to be asked. As Aid. Thompson was a member of		obtained from the local representa-	C. Uptin and Mrs. A. G. Reid
sult.	June, 1938. Sooner or later the	and Parks Committees.	the Finance Committee, which passed	The Mayor referred to the question	tive, Mr. H. Spence,	P. A. Hallett and Mrs. A.
	town would have to decide this ques- tion. A water supply was an abso-	Ald. Maloney was opposed to giv- ing the Golf Club which he thought	the accounts for approval he could not understand his reason for asking	of relief works, saying all felt deeply	KINDEE BRIDGE	A. Cumming
irkwood, C. Cumming, K. Fowler,	lute necessity in any town. With a	it should buy themselves, as council	the questions. They were the first			Bailey 95-20-75
Hicks, C. Annand, A. Kennedy, B.	decent water supply fire insurance	had use for all it had on hand. It was	questions he had been asked on no-	of proposals for the benefit of the	Mr. L O. Martin, M.L.A., Minister	
iggins, D. Lewis, B. Branch, and T. urphy.	premiums would be reduced, as with- out such an adjunct insurance com-		tice, and he thought he could follow the precedent set by the former		of Justice, has received the following letter from the Commissioner for	
		could not understand the view taken.		Castoo	AVI.	There will be a big influx of lady

	Murphy.	out such an adjunct insurance com-	Ald. Orr supported the motion. He	the precedent set by the formes	consideration. Comprised in these	letter from the Commissioner for		1.1
	All players are requested to be on	panies regarded it a big risk. If the	could not understand the view taken,	Mayor.	were improvements to the camping	Main Roads :	There will be a big influx of lady	10. 10.
	the ground at 3 p.m. sharp.	question were not considered now and	as all knew it was council property	Ald. Thompson, on notice : Was	reserve, the town beach and its fa-		golfers to Port Macquarie next week	
		advantage taken of the splendid	which was being improved.	the account sent in by Mr. Booth, in	cilities, including a look-out, altera-	August for information regarding the	to compete in the Central North	
	Two local teams, the 9.8 limit boys	terms offered, later the town may	Ald. Maloney: It is the first time I	respect of which a summons was ser-	tion of drainage from north to south	present position of the work of con-	Coast associates' annual tournament,	
	and the cup team, had wins over two	have to bear the full cost as these	knew it.		in William Street, near Owen Street		on the local golf links. Some are	1000
	teams at Wauchope last Saturday.	conditions would not prevail forever.	Ald. Orr, continuing, said the coun-			River at Kindes Crossing, I desire to	taking advantage of the school vaca-	0.027
	The young lads won by 18 points to	The Mayor said all had heard the	cil refused to improve one property.		courts and a bowling green. He had		tion to spend a week here. Play	12.20
£	5, scorers being : Port Macquarie-	able statement made by the Town			had a talk with Mr. P. Chanman.		commences on Wednesday, and will	52.34
	Ward 1, Rosenbaum 1, Anderson 2	Clerk. He moved-"That the letter	try and attract people. It was the		a very keen Taree bowler, in regard		be over 18 holes, followed by a sec-	5121
	tries, Brest 3 goals. For Wauchope,	lie on the table."			to a bowling green, who would assist		ond 18 holes on Thursday. On Fri-	
	Bransdon scored, whilst Marchment	Seconded by Ald. McCabe.	could to help to make it a success.		council in every way with such a		day ladies' foursomes will be played.	* Sa5
	kicked a goal.	Ald. Orr agreed with the motion.			proposal. He also had an old friend		On Saturday afternoon, the post-	- 義宗
	After a very poor game the Port	but felt matters should not be rush-					poned Central North Coast Associa-	-75.8
6		ed, and the whole question should be			He had also been round with Mr.		tion foursomes championship and	3.3
		bluntly discussed. The idea had been			Reid and members of the tennis club.		handlosp will be played.	 13.5
A 14		in mind for years, and several		workers' compensation insurance ?	These matters would all have to be		During their stay the visiting play-	6.8.2
		schemes being mooted, but as yet no	promised to help and should do so.	The Mayor : I refuse to answer.			ers will be entertained by the Port	- 28-29
	goal.	site had been selected. Sir John	The Mayor said that first of all			ber of this year."	Macquarie Golf Club.	1.5
	The second se	and the second second second second second		1	Terms in a remain man	Low or one Years		10000
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Item 12.01 Attachment 3

	т	HE PORT MACQUARIE	NEWS SATINDAY SET	PTEMBER 26, 1936.		
RELIGIOUS NOTICES.	MUNICIPAL COUNCIL.		duty, and this would be the means of	ASSOCIATE GOLF.	BOXING ENTERTAINMENT.	PERSONAL.
CHURCH OF ENGLAND. September 27.—Port Macquarie 9.30	(Continued from another page).	bowling green and tennis courts. These must not be considered luxur-	Aid. McCabe said he had heard the	Tuesday's competition resulted in	The second of a series of boxing	Mr, and Mrs. James Lyon and Miss
.m. (Children), Rollands Plains 11	Motions on Notice.	les, they are essentials, and just as essential as the golf links, and they	would he suggest the figures were	a win for Mrs. A. Munro, with a score of 90-25-64, four strokes below	tournaments, promoted by the Port Macquarie sub-branch of the R. S. &	Lyon have gone to Wingham for a few weeks.
.m. (H.C.), Telegraph Point 2.30 .m. (Meeting after Service), Port	Ald. Maloney moved - "That the sanitary contractor. Earl Hastings	are revenue producing. We have just	The Mayor : They were placed be-	par, thus reducing her handicap to 22. Mrs. A. J. McKee was second	S.I.L.A., was held in the Oxley Thea- tre on Friday evening last, a fair	Inspector Watson, of the CLB., Sydney, and his daughter, are the
dacquarie 7 p.m. October 4.—Port Macquarie 9.30	Hicks, be given three months notice in writing to terminate his contract,	municipal control. All tates recrea-	favoured helping the Department.	with 97-30-67. Her handicap was clipped one stroke.	stiendance being present. Patrons had good value for their money, but	guests of Mr. and Mrs. D. McLaren. Mrs. C. Dick, of West Port Mac-
.m. (Children), 11 a.m. (H.C.), The latch 2 p.m., Hamilton 3.30 p.m.,	and that applications for tenders for	tions will then be under council con- trol, and revenue producing, and if			from a financial point of view the League showed only a few shillings	quarie, received a nasty gash over the
Port Macquarie 7.30 p.m. Canon L. Gray.	a five-year term for same be invited through the "Sydney Morning Herald"	we cannot repay the monies borrowed on the exceptional terms offered by		At the Macksville open day, held on Tuesday, 22nd September, Miss	profit.	and fell against a chair.
Parish of Upper Hastings. September 27. — Wauchope 7.30	and "Port Macquarie News." The income last year to August, 1936, was	the Local Government Department.	some would favour and others that	Julie Condon, Port Macquarle, won the scratch event of 27 holes, with	Captain L. G. Liley was referee, and Mr. A. C. Elliot timekeeper.	Mr. G. Dick, who suffered an injury to a sinew in his left hand whilst
m. (H.C.), 7.30 p.m., Ellenborough	£733/11/3, being 25,065 services, at 7d, and 10 at 5/ He thought the	affairs of the ratepayers. If we avail	ed the council were trustees for the ratepayers. When you put in tanks	scores 40, 40 and 41-121, and was four strokes ahead of the runner-up,	Pacey (3-2) and Gardiner (3-4) gave a good display in three rounds	spalling in the North Shore quarry, is still off duty.
I a.m. (H.C.), Kindee 2 p.m. October 1.—Bagnoo 7.30 p.m.	figure too high. If tenders were cal- led he felt sure the price would be	be improving the town, helping the	as mentioned you know how long they	Mrs. A. G. Byrnes, Wauchope. The trophy for this event was a Royal	of a minute and a half, and put the gathering in a happy mood. The	Mr. G. Whythes, who had been off
Rev. A. Edward Warr, Th.L. CATHOLIC CHURCH.	much lower. The service at 6d. would	force of circumstances have been		Douiton dinner service. Congratula- tions, Miss Condon !	decision was a draw, and a good col-	jury began work again on Monday.
September 27 Fort Macquaric 30, Long Flat 8, Wauchope and	return the contractor with £626/12/6. a saving of £104/8/9, and mean £12	hurled back to the dole ; we will be helping the Government in their task	ing reserve was £64 for the year, and if a cottage is built there council		lection thrown on the mat was equal- ly divided.	Mr. and Mrs. E. Chisholm, of "Ea-
schwood 10 a.m. (Mass), Wauchope	a week : at 51d. the return was £574/8/13. saving £156/13/25. and	to rehabilitate these men, and it is	must get a return for the expenditure. And building, that will give employ-	GREYHOUND RACING.	Blanch (9-5) v. Mooney (9-6). This pair halled from Byabarra,	thorpe." Armidale, have been enjoy- ing a change in their cottage on the
id Port Macquarie 7.30 p.m. (Ben- Sction).	the contractor would not starve at that; at 5d, the return would be	possibly do it. I say we can do it, and we can make the works on our	ment only to about two men. Where	the tonowing in the draw, in the	and went into the fray with gusto in a four-round bout. The first	Mr. and Mrs. A. E. Moore, of Taree,
Father O'Mahony. METHODIST CHURCH.	1522/3/9, saving £208/17/6, and at 41d, £469/19/41, a saving of £261/-	programme revenue producing-they	It would not help them a bit. Road	order of box positions, for the grey- hound racing at Kempsey to-night	round was fairly even, but Blanch	Links a second to have seen
September 27Port Macquarie 11 m., Telegraph Point 2 p.m., Wauc-	1/101. In his opinion 41d. was more	will be an asset to the town, and we will be giving what every tourist ex-	sheds, etc., would not give them much,	(Friday) : First raceOcean View, Bouncing	showed out in the second, and was severe in the third, when the contest	his property at Dick's Mount.
pe 7 p.m., Rev. M. Walker. Port	fair than 7d. per service, and it was time the council woke up from its		if any, assistance. Doing up foot- paths would benefit such men, and	Bonny, Blue Blubber, Dark Sun, Pris- matic, Knoxil, Koree Queen, Black	was stopped by the police and no de- cision given.	Nurse Smith, formerly on the staff of the Hastings District Hospital,
sequarie 7 p.m., Mr. C. G. Orr. auchope 11 a.m., Mr. S. W. Se-	reverie. They must get down to a business proposition. He was not	that is service. In speaking, the Mayor said he al-	ratepayers must be the council's first	Limes.	Ramsay (11-0), Port Macquarie, and Jones (10.5), Byabarra, provided	but now at West Maitland Hospital, is visiting the town.
mbe. Week Nights.—Hibbard. Monday.	making any reflection on anyone but thought 4id, a fair thing, and left	most had to apologise to Ald. Thomp-	provide the funds. What they do	Second raceMiss Rompabon, Try Again, Domain Lad, Rhythm Queen.	the next four-round bout, which comprised a combination of boxing	Mrs. H. W. Fletcher, of Kentucky,
th : Lower Plains, Wednesday, 30th. 7.30 p.m., Rev. M. Walker.	it at that.	son for stealing his thunder. as he had quite a number of motions he had	the unemployed men, and the whole	Just Snow, Not Interested, Gallant Queen, Jack o' Diamonda.	and wrestling. Ramsay gained the	Webster and Wine & Winkers of
October 4. — Wauchope 11 a.m., sechwood 2 p.m., Port Macquarie	Ald. Hudson seconded the motion. Ald. Worrad said he would move an	suggested, which he would mention. Ald. Thompson : Why read them	matter should be thoroughly consider- ed before next meeting.	Third race. Sam Maclesy, Noisey Jane, Silent Doctor, Shirley's Gift.	decision. Clark (11-2), Begs, and Edmunds	Port Macquarie"Armidale Express"
p.m., Rev. M. Walker ; Wauchope	amendment-"That it stand over till after Christmas, for another three	out ? They are mine. The Mayor : Just to show you	The Mayor said he had gone to a	Poter's Pal, Domain Walker, Arab-	(11-0), Cessnock, were next on the programme for a four-rounder, but	Darss lean prowniow, accompanied wy
p.m., Mr. Geo. Masters. Rev. M. Walker.	months." This would allow matters to settle down after the summer	agree with my remarks. Ald. Thompson : I do not agree.	an expert did also. They could make	Fourth race Conko's Pal, Tally's	the former was disqualified at the	Mr. R. V. Dulhuniy, left for Moree last weak-end for a change, the two
PRESBYTERIAN CHURCH OF AUSTRALIA.	season. With regard to the 1731 paid	The council is five to four-you and	coning mere not animate connent mere	away.	end of the second round for hitting after the gong sounded. This appear-	former hoping to shake off colds"
September 27.—Port Macquarie 11 m., Beechwood 2 p.m., Wauchope 7	to the contractor, he did not consider him overpaid, as three men were ém-	four dummies. The Mayor : Resume your seat Ald.	not proceed any further. He had been on this matter over three weeks	Fifth raceColundar, Mirandah's Best, Ned.	ed likely to be a good fight. Ed- munds has a good left, which troub-	time past. The engagement is announced of
n.	ployed on the waggon. Ald. McCabe seconded the amend-	Thompson. As Ald. Thompson ob- jected he would not read the motions.			led Clark at times. It was announc- ed that these two men would be	Barbara Garland, second daughter of
October 4Upper Rollands Plains 30 s.m., Wauchope 2.30 p.m., Port	ment, but thought the motion out of order, as a former resolution of the	A summary of the expenditure was 1850 for the bowling green and four	per cent, interest repayable over a	OBITUARY.	matched again at the next tourna- ment.	Mrs. H. B. Madden of Armidale, and the late Canon H. B. Madden, once of
acquarie 7 p.m. Rev. G. A. Gordon.	council deferred consideration of the contract until the end of the year.	tennis courts. £50 for a pipe line for water from near the wharf to the	say the expenditure would not pay ?	MRS. MABEL AMY SECOMBE.	Groves (9-9), Wauchope, and Mc-	Fort Macquarie, to Colin James, eld- est son of Mr. and Mrs. A. A. Utz,
OF AUSTRALIA.	The Mayor, on reference to the	camping reserve, £150 for Settlement	Department the whole scheme could		Gowan (19-0), Gien Innes, fought a six-round bout. Both men went at it	of Tilbuster, Armidale. Mr. D. McLaren, who was spending
September 27Wauchope 11 a.m., oper Plains 7 p.m.	minute book, then ruled the motion out of order.	Point Road, £50 for lavatories near the green, £95 for improvements to	Ald Orr thought rather the cart	Mrs. Mabel Amy Secombe, wife of Mr. Gordon V. Secombe, wife of Mr.		a holiday with his son-in-law and
Rev. M. C. Ramsay.	Aid. Maloney bowed to the ruling, and said he would bring the matter	the Town Beach, £300 for a sea wall from near the wharf to the break-		Gordon V. Secombe, of Innes-street,	ond was again even, the contestants	at Frederickton, returned home last
September 26 (Sat.)Wauchope	up again, as he wanted ratepayers to get a fair deal.	water, and £500 for the camping re- serve. Both Messre, R. V. Dulhunty	money should be spent. He was go-	45 years of age. She was a daughter		has been confined to bed since. His
Masonic Hall) Sabbath School 9.45 m., Church Service 11 a.m., Port	The Mayor moved-"That the coun-	and D. McAlary had offered to as- sist materially with the bowling	an amendment-"That the question	Buckley, of Orange. For many years	Early in the third round, McGowan	
acquarie (Masonie Hall) Sabhaib hool 10 a.m., Church Service 11	the necessary steps to enforce pay-	green and tennis courts.	for postowing he hosthoused musi ene	Mrs. Secombe lived with her husband at Wauchope, and about three years	connected over the eye with a heavy right. After breaking shortly after a	Mr. T. Cook, son of Mr. and Mrs.
•	ment of any overdue rates exceeding £10 after thirty days' notice." The	Ald. McCabe asked where the May- or got the figures ?	It had been said it was their duty to help the Government, but he did	ago came to live in Kempsey. Be- sides her husband, three boys (Bruce,	similar blow was landed by McGowan, who knocked Groves down near the	Sydney on the 12th September by the
Pastor F. A. Alkan.	Town Clerk would be going away on 19th October and it was desired to get	The Mayor said he had been on it for a month. It had been hinted he	not think so. The care of the unem-	Keith and Allan) and one daughter	close of the round. Both men came	R.M.S. Orford for Rome, where he will continue his studies for the
IN MEMORIAM.	in as much as possible before than, especially the big amounts.	had shown favouritism, but he would	ment. He felt very much afraid most	(Beth) are left to mourn the passing of one dearly beloved. The late Mrs.	out fresh for the fourth, and landed punch for punch, Groves getting a	priesthood. He will attend the Pro- paganda College and the Lateren Uni-
Lesile Long, who was drowned 29th September, 1933.	Ald. Maloney seconded the motion	not do that, but do what was best for all. He would not do a favour	and bree when hor constant for the bod	Secombe was a woman of quiet dis- position, and gained the admiration	fine right hard to the body. McGowan smothered in his corner, with Groves	versity, within the Vatican City. He will be absent four or five years.
tander blossom was loved so dear. Eyes closed that were so bright :	appear to make any attempt to pay	for his best friend, and no one could accuse him of doing that. He had	nis courts, and many would not stroll up to the flagstaff for sport. They	of a wide circle of friends. The years of suffering that deceased ex-	attacking, and then both fought out the last part of the round. The	The Rev. H. P. Lomas, accompanied by his daughter. Miss Margaret Lo-
nother flower to make the garden	their rates and should be advised they must pay up.	endeavoured to put all plainly before the council. He would hear any	had heard a lot about a bowling green but only from a few people.		fifth opened with both fighting freely, hard punches being exchanged, and	mas, visited Port Macquarie this
fair In the great Father's sight.	The motion was carried. The Mayor moved — "That this	gricvance, and admired the man who came to him in such cases.	Some people played golf and some tennis and other sports, but the coun-	ing up under this handlcap was a tri- umph of determination and will pow-	Groves' left eye-lid was opened. In	week, and renewed many former ac- quaintances in his old parish. He is
Inserted by his loving grand-par- ts, F. and E. Matesich	council avail themselves of the terms offering under Local Government cir-	Ald. Maloney seconded the motion	cil would have to find the bowlers.	er. Miss C. R. Buckley (Sydney)	the six rounds McGowan had the best of it, Groves standing up gamely un-	now at Neilis. Victoria, between Mel-
DEATH.	cular 902, and apply for an advance	with pleasure. There was not much for him to say, but as a business man	He understood it cost about i5 a year to be a member, and cost a lot to		til knocked out with a good right- hand punch.	ing north as far as the Tweed, and will return to Victoria through the
FENN. September 21st. 1936, at	of £2000, on a basis of half grant and half loan." Last week a deputa-	he thought it a good venture. A bowling green and tennis courts	keep a green in order. It was going to be a costly matter to the town.	Adaminaby, a brother of deceased. The funeral took place on Saturday	LACTING DICTOR	tableland route.
r residence, Clarence Street, Port	tion representing over 40 men waited on me about the withdrawal of the	would produce revenue and attract people to Port Macquarie. By bor-	The estimate for the sea wall was ridiculous, and to think of building	morning last from Mr. J. R. Garland's	HASTINGS DISTRICT HOSPITAL	Mrs. Harriett Fenn, mother of Miss V. and Mr. L. Fenn, of Canborry
te year.	relief work. I can only say I do not like relief work. It was only a very	rowing this money they would help to push the town ahead, and they	one six feet high and 400 feet long was absolutely ridiculous.	conducted by Reva. J. N. V. Bullen,		Guest House, passed away peacefully on Sunday night, from heart failure,
THE	temporary measure, and when our Member, the Hon, L. O. Martin, was	could not help to progress otherwise. He did not altogether favour borrow-	The Mayor said he measured it all	M. Walker and J. B. Thorn, B.D., to the Methodist portion of the East	At a special meeting of the Hos- pital Board held on Friday night last,	aged 86 years. The deceased lady had been in ill health for some time
ort Macquaric Acws.	with us at the special meeting of the	ing money, but in this case they were	up. The average height was below als feet.	Kempsey cemetery, where the same three clergymen officiated at the	September 18th, the regulations ap- plying to the Contribution Scheme	past, and the loving cars and atten- tion administered to her by her
ATURDAY, SEPT. 26. 1936.	council a short while ago. I think I put the matter to him very plainly.	assisting the Government and helping men to employment. With full em-	Ald. Orr, continuing, said the whole of this job would have to be framed.	graveside in the presence of an as- semblage, the proportions of which	were revised and altered in one or two respects, in view of the exper-	children had prolonged her life. Her remains were quietly laid to rest on
OCAL AND GENERAL.	is helping to rehabilitate these men,	ployment he felt sure the men would give a fair day's work for a fair day's	He could not conceive other works being done within the estimate given.	paid eloquent testimony to the worth	lence of eight months working of the	Monday by Canon Gray.
"ARNCLIFFE" SOLD.	who, through no fault of their own. have been on the bread line for years.	pay. He feit sure these things would pay like the golf links. With these	He was opposed to be sitting on the fence, but was criticising now in the	Miss Buckley, at one time on the teaching staff of the Port Macquarie	fund. It was decided that the con- tributions could be permanently fixed	A THANKS OFFERING.
We have been informed that the rncliffe" home has been disposed		facilities they would help push the town along.	hope that when the council did borrow	Public School, is a sister of the de-	as at present except that the fee for single men twenty-one years of ago	
by the owners to Mrs. Griffin, of agog, who formerly conducted it	week in three, four and five. It is absurd to suggest that a man work-	The Mayor here quoted the follow-	the money would be expended on pro- fitable undertakings. It was sug-	ceased lady.	and over be fixed at 6d, per month, the same as the Metropolitan (Syd-	To Hastings District Hospital.
a guest house. Mrs. Griffin will	ing one week in five can give his best,	ing extract from Sunday's "Truth," under the heading. "Forster Forges	gested to spend £250 on a caretaker's cottage at the camping reserve. This	KINDEE.	ney) scheme. All single women and all under twenty-one years to remain	On the occasion of his eightieth
ertly go into possession.	when he is expected to live and eat for five weeks on $£3/15/6$, which is	Ahead, Progressive Resort By The Ses," which, he said, showed us :	had been done without for a long time, and the same thing could still	(Contributed).	at 3d. per week.	birthday, on 23rd September, Mr. A. E. Pountney was an inmate of the
HOSPITAL SPORTS. The Board of Directors of the Hast-	less than an invalid pensioner gets. I have had too much to do with em-	"Forster, that delightful watering place on the North Coast, some 20	apply for some time. Apart from	Since my last letter good progress	The greatest problem was the mat- ter of private hospitals. With an in-	Hastings District Hospital. To mark the event, he has forwarded the fol-
s District Hospital resolved at a eting on Friday of last week to	ployment to expect a man to be fit for hard work on that score. But,	miles from Taree, is preparing for the summer influx, which a progressive	all of this it was desirable first to finalise the electric light question.	has been made with the erection of the bridge, and the girders have now	come for the month of August of £55, accounts from 'private bospitals	lowing letter to the Board of Direct-
d a sports meeting at Port Mac- arie on Saturday, 24th October.	I do expect and I insist when a man	shire council confidently expects will	£10 was mentioned for a fence round the band rotunds, whereas the one	been placed in position. We can look forward to seeing the work com-	totalled over £30. As it was decided as impossible to continue on this	Dear Sirs,-Please accept my cheq-
programme of five races, with	that he should earn his money, and I	result from its commendable enter- prise. A 'Truth' man who visited	formerly erected and taken down cost 140. All these suggestions and ideas	pleted during the present year.	basis it was reluctantly agreed that	ue for £100 as a thanks-offering to my Heavenly Father for long life. To-
die races in between, has been wm up, and programmes will be	who is not prepared to do his fair	the locality last week was surprised to find that the Stroud Shire has con-	were going to spoil their chance to carry the electric light question to	Mr. Dan Harman is still disabled with an injured leg, as the result of	in future, as present contracts expire, while offering the same benefits as at	day I celebrate my eightieth birthday, and I realise the loving hand of Him
s will appear in a later issue.	share. When Mr. Martin was here I put this question to him, what are	structed a large swimming pool in the arm of a secluded cove between Cape	finality. He moved the amendment	an accident whilst returning home from work recently, and he is com-	present at our own and co-operating public hospitals, that the Board will	donate the necessary amount to the
ONE-LEGGED CHICKEN.		Hawke and a heavily wooded head- land. The club rooms, dreasing sheds,	to avoid this risk. The Mayor then referred to the	pelled to use crutches.	pay £1/1/0 per week for 10 weeks to licensed private hospitals.	sterilizer fund, in memory of my loving wife. With best wishes.
On Wednesday we were shown a s-legged chicken, then five days old.	storation of permanent work ? And	luncheon hall and accessories, built	revenue obtained from the golf links and quoted from the "News" of	Ellenborough tennis team played a match on the local court last Satur-	The Board is at present negotiating	It might here be mentioned that
was one from a setting of black	aspect which was giving the Cabinet	of red brick, in Spanish mission style, at a cost of £7500, are the equal	September, 1933, and a letter in reply from Mr. A. C. Elliot.	day and was defeated by the local team. I wonder why one boy did	with the Hospitals' Commission for the expenditure of about £300, half	the doner has been a regular contri- butor to the Hospital since its incep-
bington eggs laid down by Mr. G. Sk, at Port Macquarie, and was	ognised that this relief work must	of anything in the State. Now the Stroud council aided by the live wire	Ald. Thompson seconded the	not play up to his usual form ?	loan, half grant, and when the im- provements are completed the state-	tion, and was also a helper financially in the establishment of the institution.
tched out last Friday. The right is normal, and just a very short.	come to an end, and an attempt made	Forster Urban Committee, is setting the foundations for a casino, where	amendment, and thought these mat- ters should be postponed until after	last week to arrange players for the competition, and also to decide the		It has always been a pleasure to him to assist the Hospital, his interest in
mp protrudes from the body where	the sooner the better, and it is the	the purely social side of vacational	the electric light question was final-	best way of clearing the cricket oval.	was a pleasure to call at the Hastings	its welfare never relaxing.
iceable thing is that the left wing	end. We must also admit that many	life will find full scope-dancing and so forth. These works have been car-	. The amendment was put, and car- ried, Ald. Orr. Thompson, McCabe,	It was decided that Messrs Hollis Bros. and V. Elvick do the work,	District Hospital "because everything was kept in such exceptionally ex-	A TRUE CHRISTIAN.
iso slightly deformed. The chicken bears to be doing well, and man-	works that could never have seen the	ried out under the Emergency Relief Scheme. In the best sense of the term	Hibbard and Rosenbaum voting in the affirmative, and the Mayor, and Ald.	and they are making good progress. The job should be completed before	callent order" will be more than jus- tified.	Practical Christianity is practised
is to line up at feeding time, using leg and wings as a means of pro-		they are 'worth while' improvements and should be an object lesson for	Maloney, Worrad and Hudson in the negative.	the season begins. We are experiencing very dry con-	"THE PRISONER OF SHARK	quistly by the Rev. H. B. St. John, Church of England rector of Gostord,
	have had to accept the crumbs, which	Tor an entry of the second for	necrofina	mre caperiencing very gry con-	THE PRINTER OF SHARK	the second state rector of Goeford.

OPEN AIR SEATING. IN OPEN AIR SEATING. IN In this isms Mr. Reg J. Robers I advertises his intention to set up his open air stating rink in Port Mac- wards, mist to the Macquaris Hotel SC The floor comprises 2000 square feet is of space, and pinny of good states w will be available to those who care to induigs in this pleasant and exciting patime. The opening might is set as down, for Priday. October 2nd, and P for those new to the sport competent or instructors will be in attendance to instructors will be in attendance to instructors will be in attendance to for and a guiding hand and teach the art at the same time. Good music an	t the superises of the workses, when ave had to accept the crumbs, which are all that was offering. Now it is be council's duty to help these men- have not any hesitation in a stating that the Government has given us or and which this council can term any without the rategayers being shed to contribute is any way. In ort Macquarie we practically rely rots that will be all revenues produ- g, and which this council can re- may without the rategayers being shed to contribute is any way. In fort Macquarie we practically rely rots that will be all revenues produ- tor the tourists, and we cannot gave and make it attractive and workpy of he Port. With a little finance we	HIBBARD. (Contributed). After about seven weeks idleness the mill here commenced operations again on Wednesday morning last. No doubt the mill hands will be pleas- ed with this as any loss of time is seriously felt. The building of the new drogher is going along satisfactorily in the spable bands of Mr. Reckiess and party.	chaft to keep their cows in condition. The corn crops are beginning to show well in splic of the adverse- conditions. Miss R. Brocker is back at work after spending a week's boliday with her brother, Mr. W. Brocker, of Wauchope. Mr. M. Hollis has completed his contract for fencing, the road to the bridge for the Main Roads Depart- rest. There is only a small section	The Empire Theatro presents with grant pride the most stupendous mo- tion picture achievement since pic- ture making began, a production towering in its drainatic immensity over the year's great hits. "The Pri- soner of Shark Island" is a giant stride forward in picture making, vi- brant with svery human emotion, a- blase with nerve-tearing surpense, it is conveded with apectacular scenes, it is contain to be an unforgettable screen	sary thit is be shaved at least every second day. Barbers have refused to perform the office for reasons which need not be detailed. Every second day as regular as clock-work the ministor goes to the heapital with a towel wrapped around the ranor and shaving brush, and gives the man a perfect shave. The Rav. H. B. St.John mentioned	
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Item 12.01 Attachment 3

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	THE	PORT MAEQUARIE N	EWS. FRIDAY, MARCH	29, 1946	and the second second		
HOUGHT FOR THE WEEK	ASSOCIATE GOLF Miss Joyce Glen was the winner of the 18-hole stroke competition	THE FLOOD	. ESSAY COMPETITION	CORRESPONDENCE.	SCANDAL OUS	PERSONAL PARS	
e human soul against selfishness.	played on Saturday, with a card \$5- 17-66. On Saturday, an eclectic competition will be played, commenc- ing at 2 p.m.	Close to '29 Levels	Winning Entry-And Runner-up	The Editor, "Port Macquarie News." Sir—Rindly permit me to make a correction in your account in your last issue of a statement made by me at		Canon Gerry was to have taken a service at Torren's Greek on Sunday. but owing to the flood conditions this has been postponed until Sunday. 7th	
Carlyle. irths, Deaths, Memoriams	MOTHER'S CLUB STALL The members of the newly-formed	Peculiar features associated with the fixed rains at the week-end, con- sistent with the vagaries of weather elements on the coast, averted what	There were & large number of en- trants from Port Macquarie in the	last council meeting. Your report reads : "If sport is not commercial- ised or organised I am not opposed	CONDITIONS AT THE SCHOOL	April Kendall was recently the scene of a very pretty wedding, when two pop-	
ngagements, Marriages, Etc.	Mother's Club conducted a successful street on Saturday last, the takings, with competitions, amounting to £14/	may easily have been a major disan- ter in the flood history of the Hast- ings. As it was, the river (meaning	essay competition, on the subject of a water supply for the Hastings Dis- trict, tut, disappointingly, none from	or organised, I have not opposed it	REVELATIONS AT ANNUAL	ular young folk, Miss Irene, youngest daughter of Mr. and Mrs. A. Baston, of Kendall, and Mr. Roy Carrathers	
CKS - In loving memory of our	10/11). Competitions were won as follows : Cake (donated by Mrs. F. Matesich), won by Helen Ba'e : din-	generally of the Hastings) suffered substantial damage, and the losses if	E. A. Mowle, who kindly consented	brackets were understood, being the matter on debate. Personally I be-	P AND C MEETING	(AIF), son of Mr. and Mrs. C. Car- ruthers, of Laurieton, were joined in matrimony by Rev. J. E. Neville, at	1
arts that love you never forget, memory you are with us yet.	ner (donated by townspeeple), Miss Wilkinson; baby's set, won by C A. Ryan. Donations made to the stall were : Mr. J. Hennessey £1/1/0, Port	shillings and pence would be stagger- ing. Apparently, though the weather	whom we are indebted, had a busy	use is partly to blame for the wave of irreligion and consequent lowering	A bombshell was dropped and exploded resoundingly at the an-	All Saints' Church of England, Ken- dall. Their future home will be Ken- dall, where Ref. stter discharge from the army, will be resuming duties	
sisters, and grandmother. TIN In mamory of our dear	Macquarie Surf Club 11/1/0, Mrs. McWhirter 13/- Mrs Jenner 10/-, Mr. Donald 10/-, Mrs. P. Dick 5/-, Mrs.	mount of rain fell in the upper reaches of the river, and never before has it	were all far embracing, and showed wisdom of thought for the future. The f1 prize intended for the bast	day, but I have never felt justified, when sport is neither organised, rewdy, nor commercialised, in trying	nual meeting of the Parents and Citizens' Association on Wednes- day evening in the Masonic Hall	with the Camden Hoven Co-op. Dairy Co. The temptation to view the flood	12
Lorna, who passed away 29th March, 1943. Fot just a thought for to-day,	Herdson 5/-, Mr. Cunning 5/-, Mrs. Pountney 5/-, Mrs. T. Hyde 3/-, a friend 3/-, Mrs. R. Dick, Mr. Spence,		effort from a pupil in the under-50- pupil schools, from which there were no entries, has been awarded to the	to force others to my point of view. When it is, the law provides restric-	be moved so that the meeting	waters on Tuesday was too much for Charles Donaid, son of Mr. and Mrs. N. H. Donaid, and so he took a day	
But a loving memory forever. Always remembered by her mother, lers, and brother.	a friend, Mrs. W. Keens, Mrs. Brock- er 2/- each. These donations made the total takings £20/3/111. The	to sea, as there was no back-up from	We extend our congratulations to the winner. Miss Amber Steele, and	Yours &c., CHAS. G. ORR.	could be conducted—when the headmaster, Mr. T. J. Curry, re- vealed in his annual report the shocking conditions under which	off from school and wandered out Settlement Point way. Shortly after walking off the road into the gutter	
ort Macquarie Rews.	Mother's Club expresses appreciation of the generous support accorded the stall.	very little floeding in comparison came from Kooloonbung. In conse- quence, the flood waters fell almost	In passing, Mr. Mowle's third choice	THE FIRST GAME	an effective enrolment of 413 pup- its are herded together in class- rooms and corrridors and sheds	and getting wet to the armpits, he trod on a submerged glass bottle, and inflicted a masty cut beneath the big	
TURDAY, MARCH 29, 1946	DIGGER DOINGS.	as rapidly as they had risen, and where roads had been from 3 to 30 feet under water, 24 hours after the	The two winners are as under :	Football at Wauchope	designed to house about half that number. Apart from the fact that it is impossible to teach	toe on his left foot. Mr. Ifford Keena happened to be handy at the time and conveyed the lad first by	
	(Contributed) down to far milfe again. He was no-	rain ceased the water was all gone from them. The rain commenced on Saturday,	For the future development of the Hastings District a good water supply is essential. Present small town	This Saturday afternoon, on Waue- hope Park, footballers will "blow	children under such conditions, their health is constantly endar- gered, as a child could not wil-	boat, then by car, to town, and he was admitted as a patient to the H. D. Hospital. The cut, which also	
they have farewelled practically	down to farm life again. He was no- liced the other day with one of his draught hornes, drawing poles in real Digger style. These hornes of Brace's,	when Port Macquarie had six points, on Sunday 315 points, and on Mon- day 318 points, whilst Wauchope had 12 inches during the deluge and some	schemes are insufficient to meet the demands of a rapidly growing com- munity. Industries and factories can-	their bellows" — heartily, no doubt- for the first time in 1946 season, which gives promise of renewed vig- our and interest with a large num-	fully be exposed to a greater danger than being crowded with an out-of-all-proportion number	severed a tendon, needed stitching up and with his foot in plaster and em- bedded in sand bags. Charles has plenty to remind him of his first ex-	•
nt away from the district, and they welcomed them home again, and	I believe, are in good nick, and ne wonder, they get a bushel of corn- meal a feed. Wouldn't we like some	reports from the upper reaches men- tion 17 inches.	sufficient power and water for their needs. We have power, but lack the	ber of servicemen home again and anxious to learn how much they have forgetten, and how much the young-	of other children, into a drafty, ill-lit corrider. It would be as well, also, for every parent to	perience of French leave from school. Chairman of the County Cosmell, Cr. C. G. Orr, is on a visit to Syd-	. К.
a no one would suggest denying in the privilege of carrying on in ir own manner a job so nobly done,	corn for our fewla! Jack Mooney was supposed to go fishing on the Hastings and lost his	At the highest level, sround 3 s.m. on Monday morning, the flood waters were about two feet lower at Wauc- hope than the 1929 level, whilst at	taking. Why not develop our water	stors have learnt in their absence. There will be two games, two soldier sides play two civilian teams, and	know, that the public convenien- ces, particularly the girls, are not only inadequate, but far from what they should be. A sustained	ney on behalf of the council, and Mrs. Or, accompanied him. The electrical engineer, Mr. Dave Thom-	
ty deserve. A year or two ago		Hibbard the highest mark was about 15 inches lower. The district was more or less iso-	supply under similar organisation? Our local governing bodies should give their ultermost co-operation to arrange for a district water supply	there will be plenty of keenness and interest, as all players will be trying to make an impression on group and	effort is to be made to bring the disgraceful state of affairs under the notice of the proper authori-	son, is also in Sydney, attending the electrical engineers' conference. Mr. R. E. Locke has returned home	
nd welcome home, "when it would	Digger Chelman is going to try his luck on a cane farm in Queensland in a couple of week's time, and is	iated for a day and a night. The New England road was impassable, owing to the flooding and landslides,	scheme. What is to be expected of this scheme? It will be necessary to plan for a supply adequate enough to	club team selectors. Proceeds of the gate are to be split three ways, be- tween the sub-tranches of the R.S.L.	ties, but the point is this while the parents of the children are prepared to go on having this	to Narromine after visiting the town for a week or so. Mr. Locke intends to return to Port Macquarie to re-	,
a day or two off' to do it. There	wished every success from all Digger cobbers. Wilton Bale is reported to, have been dodging unfortunate cows, cal-	the Armidale bus narrowly averted being caught up in one of these, and the road to Telegraph Point was also	meet the seeds of our community, not only in the present, but for the future. The continual growth of population	at Wauchops and Port Macquarie and the Hastings District All Services As- sociation. Bailey's bus will be leav- for Wauchops shortly sitter 1 p.m.,	treatment meted out to their children without any public out- cry, those in authority will take it	side shortly, and his present inten- tions are such as will tend to relieve the accommodation problem.	
meant, nor do we think for a suite that the good-will which	ves, pigs and such like while rowing around in the flood waters recently. Not such good weather for those	impassable. A sheet of water as far as the eye could see presented itself in Wauchope lane, where at its high-	demands it. Wauchope has its own water supply coming from Blue Creek, but in dry seasons this system	and there will be room for players and supporters Any players desiring a try-out are invited.	for granted that the local condi- tions aren't so bad after all, and will allow them to continue. The	Mr. and Mrs. Rog Evans have the former's mother and sister (Jaan) staying with them. Mrs. Evans comes from the Clarence and, naturally, has	
the grand welcome home is not us to say, but it deserves the co-	choice cysters, though Wilf. Good to be up and about again, ch." Jack Staples had a great trip back	est it reached the guttering on An- urew's dairy bails in the lane, and it was impossible to get to Wauchope	fails to satisfy requirements. A sys- tem that is not always reliable is of little value to a growing community. Small town schemes must give way	The teams selected are : Services 1-E. Louis, O. Cheers, R. Andrews, Bob Byrnes, Tony Dick, V.	community owes it to their child- ren to get behind the P. and C. and tell the government they are not going to stand for their child-	had an anxious time with the flood news. Rev. Exper, the new Presbyterian	
anisation throughout the town and	from Sydney. Says "you'd think I had bought the so and so train, the time I spent in it." And then to be faced	by the main road. Measrs. Bailey and Denham, the mail contractors, managed to keep up an unbrok- en record of fifty years' survice with	to a scheme that will satisfy a'l de-	Rosenbaum, G. Campbell, E. Morcom, J. Andrews, P. Kirkwood, H. Rosen- baum, Boyd, J. Fowler, B. Nizon.	ren being exposed to such dang- ers from a health point of view. The position can only get worse,	Minister for Port Macquarie Charge, arrived on Wedneeday, and for the present is domiciled with Rev. J. D.	
bit of the "News" to suggest that	with swimming back to Port Mac- quarie. Jack dropped a line in Bar- ah's Creek on the way lack, but	the mails, by taking a best and row- ing from Zanardi's gate, at the foot of Spence's cutting, to Wauchope, and	If so, the time to act in the matter is now! The Hastings District must	Services 2.—Cant. Perrott, Brown, Graham, Hancock, Pead, Healey, Marchment, Dick, Johnson, Kilmur-	as the numbers are increasing daily. A good Blustration of this increase is the kindergarten class.	and Mrs. Walker. Mrs. Elipper is re- covering from an accident, and was unable to come with her husband, but	
g's Birthday week end. The tennis	Diggers are reminded of the foot- ball match at Wauchope on Saturday. All Diggers interested catch the bus	returning with the mails about mid- day on Mondey. They also brought across the Armidale bus passengers.	Marie Davison, BL Jospeh's Con-	ra), Mooney, Shoesmith, Brest, ReservesChapman, Avery, John- son, Lewis, Haywood, McKenzie, The Rest 1-Green, Bird, Haggerty	Half-way through last year it was contended that only about six pupils were available for a	hopes to join him shortly. The In- duction of Rev. Elpper will take place at Wauchope on Wednesday evening	101
e a city team here; the race club i id no doubt have a fixture iper-	budley Dock has been racing a- round altenting to flood mattering	who were glad to reach their destina- tion. Mr. F. Matesich came down with train passengers through Bago.	vent School, Port Macquarie. "What! No water? What kind of a holiday resort is this? Not even a	Law, Dark, Huddleston, Crozler, Rob- inson, Bransdon, Monoghan, Fowler, Pariah V, Johnson, Sparge Smith.	kindergarten class, whilst the number enrolled to day is 45. It would be an hour well spent by	next. Rev. John Walker will be absent from town periodically for the next	
icipating); the golf links will be heir best; the town could have an	plenty of extra jobs now. Dud: Otto Stewart twisted his back whilst doing a spot of painting the	In flood-time. Mr. Matesich has al- ways used this route; and has always managed to get through. The Hib-	there only seems to be set water here." With this the angry tourist walked out of the bathroom, quoting	The Rest 2-Laws, Myers, G. Fowl- er, White, Monkley, Anderson, Shoe- smith, Hayward, Ringiand, Fletcher,	any parent to visit the school and see for themselves. A full report of the headmaster's revelations,	three weeks as he will be taking charge of the Taree pulpit for that period.	
d be arranged at night there is the cus for the building up of a grand mome home. There are bundreds	other day-some story of a windmill. Dad luck, Otto, you've got the "brush- off from work for a while!	bard road was also impossible. Hibbard was our greatest canualty as there the water went into many	sarcastically : "Water, water, everywhere, But not a drop to drink."	F.etcher, Glyde, Baker, Chiaholm. All players present will be tried.	and a detailed report of the meeting, will appear in our next issue	A meeting of the Presbytery of the Hastings was held in Wascheps on Monday night. There were sever- al clergy and laymen present from	
hem who deserve our "thank- a." It may not be within our , for to provide all the material	Congratulations to Les Crisp. Ted Avern and Charlie Britt, president, secretary and treasurer respectively	houses, some having three feet of water in their homes, and the little community had no sleep on the anx-	 How many people have you heard during the holiday season make a durinp the holiday season make a similar statement to the one above? 	601	A super-primary school at Waue- hope, costing £5,500, is included in	autrounding parts, and most of them were unable to return to their homes for a few days, owing to the floods.	
of their most cheriabed dream-	of th golf glub, Be O.K., if those fei- ows don't start digging irenches and such like up there on our greens !	lous Monday night. Settlement Point was more or less inundated, as one great sheet of water spread back from the White Cross to the river at	Well, can't we do something to stop this? Of course we can, with such a river as the Hastings this could be		the Department of Education's 182,- 000,000 post-war programme.	At this meeting, Rev. Eipper, the minister succeeding Rev. J. D. Walk- er, was to have been inducted to the	
th they can live without fear, and y once again to the full their nat-	And make other members "below pac," If it's information, badges, clips,	the regatts ground and reached far along towards the point. The "water- front" was flocded, and the water was	easily done. The Hastings River has a well supplied waterahed, and has many tributaries. The greater part of	The following programme of events	RAWDON ISLAND.	charge, but on his way down from New England, he and his party were held up at Walcha by the flood condi-	
t to show our appreciation we	sominations, etc., you're after, see fack Staples, beadquarters of Port dacquarie sub-branch. Beileve VIv Oliver was the recip-	18 inches deep in Radley's shed. Much camage was done to wharves and moorings here and along the river.	this water is wasted when it runs sway to the sea. Port Macquarie's hotels and guest bouses are often left	should fully occupy our goifers during the next tribee months. Commencing 6th April an eclectic competition will	(Contributed)	tions and were unable to come on to Wauchope for the caremony. Pte, Dallas Staples is home on leave .	
is for a united effort.	ent of a bealthy smack on the seat of the pants at Challs House on Chursday last. He looked around in	The North Shore wall was under water for a time in several places. One of the greatest losses locally	short or without water. This spoils the future of Port Macquarie, which means a great deal to each of us.	be conducted and a trophy will be swarded the best score on June 30th. Another trophy (a stick donated by Mr. F. Rosendah) will be given for	at the Island with his daughter, Mrs. J. Haggetry, Mr. N. C. Knight left last week to return to his home at	with his parents, Mr. and Mrs. J. E. Staples. Dallas is a member of the occupation forces, and expects a trip	
DEMOBILISATION		washed badly, and much of the prec-	Why do we risk this? Water, which is very essential to mankind, is much needed in our town. People have to rely on their takes supply for every-	best aggregate score over ten com- pelition games played on nominated occasions between 6th April and 30th	Hobart, Taamania. Mr. Eddle Fowler suffered this week from a poisoned hand, which necesi-	to Japan. Mr. Les Kenny, secretary of the local branch of the A.L.P., is on a visit to Sydney.	
int 1945, the strength of the trailan forces was 604,584. Be-	Te turned to the defensive again, only o find his Digger cobber, Jack Stap- es, equipped with worthy walking	ious pea seed—so short in supply— was lost: slee, many, many acres of the rich red soil washed over the weir at Koolocnbung. So choeo's te-	thing. This is quite insufficient for the many purposes water is used for. So the people have to turn to well	Juse. Penalties will be inflicted for incomplete cards in this competition, which will be conducted each week-	itated medical attention. Mr. D. Knight, of Bostohrick, Dor- rigo, spent a few days with his sister.	L.A.C. Arthur ("Bungy") Warde was in town at the week-end, and ex- pects his discharge from the Air	
tal of 336,363 men and woman been demobilized. The balance	ettled it rather suitably. Digger Everingtam is at present	coloured was the water you could see the particles of soll quite plainly as the waters rushed by. The loss of	water. We'l water is sometimes im- pure, therefore it is very dangerous to drink it. If clean, fresh water is	end. Full details at the club house. A monthly trophy stroke competition will be held during the first week-end	Mrs. J. Haggetry, this week. He was accompanied on the return journey by another sister. Mrs. F. Stewart.	Force to be completed this week, Ar- thur had a narrow shave from being returned to Borneo.	
bers will be demobilised by June	leased to note that he is improving. Digger Usher is looking forward to	the soil is irreparable. Much loss was averted by timely removal of stack and other mode and	not supplied the town will become disease infested, not fit for a holiday resort. A dam should be built to	of each month, commencing April 6. The club championship will be con- ducted during July and August. All	and her daughter, Margaret, who had been holidaying at the Island for a fortnight.	Mr. Roy Kean, of Sydney, is the guest of his sister, Mrs. P. Hayward, A friend, Mr. Frank Pend, accompan-	
NORTH MAILS		chattels throughout the district on Monday.	catch the tributaries. This dam will not only be a help to the towns- people, but also the farmers. People		After many months on the Island with her relatives, Mr. and Mrs. Myles Downes, Miss Colleen Roberts has	ied him on the holiday. Mr. Kean, who worked in the late Mr. P. Hay- ward's store, left Port Macquarie in	11) (
ir. R. Grabam, reveal an improve- t in the time taken for letters to	ood haul of very nice bream on Fri-	take a long time. Stock-cattle, pigs, etc., were seen going over the bar,	are encouraged to live on farms, but they are never helped by being given a constortable and convenient home,	at 2 p.m. until 1st June, when start- ing time will be 1.50 p.m. Opening day, 7th April, an 18 hoise stroke competition will be held in the	gone on to Sydney to be with her mother, Mrs. H. Roberts. With the return of Cardinal Nor-	1915. He notes many changes after the lapse of 30 years. Mr. and Mrs. E. King left by car	
tillumbah, since the restoration of h trains. Efforts are being made ilminate the delay occurred by e	as passed the "double-header" stage Congratulations to our genial fish- ries inspector for his fine work dur-	er at Bain's Bridge lost £1500 worth of bees, boxes, and honey, a loss he can't replace. The Porbos River sus-	but instead they have to slave to get a penny'to call their own. They have to carry water from water holes. Why not give these farments a batter dealy	stroke competition will be held in the morning, starting at 9.30 a.m. Tro- phies for best off the stick and also handicap. Invitation mixed foursemes.	man Gilroy from his consecration at the Vatican, the little Catholic com- munity of the Island have a great	on Monday, an route to South Amstra- lia, and will cover the final stage of the journey by air. Mr. King will induce an experience of the later	
picked up at towns along the in a Coast line other than at the points.	tg flood conditions — up around tranded Beiliement Point, heiping olice dept. Many congrate for your	pension bridge has gone, and some idea of the height and rush of water can be gauged from the fact. At	not give these farmers a better deal? Weter is necessary for manitation, for domestic purposes and irrigation; also as a protection against fires, and to	commencing at 1.45 p.m. in the after- noon. Futting competitions during	feeling of pride in the fact that this world figure path a special visit to their church last year. On that oc-	undergo an operation in Adelaide, His many digger pais and other friends wish him every success, and a speedy recovery and return to Fort Mac-	
ABERTING OF APPEALERS	ne work, Diek. It is reported Jim Aroney has been n the end of the breakwall with-	Rawdon Island, all communication with the mainland coased. The ce- ment bridge has disappeared, and the	be able to supply the district with electricity would be a great advant- age; at present we have to rely on	able, but no luncheon provided Herewith is the programme:	casion he congratulated them on their ext,sordinary generosity towards the church.	quarte. The golf club has lost one of its very popular players in Mrs. Joan	
an advertisement calling a c	. Good luck, Jim! .	the Break-away Rocks ferry, which	Newcastle for power. This impound- ing will be very expensive, but it, like the Burrinjuck Dam, will pay	March 31-Stableford. April 6-Stroke.	Th Islanders were intrigued to read in the local press a statement attributed to the shire engineer, in reply to a question by Councillor H.	Phillips. A farewell afternoon had been arranged for her by the members but, unfortunately, Mrs. Phillip's de-	

	MEETING OF APPEALERS	It is reported Jim Aroney has been	with the mainland ceased. The ce-	electricity would be a great advant-	Herewith is the programme :	extraordinary generosity towards the	quaree.		1.1.1
	In another column in this issue ap-	on the end of the breakwall with	ment bridge has disappeared, and the			church.	The golf club has lost one of its		
	meeting to take place in the Emplies	rope and hook, "fishing" for lost box-	surranaion bridge was demaged by			Th lalanders were intrigued to	very popular players in Mrs. Joan		
	pears an advertisement calling a	es. Good luck Jim!	the treak-away Rocks ferry, which			read in the local press a statement	Phillips. A farewell afternoon had		191
	Hall on Wednesday evening next of	Radley boys had a narrow spcape	fortunately went down the backwater	ing win on very expensive, out it,		attributed to the shire engineer, in	been arranged for her by the members		100
	all those who intend appealing against		tortunately went down the backwater			reply to a question by Councillor H.	but, unfortunately, Mrs. Phillip's de-		1
	their increased valuations, and any-			back what it costs and more. This	day).		parture to Sydney was hastened, pre-		- 2
	the interested valuations, and any-	wharf collapsed in the early hours of	Damage was done to crops there, as	dam will be a great asset to the town.	April 13 Stroke.	Warlters, that the road gang was	venting the members and associates		1.
	one interested is invited to go along.	Tuesday morning. However, I believe	well as in all other parts of the river.	Tourists will not grumble then about	April 14-Canadian mixed, draw	working towards Rawdon Jaland. The	from saying good-bye to her in person		100
	Forms for appeals are obtainable	they made a good haul on Tuesday-	On the Maclesy, almost 14 inches	the water supply. We all want to		serival of the gang will not be too	on the links. A gift, in the form of		135 C
	from the town clerk.	when they hauled for pumpking	fell at Bellbrock, and 111 at Kemp-	make this town the best seaside re-		soon as far as the condition of the	pyrex ware, has been forwarded to		
		grammas, and such like.	sey. The river rose to a maximum	sort in New South Wales. We can		roads are concerned, for, as previously			18
	MR. FORDE IMPRESSED	Clive Atkins has had some bad luck	height of 41 feet, on Tuesday. It broke	do this by helping to build this dam.	April 25 Mixed four ball (invita-	written, they are in shocking order.	Madang, and carries with it kindost		1.1
	Prior to his return to Sydney on	at Settlement Point-flooded house.	over in West Kempsey above the old	of this of helping to build this territ.		One particular "puryle patch" ex-	regards to Mr. and Mrs. Philitps from		
	Wednesday, Mr. Fords, the Under	stc., but you can't keep a good man	pumping station, and a'so near the	All the people in the district should	April 27-Stableford	tends north from the hall for about	all at Port Macquarie with sincere		
-	Secretary, was taken by Mayor Ryan	down, with the result that on Tues-	rallway bridge. At 9.30 the water was	help in this wonderful scheme.		600 yards. Deep ruts criss-cross and/	wishes for health and happiness and		6
	to view the site of the proposed water	day night he was the only fi-'t-rman	running across Beigrave-street and		April 28-Four ball.	or run down the middle of the road.	good luck in their new home. Mrs.		10 M
	scheme and see Mr Cas Rebester	to brave the waters, and made good	funning scross Beigrave-street and	Lorne and Kendall at Bucton's	May 4-Flag.	and any motorist who takes the hill	Phillips sailed for Madang on Wednes-		1
	plant is action. Mr. Forde was im-	to brave the waters, and mac. : good	down to the Willows Drain, and by	Bridge.	May 5-Stroke.	at more than 10 m.p.h knows he's	day on the S.S. Ormiston.		- 11
	pressed with the possibilities, and the		10.80 it had topped the embankment	Bellingen reports a record flood,	May 11-Pour ball.		Mr. and Mrs. A. Doust and their		1.1
	present with the possibilities, and the		and was flowing over its whole length	with heavy loss of stock. Macksville,	May 12-Stableford.	been driving.	two infant daughters, of Paddy's		- 54
	Brst-hand information he obtained		At 4 p.m. it reached the front of Dr.	Grafton and Lismore also report	May 18-Mixed four ball invitation	Mr. C. Brown and son, Athel, are	Plain, Dorrigo, have been enjoying		
	will be of advantage to himself and	der, in the Clyno, when he answered	Marsh's surgery and Belgrave-street	floodings	May 19-Stroke.	making splandid recoveries from their	the Port Macquarie climate during		- 1
	the council as the scheme progresses.		was completely under water from	One of the leasons to be learnt		recent grevious accidents.	the past three weeks.		
		and found his pardner knee deep in	there to West Kempsey. No great	from the beavy rains is the damage		Mr. Neville Hopkins, of Newcastle.			- B
	FLOWER SHOW	and found his pardner keen deep in	damage was done in Kamper, but	caused to roads through faulty drain-	June 1-Stroke	is enjoying another holliday with his	The postmester, Mr. L. A. D. O'-		100
	Eagerly looked forward to year in	erime, sorry-sime,	several homes were flooded and far-	age. Overnight, more damage is	June 2-Stableford	relatives, Mr. and Mrs. F. Lyne.	Connor has been granted extended		100 12
	and year out by all garden lovers.		mers on the Lower Machen auffamil	done to a road by the wash of heavy	June 8-Four ball		sick leave, and will not be resuming		14 S.
	the Methodist Flower Show is not	Miss flunt (Lismore), telephone	anyonaly Conton Phillip Richardson	rain than 12 month's traffic wou'd		After visiting the Wingham and			19 C
	down for Thursday next, afternoon	supervisor, is at present visiting the	if an antione at the Out Barnet	do, and there are instances through-	June D-Par.	Wauchope agricultural shows, Mr.	weeks yet.	2017	200
	and evening sessions. A lovely dis-		to, all subjudge at the Owl Stores,	do, and there are instances through-	June 15-Stroke.	Myles Downes Intends to see the	Mrs. G. Smith has returned home		1000
	piny of flowers is always amassed at	Port Macquarie exchange on routine	evening, and was drowned.	out the municipality where mainten-		Tares effort this week.	from Sydney after receiving medical		128 (21)
	this for the set is always smassed at	inspection.		ance could avert this. A major lo-	June 22-Flag.	The sycionic disturbance, which had	treatment. Friends will hope she best-		100
	this function, and this year promises		At Laurieton, 10 inches of min fell,	cal problem, too, is the Settlement	June 29- Stableford.	been forecast, hit the Island on Bun-	ofits by improved health		100 March 100
	to be no exception. Anyone desiring	Blackman's Point for a number of	and local floodings resulted. The riv-	Point Road, to which much damage	June 29-Stableford.	day last and torrential rain followed	We atknowledge an invitation to		100
	to know any details is advised to get	years past, is moving on. A farewell	er broke the banks near Middlemiss'	was done, and its residents will be	June 30-Stroke.	throughout the day and night. Early	attend the Rollands Plains sports and	8 S	10 B
	in touch with the secretary, Mrs. R.	function, arranged for last Monday	farm, but no serious damage was	happy to see a start made on the re-		on the Monday morning the min	rodeo on 11th May. It is alwoys a	-3	10 million
	M. Rosen sum, who has a'l prelinin-	evening in Mr. Geo. Gardiner's barn	camped. Telephone communication	moval of the rubbinh that is at pres-	The winner of last Saturday's	showed no sim of shaling and the	pleasant day's outing at The Flains,		ALC: 3.4.
	aries running emoothly in her usual	-then flooded-is to be held on Mon-	to be interrupted. For a time, also,	ent making it untrafficable, and the	stroke competition was Alan Kennedy	danger of the Island being flooded	and they have the happy knack of "		100
	capabic style.	day evening next.	the road was impassable netween	restoration of its surface.		had not yet passed.	turning on some real entertairment.		100
				The second secon		and the party of the second se	forming on some real enteremptionent.		100
									100
									286 Bar
									C 24 17

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Item 12.01 Attachment 3

Respondent	Category	Department/Address	Response	Comments	Date Received
Transport for NSW	Public Authority	Commercial Performance and Strategy Infrastructure and Place	Nil Objection	Good Afternoon, Transport for NSW (Roads) has no objection to the proposed road closure with your reference SF21/604 dated the 27 April 2021. Thank you	18/05/2021
Private	Owner	17 Eames Avenue NORTH HAVEN NSW 2443	Nil Objection	I have a property at 17 Eames Avenue North Haven, on the corner of Granny Bell Lane. I do not object to this road closure but would ask for a right of way as I currently enjoy a side access from Granny Bell Lane to the back of my property where I store my boat. I have discussed this with the Bells and they assured me that I will maintain access, but if they ever sell I could potentially lose access.	17/05/2021
Private	Owner	Brigadoon Holiday Park - 7 Eames Avenue NORTH HAVEN NSW 2443	Objection	The residents of Brigadoon Holiday Park who front Granny Bell Lane use the road for access to the rear of their properties to move furniture etc. During the floods the public road was also a point of easy exit for them off (sic) their dwellings. I believe it is important to retain the public access for this reason. All residents of the park are elderly and need as many access points to their properties as they can get.	5/05/2021
Water NSW	Public Authority	Facilities Coordinator	Nil Objection	Hi, From the information you have provided and the list of land we hold, I have been unable to identify any WaterNSW or WAMC land as being involved or affected by this proposed road closure. Waternsw has no objections to the advised proposed road closure.	5/05/2021
				Regards,	

Attachment 1 - Summary of Responses Received to the Proposed Closure and Sale of Granny Bell Lane

Item 12.02 Attachment 1

Department of Planning	Public Authority	Property Services Team	Nil Objection	Dear Council	30/04/2021
Industry and Environment	Autority			The Department of Industry Property Services Team has no objections to the advised proposed road closure, except in cases whereby access to any land owned by or in Trust by the Water Administration Ministerial Corporation (WAMC) or its predecessors being the Water Resources Commission and the Water Conservation and Irrigation Commission is affected. From the information you have provided and the list of land we hold, I have been unable to identify any WAMC land as being involved or affected by this proposed road closure. Regards,	
Crown Lands Department of Planning Industry and Environment	Public Authority	Council Road Closure Team	Nil Objection	Dear Council, Thank you for providing notice of the proposal to close a council public road. NSW Department of Planning, Industry & Environment – Crown Lands (the department) does not object to: • the closure of the council public road • the proposed vesting upon closure. General enquiries about this submission may be directed to the Crown Lands Business Centre.	30/04/2021

Attachment 1 - Summary of Responses Received to the Proposed Closure and Sale of Granny Bell Lane

Item 12.02 Attachment 1



Kooloonbung Creek Flying-fox Camp Management Plan Final Adopted by Council June 2019

PORT MACQUARIE-HASTINGS COUNCIL



ecology / vegetation / wildlife / aquatic ecology / GIS

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Acknowledgements

Port Macquarie-Hastings Council would like to thank everyone who participated in community consultation, with all comments considered in the development of this plan and incorporated where possible. Council acknowledges input by the New South Wales Office of Environment and Heritage to the Plan in developing the template upon which this Camp Management Plan is based, and Dr Peggy Eby who provided advice which was included in the template.

PR3995 Kooloonbung Creek Camp Management Plan

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Acronyms and abbreviations

ABLV	Australian bat lyssavirus
BAM	Biodiversity Assessment Method
BC Act	Biodiversity Conservation Act 2016 (NSW)
BDAR	Biodiversity Development Assessment Report
BFF	Black flying-fox (<i>Pteropus alecto</i>)
the camp	Kooloonbung Creek flying-fox camp
CE	Critically endangered
Council	Port Macquarie-Hastings Council
DoEE	Department of the Environment and Energy (Commonwealth)
DPI	Department of Primary Industries (NSW)
E	Endangered
EEC	Endangered Ecological Communities
EP&A Act	Environmental Planning and Assessment Act 1979 (NSW)
EPA	Environment Protection Authority
EPBC Act	Environment Protection and Biodiversity Conservation Act 1999 (Commonwealth)
FKCNP	Friends of Kooloonbung Creek Nature Park
GHFF	Grey-headed flying-fox (Pteropus poliocephalus)
the Guideline	Referral guideline for management actions in grey-headed and spectacled flying-fox camps 2015 (Commonwealth)
HeV	Hendra virus
KCPoM	Kooloonbung Creek Plan of Management 2012
LEP	Local Environmental Plan
LGA	Local Government Area
LGNSW	Local Government of New South Wales
LRFF	Little red flying-fox (Pteropus scapulatus)
MNES	Matters of national environmental significance
NFFMP	National flying-fox monitoring program

PR3995 Kooloonbung Creek Camp Management Plan

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ATTACHMENT

ORDINARY COUNCIL 16/06/2021

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NPW Act	National Parks and Wildlife Act 1974 (NSW)
NPWS	National Parks and Wildlife Service (NSW)
OEH	Office of Environment and Heritage (NSW)
the Park	Kooloonbung Creek Nature Park
the Plan	this Camp Management Plan
POEO Act	Protection of the Environment Operations Act 1997 (NSW)
the Policy	Flying-fox Camp Management Policy 2015 (NSW)
SEPPs	State Environmental Planning Policies
SIS	Species impact statement
TEC	Threatened ecological community
V	Vulnerable

PR3995 Kooloonbung Creek Camp Management Plan

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1 Introduction

The Kooloonbung Creek Camp Management Plan (the Plan) provides Port Macquarie-Hastings Council (Council) with a framework for managing community impacts associated with flying-foxes roosting along Kooloonbung Creek, whilst ensuring flying-foxes and their ecological services are conserved.

Three species of flying-foxes occur in New South Wales (NSW):

- grey-headed flying-fox (Pteropus poliocephalus) (GHFF)
- black flying-fox (P. alecto) (BFF)
- little red flying-fox (*P. scapulatus*) (LRFF).

Kooloonbung Creek flying-fox camp (the camp) is mainly occupied by GHFF, and at times by BFF and the highly transient LRFF. All three species of flying-foxes, and their habitats, are protected under NSW legislation. The GHFF is also listed as Vulnerable under Commonwealth legislation, affording it additional protection.

Detail of relevant legislation and policy related to flying-foxes is provided in Appendix 1. Flying-fox ecology, species profiles and roost characteristics are provided in Appendix 2.

The Kooloonbung Creek camp is one of eight flying-fox camps in the Port Macquarie-Hastings local government area (LGA), monitored as part of the National Flying-fox Monitoring Program (NFFMP) (Figure 1).

1.1 Flying-foxes in urban areas

Flying-foxes are highly nomadic, moving across their range between a network of camps. Camps may be permanently occupied, seasonal, temporary or sporadic, and numbers can fluctuate significantly on a daily/seasonal basis. Flying-foxes may travel up to 100 km a night in search of food resources (nectar, pollen and fruit), and their occurrence within the region is tightly linked to flowering and fruiting of foraging trees. Typically, the abundance of resources within a 20–50 km radius of a camp site will be a key determinant of the size of a camp (SEQ Catchments 2012). However, understanding the availability of foraging resources is difficult because flowering and fruiting are not reliable every year and vary between locations (SEQ Catchments 2012). This highlights the need for a multi-faceted approach to management that is continually adapted as situations change or further research improves our understanding of flying-foxes and their management.

Living near a flying-fox camp can be challenging for communities, with impacts associated with noise, odour, faecal drop, damage to vegetation and concern about potential health risks. There are also challenges associated with management. State approval is required under legislation to manage a camp, and actions which may affect the GHFF must also adhere to federal policy. Attempts to relocate flying-foxes are extremely costly, and often splinter a camp to multiple undesirable locations that are difficult to predict. Flying-foxes will also regularly

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attempt to recolonise their preferred camp site when resources are available, and it is not appropriate or possible to remove all of the flowering and fruiting trees that attract them to the region.

Flying-foxes appear to be roosting and foraging in urban areas more frequently. During a study of national flying-fox camp occupation, almost three quarters of the 310 active GHFF camps (72%) were located in urban areas, 22% on agricultural land and only 4% in protected areas (Timmiss 2017). Furthermore, the number of camps increased with increasing human population densities (up to ~4000 people per km²) (Timmiss 2017).

There are many possible drivers for this urbanising trend, as summarised by Tait et al. (2014):

- · loss of native habitat and urban expansion
- opportunities presented by year-round food availability from native and exotic species found in expanding urban areas
- disturbance events such as drought, fires, cyclones
- human disturbance or culling at non-urban camps or orchards
- · urban effects on local climate
- refuge from predation
- movement advantages, e.g. ease of manoeuvring in flight due to the open nature of the habitat or ease of navigation due to landmarks and lighting.

These drivers mean that flying-foxes are likely to continuing occupying the camp into the future. Favourable habitat and food resources within the local government area (LGA) mean that camps may also establish in new locations.

Regular stakeholder engagement during the development of this plan aims to ensure the values of the Port Macquarie community are considered, and concerns of residents who have been directly impacted are addressed.

1.2 Plan objectives

The Plan has been prepared in accordance with the NSW Flying-fox Camp Management Policy (2015) framework, administered by the Office of Environment and Heritage (OEH).

The objectives of this Plan are to:

- manage community impacts and concerns associated with the camp, whilst conserving flying-foxes and their habitat
- improve community understanding and appreciation of flying-foxes, including their critical ecological role
- enable land managers and other stakeholders to use a range of suitable management responses to sustainably manage flying-foxes

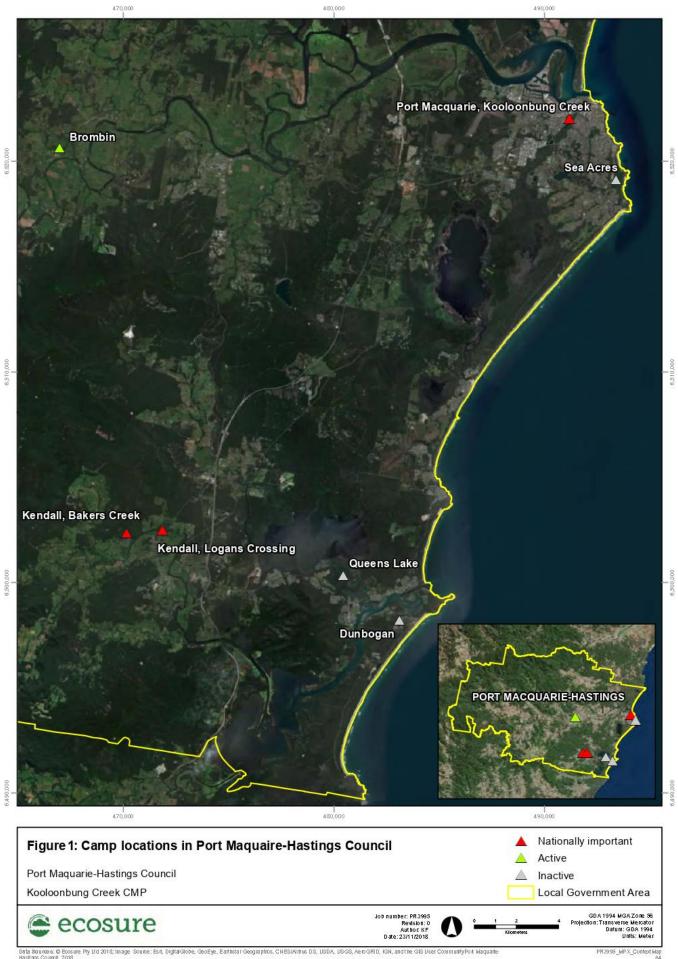
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- effectively communicate with stakeholders during planning and implementation of management activities
- clearly outline the camp management actions that have been approved and will be utilised at the camp
- ensure camp management does not contribute to loss of biodiversity or increase threats to threatened species/communities
- · ensure management actions are consistent with legislative responsibilities
- · ensure flying-fox welfare is a priority during works
- ensure long-term conservation of flying-foxes in appropriate locations.

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2 Context

2.1 Camp description

The camp is located within Kooloonbung Creek Nature Park (the Park) in Port Macquarie, along an estuarine waterway that includes Wrights Creek. The camp is bordered by residential areas to the east, west and south, a historical cemetery and recreational area adjoin the Park in the north.

Kooloonbung Creek camp meets the criteria for a Nationally Important camp under the Referral Guidelines for Management Actions in GHFF and SFF camps (DoE 2015) as it has contained over 10,000 GHFF for two consecutive years and 2,500 permanently or seasonally every year for the last 10 years (OEH 2017).

Vegetation within the camp mainly consists of:

 Broad-leaved Paperbark Swamp Woodland/Forest

•



Grey Mangrove Woodland/Forest.

Figure 2 Kooloonbung Creek

Vegetation throughout the Park also consists of Broad-leaved Paperbark – Mixed Eucalypt Swamp Forest; Sand couch Saltmarsh Grassland and Swamp Oak Coastal Floodplain Wetland Forest (Figure 5). There are also a number of mown areas between residences and the Park (Figure 3).

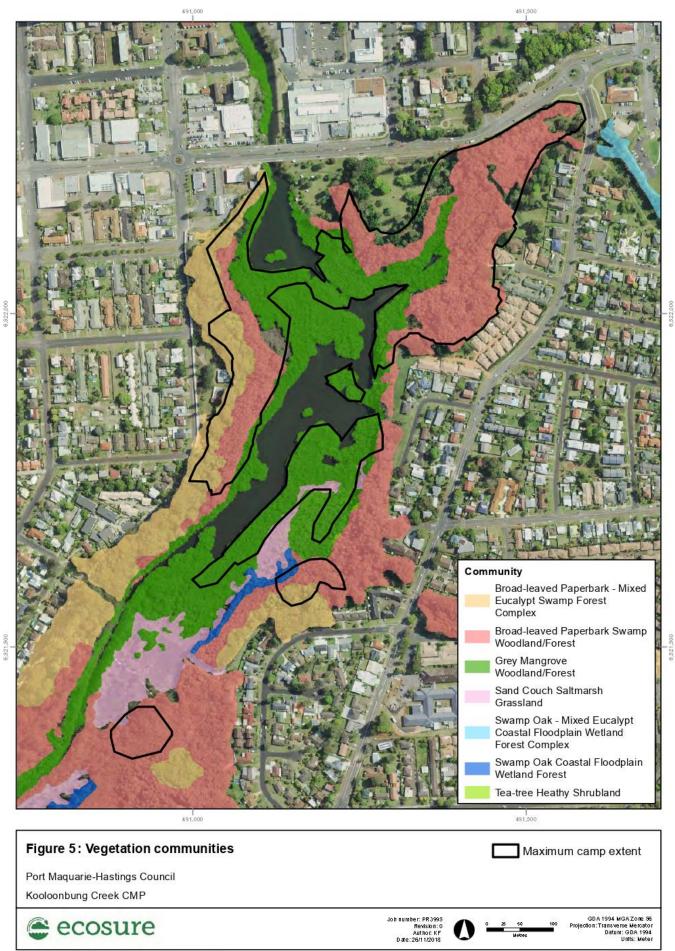


Figure 3 Mown areas between the camp and residents

Figure 4 Boardwalk through mangroves

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In 1985, volunteer group Friends of Kooloonbung Creek Nature Park (FKCNP) formed with a goal to regenerate the natural bushland and make the Park accessible to the public. FKCNP in conjunction with Council have continued with ongoing bush regeneration and park maintenance as well as establishing facilities such as seats and interpretive signage. A public footpath and extensive boardwalk network (Figure 4) extend throughout the Park offering educational and recreational opportunities for cycling, walking or bird watching. Various interpretive signage exists through the Park (Figure 6), however the only flying-fox related messaging is "Flying Foxes at rest – please do not disturb" (Figure 7).



Figure 6 Friends of Kooloonbung interpretive signage

Figure 7 Council sign



Figure 8 GHFF resting in camp

A large number of pups on mothers was observed during the November site assessment by Ecosure (Figure 8). The area occupied by flyingfoxes during the November site assessment was 2.9 ha. The maximum known camp extent is 77.01 ha (PMHC 2018). Both are shown in Figure 11.

Whilst Figure 11 illustrates the maximum camp extent, and Figure 12 shows the changes in the area occupied seasonally by flying-foxes between the years 2015 and 2018.

Some properties on Glebe Close side of the camp have 5m of cleared vegetation in the asset protection zone, although a few properties still have vegetation from the reserve close to or overhanging boundaries (Figure 9). Hollingworth road side of the camp (Figure 10) provides a 15m road buffer plus a 15m vegetative buffer between homes and the maximum known camp extent on the western side of the Park.

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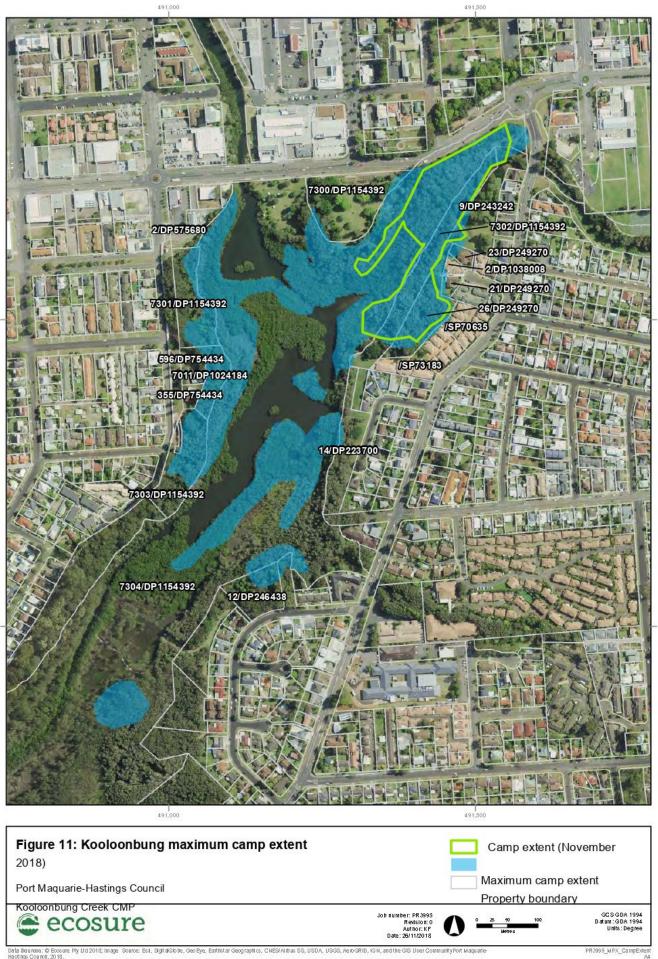
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Figure 9 Vegetation overhanging property boundaries Figure 10 Hollingworth street acts as a buffer from roosting flying-foxes in the Park.

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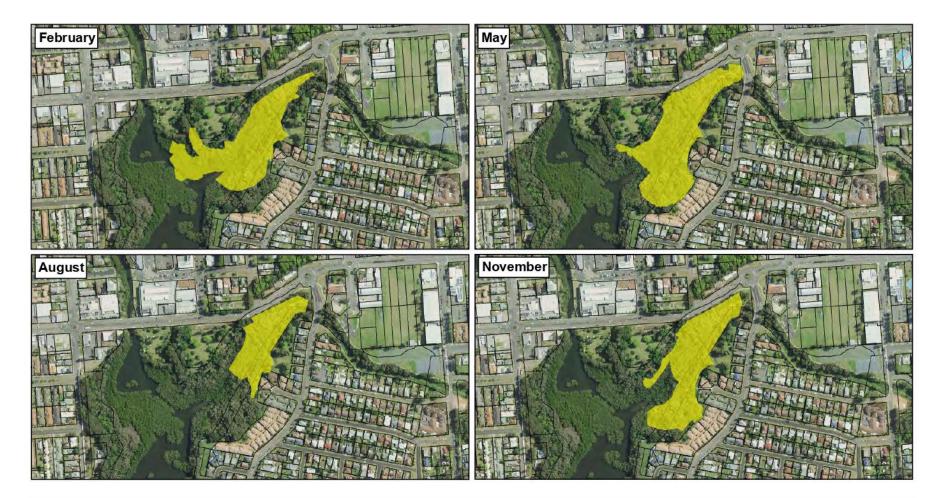


Figure 12: Seasonal camp extents 2015-2018 Port Maquarie-Hastings Council Kooloonbung Creek CMP	Camp extent Capperty boundary
ecosure	Job number: PR3995 Revision: 0 Author: KP Date: 26/11/2018 Job number: PR3995 Revision: 0 Date: 26/11/2018 GDA 1994 MGA Zone Poleotor: Transverse Merco Units: Markets Units: Markets Units: Markets
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2.2 Tenure

The camp is located on multiple land parcels including freehold, Council land and Crown land for which Council is appointed Trustee. Table 1 provides a list of properties that comprise the maximum camp extent. This will assist in identifying the location/s for potential management actions.

Table 1 Properties comprising the	he maximum camp extent. I	Lot and plans are mapped in Section 2.1.

	1 5	
Lot and DP	Tenure	Zoning
7300/DP1154392	Council	RE1 Public recreation (Historical cemetery)
7301/DP1154392	Council	E2 Environmental conservation
7302/DP1154392	Council	E2 Environmental conservation
7303/DP1154392	Council	E2 Environmental conservation
7304/DP1154392	Crown	E2 Environmental Conservation (Kooloonbung Creek Nature Park)
9/DP243243	Freehold	R2 Low Density Residential
21/DP249270	Freehold	R1 General residential
23/DP249270	Freehold	R1 General residential
26/DP249270	Council	RE1 Public recreation
7011/DP1024184	Council	E2 Environmental conservation (behind girl guides)
14/DP223700	Council	E2 Environmental conservation
12/DP246438	Freehold	E2 Environmental conservation
355/DP754434	Crown	E2 Environmental conservation
596/DP754434	Crown	E2 Environmental conservation
2/DP575680	Freehold	B2 Local centre
SP70635	Strata Freehold	R1 General residential
SP73183	Strata Freehold	R1 General residential
2/DP1038008	Freehold	R1 General residential

2.3 Other ecological values

Thirty-three threatened species are known to occur or have been recorded within 1 km of Kooloonbung Creek camp (Table 2) (Figure 13). Migratory and marine species found within the area have been excluded from this list but are provided in Appendix 3. The Park also contains five threatened plant communities; subtropical coastal floodplain forest (Endangered Ecological Community {EEC}), freshwater wetlands on coastal floodplains (EEC), coastal saltmarsh (EEC), swamp oak floodplain forest (EEC) and coastal upland swamp (EEC) (PMHC 2012) (Figure 14).

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Protection level	Source	Category	Values/significance	Details
Federal NFFMP (DoEE 2018)		Nationally important camp	See definition Appendix 1.	Site meets criteria.
	Protected Matters Search Tool (DoEE 2018) Atlas of Living Australia 2019	Threatened species^	Regent Honeyeater (<i>Anthochaera phrygia</i>) (CE) Australasian Bittern (<i>Botaurus poiciloptilus</i>) (E) Red Knot (<i>Calidris canutus</i>) (E) Curlew Sandpiper (<i>Calidris ferruginea</i>) (CE) Lesser Sand Plover (<i>Charadrius mongolus</i>) (E) Eastern Curlew (<i>Numenius madagascariensis</i>) (CE) Fairy Prion (<i>Pachyptila turtur subantarctica</i>) (V) Spot-tailed Quoll (<i>Dasyurus maculatus maculatus</i>) (E) Koala (<i>Phascolarctos cinereus</i>) (V) <i>Allocasuarina thalassoscopica</i> (E)	10 species (2 mammals, 7 birds and 1 plant) known to occur within the area (SPRAT data not mapped)
State	Bionet (OEH 2018) Atlas of Living Australia 2019	Threatened species	Magpie Goose (Anseranas semipalmata) (V) Bush Stone-Curlew (Burhinus grallarius) (E) Glossy Black Cockatoo (Calyptorhychus lathami) (V) White-eared Monarch (Carterornis leucotis) (V) Spotted Harrier (Cicus assimilis) (V) Barred Cuckoo-shrike (Coracina lineata) (V) Varied Sittella (Daphoenositta chrysoptera) (V) Black-necked Stork (Ephippiorhychus asiaticus) (E) Black Falcon (Falco subniger) (V) Pied Oystercatcher (Haemtopus longirostris) (V) Swift Parrot (Lathamus discolour) (E) Square-tailed Kite (Lophoictinia isura) (V) Eastern Curlew (Numenius madagascariensis) (CE) Eastern Osprey (Pandion cristatus) (V) Little Lorikeet (Parvipsitta pusilla) (V) Koala (Phascolarctos cinereus) (V)	15 species (13 birds, 1 mammal and 1 plant) have been recorded within 1 km of camp
Local	KCPoM (2012)	Threatened species identified in the reserve	Black-necked Stork (<i>Ephippiorhynchus asiaticus</i>) (E) Black Bittern (<i>Ixobrychus flavicollis</i>) (V) Freckled Duck (<i>Stictonetta naevosa</i>) (V) Eastern Osprey (<i>Pandion cristatus</i>) (V) Little Bent-wing Bat (<i>Miniopterus australis</i>) (V) Eastern Bent-wing Bat (<i>Miniopterus schreibersii</i> <i>oceanensis</i>) (V) Eastern Freetail-bat (<i>Mormopterus norfolkensis</i>) (V) Southern Myotis (<i>Myotis macropus</i>) (V) Greater Broad-nosed Bat (<i>Scoteanax rueppellii</i>) (V) Biconvex paperbark (<i>Melalueca biconvexa</i>) (V)	10 species identified in the park (not recorded in database searches)

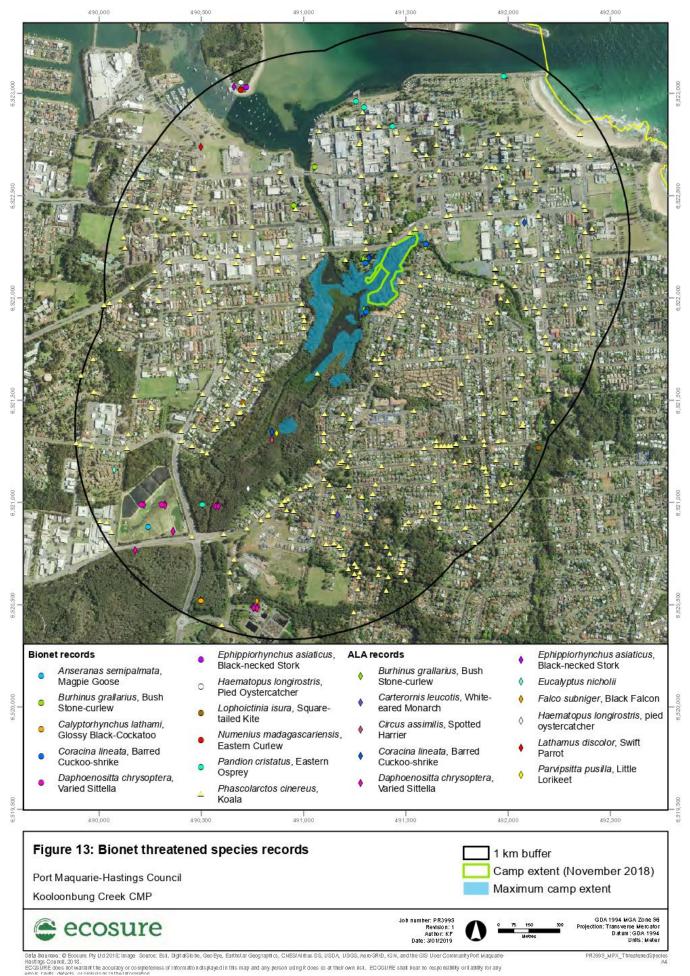
Table 2 Other ecological values known to occur or recorded within 1km of the camp

^ listing status: CE - Critically Endangered, EN - Endangered, VU - Vulnerable

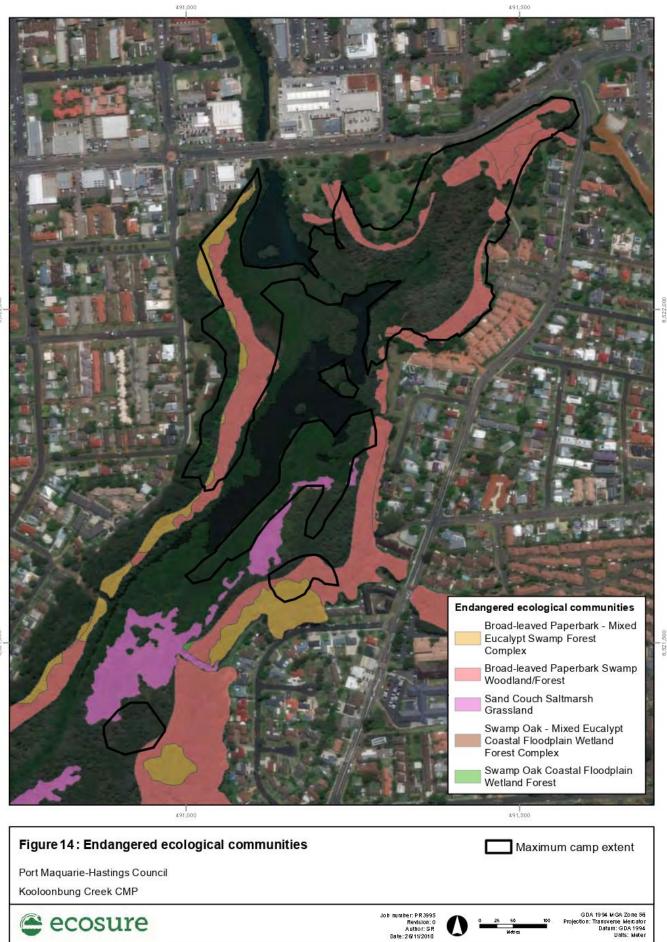
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2.4 History of the camp

Flying-foxes arrived at the Kooloonbung Creek camp in the 1990s (PMHC 2012). Prior to this, flying-foxes had occupied Sea Acres Nature Reserve, approximately 3.5 km to the south east (Figure 1). The camp has recorded a maximum of 173,000 flying-foxes in January 2014 since quarterly monitoring began in 2012 as part of the NFFMP (OEH 2017). This influx mainly comprised the highly nomadic LRFF. Camp numbers are usually below 20,000 with some influxes over 100,000 flying-foxes in 2014 and 2016. As this site is a maternity camp, it is likely that some individuals have a strong site fidelity, returning year after year as part of seasonal migrations.

The GHFF population will generally move south within their range (see Appendix 2) in spring and summer, then return to the coastal forests of north-east NSW and south-east Queensland in winter (Ratcliffe 1932; Eby 1991; Parry-Jones & Augee 1992; Roberts et al. 2012). In autumn they occupy primarily coastal lowland camps. LRFF travel south within their range (see Appendix 2) to visit the coastal areas of south-east Queensland and NSW during the summer months.

The LRFF has the most nomadic distribution, strongly influenced by availability of food resources (predominantly the flowering of eucalypt species) (Churchill 2008), which means the duration of their stay in any one place is generally very short. LRFF travel south to visit the coastal areas of south-east Queensland and NSW during the summer months. Flying-foxes also occupy Brombin camp and two camps in Kendall: Bakers Creek and Logans crossing.

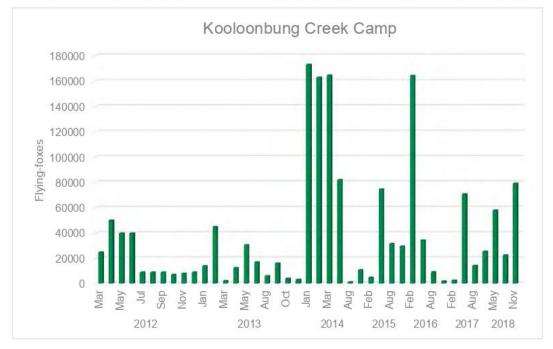


Figure 15 Numbers of flying-foxes at Kooloonbung Creek camp (Source: OEH 2017; PMHC 2018). Count methods are consistent with those described in Westcott et al. 2011.

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2.5 Sensitive receptors

Sensitive receptors are those locations that host vulnerable people where risks need to be managed. Sensitive receptors (i.e. schools, hospitals with helipads, airports, equine facilities) within 2 km of the maximum camp extent and within 13 km for aerodromes are provided in Table 3 and Figure 16. Identifying sensitive receptors is necessary with regards to any management actions that could inadvertently cause the camp to splinter to undesirable or sensitive locations surrounding the camp.

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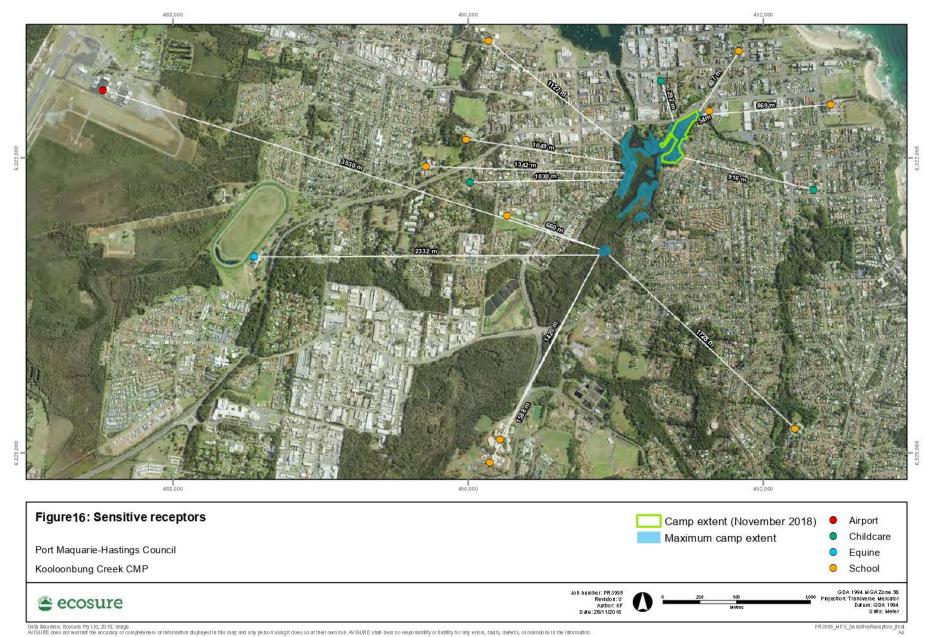
Sensitive receptor	Distance from maximum camp extent (m)
Port Macquarie community preschool	54
Hastings family day care	282
Port Macquarie public school	487
St Agnes primary school	660
Hastings Secondary College	869
Hastings preschool and long day care	1030
Hastings Secondary College Westport campus	1049
Portside preschool and long day care	1057
St Josephs primary school	1127
Westport public school	1342
MacKillop College	1420
St Peters primary school	1588
Hastings public school	1728
Port Macquarie race club	2332
Port Macquarie airport	3530

Table 3 Sensitive receptors surrounding the camp

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2.6 Management to date

A number of plans have been developed for the park:

- Catchment Management Plan 1992
- Reserve Management Plan 1996
- Kooloonbung Creek Nature Park Plan of Management (KCPoM) (Port-Macquarie-Hastings Council 2012).

During the development of the 2012 KCPoM, Dr Peggy Eby provided a presentation on bat ecology to residents at a community forum. Council also provided an on-line survey and subsequent issues paper (PVHC 2012). The KCPoM produced a number of management actions to address flying-fox issues. Table 4 provides a review on the progress of actions outlined in the KCPoM for consideration within this Plan.

Bush fire management and weed removal also have consequences for roost occupation patterns, and vegetation removal may exacerbate the effects of Heat Stress Events (HSE). Friends of Kooloonbung (FOK) have been undertaking bush regeneration (Figure 17) in the Park since 1985, improving public access, facilities and providing interpretational signage. Although FOK signage includes a flying-fox logo/illustration, no signs with flying-fox ecology information were observed in the Park.

The Inaugural Committee Meeting for the Flying-fox Camp Management Plan was held on Tuesday 4th September 2018 opened by Mayor Peta Pinson, with attendance by the Community Consultative Committee and Ecosure. Residents were provided legislative background on flying-fox management and information regarding the ecological importance and behaviour of flying-foxes.

When Council commissioned the development of this Plan, two further community consultative meetings were scheduled. Outcomes of these meetings held on Wednesday 14 November 2018 and 12 December 2018 are provided in Section 3 Community Engagement.



Table 4 Progress of 2012 actions for flying-fox issues. Adapted from Table 2 and Appendix A KCPoM (PMHC 2012)

Management action	Actions to be taken	Progress	Ecosure review
Undertake monthly flying-fox habitat mapping and broad population size estimations	Fluctuations in population and roosting area monitored; roost boundaries adjacent to residents identified. Mapping uploaded to Council Website for Community access	Commenced: Council undertakes quarterly monitoring of camp and produces. The public is able to access this information on the interactive flying fox mapping tool on the Department of Environment and Energy's website. http://www.environment.gov.au/webgis- framework/apps/ffc-wide/ffc-wide.jsf	Highly informative and a beneficial action to continue. Enables temporal and spatial analysis for this Plan and future management.
Community Health Risk	Annual inspection of advisory signage. Annual check on recorded Lyssavirus. Hendra Virus, Menangle Virus and Leptospirosis infections	These actions have not been initiated	Educational messaging including prevention of risk is better than the collation of incident data alone. Residents should be informed that disease transmission is extremely low and vaccines are available for post exposure.
Community and Council monitoring of the Flying- fox population	Develop a web based community monitoring system and movements of flying foxes in the Park and to record complaints.	Commenced: Council undertake the census which records movements of the camp. Council website has not been updated with this information.	Update Council's webpage with quarterly monitoring of camp extent. Recording and promoting positive stories of rehabilitation, educational visits and tours or observations of fly outs (not just complaints).
Community education	Install signage on bats to inform park users to avoid contact and to not disturb them. Provide NSW Dept. of Health fact sheets for all Flying Fox diseases to adjacent residents.	Commenced: Some basic warning signage installed Facts sheets produced in 2013	Avoidance messaging on its own is inadequate and promotes negative attitudes and fear towards flying-foxes. Educational signage could be interwoven with indigenous values for flying-foxes. The bat logo/ illustration on Friends of Kooloonbung signage should be continued on future signage.

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Management action	Actions to be taken	Progress	Ecosure review
Vegetation removal	Glebe Close (1st priority) Removal of vegetation within 10m of the Glebe Close properties. Hollingworth Street. (2nd Priority) Vegetation removal within the Hollingworth Street road reserve between Table Street and Koorong Avenue intersections (equates to a 25 - 30m buffer from residential properties). Vegetation removal within the Hollingworth Street Road Reserve between Ackroyd and Table Street intersections to the change of grade on roadside batter (equates to a 15m wide buffer from residential boundaries). Fischer Street (3rd Priority) Removal of overhanging branches back the trunk of trees that are within 10m of the Fischer Street property boundary.	Completed: 1st Priority in Glebe Close (figure 18) 2nd Priority Hollingworth Street	Trees overhanging properties in Glebe are mapped EECs (figure 17). The previously proposed buffer distances are considered adequate to provide relief from issues such as faecal drop to improve amenity. However, canopy area inside Kooloonbung Park must be considered with regards for the available roosting area for maximum number of bats. Noise and odour may need to be addressed using other management options such as improving building insulation and using air deodorisers. Important for residents to acknowledge the mobility of camp and the temporary nature of impacts in order to build resilience within community
Desk top assessment	Additional roosting site for the flying foxes	Not yet initiated	Whilst assessing suitable nearby vegetation may be beneficial for identifying potential habitat, it can not be guaranteed that flying-fox will take up alternative sites either temporarily or permanently.
Bush regeneration/ weed control	Proposed bush regeneration works zones	Ongoing	Any habitat improvement will benefit the flying-foxes however the timing of work zones ought to consider areas occupied by bats especially during breeding and summer months. Need to coordinate with Council mapping that shows seasonally occupied roost space.
			Do volunteers know what to do if they see a sick or injured bat?

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Figure 17 Bush restoration works in the understory of the Park



Figure 19 Asset protection zone behind properties in Glebe Close



Figure 18 Vegetation mapped as EEC overhanging behind Glebe Close properties

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3 Community engagement

3.1 Stakeholders

There are a range of stakeholders directly or indirectly affected by flying-foxes, or who are interested in management of the camp. Stakeholders include those shown in Table 5.

Stakeholder group	Stakeholder	Interest/reported impacts
Community	Residents and business owners	The location of the camp within a residential area means there are many residents affected by amenity issues (e.g. noise, smell, faecal drop), including residents of:
		Glebe Close
		Gordon Street
		Hollingworth St
		Lake Road
		Anita Crescent
		Lake Road
	Conservation groups	Friends of Kooloonbung have been undertaking bush regeneration in the Park since 1985
	Indigenous community	Traditional owners have a general interest in flying-foxes, including the ecological services they provide and the potential for sustainable harvesting for food or medicinal purposes.
Industry	Horse owners and managers	Horse owners, equine facility managers and local vets should be aware that Hendra virus risk is associated with foraging flying-foxes (e.g. risk is present across the entire flying-fox range), and appropriate mitigation measures.
	Orchardists and fruit growers	Fruit growers may be impacted by flying-foxes raiding orchards and should have access to wildlife friendly netting information.
	Port Macquarie Airport	Airport managers have a responsibility to reduce the risk of wildlife-aircraft strike. Port Macquarie Airport is located 3.5 km to the west of Kooloonbung camp and should be consulted regarding any management that may influence flying-fox movements or behaviour.
Government	Port Macquarie-Hastings Council	Council is responsible for administering local laws, plans and policies, and appropriately managing assets (including land) for which it is responsible.
	OEH	OEH is responsible for administering state legislation relating to (among other matters) the conservation and management of native plants and animals, including threatened species and ecological communities.
	Commonwealth Department of the Environment and Energy (DoEE)	DoEE is responsible for administering federal legislation relating to matters of national environmental significance, such as the grey-headed flying-fox which roosts at Kooloonbung Creek camp.
	Local Government NSW (LGNSW)	LGNSW is an industry association that represents the interests of councils in NSW. LGNSW also administered funds under the NSW Flying-fox Grants Program.
Non- government organisations	Wildlife carers and conservation organisations	Wildlife carers and conservation organisations have an interest in flying-fox welfare and conservation of flying-foxes and their habitat.
	Researchers/universities/CSIRO	Researchers have an interest in flying-fox behaviour, biology and conservation.

Table 5 Stakeholders of Kooloonbung Creek camp

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3.2 Engagement methods and results

Extensive effort has been made to engage with the community in the development of this plan. The aim of the engagement was to:

- seek feedback from the community concerning the impact of flying-foxes and advice on potential mitigation options
- understand the impacts, positive and negative, directly and indirectly affecting the community
- · share information and seek ideas about possible future management options
- correct misinformation and alleviate fears regarding health risks to people and domestic animals (see Appendix 4 for information about human and animal health)
- raise awareness about the ecological importance of flying-foxes.

During development of the Plan, three methods will be used to engage with the Port Macquarie community; an online survey, community consultation meetings and public exhibition of the draft Plan (Table 6).

Engagement method	Key dates	Outcomes
Inaugural Committee Meeting	4 September 2018	21 attendees. Minutes of Flying-fox Camp Management Plan 2018 Inaugural Committee Meeting available from Council.
Community consultation meeting #2	14 November 2018	23 attendees provided feedback to Ecosure on their preferred management options for inclusion in the Plan.
Online survey	20-30 November 2018	Online survey results will be included in section 3.2.2
Community consultation meeting #3	12 December 2018	Outcomes of meeting included in section 3.2.1
Public exhibition of draft Plan	5 December 2018 – 11 February	Submissions received by Council on the draft Plan incorporated into the final plan
Community consultation meeting #4	27 February 2018	ТВА

Table 6 Community engagement methods

3.2.1 Community consultation meeting

Concerns reported by the Port Macquarie-Hastings community include:

- · fear of disease transfer to humans and domestic animals
- excessive noise, particularly depriving sleep and contributing to other health issues, and reduced amenity
- · odour entering homes and reduced lifestyle amenity
- · faecal drop on vehicles and outdoor areas
- damage to vegetation.

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Impacts reported by residents are confined to the northern extent of Kooloonbung Creek Nature Park, particularly behind properties on Glebe Close, the Glebe Terraces Villas and Grove Terraces and the northern most end of Lake Road (Figure 20).

Attendees at the consultation meeting on November 14, 2018 provided feedback to Ecosure on which camp management actions (Section 4) to prioritise or disregard. Attendees were also invited to suggest how or where best to implement those actions. Comments in 'Actions to Priortise', Actions to Disregard' and 'How and Where to Implement' have come directly from the consultation committee (Appendix 7). Actions were attributed to a corresponding level of management outlined in Section 4.

Meeting 2 revealed that the committee preferred, or were interested in learning more about, the following management actions:

- · education and awareness programs specifically:
 - removal of non-natives
 - information on flying-fox diseases
 - advice on tree trimming
 - banning barbed wire in residential areas
 - promoting wildlife friendly netting.
- property acquisition
- · creating alternative habitat or roost site
- · subsidies for water and power use
- · buffers through tree trimming or removal
- canopy-mounted sprinklers.

Concerns regarding flying-fox disease need to be addressed as part of an educational strategy. One resident at the meeting enquired about bat dander. Dander is an airborne allergen associated with the shedding of fur, hair or feathers (ALA 2017). The two flying-fox diseases; Australian Bat Lyssavirus and Hendra Virus (Appendix 4) cannot be transmitted through air-borne particles. Suspected allergies to flying-fox dander should be treated the same way as reactions to other general allergens such as dust and pollen. It is also possible that allergies perceived to be associated with flying-foxes may be due to coinciding increases in flowering (and pollen) that attracted an increased number of flying-foxes.

Although some committee members indicated that signage had already been installed and was an action that could be disregarded, the current signage only states, 'do not disturb' and does not improve knowledge or awareness of flying-fox ecology or conservation.

Some attendees had concern for removal of vegetation and making sure that the Park's other values such as koala movement was not impeded through actions such as noise attenuation fencing. Endangered ecological communities mapped to the border of some property boundaries exclude the option of tree removal within asset protection zones according the

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Council Development Control Plan (Appendix 1).

The committee is in favour of subsidies to assist with the costs associated with water for cleaning and high electricity use from air-conditioners due to not being able to open windows when the camp is emitting odour. A distance-based subsidy program could be created for residents to apply for financial assistance or professional advice regarding building insulation or double-glazed windows.

The third community consultative meeting discussed a dispersal scenario for the camp with reference to other Councils' dispersal results between 1990-2013 (Roberts and Eby 2013). Matters for consideration included financial cost, duration and resources required, likelihood of long-term success and, consequences of moving the camp to unknown or more problematic locations.

In 2013, Roberts and Eby summarised dispersals between 1990 and 2013:

- In all cases, dispersed animals did not abandon the local area.
- In 16 of the 17 cases, dispersals did not reduce the number of flying-foxes in the local area.
- Dispersed animals did not move far (63% of the time within 600m).
- In 85% of cases, new camps established nearby.
- In all cases it was not possible to predict where replacement camps would form.
- Conflict was often not resolved. In 71% of cases conflict still being reported either at the original site or within the local area years after the initial dispersal actions.
- Repeat dispersal was always required except where extensive vegetation removal occurred.

3.2.2 Survey responses

The community was invited to participate in an online survey linked to Council's website. The survey period was open for two weeks and contained 21 questions that comprised:

- flying-fox awareness (7 questions)
- flying-fox interaction (3 questions)
- flying-fox management (8 questions)
- respondent demographic (2 questions)
- open comment (1).

Results from the community survey are provided in Appendix 5.

A total of 43 survey respondents answered all or some of the questions. Some questions were optional while others requested more details depending upon the response, e.g. if a respondent answered 'no' to 'is the protection of flying-foxes important to you as a community member?' they were then prompted to respond to questions about why they had responded



in this way i.e. why they did not consider the protection of flying-foxes important. There were also opportunities for respondents to provide additional information if their points of view were not included in the available options.

3.2.3 Public submissions on draft Plan

The public submission period for the draft Plan is 5 December 2018 – 11 February 2019.

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4 Camp management options

Below is an overview of management options commonly used throughout NSW and Australia which were considered in the development of the Plan. These are categorised as Level 1, 2 or 3 in accordance with the Policy.

4.1 Level 1 actions: routine camp management

4.1.1 Education and awareness programs

This management option involves undertaking a comprehensive and targeted flying-fox education and awareness program to provide accurate information to the local community about flying-foxes.

Such a program would include information about managing risk and alleviating concern about health and safety issues associated with flying-foxes, options available to reduce impacts from roosting and foraging flying-foxes, an up-to-date program of works being undertaken at the camp, and information about flying-fox numbers and flying-fox behaviour at the camp.

Residents should also be made aware that faecal drop and noise at night is mainly associated with plants that provide food, independent of camp location. Staged removal of foraging species such as fruit trees and palms from residential yards, or management of fruit (e.g. bagging, pruning) will greatly assist in mitigating this issue.

Collecting and providing information should always be the first response to community concerns in an attempt to alleviate issues without the need to actively manage flying-foxes or their habitat. Where it is determined that management is required, education should similarly be a key component of any approach.

The likelihood of improving community understanding of flying-fox issues is high. However, the extent to which that understanding will help alleviate conflict issues is probably less so. Extensive education for decision-makers, the media and the broader community may be required to overcome negative attitudes towards flying-foxes.

It should be stressed that a long-term solution to the issue resides with better understanding flying-fox ecology and applying that understanding to careful urban planning and development.

An education program may include components shown in Figure 21.



Figure 21 Possible components of an education program

4.1.2 Property modification without subsidies

The managers of land on which a flying-fox camp is located would promote or encourage the adoption of certain actions on properties adjacent to or near the camp to minimise impacts from roosting and foraging flying-foxes:

- Create visual/sound/smell barriers with fencing or hedges. To avoid attracting flyingfoxes, species selected for hedging should not produce edible fruit or nectar-exuding flowers, should grow in dense formation between two and five metres (Roberts 2006) (or be maintained at less than 5 metres). Vegetation that produces fragrant flowers can assist in masking camp odour where this is of concern.
- Manage foraging trees (i.e. plants that produce fruit/nectar-exuding flowers) within
 properties through pruning/covering with bags or wildlife friendly netting, early
 removal of fruit, or tree replacement.
- Cover vehicles, structures and clothes lines where faecal contamination is an issue, or remove washing from the line before dawn/dusk.
- Move or cover eating areas (e.g. BBQs and tables) within close proximity to a camp or foraging tree to avoid contamination by flying-foxes.

- Install double-glazed windows, insulation and use air-conditioners when needed to reduce noise disturbance and smell associated with a nearby camp.
- Follow horse husbandry and property management guidelines provided at the NSW Department of Primary Industries Hendra virus web page (DPI 2015a).
- Include suitable buffers and other provisions (e.g. covered car parks) in planning of new developments.
- Turn off lighting at night which may assist flying-fox navigation and increase fly-over impacts.
- Consider removable covers for swimming pools and ensure working filter and regular chlorine treatment.
- Appropriately manage rainwater tanks, including installing first-flush systems.
- Avoid disturbing flying-foxes during the day as this will increase camp noise.

The cost would be borne by the person or organisation who modifies the property; however, opportunities for funding assistance (e.g. environment grants) may be available for management activities that reduce the need to actively manage a camp.

4.1.3 Property modification subsidies

Fully funding or providing subsidies to property owners for property modifications may be considered to manage the impacts of the flying-foxes. Providing subsidies to install infrastructure may improve the value of the property, which may also offset concerns regarding perceived or actual property value or rental return losses.

The level and type of subsidy would need to be agreed to by the entity responsible for managing the flying-fox camp.

4.1.4 Service subsidies

This management option involves providing property owners with a subsidy to help manage impacts on the property and lifestyle of residents. The types of services that could be subsidised include clothes washing, cleaning outside areas and property, car washing or power bills. Rate reductions could also be considered.

Critical thresholds of flying-fox numbers at a camp and distance to a camp may be used to determine when subsidies would apply.

4.1.5 Routine camp maintenance and operational activities

Examples of routine camp management actions (permissible subject to animal welfare measures listed in Appendix 6) are provided in the Policy. These include:

 removal of tree limbs or whole trees that pose a genuine health and safety risk, as determined by a qualified arborist

- weed removal, including removal of noxious weeds under the *Noxious Weeds Act 1993*, or species listed as undesirable by a council
- · trimming of understorey vegetation
- · the planting of vegetation
- · minor habitat augmentation for the benefit of the roosting animals
- mowing of grass and similar grounds-keeping actions that will not create a major disturbance to roosting flying-foxes
- application of mulch or
- · removal of leaf litter or other material on the ground.

Protocols should be developed for carrying out operations that may disturb flying-foxes, which can result in excess camp noise. Such protocols could include limiting the use of disturbing activities to certain days or certain times of day in the areas adjacent to the camp and advising adjacent residents of activity days. Such activities could include lawn-mowing, using chainsaws, whipper-snippers, using generators and testing alarms or sirens.

4.1.6 Revegetation and land management to create alternative habitat

This management option involves revegetating and managing land to create alternative flyingfox roosting habitat through improving and extending existing low-conflict camps or developing new roosting habitat in areas away from human settlement.

Selecting new sites and attempting to attract flying-foxes to them has had limited success in the past, and ideally habitat at known camp sites would be dedicated as a flying-fox reserve. However, if a staged and long-term approach is used to make unsuitable current camps less attractive, whilst concurrently improving appropriate sites, it is a viable option (particularly for the transient and less selective LRFF). Supporting further research into flying-fox camp preferences may improve the potential to create new flying-fox habitat.

When improving a site for a designated flying-fox camp, preferred habitat characteristics detailed in Appendix 3 Alternative camp site investigation report Section 1.3 Roosting preferences should be considered.

Foraging trees planted amongst and surrounding roost trees (excluding in/near horse paddocks) may help to attract flying-foxes to a desired site. They will also assist with reducing foraging impacts in residential areas. Consideration should be given to tree species that will provide year-round food, increasing the attractiveness of the designated site. Depending on the site, the potential negative impacts to a natural area will need to be considered if introducing non-indigenous plant species.

The presence of a water source is likely to increase the attractiveness of an alternative camp location. Supply of an artificial water source should be considered if unavailable naturally, however this may be cost-prohibitive.

Potential habitat mapping using camp preferences and suitable land tenure can assist in initial

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alternative site selection. A feasibility study would then be required prior to site designation to assess likelihood of success and determine the warranted level of resource allocated to habitat improvement.

4.1.7 Provision of artificial roosting habitat

This management option involves constructing artificial structures to augment roosting habitat in current camp sites or to provide new roosting habitat. Trials using suspended ropes have been of limited success as flying-foxes only used the structures that were very close to the available natural roosting habitat. It is thought that the structure of the vegetation below and around the ropes is important.

4.1.8 Protocols to manage incidents

This management option involves implementing protocols for managing incidents or situations specific to particular camps. Such protocols may include monitoring at sites within the vicinity of aged care or child care facilities, management of compatible uses such as dog walking or sites susceptible to heat stress incidents (when the camp is subjected to extremely high temperatures leading to flying-foxes changing their behaviour and/or dying).

4.1.9 Participation in research

This management option involves participating in research to improve knowledge of flying-fox ecology to address the large gaps in our knowledge about flying-fox habits and behaviours and why they choose certain sites for roosting. Further research and knowledge sharing at local, regional and national levels will enhance our understanding and management of flying-fox camps.

4.1.10 Appropriate land-use planning

Land-use planning instruments may be able to be used to ensure adequate distances are maintained between future residential developments and existing or historical flying-fox camps. While this management option will not assist in the resolution of existing land-use conflict, it may prevent issues for future residents.

4.1.11 Property acquisition

Property acquisition may be considered if negative impacts cannot be sufficiently mitigated using other measures. This option will clearly be extremely expensive, however is likely to be more effective than dispersal and in the long-term may be less costly.

4.1.12 Do nothing

The management option to 'do nothing' involves not undertaking any management actions in relation to the flying-fox camp and leaving the situation and site in its current state.

4.2 Level 2 actions: in-situ management

4.2.1 Buffers

Buffers can be created through vegetation removal and/or the installation of permanent/semipermanent deterrents.

Creating buffers may involve planting low-growing or spiky plants between residents or other conflict areas and the flying-fox camp. Such plantings can create a visual buffer between the camp and residences or make areas of the camp inaccessible to humans.

The Kooloonbung Creek camp exists in relatively narrow strips of vegetation around a treeless wetland and therefore it is necessary to devise a suitable buffer distance that provides relief for residents and avoids forcing flying-foxes into backyards or splintering the camp to other problematic locations, and maintains the ecological and amenity values of the vegetation. This requires consideration of the approximate total area of the camp, the area that would need to be modified/removed to create a suitable buffer, and whether there is an equivalent replacement area available in an appropriate location for the displaced flying-foxes.

Previous studies have recommended that vegetation buffers consisting of habitat not used by flying-foxes, should be 300 m or as wide as the site allows to mitigate amenity impacts for a community (SEQ Catchments 2012). Buffers need to take into consideration the variability of use of a camp site by flying-foxes within and across years, including large, seasonal influxes of flying-foxes. The usefulness of a buffer declines if the flying-fox camp is within 50 m of human habitation (section 4.2.1).

Buffers through vegetation removal

Vegetation removal aims to alter the area of the buffer habitat sufficiently so that it is no longer suitable as a camp. The amount required to be removed varies between sites and camps, ranging from some weed removal to removal of most of the canopy vegetation.

Any vegetation removal should be done using a staged approach, with the aim of removing as little native vegetation as possible. This is of particular importance at sites with other values (e.g. ecological or amenity), and in some instances the removal of any native vegetation will not be appropriate. Thorough site assessment will inform whether vegetation management is suitable (e.g. can impacts to other wildlife and/or the community be avoided?).

Removing vegetation can also increase visibility into the camp and noise issues for neighbouring residents which may create further conflict.

Suitable experts should be consulted to assist selective vegetation trimming/removal to minimise vegetation loss and associated impacts.

The importance of under- and mid-storey vegetation in the buffer area for flying-foxes during heat stress events also requires consideration.

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Buffers without vegetation removal

Permanent or semi-permanent deterrents can be used to make buffer areas unattractive to flying-foxes for roosting, without the need for vegetation removal. This is often an attractive option where vegetation has high ecological or amenity value.

While many deterrents have been trialled in the past with limited success, there are some options worthy of further investigation:

Visual deterrents – Visual deterrents such as plastic bags, fluoro vests (GeoLINK 2012) and balloons (Ecosure, pers. comm.) in roost trees have shown to have localised effects, with flying-foxes deterred from roosting within 1–10 metres of the deterrents. The type and placement of visual deterrents would need to be varied regularly to avoid habituation. Potential for litter pollution should be considered and managed when selecting the type and placement of visual deterrents. In the absence of effective maintenance, this option could potentially lead to an increase in rubbish in the natural environment.

- Noise emitters on timers Noise needs to be random, varied and unexpected to avoid flying-foxes habituating. As such these emitters would need to be portable, on varying timers and a diverse array of noises would be required. It is likely to require some level of additional disturbance to maintain its effectiveness, and ways to avoid disturbing flying-foxes from desirable areas would need to be identified. This is also likely to be disruptive to nearby residents.
- Smell deterrents For example, bagged python excrement hung in trees has
 previously had a localised effect (GeoLINK 2012). The smell of certain deterrents
 may also impact nearby residents, and there is potential for flying-foxes to habituate.
- Canopy-mounted water sprinklers This method has been effective in deterring flying-foxes during dispersals (Ecosure personal experience), and current trials in Queensland are showing promise for keeping flying-foxes out of designated buffer zones. This option can be logistically difficult (installation and water sourcing) and may be cost-prohibitive. Design and use of sprinklers need to be considerate of animal welfare and features of the site. For example, misting may increase humidity and exacerbate heat stress events, and overuse may impact other environmental values of the site.

Note that any deterrent with a high risk of causing inadvertent dispersal may be considered a Level 3 action.

4.2.2 Noise attenuation fencing

Noise attenuation fencing could be installed in areas where the camp is particularly close to residents. This may also assist with odour reduction, and perspex fencing could be investigated to assist fence amenity. Although expensive to install, this option could negate the need for habitat modification, maintaining the ecological values of the site, and may be more cost-effective than ongoing management.

4.3 Level 3 actions: disturbance or dispersal

4.3.1 Nudging

Noise and other low intensity active disturbance restricted to certain areas of the camp can be used to encourage flying-foxes away from high conflict areas. This technique aims to actively 'nudge' flying-foxes from one area to another, while allowing them to remain at the camp site.

Unless the area of the camp is very large, nudging should not be done early in the morning as this may lead to inadvertent dispersal of flying-foxes from the entire camp site. Disturbance during the day should be limited in frequency and duration (e.g. up to four times per day for up to 10 minutes each) to avoid welfare impacts. As with dispersal, it is also critical to avoid periods when dependent young are present (as identified by a flying-fox expert).

4.3.2 Dispersal

Dispersal aims to encourage a camp to move to another location, through either disturbance or habitat modification.

There is a range of potential risks, costs and legal implications that are greatly increased with dispersal (compared with in-situ management as above). See Appendix 6 for more details. These include:

- impact on animal welfare and flying-fox conservation
- · splintering the camp into other locations that are equally or more problematic
- · shifting the issue to another area
- impact on habitat value
- effects on the flying-fox population, including disease status and associated public health risk
- impacts to nearby residents associated with ongoing dispersal attempts
- · excessive initial and/or ongoing capacity and financial investment
- negative public perception and backlash
- · increased aircraft strike risk associated with changed flying-fox movement patterns
- unsuccessful management requiring multiple attempts, which may exacerbate all of the above.

Despite these risks, there are some situations where camp dispersal may be considered. Dispersal can broadly be categorised as 'passive' or 'active' as detailed below.

Passive dispersal

Removing vegetation in a staged manner can be used to passively disperse a camp, by gradually making the habitat unattractive so that flying-foxes will disperse of their own accord over time with little stress (rather than being more forcefully moved with noise, smoke, etc.).

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This is less stressful to flying-foxes, and greatly reduces the risk of splinter colonies forming in other locations (as flying-foxes are more likely to move to other known sites within their camp network when not being forced to move immediately, as in active dispersal).

Generally, a significant proportion of vegetation needs to be removed in order to achieve dispersal of flying-foxes from a camp or to prevent camp re-establishment. For example, flying-foxes abandoned a camp in Bundall, Queensland once 70% of the canopy/mid-storey and 90% of the understorey had been removed (Ecosure 2011). Ongoing maintenance of the site is required to prevent vegetation structure returning to levels favourable for colonisation by flying-foxes. Importantly, at nationally important camps (Appendix 1) sufficient vegetation must be retained to accommodate the maximum number of flying-foxes recorded at the site.

This option may be preferable in situations where the vegetation is of relatively low ecological and amenity value, and alternative known permanent camps are located nearby with capacity to absorb the additional flying-foxes. While the likelihood of splinter colonies forming is lower than with active dispersal, if they do form following vegetation modification there will no longer be an option to encourage flying-foxes back to the original site. This must be carefully considered before modifying habitat.

There is also potential to make a camp site unattractive by removing access to water sources. However, at the time of writing this method had not been trialled so the likelihood of this causing a camp to be abandoned is unknown. It would also likely only be effective where there are no alternative water sources in the vicinity of the camp.

Active dispersal through disturbance

Dispersal is more effective when a wide range of tools are used on a randomised schedule with animals less likely to habituate (Ecosure pers. obs. 1997–2015). Each dispersal team member should have at least one visual and one aural tool that can be used at different locations on different days (and preferably swapped regularly for alternate tools). Exact location of these and positioning of personnel will need to be determined on a daily basis in response to flying-fox movement and behaviour, as well as prevailing weather conditions (e.g. wind direction for smoke drums).

Active dispersal will be disruptive for nearby residents given the timing and nature of activities, and this needs to be considered during planning and community consultation.

This method does not explicitly use habitat modification as a means to disperse the camp, however if dispersal is successful, some level of habitat modification should be considered. This will reduce the likelihood of flying-foxes attempting to re-establish the camp and the need for follow-up dispersal as a result. Ecological and aesthetic values will need to be considered for the site, with options for modifying habitat the same as those detailed for buffers above.

Early dispersal before a camp is established at a new location

This management option involves monitoring local vegetation for signs of flying-foxes roosting in the daylight hours and then undertaking active or passive dispersal options to discourage the animals from establishing a new camp. Even though there may only be a few animals

initially using the site, this option is still treated as a dispersal activity, however it may be simpler to achieve dispersal at these new sites than it would in an established camp. It may also avoid considerable issues and management effort required should the camp be allowed to establish in an inappropriate location.

It is important that flying-foxes feeding overnight in vegetation are not mistaken for animals establishing a camp.

Maintenance dispersal

Maintenance dispersal refers to active disturbance following a successful dispersal to prevent the camp from re-establishing. It differs from initial dispersal by aiming to discourage occasional over-flying individuals from returning, rather than attempting to actively disperse animals that have been recently roosting at the site. As such, maintenance dispersal may have fewer timing restrictions than initial dispersal, provided that appropriate mitigation measures are in place.

4.4 Unlawful activities

4.4.1 Culling

Culling is addressed here as it is often raised by community members as a preferred management method; however, culling is contrary to the object of the *Biodiversity Conservation Act* and will not be permitted as a method to manage flying-fox camps.

Culling was used in the early 1800's and into the 1920s.



4.5 Camp management options analysis

Table 7 provides an analysis of the camp management options described in Section 4 and their suitability for implementation at Kooloonbung Creek Camp. An appraisal is provided for the options to be either adopted, investigated further or disregarded within this plan.

Management options	Relevant impacts	Cost \$-\$\$\$ Low-high	Advantages	Disadvantages	Suitability for site
Level 1 options					
Education and awareness programs	Fear of disease Noise Smell Faecal drop	\$		mitigate all issues, and on its own would not be acceptable to the	There appears to be some fears and concerns regarding disease risk that needs addressing. Ongoing education is a good way to manage expectations around achievable flying-fox management actions
Property modification / service subsidies	Noise Smell Faecal drop Health/wellbeing Property devaluation Lost rental return	\$-\$\$		landholders, however subsidies would assist.	Appraisal: Adopt Property modification can assist in reducing all amenity concerns, although may be cost prohibitive for residents – subsidies would assist. Appraisal: Adopt distance-based subsidies program.

Table 7 Camp management options analysis

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Management options	Relevant impacts	Cost \$-\$\$\$ Low-high	Advantages	Disadvantages	Suitability for site
			Council could provide car covers, clothesline covers, free hire of pressure cleaners or consider rate reductions to assist with faecal drop impacts.		
Odour reducing / masking plants	Noise Smell Health/wellbeing Property devaluation	\$		the desired effect. May not work effectively for residents located immediately next to the camp but	Residents could be encouraged to modify properties by planting dense screens and fragrant plants. If paired in conjunction with cleared buffers, could replace gaps with low shrubs to mitigate visual or odour impacts from camp. Appraisal: Adopt
Routine camp management	Health/well-being	\$	Weed removal has the potential to reduce roost availability and reduce numbers of roosting FFs.		Friends of Kooloonbung and Council regularly undertake bush regeneration and maintenance in the Park Appraisal: Adopt (ongoing)
Alternative habitat creation	Noise Smell Faecal drop Health/wellbeing Property devaluation Lost rental return	\$\$-\$\$\$	low conflict areas will mitigate all impacts	approach so cannot be undertaken quickly, previous attempts to attract FFs to a new site have not been known to succeed.	Undertake a desktop investigation into nearby suitable flying-fox habitat and potential alternative camp sites. A risk assessment of the alternative sites against the current site would help in determining feasibility of moving the camp. Appraisal: Adopt

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Management options	Relevant impacts	Cost \$-\$\$\$ Low-high	Advantages	Disadvantages	Suitability for site
Provision of artificial roosting habitat	Noise Smell Faecal drop Health/wellbeing Property devaluation Lost rental return	\$-\$\$		would use artificial habitat, but	Not enough evidence to explore this as a viable option to support habitat restoration. Appraisal: Disregard
Protocols to manage incidents	Health/wellbeing Fear of disease	\$	Low cost, will reduce actual risk of negative human/pet-FF interactions, promotes conservation of FFs, can be undertaken quickly.	but will reduce fear of disease.	Previous management actions (Table 4) recommended annual check on recorded Lyssavirus. Hendra Virus, Menangle Virus and Leptospirosis infections. Need to develop understanding of records or incidents and whether protocols are needed to reduce exposure to risk such as for staff, subcontractors or volunteers working in the Park.
Research	Noise Smell Faecal drop Health/wellbeing Property devaluation Lost rental return	\$			Appraisal: Investigate further Not considered an urgent action at this site. Council will endeavour to stay up to date with contemporary research as it arises. Appraisal: Investigate further

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Management options	Relevant impacts	Cost \$-\$\$\$ Low-high	Advantages	Disadvantages	Suitability for site
Appropriate land- use planning	Noise Smell Faecal drop Health/wellbeing Property devaluation Lost rental return	\$	Suitable planning for future development will reduce potential for future conflict. Identification of degraded sites that may be suitable for long-term rehabilitation for FFs could reduce impacts.	impacts.	Council may consider including additional management buffer zones within their codes in future planning scheme updates where properly documented and justified. Appraisal: Investigate further
Property acquisition	All for specific property owners Nil for broader community	\$\$\$	Option to lease properties back to community	Extremely expensive	Some consultation committee members wanted this option considered. Appraisal: Investigate further
Do nothing	Nil	Nil	No resource expenditure.	Will not mitigate impacts and would not be considered acceptable by impacted members of the community.	Not appropriate. Appraisal: Disregard

Level 2 options

Buffers through vegetation removal	Noise Smell Health/wellbeing	\$-\$\$	using a staged approach, with the aim of removing as little native vegetation as possible and only in vegetation directly affecting residents subject to a Vegetation Management Plan.	increase visibility into the camp and noise issues for neighbouring residents which may create further conflict. No certainty that removal of vegetation will improve condition for impacted residents. Vegetation removed too quickly could cause	Development Assessment Report in accordance with a Biodiversity Assessment Method. Further assessment
Buffers without vegetation removal – canopy-mounted	Noise Smell Health/wellbeing	\$\$	method has been effective in deterring	difficult regarding installation and water sourcing. Could alter design	Appraisal: Investigate further Some residents were in favour of this technique. TS licence require on private property or assessed under Part 5 EP&A Act on public land

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Management options	Relevant impacts	Cost \$-\$\$\$ Low-high	Advantages	Disadvantages	Suitability for site
sprinklers	Damage to vegetation or habitat			require guidelines regarding frequency and duration of use to mitigate exacerbating HSE effects.	Appraisal: Investigate further for boundary between Lot 26/DP249270 and 3/SP70635 and 3/SP73183
Noise attenuation fencing	Noise Smell Health/wellbeing Property devaluation Lost rental return/income	\$\$	Standard noise attenuation fencing is intended to alleviate amenity issues for residents. Advice from an acoustic consultant may provide site-specific alternatives (see Section 9).	and can be considered unsightly for property fencing.	
Level 3 options	1	1	1		·
Nudging All		\$\$-\$\$\$	Can encourage flying-foxes to shift away from high conflict areas next to residential areas.		Level 2 management actions that have been proposed will serve to nudge flying- foxes away from high impact areas. Active nudging will only be considered if Level 2 actions are deemed to have been unsuccessful for the community.
					Appraisal: Investigate further
Active dispersal	All at that site but not generally appropriate for amenity impacts only	\$\$\$	If successful can mitigate all impacts at that site.		This option is not considered appropriate at this site. Appraisal: Disregard

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Man opti	nagement ons	Relevant impacts	Cost \$-\$\$\$ Low-high	Advantages	Disadvantages	Suitability for site
					permanently splintered to numerous sites as a result of dispersal in Maclean, including remaining at the original site).	

If flying-foxes are being unlawfully and intentionally disturbed, this should be reported to NSW Office of Environment and Heritage's Environment Line by calling 131 555. Native animals are protected under the BC Act 2016 and EPBC Act and there are penalties for people found guilty of an offence. Offences are dealt with before the Local Court or before the Land and Environment Court. The RSPCA (NSW) is legally able to prosecute infringements that directly contradict the *Prevention to Cruelty Act 1979*.

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5 Planned management approach

Council will take a risk-based approach to management based on:

- · potential health, safety, wellbeing and economic implications for the community
- likelihood of management success
- · potential flying-fox welfare and conservation impacts
- cost of management, and who would contribute to these costs
- risk of splintering the camp to other locations that are equally or more problematic.

Management options provided herein are intended primarily to provide relief for residents living in close proximity to the camp. Management options have been staged to prevent exacerbating issues associated with the camp whilst ensuring the welfare of flying-foxes.

Management options for adoption or further investigation are outlined in Actions in Table 8 contingent on securing funds and staff resourcing levels. Funds for actions will be sourced via grants and Council commitments.

Table 8 along with proposed timing: short-term (within 12 months), medium-term (within 2 years) or long-term (within 3-5 years). Note also that all management, and routine maintenance, must be done in accordance with mitigation measures detailed in Appendix 6.

The proposed management approach considers feedback received during community engagement, with the community indicating a preference for those below.

N.B. All management must be consistent with legislation detailed in Appendix 1, and any other applicable legislation.

5.1 Level 1 management

Level 1 management actions include:

- · education and awareness programs specifically:
 - options for property modification such as removal of non-native foraging vegetation
 - information on flying-fox diseases
 - wearing a hat when walking through the park to reduce contact with faecal drop
 - advice on tree trimming or odour masking plants on private properties
 - banning barbed wire in residential areas
 - promoting wildlife friendly netting.
- property acquisition
- · creating alternative habitat or camp site

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• subsidies for water and power use.

Misconceptions that flying-foxes are not threatened, are pests and about health risks can be reduced by adopting an education and awareness program to reduce misconceptions and appease fears. Residents are in favour of upgrading of the boardwalk to improve their enjoyment of and ability to use the park safely. This work will be undertaken as a separate item to this Plan.

A distance based subsidy program will be investigated to assist with cost associated cleaning and using air-conditioning. For instance, power rebates for residents living within 50m (Figure 22) and then within 100m. The community survey indicated a preference for driveway and roof cleaning.

Finding an alternative camp site for the flying-foxes to inhabit could be beneficial to residents living around Kooloongbung Creek. A desktop investigation of alternative flying-fox camp sites will be undertaken by Council along with a comparable risk assessment of alternative sites against Kooloonbung camp.

5.2 Level 2 management

Level 2 management actions include:

- buffers through tree trimming or removal
- · canopy-mounted sprinklers.

Level 2 buffers may consist of a combination of vegetation removal and sprinklers where tree removal is not preferred or constrained by the presence of EECs (Figure 22). Level 2 actions require OEH authorisation prior to commencing (Appendix 1). Additional approvals may also be required under other legislation (Appendix 1). An arborist should be engaged to investigate which trees are suitable for trimming or removal in line with the aforementioned provisions.

The Code of Practice Authorising Flying-fox Camp Management Actions 2018 under the Biodiversity Conservation Regulation does not exempt Council from meeting its responsibilities under the *Biodiversity Conservation Act 2016* and *Environmental Planning and Assessment Act 1979*.

Due to the presence of EECs behind the villas in Lake Road, the installation of canopymounted sprinklers in this area may trigger the need for a threatened species 'test of significance' under the *Biodiversity Conservation Act 2016*. Alternative or innovative designs for the sprinkler installation may include using poles instead of attaching to sprinklers to trees, and locating the poles on the border of Council and private property to avoid impacts to the EEC.

5.2.1 Canopy-mounted sprinklers

Canopy sprinklers have been used successfully elsewhere to deter flying-foxes from areas of

conflict. It is not the intention to disperse flying-foxes away from the camp, but maintain a buffer between residents and the flying-fox camp.

Canopy sprinklers have been effectively used at several camps in south-east Queensland, with adjacent residents given the option to activate sprinklers for short periods during the day if flying-foxes enter the buffer zone. By limiting flying-foxes in the buffer zones (the high conflict areas), there was also less disturbance of the camp, which provided the secondary benefit of reduced noise, smell, daytime fly-overs and faecal drop. Residents report a sense of regained control, which combined with the increased distance to roosting flying-foxes achieved with the sprinklers, has greatly assisted in reducing conflict with the camp. It is recommended residents near the Kooloonbung Creek camp should be able to activate sprinklers when necessary (with consideration to guidelines below).

Provided that adequate water pressure can be achieved (with a pump station), each sprinkler should have approximately a 13-15 m reach (radius). Figure 22 shows approximate locations where four sprinklers are planned for installation as soon as practicable to minimise current conflict.

Note that consultation is still required with irrigation/sprinkler specialists to confirm feasibility at this site, however based on previous Ecosure experience this option should be achievable.

Installation costs for similar programs elsewhere, including all infrastructure and eight sprinklers, were approximately \$30,000. The majority of this cost is in infrastructure (pump shed, control board, plumbing, etc.) with individual sprinklers costing less than \$1,000. As such the installation of four sprinklers, and associated infrastructure (pump, control board, etc.) will cost an estimated \$20,000 (plus maintenance and operation costs, including ~100 L water/week/sprinkler).

5.2.1.1 Installation and operational considerations

Placement - Exact placement will be dependent on finding suitable location to install poles These will be designed to withstand high wind and vegetation debris fall, and will be highly visible to flying-foxes to avoid collisions.

Water pressure – Water pressure must be firm so it is sufficient to deter flying-foxes, however must not risk injuring flying-foxes (or other fauna) or knocking an animal from the tree. Water misting should be minimised as this is unlikely to deter flying-foxes.

Noise – Sprinklers should release a jet of air prior to water, as an additional deterrent and to cue animals to move prior to water being released. The intention of the sprinklers is to make the buffer unattractive, and effectively 'train' individuals to stay out of the buffer area.

Potential for additional sprinklers – Infrastructure should accommodate additional sprinklers if possible should they be required in the future.

Residents involved in a similar approved trial elsewhere also reported noise impacts associated with the water hammer.

Access for maintenance/adjustments – Sprinklers should be designed and attached in a way that allows the easiest possible access for future maintenance, replacement and sprinkler head adjustments.

Sprinkler control – The system control station should allow independent programming of each individual sprinkler. The number of times per day each sprinkler is activated, duration of each activation and sequence of sprinkler activation needs to be fully adjustable (minutes and seconds programming required). The operational time of day also needs to be adjustable. Ideally water pressure to individual sprinklers could also be adjusted.

Sprinklers will operate on a random schedule, and in a staggered manner (i.e. not all sprinklers operating at the same time, to avoid excessive disturbance). Each activation will be for approximately 20 seconds per sprinkler. It is anticipated each sprinkler will be activated up to four times per hour between 0600 and 1700, totalling approximately 15 minutes run time per sprinkler per day. Sprinklers will not operate during fly-in or fly-out periods to avoid inadvertent dispersal.

Sprinkler settings will need to be changed regularly to avoid flying-foxes habituating, and to account for seasonal changes (e.g. not in the heat of the day during summer when they may be an attractant). Individual sprinklers may also need to be temporarily turned off depending on location of creching young, or if it appears likely that animals will be displaced to undesirable locations.

Flying-fox heat stroke generally occurs when the temperature reaches 42°C, however can occur at lower temperatures in more humid conditions (Bishop 2015). Given that humidity is most likely to be increased with water mist, if sprinkler design cannot limit mist, sprinklers may need to be turned off in higher temperatures (e.g. >30°C) to avoid exacerbating heat stress.

5.3 Level 3 management

Some people have an expectation that flying-foxes can be moved or controlled. Level 3 dispersal actions are extremely expensive, resource intense and very rarely successful in the long term due to flying-foxes returning year after year. Dispersal should only be considered once Level 1 and Level 2 management methods have been implemented and impacts have not been mitigated. OEH approval would be required.

Actions in Table 8 contingent on securing funds and staff resourcing levels. Funds for actions will be sourced via grants and Council commitments.

Table 8 Staged approach to management at Kooloonbung Creek flying-fox camp. Note all actions must be done in accordance with measures in Appendix 6. Authorisation for Level 2 and 3 actions is required, and additional approvals may be required under other legislation (see Appendix 1).

Management option	Management level	Appraisal	Action(s)	Timing	Estimated cost
Education and	Level 1	Adopt	Increase community understanding and improve perceptions of flying-foxes.	Short term	\$4,000
awareness programs			mail out flyers; disease risk; removal of non-natives in gardens (i.e. Cocos palms), removal of barbed wire fencing, using wildlife friendly netting	(within 12 months)	\$2,000
			produce webpage on Council's website summarising Council's approach to flying-fox management (or refer to sites such as http://littleaussiebat.com.au/health/)		\$10,000
			design and install signage promoting flying-fox ecology and conservation		\$2,000
			train staff on positive flying-fox messaging		\$18,000
			Continue to consult with affected community members.		
Property modification / service subsidies	Level 1	Adopt	Investigate a subsidy program to assist with property modification for PMHC residents. The subsidy program will identify an eligibility matrix for subsidies based on the following parametres	Short term (within 12 months)	
			location of flying-fox camp		
			proximity to residential areas		\$20,000
			 level of likely impacts from noise, odour, visibility 		
			Rebates/subsidies will be offered for		\$5,000
			water or electricity bills		
			double glazed windows		\$25,000
			cleaning services		
			· Car covers		
Routine camp management	Level 1	Adopt (ongoing)	Friends of Kooloonbung have been undertaking bush regeneration in the Park since 1985.	(ongoing)	Friends of Kooloonbung in kind
Alternative habitat creation	Level 1	Adopt	Stage 1 - Undertake an investigation of nearby alternative flying-fox camp sites to determine feasibility of identified sites. This will include a risk	Short term	\$10,000 (assumes up to three sites, additional

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Management option	Management level	Appraisal	Action(s)	Timing	Estimated cost
			assessment to determine sensitive receptor areas, zoning conflicts and potential current and future use conflicts, assessment of current vegetation condition to determine what resource investment would be required to improve the sites suitability. Stage 2 – Select feasible sites for investment. Write plan of management / rehabilitation plans for selected sites. Stage 3 – Implementation of plans of management.		\$5,000 per additional site)
Canopy-mounted sprinklers	Level 2	Investigate further	Liaise with landholders and an irrigation specialist regarding feasibility and costs associated with installing canopy-mounted sprinklers in the buffer zone between Lot 26/DP249270 and 3/SP70635 and 3/SP73183 Design and frequency/ duration of use must also be considerate of animal welfare and other ecological values of the site. Level 2 action and so licence from OEH required prior to installing. Action may trigger a test of significance.	Medium term (within 2-3 years)	~ \$10,000 for each sprinkler including installation.
Buffers through vegetation trimming and removal	Level 2	Investigate further	Improve buffers for residents in high impact areas in Lots 3/SP70635 and 3/SP73183. An arborist should be engaged to investigate which trees are suitable for trimming or removal in line with Council's Planning Provisions. Level 2 action and authorisation from OEH required prior to commencing.	Long term (4-5 years after sprinklers deemed ineffective)	\$50,000 Estimate based on previous vegetation works
Protocols to manage ncidents	Level 1	Investigate further	Previous management actions (Table 4) recommended annual check on recorded Lyssavirus. Hendra Virus, Menangle Virus and Leptospirosis infections. Develop understanding of records or incidents and whether protocols are needed to reduce exposure to risk such as for staff, subcontractors or volunteers working in the Park. Gather data during heat stress events and provide to researchers via https://www.animalecologylab.org/heat-stress-data-form.html	Short term (1-2 years)	Council in kind
Research	Level 1	Investigate further	Support research through continued flying-fox census data collection. Develop understanding of native flowering events in area.	Long term (ongoing)	Council in kind
Appropriate land-use planning	Level 1	Investigate further	Identification of zoning for FFs to mitigate impacts to residents.	Long term (within 3 years)	Council in kind

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Management option	Management level	Appraisal	Action(s)	Timing	Estimated cost
0			Measures to avoid future impacts will be considered when assessing development applications.		

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6 Evaluation and review

This Plan will be in operation for five years (2019-2023) with annual review of management actions set out in Section 5.

The following will trigger a reactive internal review of the Plan:

- · completion of a management activity
- · progression to a higher level of management
- · changes to relevant policy/legislation
- · new management techniques becoming available
- · outcomes of research that may influence the Plan
- · incidents associated with the camp.

Monitoring and reporting requirements are detailed in Section 7.

If the Plan is to remain current, a full review including stakeholder consultation and expert input will be undertaken in the final year of the Plan prior to being re-submitted to OEH.

7 Plan administration

7.1 Monitoring of the camp

Reports for Level 1 actions that comply with this Plan are not required to be submitted to OEH. It is recommended that Council keep internal records to allow the effectiveness of each management action to be evaluated.

Reports for Level 2 actions will be submitted to OEH one month after commencement of works and then quarterly in periods where works have occurred. Each report is to include:

- results of pre- and post-work population monitoring
- any information on new camps that have formed in the area
- further management actions planned to include a schedule of works
- an assessment of how the community responded to the works, including details on the number and nature of complaints before and after the works
- · detail on any compensatory planting
- · expenditure and contributors
- outcomes from evaluation and review (Section 6).

7.2 Responsibilities

Council is responsible for implementation of the Plan once it has been endorsed by OEH, licences have been obtained for Level 2 actions and resources have been allocated for implementation. Council will seek advice from OEH and other flying-fox experts as required during implementation.

Administration may also include determining residents' eligibility for subsidies for services such as arborist advice, vegetation trimming/removal and green waste removal or providing details of suppliers for canopy-mounted sprinklers.

All Council personnel, contractors and volunteers working in Kooloonbung Creek are responsible for complying with mitigation measures detailed in Appendix 6. Council will ensure non-Council staff and volunteers are aware of this responsibility and will provide assistance if required. All on-ground works towards implementation of this Plan, will be performed in accordance with a Safe Work Method Statement that includes risks and mitigation measures for working in a flying-fox camp.

7.3 Funding commitment

Council will commit available funds on an annual basis over the life of the five year Plan to implement actions in Table 8. Allocation of Council funding will be dependent on resources available and annual priorities. Council will also seek opportunities for funding through relevant grant programs, such as the NSW Flying-fox Grants Program.

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Appendix 1 Legislation

Local

Port Macquarie-Hastings Council Local Environmental Plan 2011

The Park is zoned E2 (Environmental Conservation). The objectives of this zone are to:

- To protect, manage and restore areas of high ecological, scientific, cultural or aesthetic values.
- To prevent development that could destroy, damage or otherwise have an adverse effect on those values.
- To protect coastal wetlands and littoral rainforests.
- To protect land affected by coastal processes and environmentally sensitive land.
- To prevent development that adversely affects, or would be adversely affected by, coastal processes.
- To enable development of public works and environmental facilities where such development would not have an overall detrimental impact on ecological, scientific, cultural or aesthetic values.

Port Macquarie-Hastings Development Control Plan 2013

For actions relating to trees and vegetations, the Tree Management Provisions in Port Macquarie-Hastings Council Development Control Plan 2013 apply to the reserve and any development in the Park by regulating the removal of trees requiring consent where exemptions do not apply.

Environmental Management Areas and Buffers must be considered in relation to potential vegetation works around the camp.

Development Provisions

a) For coastal floodplain endangered ecological communities a minimum, fully vegetated buffer of 35m must be provided.

b) For Freshwater Wetland on Coastal Floodplain endangered ecological community a fully vegetated buffer of 100m is to be provided.

c) For all other endangered ecological communities, a fully vegetated buffer of 50m must be provided.

e) Fully vegetated buffers cannot contain road infrastructure or an asset protection zone.

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State

Flying-fox Camp Management Policy 2015

The Flying-fox Camp Management Policy 2015 (the Policy) has been developed to empower land managers, primarily local councils, to work with their communities to manage flying-fox camps effectively. It provides the framework within which OEH will make regulatory decisions. In particular, the Policy strongly encourages local councils and other land managers to prepare Camp Management Plans for sites where the local community is affected.

Flying-fox Camp Management 2018

OEH has prepared a Code of Practice under the *Biodiversity Conservation Regulation 2017* authorising camp management actions on public land. The code defines standards for effective and humane management of flying-fox camps.

Camp management actions can only be implemented under the Code in accordance with a camp management plan endorsed by the Environment Agency Head (i.e. OEH).

The objective of the code is to enable camp managers to act quickly if flying-fox camps are causing a concern on public land. If camp management actions are consistent with the code, a Biodiversity Conservation licence will not be required.

Biodiversity Conservation Act 2016

The purpose of the *Biodiversity Conservation Act 2016* (BC Act) is to maintain a healthy, productive and resilient environment for the greatest well-being of the community, now and into the future, consistent with the principles of ecologically sustainable development including conserving biodiversity, maintaining the diversity and quality of ecosystems, regulating human interactions with wildlife, and supporting conservation and threat abatement action to slow the rate of biodiversity loss and conserve threatened species and ecological communities in nature.

The Grey-headed Flying-fox is listed as a threatened species under the BC Act.

Part 2 Division 3 of the BC Act provides for the issuing of Biodiversity Conservation Licences to authorise the doing of an act likely to result in one or more of the following:

- a. harm or attempted harm to any animal that is of a threatened species or is part of threatened ecological community
- b. harm or attempted harm, dealing in, or liberating a protected animal
- c. the picking of any plant that is of a threatened species or is part of threatened ecological community
- d. picking or dealing in protected plants
- e. damage to declared areas of outstanding biodiversity value
- f. damage to any habitat of a threatened species or threatened ecological community.

Part 7 of the BC Act provides for the biodiversity assessment and approvals required under the *Environmental Planning and Assessment Act 1979* for development other than complying

development, activities and state significant development and infrastructure.

An assessment of impacts is required for any threatened species or threatened ecological community, or their habitats, that are likely to be harmed by the doing of an act proposed in the Plan.

Note: that the definition of 'harm' includes kill, injure or capture the animal, but does not include harm by changing the habitat of the animal, and attempt to harm an animal includes hunting or pursuing, or using anything, for the purpose of harming the animal. The definition of 'pick' includes to gather, take, cut, remove from the ground, destroy, poison, crush or injure the plant or any part of the plant. The definition of habitat includes an area periodically or occasionally occupied by a species or ecological community and the biotic and abiotic components of an area.

Local Government Act 1993

The primary purpose of this Act is to provide the legal framework for an effective, efficient and environmentally responsible, open system of local government. Most relevant to flying-fox management is that it also provides encouragement for the effective participation of local communities in the affairs of local government and sets out guidance on the use and management of community land which may be applicable to land which requires management of flying-foxes.

National Parks and Wildlife Act 1974

The National Parks and Wildlife Act 1974 (NPW Act) provides for the conservation of nature, objects, places or features of cultural value and the management of land reserved under this Act. The Act protects Aboriginal objects and declared Aboriginal Places. An Aboriginal Heritage Impact Permit may be required under this Act to authorise camp management actions that may harm Aboriginal objects a declared Aboriginal Places.

Prevention of Cruelty to Animals Act 1979

It may be an offence under this Act if there is evidence of unreasonable/unnecessary torment associated with management activities. Adhering to welfare and conservation measures provided in Section 10.3 will ensure compliance with this Act.

Environmental Planning and Assessment Act 1979

The objects of the *Environmental Planning and Assessment Act 1979* (EP&A Act) are to encourage proper management, development and conservation of resources, for the purposes of the social and economic welfare of the community and a better environment. It also aims to share responsibility for environmental planning between different levels of government and promote public participation in environmental planning and assessment.

The EP&A Act is administered by the NSW Department of Planning and Environment.

Development control plans under the EP&A Act should consider flying-fox camps so that planning, design and construction of future developments is appropriate to avoid future conflict.

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Development given consent under Part 4 or activities assessed under Part 5 of the EP&A Act do not require licensing under the BC Act. Consent and determining authorities are required to consider the impacts of such proposals on threatened species, threatened ecological communities, and their habitats in accordance with Part 7 of the BC Act.

Where development consent under Part 4 or assessment under Part 5 of the EP&A Act is not required, a licence under the BC Act may be required to authorise the doing of an act that harms protected animals, threatened species, or threatened ecological community, or which damages the habitat of a threatened species or ecological community. This includes the doing of an act likely to harm any flying-fox or damaging the habitat of grey-headed flying-foxes.

Where a proposal to manage a flying-fox camp involves the cutting down, destruction, lopping or removal of a substantial part of a tree or other vegetation that is not covered by a development consent or assessment under Part 5 it may still require authorisation. Depending on the land on which the vegetation occurs and the character of the vegetation, it may require an approval or a permit under the *State Environmental Planning Policy* (*Vegetation in Non-Rural Areas*) 2017 (SEPP) or an approval under the *Local Land Services Act 2013*.

Where flying-fox camps occur or impact on private land, private land owners are advised to contact their local council to explore management options and the appropriate approval processes for addressing arising issues.

State Environmental Planning Policy (Vegetation in Non-Rural Areas) 2017

This policy aims to protect the biodiversity, and amenity values of trees, and other vegetation in non-rural areas of the State. A person must not cut down, fell, up root, kill, poison, ringbark, burn or otherwise destroy the vegetation, or lop or otherwise remove a substantial part of the vegetation to which this Policy applies without a permit granted by council, or in the case of vegetation clearing exceeding the biodiversity offset thresholds (as stated in Part 7 of the *Biodiversity Conservation Regulation 2017*), approval by the Native Vegetation Panel.

Proponents will need to consider whether the SEPP (Vegetation in Non-Rural Areas) applies to their proposal, and if any approvals under the BC Act.

Commonwealth

Environment Protection and Biodiversity Conservation Act 1999

The Commonwealth's EPBC Act provides protection for the environment, specifically matters of national environmental significance (MNES). A referral to the Commonwealth DoE is required under the EPBC Act for any action that is likely to significantly impact on an MNES.

MNES under the EPBC Act that relate to flying-foxes include:

- · world heritage sites (where those sites contain flying-fox camps or foraging habitat)
- wetlands of international importance (where those wetlands contain flying-fox camps or foraging habitat)
- nationally threatened species and ecological communities.

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The GHFF is listed as a vulnerable species under the EPBC Act, meaning it is an MNES. It is also considered to have a single national population. DoE has developed the Referral guideline for management actions in GHFF and SFF camps (DoE 2015) (the Guideline) to guide whether referral is required for actions pertaining to the GHFF.

The Guideline defines a nationally important GHFF camp as one that has either:

- contained ≥10,000 GHFF in more than one year in the last 10 years, or
- been occupied by more than 2500 GHFF permanently or seasonally every year for the last 10 years.

Provided that management at nationally important camps follows the mitigation standards below, DoE has determined that a significant impact to the population is unlikely, and referral is not likely to be required.

Referral will be required if a significant impact to any other MNES is considered likely as a result of management actions outlined in the Plan. Self-assessable criteria are available in the Significant Impact Guidelines 1.1 (DoE 2013) to assist in determining whether a significant impact is likely; otherwise consultation with DoEE will be required.

Mitigation standards

The action must not occur if the camp contains females that are in the late stages of pregnancy or have dependent young that cannot fly on their own.

The action must not occur during or immediately after climatic extremes (heat stress event, cyclone event), or during a period of significant food stress.

Disturbance must be carried out using non-lethal means, such as acoustic, visual and/or physical disturbance or use of smoke.

Disturbance activities must be limited to a maximum of 2.5 hours in any 12-hour period, preferably at or before sunrise or at sunset.

Trees are not felled, lopped or have large branches removed when flying-foxes are in or near to a tree and likely to be harmed.

The action must be supervised by a person with knowledge and experience relevant to the management of flying-foxes and their habitat, who can identify dependent young and is aware of climatic extremes and food stress events. This person must make an assessment of the relevant conditions and advise the proponent whether the activity can go ahead consistent with these standards.

The action must not involve the clearing of all vegetation supporting a nationally-important flying-fox camp. Sufficient vegetation must be retained to support the maximum number of flying-foxes ever recorded in the camp of interest.

These standards have been incorporated into mitigation measures detailed in Section 10.3. If actions cannot comply with these mitigation measures, referral for activities at nationally important camps is likely to be required.

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Appendix 2 Flying-fox ecology & behaviour

Ecological role

Flying-foxes, along with some birds, make a unique contribution to ecosystem health through their ability to move seeds and pollen over long distances (Southerton et al. 2004). This contributes directly to the reproduction, regeneration and viability of forest ecosystems (DoE 2016a). It is estimated that a single flying-fox can disperse up to 60,000 seeds in one night (ELW&P 2015). Some plants, particularly Corymbia spp., have adaptations suggesting they rely more heavily on nocturnal visitors such as bats for pollination than daytime pollinators (Southerton et al. 2004).

GHFF may travel 100 km in a single night with a foraging radius of up to 50 km from their camp (McConkey et al. 2012) and have been recorded travelling over 500 km in two days between camps (Roberts et al. 2012). In comparison bees, another important pollinator, move much shorter foraging distances of generally less than one kilometre (Zurbuchen et al. 2010).

Long-distance seed dispersal and pollination makes flying-foxes critical to the long-term persistence of many plant communities (Westcott et al. 2008; McConkey et al. 2012), including eucalypt forests, rainforests, woodlands and wetlands (Roberts et al. 2006). Seeds that are able to germinate away from their parent plant have a greater chance of growing into a mature plant (EHP 2012). Long-distance dispersal also allows genetic material to be spread between forest patches that would normally be geographically isolated (Parry-Jones & Augee 1992; Eby 1991; Roberts 2006). This genetic diversity allows species to adapt to environmental change and respond to disease pathogens. Transfer of genetic material between forest patches is particularly important in the context of contemporary fragmented landscapes.

Flying-foxes are considered 'keystone' species given their contribution to the health, longevity and diversity among and between vegetation communities. These ecological services ultimately protect the long-term health and biodiversity of Australia's bushland and wetlands. In turn, native forests act as carbon sinks, provide habitat for other fauna and flora, stabilise river systems and catchments, add value to production of hardwood timber, honey and fruit (e.g. bananas and mangoes; Fujita 1991), and provide recreational and tourism opportunities worth millions of dollars each year (EHP 2012; ELW&P 2015).

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Black flying-fox (Pteropus alecto)



Figure 23 Black flying-fox indicative species distribution, adapted from OEH 2015a

The black flying-fox (BFF) (Figure 23) has traditionally occurred throughout coastal areas from Shark Bay in Western Australia, across Northern Australia, down through Queensland and into NSW (Churchill 2008; OEH 2015a). Since it was first described there has been a substantial southerly shift by the BFF (Webb & Tidemann 1995). This shift has consequently led to an increase in indirect competition with the threatened GHFF, which appears to be favouring the BFF (DoE 2016a).

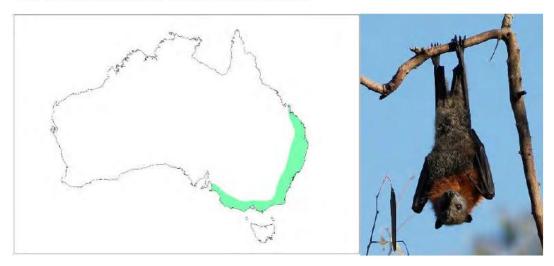
They forage on the fruit and blossoms of native and introduced plants (Churchill 2008; OEH 2015a), including orchard species at times.

BFFs are largely nomadic animals with movement and local distribution influenced by climatic variability and the flowering and fruiting patterns of their preferred food plants. Feeding commonly occurs within 20 km of the camp site (Markus & Hall 2004).

BFFs usually roost beside a creek or river in a wide range of warm and moist habitats, including lowland rainforest gullies, coastal stringybark forests and mangroves. During the breeding season camp sizes can change significantly in response to the availability of food and the arrival of animals from other areas.

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Grey-headed flying-fox (Pteropus poliocephalus)

Figure 24 Grey-headed flying-fox indicative species distribution, adapted from OEH 2015a

The GHFF (Figure 24) is found throughout eastern Australia, generally within 200 kilometres of the coast, from Finch Hatton in Queensland to Melbourne, Victoria (OEH 2015d). This species now ranges into South Australia and has been observed in Tasmania (DoE 2016a). It requires foraging resources and camp sites within rainforests, open forests, closed and open woodlands (including melaleuca swamps and banksia woodlands). This species is also found throughout urban and agricultural areas where food trees exist and will raid orchards at times, especially when other food is scarce (OEH 2015a).

All the GHFF in Australia are regarded as one population that moves around freely within its entire national range (Webb & Tidemann 1996; DoE 2015). GHFF may travel up to 100 kilometres in a single night with a foraging radius of up to 50 kilometres from their camp (McConkey et al. 2012). They have been recorded travelling over 500 kilometres over 48 hours when moving from one camp to another (Roberts et al. 2012). GHFF generally show a high level of fidelity to camp sites, returning year after year to the same site, and have been recorded returning to the same branch of a particular tree (SEQ Catchments 2012). This may be one of the reasons flying-foxes continue to return to small urban bushland blocks that may be remnants of historically-used larger tracts of vegetation.

The GHFF population has a generally annual southerly movement in spring and summer, with their return to the coastal forests of north-east NSW and south-east Queensland in winter (Ratcliffe 1932; Eby 1991; Parry-Jones & Augee 1992; Roberts et al. 2012). This results in large fluctuations in the number of GHFF in NSW, ranging from as few as 20% of the total population in winter up to around 75% of the total population in summer (Eby 2000). They are widespread throughout their range during summer, but in spring and winter are uncommon in the south. In autumn they occupy primarily coastal lowland camps and are uncommon inland and on the south coast of NSW (DECCW 2009).

There is evidence the GHFF population declined by up to 30% between 1989 and 2000 (Birt 2000; Richards 2000 cited in OEH 2011a). There is a wide range of ongoing threats to the survival of the GHFF, including habitat loss and degradation, deliberate destruction associated

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with the commercial horticulture industry, conflict with humans, infrastructure-related mortality (e.g. entanglement in barbed wire fencing and fruit netting, power line electrocution, etc.) and competition and hybridisation with the BFF (DECCW 2009). For these reasons it is listed as vulnerable to extinction under NSW and federal legislation (see Section 3).



Little red flying-fox (Pteropus scapulatus)

Figure 25 Little red flying-fox indicative species distribution, adapted from OEH 2015a

The little red flying-fox (LRFF) (Figure 25) is widely distributed throughout northern and eastern Australia, with populations occurring across northern Australia and down the east coast into Victoria.

The LRFF forages almost exclusively on nectar and pollen, although will eat fruit at times and occasionally raids orchards (Australian Museum 2010). LRFF often move sub-continental distances in search of sporadic food supplies. The LRFF has the most nomadic distribution, strongly influenced by availability of food resources (predominantly the flowering of eucalypt species) (Churchill 2008), which means the duration of their stay in any one place is generally very short.

Habitat preferences of this species are quite diverse and range from semi-arid areas to tropical and temperate areas, and can include sclerophyll woodland, melaleuca swamplands, bamboo, mangroves and occasionally orchards (IUCN 2015). LRFF are frequently associated with other *Pteropus* species. In some colonies, LRFF individuals can number many hundreds of thousands and they are unique among *Pteropus* species in their habit of clustering in dense bunches on a single branch. As a result, the weight of roosting individuals can break large branches and cause significant structural damage to roost trees, in addition to elevating soil nutrient levels through faecal material (SEQ Catchments 2012).

Throughout its range, populations within an area or occupying a camp can fluctuate widely. There is a general migration pattern in LRFF, whereby large congregations of over one million individuals can be found in northern camp sites (e.g. Northern Territory, North Queensland) during key breeding periods (Vardon & Tidemann 1999). LRFF travel south to visit the coastal areas of south-east Queensland and NSW during the summer months. Outside these periods

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LRFF undertake regular movements from north to south during winter–spring (July–October) (Milne & Pavey 2011).

Reproduction

Black and grey-headed flying-foxes

Males initiate contact with females in January with peak conception occurring around March to April/May; this mating season represents the period of peak camp occupancy (Markus 2002). Young (usually a single pup) are born six months later from September to November (Churchill 2008). The birth season becomes progressively earlier, albeit by a few weeks, in more northerly populations (McGuckin & Blackshaw 1991), however out of season breeding is common with births occurring later in the year.

Young are highly dependent on their mother for food and thermoregulation. Young are suckled and carried by the mother until approximately four weeks of age (Markus & Blackshaw 2002). At this time, they are left at the camp during the night in a crèche until they begin foraging with their mother in January and February (Churchill 2008) and are usually weaned by six months of age around March. Sexual maturity is reached at two years of age with a life expectancy up to 20 years in the wild (Pierson & Rainey 1992).

As such, the critical reproductive period for GHFF is generally from August (when females are in final trimester) to the end of peak conception around April. Dependent pups are usually present from September to March (Figure 26).

Little red flying-fox

The LRFF breeds approximately six months out of phase with the other flying-foxes. Peak conception occurs around October to November, with young born between March and June (McGuckin & Blackshaw 1991; Churchill 2008) (Figure 26). Young are carried by their mother for approximately one month then left at the camp while she forages (Churchill 2008). Suckling occurs for several months while young are learning how to forage. LRFF generally birth and rear young in temperate areas (rarely in NSW).

	Jan	Feb	Mar	Apr	Мау	Jun	Jul	Aug	Sep	Oct	Nov	Dec
GHFF				•	-		-					
BFF				•								
LRFF												

Peak conception Final trimester Peak birthing Crèching (young left at roost) Lactation

Figure 26 Indicative flying-fox reproductive cycle.

Note that LRFF rarely birth and rear young in NSW. The breeding season of all species is variable between years and location, and expert assessment is required to accurately determine phases in the breeding cycle and inform appropriate management timing.

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Heat stress events

Flying-foxes suffer from heat stress when the ambient temperature exceeds the physiological limits flying-foxes can endure for maintaining a comfortable body temperature (Bishop 2014). Flying-foxes are susceptible to heat stress due to their inability to sweat (Snoyman et al 2012), therefore they need to expend energy on cooling mechanisms such as fanning. BFF are considered to be more susceptible to HSE than GHFF due to the southern expansion of their range with temperature extremes increasing in severity with latitude in eastern Australia (Welbergen et al 2008).

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Appendix 3 Protected Matters

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EPBC Act Protected Matters Report

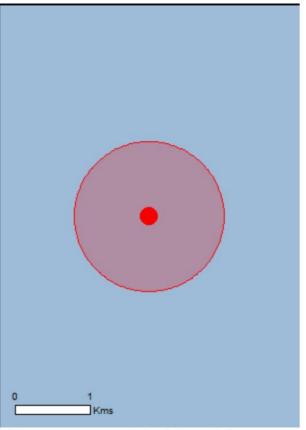
This report provides general guidance on matters of national environmental significance and other matters protected by the EPBC Act in the area you have selected.

Information on the coverage of this report and qualifications on data supporting this report are contained in the caveat at the end of the report.

Information is available about <u>Environment Assessments</u> and the EPBC Act including significance guidelines, forms and application process details.

Report created: 16/11/18 16:27:53

Summary Details Matters of NES Other Matters Protected by the EPBC Act Extra Information Caveat Acknowledgements



This map may contain data which are ©Commonwealth of Australia (Geoscience Australia), ©PSMA 2010



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Summary

Matters of National Environmental Significance

This part of the report summarises the matters of national environmental significance that may occur in, or may relate to, the area you nominated. Further information is available in the detail part of the report, which can be accessed by scrolling or following the links below. If you are proposing to undertake an activity that may have a significant impact on one or more matters of national environmental significance then you should consider the Administrative Guidelines on Significance.

World Heritage Properties:	None
National Heritage Places:	None
Wetlands of International Importance:	None
Great Barrier Reef Marine Park:	None
Commonwealth Marine Area:	None
Listed Threatened Ecological Communities:	2
Listed Threatened Species:	52
Listed Migratory Species:	57

Other Matters Protected by the EPBC Act

This part of the report summarises other matters protected under the Act that may relate to the area you nominated. Approval may be required for a proposed activity that significantly affects the environment on Commonwealth land, when the action is outside the Commonwealth land, or the environment anywhere when the action is taken on Commonwealth land. Approval may also be required for the Commonwealth or Commonwealth agencies proposing to take an action that is likely to have a significant impact on the environment anywhere.

The EPBC Act protects the environment on Commonwealth land, the environment from the actions taken on Commonwealth land, and the environment from actions taken by Commonwealth agencies. As heritage values of a place are part of the 'environment', these aspects of the EPBC Act protect the Commonwealth Heritage values of a Commonwealth Heritage place. Information on the new heritage laws can be found at http://www.environment.gov.au/heritage

A <u>permit</u> may be required for activities in or on a Commonwealth area that may affect a member of a listed threatened species or ecological community, a member of a listed migratory species, whales and other cetaceans, or a member of a listed marine species.

Commonwealth Land:	2
Commonwealth Heritage Places:	None
Listed Marine Species:	61
Whales and Other Cetaceans:	1
Critical Habitats:	None
Commonwealth Reserves Terrestrial:	None
Australian Marine Parks:	None

Extra Information

This part of the report provides information that may also be relevant to the area you have nominated.

State and Territory Reserves:	None
Regional Forest Agreements:	1
Invasive Species:	35
Nationally Important Wetlands:	None
<u>Key Ecological Features (Marine)</u>	None

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Details

Matters of National Environmental Significance

Listed Threatened Ecological Communities		[Resource Information]
For threatened ecological communities where the distribution plans, State vegetation maps, remote sensing imagery a community distributions are less well known, existing very produce indicative distribution maps.	and other sources. Where	threatened ecological
Name	Status	Type of Presence
Coastal Swamp Oak (Casuarina glauca) Forest of New South Wales and South East Queensland ecological	Endangered	Community likely to occur within area
<u>community</u> Lowland Rainforest of Subtropical Australia	Critically Endangered	Community may occur within area
Listed Threatened Species		[Resource Information]
Name	Status	Type of Presence
Birds		
Anthochaera phrygia		
Regent Honeyeater [82338]	Critically Endangered	Species or species habitat known to occur within area
Botaurus poiciloptilus		
Australasian Bittern [1001]	Endangered	Species or species habitat known to occur within area
Calidris canutus		
Red Knot, Knot [855]	Endangered	Species or species habitat known to occur within area
Calidris ferruginea		
Curlew Sandpiper [856]	Critically Endangered	Species or species habitat known to occur within area
Charadrius mongolus		
Lesser Sand Plover, Mongolian Plover [879]	Endangered	Foraging, feeding or related behaviour known to occur within area
<u>Dasyornis brachypterus</u> Eastern Bristlebird [533]	Endangered	Species or species habitat likely to occur within area
Diomedea antipodensis		
Antipodean Albatross [64458]	Vulnerable	Foraging, feeding or related behaviour likely to occur within area
Diomedea antipodensis gibsoni		
Gibson's Albatross [82270]	Vulnerable	Foraging, feeding or related behaviour likely to occur within area
Diomedea epomophora		
Southern Royal Albatross [89221]	Vulnerable	Foraging, feeding or related behaviour likely to occur

<u>Diomedea exulans</u>

Wandering Albatross [89223]

Vulnerable

Foraging, feeding or related behaviour likely to occur within area

within area

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Name	Status	Type of Presence
Diomedea sanfordi		
Northern Royal Albatross [64456]	Endangered	Foraging, feeding or related behaviour likely to occur within area
Erythrotriorchis radiatus		
Red Goshawk [942]	Vulnerable	Species or species habitat likely to occur within area
Grantiella picta		
Painted Honeyeater [470]	Vulnerable	Species or species habitat may occur within area
Limosa lapponica baueri		
Bar-tailed Godwit (baueri), Western Alaskan Bar-tailed Godwit [86380]	Vulnerable	Species or species habitat likely to occur within area
Limosa lapponica menzbieri		
Northern Siberian Bar-tailed Godwit, Bar-tailed Godwit (menzbieri) [86432]	Critically Endangered	Species or species habitat may occur within area
Macronectes giganteus		
Southern Giant-Petrel, Southern Giant Petrel [1060]	Endangered	Species or species habitat may occur within area
Macronectes halli		
Northern Giant Petrel [1061]	Vulnerable	Species or species habitat may occur within area
Numenius madagascariensis		
Eastern Curlew, Far Eastern Curlew [847]	Critically Endangered	Species or species habitat known to occur within area
Pachyptila turtur subantarctica		
Fairy Prion (southern) [64445]	Vulnerable	Species or species habitat known to occur within area
Rostratula australis		
Australian Painted-snipe, Australian Painted Snipe [77037]	Endangered	Species or species habitat may occur within area
Thalassarche bulleri		
Buller's Albatross, Pacific Albatross [64460]	Vulnerable	Species or species habitat may occur within area
Thalassarche bulleri platei		
Northern Buller's Albatross, Pacific Albatross [82273]	Vulnerable	Species or species habitat may occur within area
Thalassarche cauta cauta		
Shy Albatross, Tasmanian Shy Albatross [82345]	Vulnerable	Species or species habitat may occur within area
Thalassarche cauta steadi		
White-capped Albatross [82344]	Vulnerable	Foraging, feeding or related behaviour likely to occur within area
Chatham Albatross [64457]	Endangered	Species or species habitat
		may occur within area

may occur within area

Thalassarche impavida

Campbell Albatross, Campbell Black-browed Albatross Vulnerable [64459]

Thalassarche melanophris Black-browed Albatross [66472]

Vulnerable

Species or species habitat may occur within area

Species or species habitat may occur within area

<u>Thalassarche salvini</u> Salvin's Albatross [64463]

Fish

Vulnerable

Foraging, feeding or related behaviour likely to occur within area

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Name	Status	Type of Presence
Epinephelus daemelii	Oldius	Type of Fresence
Black Rockcod, Black Cod, Saddled Rockcod [68449]	Vulnerable	Species or species habitat likely to occur within area
Frogs		
Litoria aurea		
Green and Golden Bell Frog [1870]	Vulnerable	Species or species habitat likely to occur within area
Insects		
Argynnis hyperbius inconstans		
Australian Fritillary [88056]	Critically Endangered	Species or species habitat likely to occur within area
Mammals		
<u>Chalinolobus dwyeri</u>		
Large-eared Pied Bat, Large Pied Bat [183]	Vulnerable	Species or species habitat likely to occur within area
Dasyurus maculatus maculatus (SE mainland populati	<u>on)</u>	
Spot-tailed Quoll, Spotted-tail Quoll, Tiger Quoll (southeastern mainland population) [75184]	Endangered	Species or species habitat known to occur within area
<u>Petauroides volans</u>		
Greater Glider [254]	Vulnerable	Species or species habitat likely to occur within area
Phascolarctos cinereus (combined populations of Qld,	NSW and the ACT)	
Koala (combined populations of Queensland, New South Wales and the Australian Capital Territory) [85104]	Vulnerable	Species or species habitat known to occur within area
Potorous tridactylus tridactylus		
Long-nosed Potoroo (SE mainland) [66645]	Vulnerable	Species or species habitat likely to occur within area
Pseudomys novaehollandiae		
New Holland Mouse, Pookila [96]	Vulnerable	Species or species habitat likely to occur within area
Pteropus poliocephalus		
Grey-headed Flying-fox [186]	Vulnerable	Roosting known to occur within area
Plants		
<u>Acronychia littoralis</u> Scented Acronychia [8582]	Endangered	Species or species habitat likely to occur within area
<u>Allocasuarina thalassoscopica</u> [21927]	Endangered	Species or species habitat known to occur within area
<u>Cryptostylis hunteriana</u> Leafless Tongue-orchid [19533]	Vulnerable	Species or species habitat likely to occur within area
<u>Euphrasia arguta</u> [4325]	Critically Endangered	Species or species habitat may occur within area

Macadamia integrifolia

Macadamia Nut, Queensland Nut Tree, Smoothshelled Macadamia, Bush Nut, Nut Oak [7326]

Melaleuca biconvexa

Biconvex Paperbark [5583]

Vulnerable

Species or species habitat may occur within area

Vulnerable

Species or species habitat may occur within area

Phaius australis Lesser Swamp-orchid [5872]

Endangered

Species or species habitat may occur within area

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Name	Status	Type of Presence
<u>Syzygium paniculatum</u> Magenta Lilly Pilly, Magenta Cherry, Daguba, Scrub Cherry, Creek Lilly Pilly, Brush Cherry [20307]	Vulnerable	Species or species habitat may occur within area
<u>Thesium australe</u> Austral Toadflax, Toadflax [15202]	Vulnerable	Species or species habitat likely to occur within area
Reptiles		
Caretta caretta Loggerhead Turtle [1763]	Endangered	Species or species habitat known to occur within area
<u>Chelonia mydas</u> Green Turtle [1765]	Vulnerable	Species or species habitat known to occur within area
<u>Dermochelys coriacea</u> Leatherback Turtle, Leathery Turtle, Luth [1768]	Endangered	Species or species habitat known to occur within area
Eretmochelys imbricata Hawksbill Turtle [1766]	Vulnerable	Species or species habitat known to occur within area
<u>Natator depressus</u> Flatback Turtle [59257]	Vulnerable	Breeding likely to occur within area
Listed Migratory Species		[Resource Information]
* Species is listed under a different scientific name o	n the EPBC Act - Threa	
Name	Threatened	Type of Presence
Migratory Marine Birds		
Anous stolidus Common Noddy [825]		Species or species habitat likely to occur within area
Apus pacificus Fork-tailed Swift [678]		Species or species habitat likely to occur within area
Calonectris leucomelas Streaked Shearwater [1077]		Species or species habitat may occur within area
Diomedea antipodensis Antipodean Albatross [64458]	Vulnerable	Foraging, feeding or related behaviour likely to occur within area
Diomedea epomophora Southern Royal Albatross [89221]	Vulnerable	Foraging, feeding or related behaviour likely to occur within area
<u>Diomedea exulans</u> Wandering Albatross [89223]	Vulnerable	Foraging, feeding or related behaviour likely to occur within area
Diomedea sanfordi		
Northern Poyal Albatrass [64456]	Endangered	Eorgaina, feeding or related

Northern Royal Albatross [64456]

Endangered

Foraging, feeding or related behaviour likely to occur within area

Species or species habitat known to occur within area

Species or species habitat likely to occur within area

Species or species habitat may occur within area

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<u>Fregata ariel</u> Lesser Frigatebird, Least Frigatebird [1012]

<u>Fregata minor</u> Great Frigatebird, Greater Frigatebird [1013]

Macronectes giganteus Southern Giant-Petrel, Southern Giant Petrel [1060] Endangered

angered

Name	Threatened	Type of Presence
Macronectes halli		
Northern Giant Petrel [1061]	Vulnerable	Species or species habitat may occur within area
Other work and the former		
Sternula albifrons		
Little Tern [82849]		Species or species habitat
		may occur within area
Thalassarche bulleri		
Buller's Albatross, Pacific Albatross [64460]	Vulnerable	Species or species habitat
		may occur within area
Thalassarche cauta		
Tasmanian Shy Albatross [89224]	Vulnerable*	Species or species habitat
		may occur within area
<u>Thalassarche eremita</u> Chatham Albatross [64457]	Endangered	Species or species habitat
	Lindangered	may occur within area
		may occur within alea
Thalassarche impavida		
Campbell Albatross, Campbell Black-browed Albatross	Vulnerable	Species or species habitat
[64459]		may occur within area
Thalassarche melanophris		
Black-browed Albatross [66472]	Vulnerable	Species or species habitat
		may occur within area
Thalassarche salvini		
	Vulparable	Foreging fooding or related
Salvin's Albatross [64463]	Vulnerable	Foraging, feeding or related
		behaviour likely to occur within area
Thalassarche steadi		
White-capped Albatross [64462]	Vulnerable*	Foraging, feeding or related
		behaviour likely to occur
Minnehem Manine Onesies		within area
Migratory Marine Species Caretta caretta		
Loggerhead Turtle [1763]	Endangered	Species or species habitat
Loggerread Tutte [1700]	Lindingered	known to occur within area
Chelonia mydas		
Green Turtle [1765]	Vulnerable	Species or species habitat
		known to occur within area
Dermochelys coriacea		
Leatherback Turtle, Leathery Turtle, Luth [1768]	Endangered	Species or species habitat
		known to occur within area
Dugong dugon		Opening on angeling habit-t
Dugong [28]		Species or species habitat
		may occur within area
Eretmochelys imbricata		
Hawksbill Turtle [1766]	Vulnerable	Species or species habitat
-		known to occur within area
Lamna nasus		

Porbeagle, Mackerel Shark [83288]

Manta alfredi

Reef Manta Ray, Coastal Manta Ray, Inshore Manta Ray, Prince Alfred's Ray, Resident Manta Ray [84994]

Manta birostris

Giant Manta Ray, Chevron Manta Ray, Pacific Manta Ray, Pelagic Manta Ray, Oceanic Manta Ray [84995]

Natator depressus Flatback Turtle [59257]

Vulnerable

Species or species habitat may occur within area

Species or species habitat may occur within area

Species or species habitat may occur within area

Breeding likely to occur within area

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Name	Threatened	Type of Presence
Sousa chinensis		
Indo-Pacific Humpback Dolphin [50]		Species or species habitat likely to occur within area
Migratory Terrestrial Species		
<u>Cuculus optatus</u>		
Oriental Cuckoo, Horsfield's Cuckoo [86651]		Species or species habitat may occur within area
<u>Hirundapus caudacutus</u>		
White-throated Needletail [682]		Species or species habitat known to occur within area
<u>Monarcha melanopsis</u>		
Black-faced Monarch [609]		Species or species habitat known to occur within area
Monarcha trivirgatus		
Spectacled Monarch [610]		Species or species habitat known to occur within area
<u>Myiagra cyanoleuca</u>		
Satin Flycatcher [612]		Species or species habitat known to occur within area
Rhipidura rufifrons		
Rufous Fantail [592]		Species or species habitat known to occur within area
Migratory Wetlands Species		
Actitis hypoleucos		
Common Sandpiper [59309]		Species or species habitat known to occur within area
Arenaria interpres		
Ruddy Turnstone [872]		Foraging, feeding or related behaviour known to occur within area
Calidris acuminata		o · · · · · · · · · · · ·
Sharp-tailed Sandpiper [874]		Species or species habitat known to occur within area
<u>Calidris canutus</u>	_	
Red Knot, Knot [855]	Endangered	Species or species habitat known to occur within area
Calidris ferruginea		
Curlew Sandpiper [856]	Critically Endangered	Species or species habitat known to occur within area
<u>Calidris melanotos</u>		
Pectoral Sandpiper [858]		Species or species habitat may occur within area
<u>Calidris ruficollis</u>		
Red-necked Stint [860]		Foraging, feeding or related behaviour known to occur within area
Charadrius bicinctus		

Double-banded Plover [895]

<u>Charadrius mongolus</u> Lesser Sand Plover, Mongolian Plover [879]

Gallinago hardwickii Latham's Snipe, Japanese Snipe [863]

Gallinago megala Swinhoe's Snipe [864] Endangered

Foraging, feeding or related behaviour known to occur within area

Foraging, feeding or related behaviour known to occur within area

Foraging, feeding or related behaviour may occur within area

Foraging, feeding or related behaviour likely

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Name	Threatened	Type of Presence
		to occur within area
Gallinago stenura		
Pin-tailed Snipe [841]		Foraging, feeding or relate behaviour likely to occur within area
Limosa lapponica		
Bar-tailed Godwit [844]		Species or species habitat known to occur within area
Numenius madagascariensis		
Eastern Curlew, Far Eastern Curlew [847]	Critically Endangered	Species or species habitation known to occur within area
Numenius minutus		
Little Curlew, Little Whimbrel [848]		Foraging, feeding or relate behaviour likely to occur within area
Numenius phaeopus		
Whimbrel [849]		Foraging, feeding or relate behaviour known to occur within area
Pandion haliaetus		
Osprey [952]		Breeding known to occur within area
<u>Pluvialis fulva</u>		
Pacific Golden Plover [25545]		Foraging, feeding or relate behaviour known to occur within area
Pluvialis squatarola		
Grey Plover [865]		Foraging, feeding or relate behaviour known to occur within area
Tringa brevipes		
Grey-tailed Tattler [851]		Foraging, feeding or relate behaviour known to occur within area
Tringa nebularia		.
Common Greenshank, Greenshank [832]		Species or species habitation known to occur within area
Xenus cinereus		
Terek Sandpiper [59300]		Foraging, feeding or relate behaviour known to occur within area

Other Matters Protected by the EPBC Act

Commonwealth Land

[Resource Information]

The Commonwealth area listed below may indicate the presence of Commonwealth land in this vicinity. Due to the unreliability of the data source, all proposals should be checked as to whether it impacts on a Commonwealth area, before making a definitive decision. Contact the State or Territory government land department for further information.

Name

Commonwealth Land - Australian Postal Commission Commonwealth Land - Australian Telecommunications Commission

Listed Marine Species

[Resource Information]

likely to occur within area

* Species is listed under a different estentific name on the EDBC Act. Threatened Species list

opecies is listed under a unterent scientific i	lattle off the EFDC Act - Thea	aleneu opecies list.
Name	Threatened	Type of Presence
Birds		
Actitis hypoleucos		
Common Sandpiper [59309]		Species or species habitat known to occur within area
Anous stolidus		
Common Noddy [825]		Species or species habitat

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Name	Threatened	Type of Presence
Apus pacificus		
Fork-tailed Swift [678]		Species or species habitat likely to occur within area
Ardea alba		
Great Egret, White Egret [59541]		Species or species habitat known to occur within area
<u>Ardea ibis</u>		
Cattle Egret [59542]		Species or species habitat may occur within area
Arenaria interpres		
Ruddy Turnstone [872]		Foraging, feeding or related behaviour known to occur within area
<u>Calidris acuminata</u>		
Sharp-tailed Sandpiper [874]		Species or species habitat known to occur within area
<u>Calidris canutus</u>		
Red Knot, Knot [855]	Endangered	Species or species habitat known to occur within area
Calidris ferruginea		
Curlew Sandpiper [856]	Critically Endangered	Species or species habitat known to occur within area
<u>Calidris melanotos</u>		
Pectoral Sandpiper [858]		Species or species habitat may occur within area
<u>Calidris ruficollis</u>		
Red-necked Stint [860]		Foraging, feeding or related behaviour known to occur within area
Calonectris leucomelas		
Streaked Shearwater [1077]		Species or species habitat may occur within area
Charadrius bicinctus		
Double-banded Plover [895]		Foraging, feeding or related behaviour known to occur within area
Charadrius mongolus	F ueden ere et	
₋esser Sand Plover, Mongolian Plover [879]	Endangered	Foraging, feeding or related behaviour known to occur within area
Charadrius ruficapillus		
Red-capped Plover [881]		Foraging, feeding or related behaviour known to occur within area
<u>Diomedea antipodensis</u> Antipodean Albatross [64458]	Vulnerable	Foraging, feeding or related
	Vanorabio	behaviour likely to occur within area
<u>Diomedea epomophora</u> Southern Royal Albatross [89221]	Vulnerable	Foraging feeding or related
Southern Royal Albatross [09221]	vuinerable	Foraging, feeding or related behaviour likely to occur

Diomedea exulans

Wandering Albatross [89223]

Diomedea gibsoni

Gibson's Albatross [64466]

Diomedea sanfordi Northern Royal Albatross [64456] Vulnerable

Vulnerable*

Endangered

Foraging, feeding or related behaviour likely to occur within area

within area

Foraging, feeding or related behaviour likely to occur within area

Foraging, feeding or related behaviour likely to occur within area

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Name	Threatened	Type of Presence
Fregata ariel		.,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,
Lesser Frigatebird, Least Frigatebird [1012]		Species or species habitat known to occur within area
Fregata minor		
Great Frigatebird, Greater Frigatebird [1013]		Species or species habitat likely to occur within area
Gallinago hardwickii		
Latham's Snipe, Japanese Snipe [863]		Foraging, feeding or related behaviour may occur withir area
<u>Gallinago megala</u>		
Swinhoe's Snipe [864]		Foraging, feeding or related behaviour likely to occur within area
<u>Gallinago stenura</u>		
Pin-tailed Snipe [841]		Foraging, feeding or related behaviour likely to occur within area
Haliaeetus leucogaster		
White-bellied Sea-Eagle [943]		Species or species habitat known to occur within area
Heteroscelus brevipes		
Grey-tailed Tattler [59311]		Foraging, feeding or related behaviour known to occur within area
<u>Hirundapus caudacutus</u>		
White-throated Needletail [682]		Species or species habitat known to occur within area
Limosa lapponica		
Bar-tailed Godwit [844]		Species or species habitat
		known to occur within area
Macronectes giganteus		
Southern Giant-Petrel, Southern Giant Petrel [1060]	Endangered	Species or species habitat may occur within area
Macronectes halli		
Northern Giant Petrel [1061]	Vulnerable	Species or species habitat may occur within area
Merops ornatus		
Rainbow Bee-eater [670]		Species or species habitat may occur within area
Monarcha melanopsis		
Black-faced Monarch [609]		Species or species habitat known to occur within area
Monarcha trivirgatus		
Spectacled Monarch [610]		Species or species habitat known to occur within area
Myiagra cyanoleuca		
Satin Flycatcher [612]		Species or species habitat known to occur within area

Numenius madagascariensis

Eastern Curlew, Far Eastern Curlew [847]

Critically Endangered

Species or species habitat known to occur within area

known to occur within area

Numenius minutus Little Curlew, Little Whimbrel [848]

Numenius phaeopus Whimbrel [849] Foraging, feeding or related behaviour likely to occur within area

Foraging, feeding or related behaviour known to occur within area

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Name	Threatened	Type of Presence
Pachyptila turtur Fairy Prion [1066]		Species or species habitat known to occur within area
<u>Pandion haliaetus</u> Osprey [952]		Breeding known to occur within area
<u>Pluvialis fulva</u> Pacific Golden Plover [25545]		Foraging, feeding or related behaviour known to occur within area
<u>Pluvialis squatarola</u> Grey Plover [865]		Foraging, feeding or related behaviour known to occur within area
<u>Rhipidura rufifrons</u> Rufous Fantail [592]		Species or species habitat known to occur within area
<u>Rostratula benghalensis (sensu lato)</u> Painted Snipe [889]	Endangered*	Species or species habitat may occur within area
<u>Sterna albifrons</u> Little Tern [813]		Species or species habitat may occur within area
<u>Thalassarche bulleri</u> Buller's Albatross, Pacific Albatross [64460]	Vulnerable	Species or species habitat may occur within area
<u>Thalassarche cauta</u> Tasmanian Shy Albatross [89224]	Vulnerable*	Species or species habitat may occur within area
<u>Thalassarche eremita</u> Chatham Albatross [64457]	Endangered	Species or species habitat may occur within area
<u>Thalassarche impavida</u> Campbell Albatross, Campbell Black-browed Albatross [64459]	Vulnerable	Species or species habitat may occur within area
<u>Thalassarche melanophris</u> Black-browed Albatross [66472]	Vulnerable	Species or species habitat may occur within area
<u>Thalassarche salvini</u> Salvin's Albatross [64463]	Vulnerable	Foraging, feeding or related behaviour likely to occur within area
<u>Thalassarche sp. nov.</u> Pacific Albatross [66511]	Vulnerable*	Species or species habitat may occur within area
<u>Thalassarche steadi</u> White-capped Albatross [64462]	Vulnerable*	Foraging, feeding or related behaviour likely to occur within area

<u>Tringa nebularia</u>

Common Greenshank, Greenshank [832]

Xenus cinereus Terek Sandpiper [59300] Species or species habitat known to occur within area

within area

Foraging, feeding or related behaviour known to occur within area

Species or species habitat may occur within area

Reptiles

Mammals Dugong dugon

Dugong [28]

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Name	Threatened	Type of Presence
<u>Caretta caretta</u> Loggerhead Turtle [1763]	Endangered	Species or species habitat known to occur within area
<u>Chelonia mydas</u> Green Turtle [1765]	Vulnerable	Species or species habitat
Dermochelys coriacea		known to occur within area
Leatherback Turtle, Leathery Turtle, Luth [1768]	Endangered	Species or species habitat known to occur within area
<u>Eretmochelys imbricata</u> Hawksbill Turtle [1766]	Vulnerable	Species or species habitat known to occur within area
<u>Natator depressus</u> Flatback Turtle [59257]	Vulnerable	Breeding likely to occur
Whales and other Cetaceans		[Resource Information]
Name	Status	Type of Presence
Mammals		
Sousa chinensis		
Indo-Pacific Humpback Dolphin [50]		Species or species habitat likely to occur within area

Extra Information

Regional Forest Agreements	[Resource Information]
Note that all areas with completed RFAs have been incl	uded.
Name	State
North East NSW RFA	New South Wales
Invasive Species	[Resource Information]

Invasive Species

Weeds reported here are the 20 species of national significance (WoNS), along with other introduced plants that are considered by the States and Territories to pose a particularly significant threat to biodiversity. The following feral animals are reported: Goat, Red Fox, Cat, Rabbit, Pig, Water Buffalo and Cane Toad. Maps from Landscape Health Project, National Land and Water Resouces Audit, 2001.

Name	Status	Type of Presence
Birds		
Acridotheres tristis		
Common Myna, Indian Myna [387]		Species or species habitat likely to occur within area
Anas platyrhynchos		
Mallard [974]		Species or species habitat likely to occur within area

Carduelis carduelis European Goldfinch [403]

Columba livia Rock Pigeon, Rock Dove, Domestic Pigeon [803]

Lonchura punctulata Nutmeg Mannikin [399]

Species or species habitat likely to occur within area

Species or species habitat likely to occur within area

Species or species habitat likely to occur

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		T (D
Name	Status	Type of Presence
Passer domesticus		within area
House Sparrow [405]		Species or species habitat likely to occur within area
Pycnonotus jocosus Red-whiskered Bulbul [631]		Species or species habitat likely to occur within area
Streptopelia chinensis		·
Spotted Turtle-Dove [780]		Species or species habitat likely to occur within area
Sturnus vulgaris Common Starling [389]		Species or species habitat likely to occur within area
Turdus merula Common Blackbird, Eurasian Blackbird [596]		Species or species habitat likely to occur within area
Frogs		
Rhinella marina Cane Toad [83218]		Species or species habitat known to occur within area
Mammals		
Bos taurus Domestic Cattle [16]		Species or species habitat likely to occur within area
Canis lupus familiaris Domestic Dog [82654]		Species or species habitat likely to occur within area
Felis catus Cat, House Cat, Domestic Cat [19]		Species or species habitat likely to occur within area
Feral deer Feral deer species in Australia [85733]		Species or species habitat likely to occur within area
Lepus capensis Brown Hare [127]		Species or species habitat likely to occur within area
Mus musculus		
House Mouse [120]		Species or species habitat likely to occur within area
Rattus norvegicus Brown Rat, Norway Rat [83]		Species or species habitat likely to occur within area

Rattus rattus Black Rat, Ship Rat [84]

Species or species habitat likely to occur within area

Vulpes vulpes Red Fox, Fox [18]

Plants

Alternanthera philoxeroides Alligator Weed [11620]

Anredera cordifolia Madeira Vine, Jalap, Lamb's-tail, Mignonette Vine, Anredera, Gulf Madeiravine, Heartleaf Madeiravine, Potato Vine [2643] Species or species habitat likely to occur within area

Species or species habitat likely to occur within area

Species or species habitat likely to occur within area

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Name	Status	Type of Presence
Asparagus aethiopicus		
Asparagus Fern, Ground Asparagus, Basket Fern, Sprengi's Fern, Bushy Asparagus, Emerald Asparagus 62425] Asparagus plumosus		Species or species habitat likely to occur within area
limbing Asparagus-fern [48993]		Species or species habitat likely to occur within area
Cabomba caroliniana Cabomba, Fanwort, Carolina Watershield, Fish Grass, Vashington Grass, Watershield, Carolina Fanwort, Common Cabomba [5171] Chrysanthemoides monilifera		Species or species habitat likely to occur within area
Bitou Bush, Boneseed [18983]		Species or species habitat likely to occur within area
Chrysanthemoides monilifera subsp. rotundata		
Bitou Bush [16332]		Species or species habitat likely to occur within area
ichhornia crassipes		
Vater Hyacinth, Water Orchid, Nile Lily [13466]		Species or species habitat likely to occur within area
Genista sp. X Genista monspessulana		
Broom [67538]		Species or species habitat may occur within area
antana camara		
Lantana, Common Lantana, Kamara Lantana, Large- eaf Lantana, Pink Flowered Lantana, Red Flowered Lantana, Red-Flowered Sage, White Sage, Wild Sage 10892] Pinus radiata		Species or species habitat likely to occur within area
Radiata Pine Monterey Pine, Insignis Pine, Wilding Pine [20780]		Species or species habitat may occur within area
Rubus fruticosus aggregate		
Blackberry, European Blackberry [68406]		Species or species habitat likely to occur within area
Sagittaria platyphylla		
0elta Arrowhead, Arrowhead, Slender Arrowhead 58483]		Species or species habitat likely to occur within area
Salvinia molesta Salvinia, Giant Salvinia, Aquarium Watermoss, Kariba Veed [13665]		Species or species habitat likely to occur within area
Senecio madagascariensis Fireweed, Madagascar Ragwort, Madagascar		Species or species habitat

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Caveat

The information presented in this report has been provided by a range of data sources as acknowledged at the end of the report.

This report is designed to assist in identifying the locations of places which may be relevant in determining obligations under the Environment Protection and Biodiversity Conservation Act 1999. It holds mapped locations of World and National Heritage properties, Wetlands of International and National Importance, Commonwealth and State/Territory reserves, listed threatened, migratory and marine species and listed threatened ecological communities. Mapping of Commonwealth land is not complete at this stage. Maps have been collated from a range of sources at various resolutions.

Not all species listed under the EPBC Act have been mapped (see below) and therefore a report is a general guide only. Where available data supports mapping, the type of presence that can be determined from the data is indicated in general terms. People using this information in making a referral may need to consider the qualifications below and may need to seek and consider other information sources.

For threatened ecological communities where the distribution is well known, maps are derived from recovery plans, State vegetation maps, remote sensing imagery and other sources. Where threatened ecological community distributions are less well known, existing vegetation maps and point location data are used to produce indicative distribution maps.

Threatened, migratory and marine species distributions have been derived through a variety of methods. Where distributions are well known and if time permits, maps are derived using either thematic spatial data (i.e. vegetation, soils, geology, elevation, aspect, terrain, etc) together with point locations and described habitat; or environmental modelling (MAXENT or BIOCLIM habitat modelling) using point locations and environmental data layers.

Where very little information is available for species or large number of maps are required in a short time-frame, maps are derived either from 0.04 or 0.02 decimal degree cells; by an automated process using polygon capture techniques (static two kilometre grid cells, alpha-hull and convex hull); or captured manually or by using topographic features (national park boundaries, islands, etc). In the early stages of the distribution mapping process (1999-early 2000s) distributions were defined by degree blocks, 100K or 250K map sheets to rapidly create distribution maps. More reliable distribution mapping methods are used to update these distributions as time permits.

Only selected species covered by the following provisions of the EPBC Act have been mapped:

- migratory and
- marine

The following species and ecological communities have not been mapped and do not appear in reports produced from this database:

- threatened species listed as extinct or considered as vagrants
- some species and ecological communities that have only recently been listed
- some terrestrial species that overfly the Commonwealth marine area
- migratory species that are very widespread, vagrant, or only occur in small numbers

The following groups have been mapped, but may not cover the complete distribution of the species:

- non-threatened seabirds which have only been mapped for recorded breeding sites
- seals which have only been mapped for breeding sites near the Australian continent

Such breeding sites may be important for the protection of the Commonwealth Marine environment.

Coordinates

-31.436161 152.908569

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Acknowledgements

This database has been compiled from a range of data sources. The department acknowledges the following custodians who have contributed valuable data and advice:

-Office of Environment and Heritage, New South Wales -Department of Environment and Primary Industries, Victoria -Department of Primary Industries, Parks, Water and Environment, Tasmania -Department of Environment, Water and Natural Resources, South Australia -Department of Land and Resource Management, Northern Territory -Department of Environmental and Heritage Protection, Queensland -Department of Parks and Wildlife, Western Australia -Environment and Planning Directorate, ACT -Birdlife Australia -Australian Bird and Bat Banding Scheme -Australian National Wildlife Collection -Natural history museums of Australia -Museum Victoria -Australian Museum -South Australian Museum -Queensland Museum -Online Zoological Collections of Australian Museums -Queensland Herbarium -National Herbarium of NSW -Royal Botanic Gardens and National Herbarium of Victoria -Tasmanian Herbarium -State Herbarium of South Australia -Northern Territory Herbarium -Western Australian Herbarium -Australian National Herbarium, Canberra -University of New England -Ocean Biogeographic Information System -Australian Government, Department of Defence Forestry Corporation, NSW -Geoscience Australia -CSIRO -Australian Tropical Herbarium, Cairns -eBird Australia -Australian Government – Australian Antarctic Data Centre -Museum and Art Gallery of the Northern Territory -Australian Government National Environmental Science Program -Australian Institute of Marine Science -Reef Life Survey Australia -American Museum of Natural History -Queen Victoria Museum and Art Gallery, Inveresk, Tasmania -Tasmanian Museum and Art Gallery, Hobart, Tasmania -Other groups and individuals

The Department is extremely grateful to the many organisations and individuals who provided expert advice and information on numerous draft distributions.

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Appendix 4 Human and animal health

Human and animal health

Flying-foxes, like many animals, carry pathogens that may pose human health risks. Many of these are viruses which cause only asymptomatic infections in flying-foxes themselves but may cause significant disease in humans or other animals that are exposed. In Australia, the most well-defined of these include Australian bat lyssavirus (ABLV), Hendra virus (HeV) and Menangle virus. Specific information on these viruses is provided below.

Excluding those people whose occupations require contact with bats, such as wildlife carers and vets, human exposure to ABLV, HeV and Menangle virus, their transmission and frequency of infection is extremely rare. HeV infection in humans requires transfer from an infected intermediate equine host (i.e. close contact with an infected horse) and spread of the virus directly from bats to humans has not been reported.

These diseases are also easily prevented through vaccination, personal protective equipment, safe flying-fox handling (by trained and vaccinated personnel only) and appropriate horse husbandry. Therefore, despite the fact that human infection with these agents can be fatal, the probability of infection is extremely low, and the overall public health risk is also judged to be low (Qld Health 2016).

Disease and flying-fox management

A recent study at several camps before, during and after disturbance (Edson et al. 2015) showed no statistical association between HeV prevalence and flying-fox disturbance. However, the consequences of chronic or ongoing disturbance and harassment and its effect on HeV infection were not within the scope of the study and are therefore unknown.

The effects of stress are linked to increased susceptibility and expression of disease in both humans (AIHW 2012) and animals (Henry & Stephens-Larson 1985; Aich et. al. 2009), including reduced immunity to disease.

Therefore, it can be assumed that management actions which may cause stress (e.g. dispersal), particularly over a prolonged period or at times where other stressors are increased (e.g. food shortages, habitat fragmentation, etc.), are likely to increase the susceptibility and prevalence of disease within the flying-fox population, and consequently the risk of transfer to humans.

Furthermore, management actions or natural environmental changes may increase disease risk by:

- forcing flying-foxes into closer proximity to one another, increasing the probability of disease transfer between individuals and within the population.
- resulting in abortions and/or dropped young if inappropriate management methods are used during critical periods of the breeding cycle. This will increase the

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likelihood of direct interaction between flying-foxes and the public, and potential for disease exposure.

adoption of inhumane methods with potential to cause injury which would increase the likelihood of the community coming into contact with injured/dying or deceased flying-foxes.

The potential to increase disease risk should be carefully considered as part of a full risk assessment when determining the appropriate level of management and the associated mitigation measures required.

Australian bat lyssavirus

ABLV is a rabies-like virus that may be found in all flying-fox species on mainland Australia. It has also been found in an insectivorous microbat and it is assumed it may be carried by any bat species. The probability of human infection with ABLV is very low with less than 1% of the flying-fox population being affected (DPI 2013) and transmission requiring direct contact with an infected animal that is secreting the virus. In Australia three people have died from ABLV infection since the virus was identified in 1996 (NSW Health 2013).

Domestic animals are also at risk if exposed to ABLV. In 2013, ABLV infections were identified in two horses (Shinwari et al. 2014). There have been no confirmed cases of ABLV in dogs in Australia; however, transmission is possible (McCall et al. 2005) and consultation with a veterinarian should be sought if exposure is suspected.

Transmission of the virus from bats to humans is through a bite or scratch but may have potential to be transferred if bat saliva directly contacts the eyes, nose, mouth or broken skin. ABLV is unlikely to survive in the environment for more than a few hours, especially in dry environments that are exposed to sunlight (NSW Health 2013).

Transmission of closely related viruses suggests that contact or exposure to bat faeces, urine or blood does not pose a risk of exposure to ABLV, nor does living, playing or walking near bat roosting areas (NSW Health 2013).

The incubation period in humans is assumed similar to rabies and variable between two weeks and several years. Similarly, the disease in humans presents essentially the same clinical picture as classical rabies. Once clinical signs have developed the infection is invariably fatal. However, infection can easily be prevented by avoiding direct contact with bats (i.e. handling). Pre-exposure vaccination provides reliable protection from the disease for people who are likely to have direct contact with bats, and it is generally a mandatory workplace health and safety requirement that all persons working with bats receive pre-vaccination and have their level of protection regularly assessed. Like classical rabies, ABLV infection in humans also appears to be effectively treated using post-exposure vaccination and so any person who suspects they have been exposed should seek immediate medical treatment. Post-exposure vaccination is usually ineffective once clinical manifestations of the disease have commenced.

If a person is bitten or scratched by a bat they should:

wash the wound with soap and water for at least five minutes (do not scrub)

contact their doctor immediately to arrange for post-exposure vaccinations.

If bat saliva contacts the eyes, nose, mouth or an open wound, flush thoroughly with water and seek immediate medical advice.

Hendra virus

Flying-foxes are the natural host for Hendra virus (HeV), which can be transmitted from flyingfoxes to horses. Infected horses sometimes amplify the virus and can then transmit it to other horses, humans and on two occasions, dogs (DPI 2014). There is no evidence that the virus can be passed directly from flying-foxes to humans or to dogs (AVA 2015). Clinical studies have shown cats, pigs, ferrets and guinea pigs can carry the infection (DPI 2015a).

Although the virus is periodically present in flying-fox populations across Australia, the likelihood of horses becoming infected is low and consequently human infection is extremely rare. Horses are thought to contract the disease after ingesting forage or water contaminated primarily with flying-fox urine (CDC 2014).

Humans may contract the disease after close contact with an infected horse. HeV infection in humans presents as a serious and often fatal respiratory and/or neurological disease and there is currently no effective post-exposure treatment or vaccine available for people. The mortality rate in horses is greater than 70% (DPI 2014). Since 1994, 81 horses have died, and four of the seven people infected with HeV have lost their lives (DPI 2014).

Previous studies have shown that HeV spillover events have been associated with foraging flying-foxes rather than camp locations. Therefore, risk is considered similar at any location within the range of flying-fox species and all horse owners should be vigilant. Vaccination of horses can protect horses and subsequently humans from infection (DPI 2014), as can appropriate horse husbandry (e.g. covering food and water troughs, fencing flying-fox foraging trees in paddocks, etc.).

Although all human cases of HeV to date have been contracted from infected horses and direct transmission from bats to humans has not yet been reported, particular care should be taken by select occupational groups that could be uniquely exposed. For example, persons who may be exposed to high levels of HeV via aerosol of heavily contaminated substrate should consider additional PPE (e.g. respiratory filters), and potentially dampening down dry dusty substrate.

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Appendix 5 Survey results

The highest proportion of respondents were not aware of the community engagement for the development of the 'Kooloonbung Creek Nature Park Plan of Management' in 2012 (17) while the remaining had participated (11) or did know about it but had not participated (15) (Figure 1).

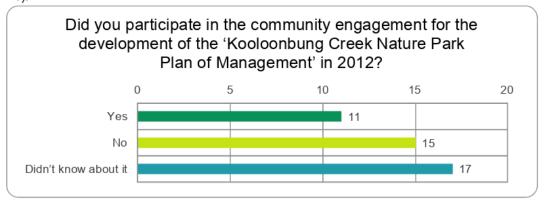


Figure 27 Responses regarding participation in the development of 'Kooloonbung Creek Nature Park Plan of Management' in 2012

Survey responses indicated that the majority were aware that the camp constitutes a Nationally Important Camp providing critical habitat (69.7% answered yes, 11.6% responded no and 18.6% did not care) (Figure 2).

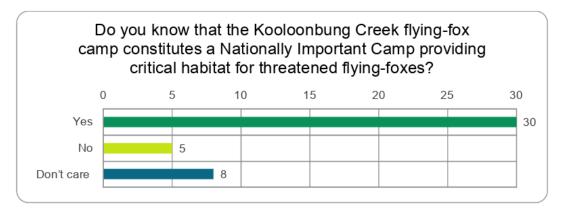


Figure 28 Knowledge of the classification of the camp as a Nationally Important Camp

The majority of respondents were aware that flying-foxes are a protected native species (39, 92.8%) while the remaining didn't care (Figure 3).

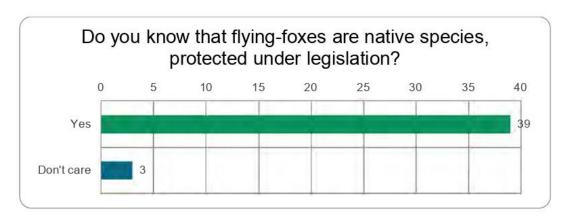


Figure 29 Knowledge of the status of flying-foxes

Similar results were provided regarding the species' role in long distance seed dispersal and pollination (86% answered yes, 6.9% responded no, 6.9% didn't care or understand the question). The majority of respondents are aware that disease can be prevented by not handling flying-foxes and through appropriate horse husbandry (83.3%), with the remaining answering no (Figure 4). When asked if the respondents knew that the GHFF is the main species of flying-fox that uses the Koolonbung creek camp site, 60.9% responded positively, while 21.9% did not know, and 17.1% didn't care.

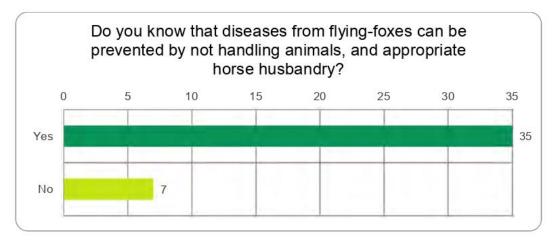


Figure 30 Disease from flying-foxes

Survey results indicated a majority of respondents held positive feelings towards the protection of flying-foxes (62.8%), with 37.2% indicating that it is not important to them (Figure 5).

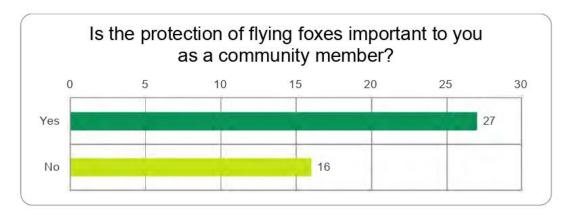


Figure 31 Protection of flying-foxes

54.7% of respondents indicated that their experience or interaction with flying-foxes in the Port Macquarie area has been negative, 35.7% responded positively and 1% were neither positive nor negative (Figure 6).

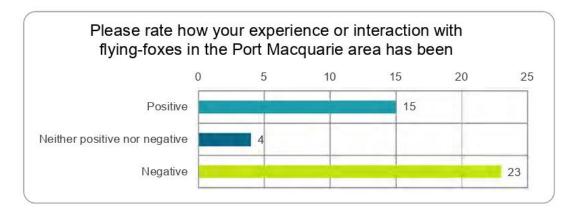


Figure 32 Experience with flying-foxes

Respondents who indicated that they are negatively impacted (23), were being most impacted around the home. These respondents identified the majority of impacts related to smell, excrement and noise (Figure 7). Twelve respondents (who selected 'other' in the survey) added 'quality of life', 'constant health issues', 'isolation due to family and friends not wanting to visit' and 'destruction of habitat for other species i.e. small birds' being of concern.

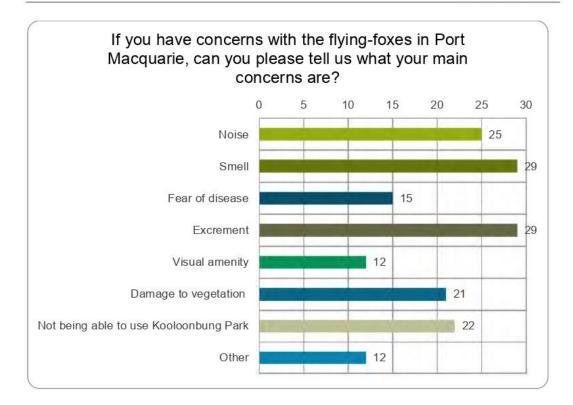


Figure 33 Main concerns about flying-foxes

In relation to costs associated with any management actions, the majority of respondents answered that is was moderately important that it was of low cost to ratepayers (48%), while 33% considered this not at all important, and 19% responded as very important (Figure 8).

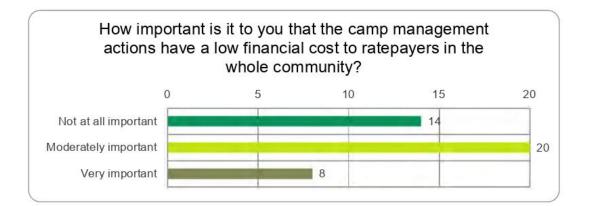


Figure 34 Financial impacts to ratepayers

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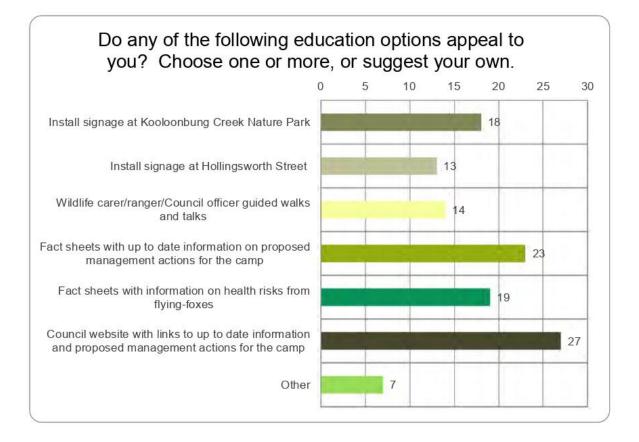


Figure 35 Educational options

Respondents were asked to choose one or more education options, or suggest their own. 'Council websites with links to up to date information and proposed management actions for the camp' received the highest number of votes (64%). The second-most highly rated option was 'fact sheets with up to date information on proposed management actions for the camp' (54%). All other options received similar ranking priority (Figure 9). Other suggested options were, 'action to reduce noise/removing the bats', 'cull a proportion of the flying-fox population' and for 'Friends of Kooloonbung Creek Nature Park' (FKCNP) to take guided walks through the park.

The majority of the respondents identified as 'not living within 100 metres of the camp' (67.4%). All (14) respondents answered 'no' when asked if more information was required concerning plants that may be attracting flying-foxes to their backyard. Of those 14 respondents, when asked 'Would receiving service subsidies help in reducing flying-fox impacts on your property if funding assistance was provided in some way?'; 9 answered 'yes' and 5 responded 'no'.

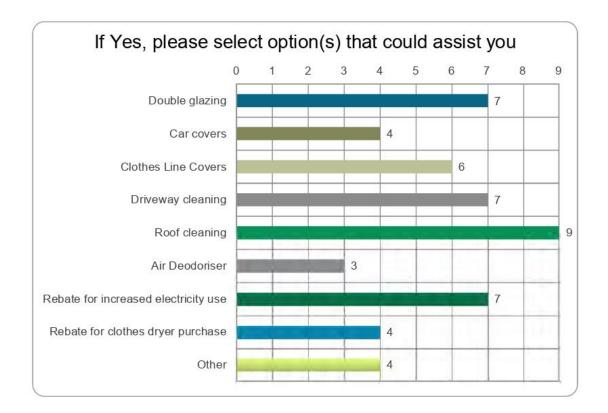


Figure 36 Management options to assist

In regards to which options could assist residents if funding assistance was provided in some way; roof cleaning had the most selections (9 selections from each of the 9 respondents) (Figure 10). The next rated options after roof cleaning were 'double glazing', 'driveway cleaning' and 'rebate for increased electricity use' with 7 votes each. Four respondents (who selected 'other' in the survey) added water rebate for hosing off outdoor areas, rebate for air-conditioner installation or solar panels to reduce electricity costs when residences must be closed due to odour/noise and rebate for regular cleaning of solar panels.

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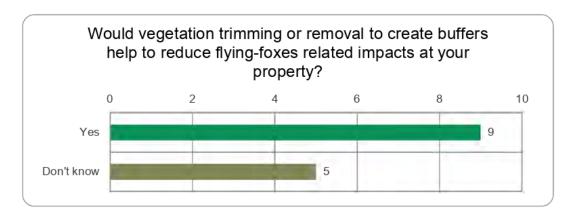


Figure 37 Buffer creation to reduce flying-fox related impacts

The majority of respondents (64.3%) answered 'yes' to would vegetation trimming or removal to create buffers would help to reduce flying-fox related impacts at their property (Figure 11). Nine respondents (who selected 'yes') added what street they live on to the survey. Addresses included; Lake Road, Glebe Close, Fischer Street and Anita Crescent.

Respondents who indicated that they are interested in learning more about management options (39), were most interested in managing impacts to amenities. Respondents selected 'improving the boardwalk to prevent slipping' as the most preferred management option (Figure 12). Nineteen respondents were interested to learn about canopy-mounted sprinklers.

Fourteen respondents (who selected 'other' in the survey) added *"relocate the colony"*, *"*change conditions to move flying-fox elsewhere", "clear roosting trees along the cutting path through Kooloonbung so that the walkway and bridge can be used without the fear of slipping in excrement", *"replant flying-fox roosts trees to replace those lost during the nudging process"*, "cull numbers of flying-fox", *"minimise the camp size"*, *"relocate the camp to state forests and national parks"*, *"clear the area of trees due to bushfire concerns"*, *"smoke bombs, low humming motors" and "develop an eradication program"*.

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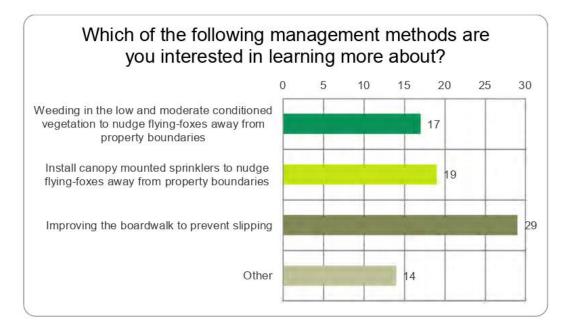


Figure 39 Management options that participants are interested in learning

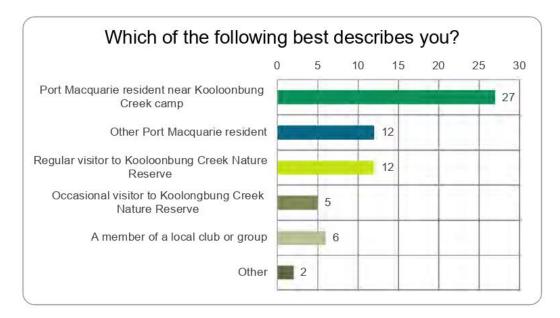


Figure 38 Survey distribution.

'Port Macquarie resident near Kooloonbung Creek camp' was the highest response group in the survey (62.8%) (Figure 13). 62.8% of participants were aged 50-75, 18.6% were 36-50, 13.9% were 76+ and 18-35 and >18 were both 2.3% of the participants.

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Table 9 Responses to question; Do any of the proposed management options not appeal to you? And if so which ones and for what reason?

Response type	Respondents comments
Remove flying-fox	"Just get rid of the problem".
Remove flying-fox	"NONE - get rid of them. All of these management options cost money and don't solve the problem".
Remove flying-fox	"Rebates for water, double glazing, etc. I want population reduced and none of the options listed will achieve that".
Remove flying-fox	"Weeding - can't see how this helps unless it involves removal of future roosting & paper bark tree seedlings".
Remove flying-fox	"I would to have them removed as the smell is repulsive".
Remove flying-fox	"In Kooloonbung there are 2 endangered things. The bat camp & the rainforest, & only one of those species can be moved & or minimised. One of your options mystifies me, what do you mean by weeding ?? To my mind bats are not what you would classify as ground foragers".
Remove flying-fox	"None of the service subsidies required if bat colony persuaded to live elsewhere. Financial grants, if I understand correctly, were made to combat the problems we have. Why was no action taken? Personally, I think incompetency at a higher level should not be left unuttered. Trimming, pruning to create a buffer would only hasten the demise of our beautiful arboreal worldWe don't need to axe the rain forest the bats execute the demise effortlessly".
Remove flying-fox	"I lived in Albury before moving to Port Macquarie. We had a colony of bats move into the Botanic Gardens. The council was given permission to move the bats to another location. This was done by making a lot of noise each morning with power tools, banging tin lids etc. The bats moved to another location that was not near any homes. I feel that the people who live near the bats must find the smell unbearable".
Remove flying-fox	"Sydney moved their population along, we can do the same, within the guidelines set out by N.S.W Gov. We do not propose to killing the bats, just moving them on to a less populated area".
Remove flying-fox / cull	"as above remove them completely or cull them. The national parks condone culling of brumbies and we are allowed to cull Kangaroos why is the fox different? they have a far worse impact".
Remove flying-fox / cull	"Don't see how it will work as the smell and flying foxes will still be there with the above options".
Remove flying-fox / cull	"Remove them completely. Thats it - get them out of there".
Remove flying-fox / cull	"No none of these address the real issue. Management is not on my agenda! Eradication is is is feasible and achievable!".
Management method	"This community problem has been well known for many years and the time has come for proactive responses. Council already has the views of 10,000 residents who have Petitioned for action. This survey must recognise those voices. Options limited to information about PROPOSED management actions are insufficient. Citizens now also need to know what has actually been done and how effective (or not) that has been". "Education alone does not solve the issue"
Management method	"Sprinklers - water costs money".
Management method	"Canopy-mounted sprinklers sounds like it would require a lot of infrastructure to be

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Response type	Respondents comments
	installed, this may be expensive and require vegetation to be removed or disturbed".
Management method	"Sprinklers or any other method that negatively impacts the bats' bodies, habitat or flight paths should not be used as this is a threatened species that needs as much protection as we can give it. While the bats like to roost above water, being wet could reduce their ability to keep warm and perhaps to fly and to forage. Peggy Eby told us that they are extremely loyal to their roost and are unlikely to desert it in spite of interference by humans. They occupy only a tiny part of the town and they "pay rent" in the form of the ecosystem services they deliver"
Management method	"FKCNP are already involved with the other management methods listed above".

Table 10 Further comments to survey from participants

Theme	Respondents comment
Relocate flying-fox	"Relocate them to a nonresidential or recreational area if possible".
Relocate flying-fox	"It is my understanding that the colony was moved from the Sea Acres NP in the eighties, because the NPWS did not like having them around. Back then, Kooloonbung Creek Nature Reserve was beautiful, with no fruit bat colony established until the Sea Acres 'eviction'. Additionally, back in those days, the water in Kooloonbung Creek was more brackish than it is now. The ever-increasing discharge of treated effluent into Kooloonbung Creek has diluted the naturally-occurring brackish water in the inter-tidal zone. This has provided the colony with a ready supply of drinking water that has provided the foundation for the fruit bat colony to grow more than it otherwise might have been. The fruit bat colony has ruined the amenity of a beautiful area, and it is my opinion that they should be moved from that location. Council should at least salinate the water they discharge into the creek so that it does not alter the natural state of the inter-tidal zone".
Relocate flying-fox	"I want Council \$ spent on reducing population of FFs in KCR, not on education or writing management reports with no tangible outcomes".
Relocate flying-fox	"Don't hold much hope of a council solution to relocating bats, there are avenues for this, but think the problem has become too complicated for this council. Another tourist venue destroyed in PM, only ones these days are the khaki hat brigade. No longer a tranquil spot for wedding photos etc. used to be a wonderful walk through boardwalk with visitors, no longer"!
Relocate flying-fox	"I am sure the bats will move on to another site when it suits them, when is another question".
Relocate flying-fox	"Please do something urgently, they have moved and settled into our area in the last 12 months"!!
Relocate flying-fox	"GET RID OF THEM!!!!! WE WANT OUR ENVIRONMENT BACK SO WE CAN USE IT AND BE SAFE".
Relocate flying-fox	"I have excrement drop on my house and driveway. There are palm trees, next door, which attract the bats. The removal of these non-native plants would help, in my case. The decline of smaller native birds is very noticeable, since the bats took over the reserve. I will not use the boardwalk, since the bats took over. It used to be a special place for me. My visitors cannot enjoy the renovated picnic area, because of the noise, smell and the excrement. There have been cases where other N.S.W Councils have moved the bats on, we should be able to do the same".
Engagement process	"How is the community to comment if they do not happen to log into the website. I have not seen any notification in the local paper.

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Theme	Respondents comment
	As a resident who has already expressed concern on these issues it is important to know that community engagement has been openly sought before the deadline closes".
Engagement process	"Proof read your surveys before sending them out! Too repetitive".
Engagement process / relocate flying-fox	"Your survey is false and deliberately based on lies at worst, misinformation at best. There are plenty of colonies that can be studied in this area, Wingham to Kempsey, and NSW Govt will assist with funding to relocate inappropriately located colonies. The time frame for this survey seems very short, deliberately structured to sabotage the effectiveness of this exercise. You should be ashamed. Don't you understand your responsibility as a council servant".
Park closure	"e.g Singleton area - They had to close their Park & some Businesses due to flying foxes? is this going to happen in Port Macquarie and who pays for this? Our main street and area will be abandon".
Health problems	"This year has caused health problems which I have discussed with my G.P. I am a non- smoker. I have a permanent cough and have been under great stress through lack of/disturbed sleep, sore ears from the use of ear plugs and general depression at not being able to enjoy the lifestyle in our own backyard. Our interstate visitors are not keen to return to Port because of the strong bat smell in this area; commenting on why a beautiful holiday destination is so spoilt by the stench of the flying foxes
Flying-fox advocate	"I would like to see the retention of this naturally occurring FF population within KCNP. As a rate-payer with a 17 year affiliation with the Park, I know that the FF population numbers and species are variable, and truly hope that no drastic action is taken to remove the colony to appease relatively short-term residents who chose to live next to a bushland area and wildlife corridor".
Flying-fox advocate	"if people had the opportunity to interact like the koala hospital they would realise there is more to them then the smell".
Flying-fox advocate	"To have a colony of wild animals within the township is an asset and already a tourist attraction which could be better managed and promoted. How about a Flying Fox Cafe nearby with fruit specialties on the menu, souvenirs and photos? Let's promote these gentle, intelligent and intriguing animals the way we promote our koalas. We need more signage to educate people about the bats' role in making our forests. Encourage people to wash their hands and clean their shoes after walking through, while explaining the facts about possible disease risks to address fear and disinformation. Replacing the timber boardwalk with the same black fibreglass mesh used at Sea Acres and parts of Kooloonbung would greatly improve safety and allow droppings to wash off because they can make the boardwalk slippery. If walkers are still concerned about droppings, suggest they use an umbrella or choose an alternative walking route, e.g. beside Lake Road. The proximity of homes to the bat roost is due to development being permitted too close to the creek: this is the fault of humans, not the bats, which are suffering from habitat loss throughout their range, exacerbated by climate change".
Flying-fox advocate	"FKCNP strongly support the protection of the Flying Fox camp in Kooloonbung Creek NP. Although some trees may be adversely affected by the FF's, there is also much evidence of regenerating native plants in the Reserve as a result of the FF foraging through and beyond the Reserve. FKCNP recognises the critical role that FF's play in maintaining healthy ecological processes in native forests. Having said this our group acknowledges the significant issues affecting immediate neighbours to the Reserve and is keen to work with Council and these residents to develop management actions which may alleviate some of the problems faced by the residents while at the same time minimising major disturbance to the FF camp".
Flying-fox advocate	"I think the Bats are wonderful creatures who have a vital role to play in the Eco System & I hope we have a win win situation for both bats & man in this survey".
Support for reserve	"Please save the Reserveit is unique to Port Macquarie and should be available for all to enjoy".

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Theme	Respondents comment
Support for education	"I applaud the educational approach adopted by the council, education is the best option to work with people who live in the area".
Support for education	"We have neighbors that purposely disturbed the bats regularly making banging sounds with pots and pans and whistles they wake the bats in the day it's realy annoying as the bats are usually fine sleeping in the day, when they are right at our back fence I have to use the dryer or hang clothes inside but I'm happy having the bats there in fact I believe we are lucky to be able to live near them and have a duty to protect them as we are the ones who have destroyed allot of their natural habitat anyway. Just wish there was more education for those who live on the creek and penalties for those who disturb the bats".
Support for education	"I understand the difficult nature of flying fox camps in close proximity to houses but very strongly support community education regarding the importance of these species on long term forest health as well as the least invasive methods to gently nudge them away from roosting close to homes".
Council responsiveness	 "1 - This community problem has been well known for many years and the time has come for proactive responses. Spending more years on education without action to control the adverse impacts is not sufficient. Council already has the views of 10,000 residents who have Petitioned for action. This survey/report to Council must also recognise those very patient voices. 2 - The cost of attending to the problem has to be considered in the same way that all worthwhile Council initiatives are considered in the Budget - cost effective activity that addresses a recognised need. Looking for a 'low financial cost' as this survey suggests should not be the approach for any project - identifying best value outcomes is important. 3 - Intending to seek funding from the State Government, without mentioning in this survey, the relevance of the State Government's Code seems odd. 4 - To engage, in the future, only with the community 'around a camp' is limiting. Flying-foxes have impacted the broader community including tourism operators, community groups and walkers. Broader engagement is required. 5 - Engaging and educating community members is inadequate where it relates only to some input and little after that. Council should commit to: informing the community of decisions taken; planned actions and timeframes; performance measures and the results of the actions. A considered management plan could do this. 6 - All the options identified in the NSW Government's Draft Code of Practice Authorising Flying-Fox Camp Management Actions 2018 including the many practical options not identified in this survey, must be considered".
Council responsiveness	"I encourage Council to effectively deal with this problem in the near future as residents, particularly those close to Koolongbung Creek, have suffered the effects for far too long".
Council responsiveness	"We hold council totally responsible for the terrible situation we are now in. If council had acted many years ago we would not be in this position. council now needs to act with URGENCY".
Council responsiveness	"Yes, please get on with it, please don't just have this survey for some people in Council to justify their job or position, & seem to say, look what we are doing to help. You can if you wish, contact me if you need any clarification on my thoughts. Thank you for the opportunity

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Appendix 6 Standard measures to avoid impacts to flying-foxes

The following mitigation measures will be complied with at all times during implementation of any activities within or immediately adjacent the camp.

- All personnel will be appropriately experienced, trained and inducted. Induction will include each person's responsibilities under this Plan.
- All personnel will be briefed prior to the action commencing each day and debriefed at the end of the day.
- Works will cease and OEH consulted in accordance with the 'stop work triggers' section of the Plan.
- · Large crews will be avoided where possible.
- The use of loud machinery and equipment that produces sudden impacts/noise will be limited. Where loud equipment (e.g. chainsaws) is required they will be started away from the camp and allowed to run for a short time to allow flying-foxes to adjust.
- Activities that may disturb flying-foxes at any time during the year will begin as far from the camp as possible, working towards the camp gradually to allow flying-foxes to habituate.
- Any activity likely to disturb flying-foxes so that they take flight will be avoided during the day during the sensitive GHFF/BFF birthing period (i.e. when females are in final trimester or the majority are carrying pups, generally August December) and avoided altogether during crèching (generally November/December to February). Where works cannot be done at night after fly-out during these periods, it is preferable they are undertaken in the late afternoon close to or at fly-out. If this is also not possible, a person experienced in flying-fox behaviour will monitor the camp for at least the first two scheduled actions (or as otherwise deemed to be required by that person) to ensure impacts are not excessive and advise on the most appropriate methods (e.g. required buffer distances, approach, etc.).
- OEH will be immediately contacted if LRFF are present between March and October or are identified as being in final trimester / with dependent young.
- Non-critical maintenance activities will ideally be scheduled when the camp is
 naturally empty. Where this is not possible (e.g. at permanently occupied camps)
 they will be scheduled for the best period for that camp (e.g. when the camp is
 seasonally lower in numbers and breeding will not be interrupted, or during the nonbreeding season, generally May to July).
- Works will not take place in periods of adverse weather including strong winds, sustained heavy rains, in very cold temperatures or during periods of likely population stress (e.g. food bottlenecks). Wildlife carers will be consulted to determine whether the population appears to be under stress.

- Works will be postponed on days predicted to exceed 35°C (or ideally 30°C), and for one day following a day that reached ≥35°C. If an actual heat stress event has been recorded at the camp or at nearby camps, a rest period of several weeks will be scheduled to allow affected flying-foxes to fully recover. See the OEH fact sheet on Responding to heat stress in flying-fox camps.
- Any proposed variations to works detailed in the Plan will be approved, in writing, by OEH before any new works occur.
- OEH may require changes to methods or cessation of management activities at any time.
- Ensure Level 2 management actions and results are recorded to inform future planning. See the OEH fact sheet on Monitoring, evaluating and reporting.

Vegetation trimming/removal (if required)

- · Dead wood and hollows will be retained on site where possible as habitat.
- Vegetation chipping/mulching is to be undertaken as far away from roosting flyingfoxes as possible (at least 100 m).

Canopy vegetation trimming/removal (if required)

Prior to works

• Trees to be removed or lopped will be clearly marked (e.g. with flagging tape) prior to works commencing, to avoid unintentionally impacting trees to be retained.

During works

- Any tree lopping, trimming or removal is undertaken under the supervision of a suitably qualified arborist (minimum qualification of Certificate III in Horticulture (Arboriculture) who is a member of an appropriate professional body such as the National Arborists Association) (e.g. Highland Arbor; Chris Watchirs).
- Trimming will be in accordance with relevant Australian Standards (e.g. AS4373 Pruning of Amenity Trees), and best practice techniques used to remove vegetation in a way that avoids impacting other fauna and remaining habitat.
- No tree in which a flying-fox is roosting will be trimmed or removed. Works may
 continue in trees adjacent to roost trees only where a person experienced in flyingfox behaviour assesses that no flying-foxes are at risk of being harmed. A person
 experienced in flying-fox behaviour is to remain on site to monitor, when canopy
 trimming/removal is required within 50 metres of roosting flying-foxes.
- While most females are likely to be carrying young (generally September January) vegetation removal within 50 metres of the camp will only be done in the evening after fly-out, unless otherwise advised by a flying-fox expert.
- Tree removal as part of management will be offset at a ratio of at least 2:1. Where threatened vegetation removal is required, the land manager will prepare an Offset

Strategy to outline a program of restoration works in other locations (in addition to existing programs). The strategy will be submitted to OEH for approval at least two months prior to commencing works.

Bush regeneration

- All works will be carried out by suitably qualified and experienced bush regenerators (i.e. Landcare groups), with at least one supervisor knowledgeable about flying-fox habitat requirements (and how to retain them for Level 1 and 2 actions) with knowledge regarding working under a camp.
- Vegetation modification, including weed removal, will not alter the conditions of the site such that it becomes unsuitable flying-fox habitat for Level 1 and 2 actions.
- Weed removal should follow a mosaic pattern, maintaining refuges in the mid- and lower storeys at all times.
- Weed control in the core habitat area will be undertaken using hand tools only (or in the evening after fly-out while crèching young are not present).
- Species selected for revegetation will be consistent with the habitat on site, and in buffer areas or conflict areas should be restricted to small shrubs/understorey species to reduce the need for further roost tree management in the future.

Stop work triggers

Management activities in or near Kooloonbung Creek camp will cease and will not recommence without consulting OEH if:

- any of the animal welfare triggers occur on more than two days during the program, such as unacceptable levels of stress (Table 10)
- there is a flying-fox injury or death
- a new camp/camps appear to be establishing
- · impacts are created or exacerbated at other locations
- there appears to be potential for conservation impacts (e.g. reduction in breeding success identified through independent monitoring)
- · standard measures to avoid impacts cannot be met.
- · Management may also be terminated at any time if:
 - unintended impacts are created for the community around the camp
 - allocated resources are exhausted.

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Table 11 Planned action for potential impacts during any works under or near the camp. A person with experience in flying-fox behaviour (as per Appendix 6) will monitor for welfare triggers and direct works in accordance with the criteria below.

Welfare trigger	Signs	Action
Unacceptable levels of stress	If any individual is observed:	Works to cease for the day.
Fatigue	 In-situ management more than 30% of the camp takes flight individuals are in flight for more than 5 minutes flying-foxes appear to be leaving the camp 	In-situ management Works to cease and recommence only when flying-foxes have settled* / move to alternative locations at least 50 m from roosting animals.
Injury/death	 A flying-fox appears to have been injured/killed on site (including aborted foetuses) dependent/crèching young present and adults likely to take flight or abandoned camp 	Works to cease immediately and OEH notified AND rescheduled OR adapted sufficiently so that significant impacts (e.g. death/injury) are highly unlikely to occur, as confirmed by an independent expert OR stopped indefinitely and alternative management options investigated.

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Appendix 7 Community feedback

Table 12 Community feedback from consultative meeting 2

Level	Theme	Actions to prioritise	Actions to disregard	How or where to implement
	Education and awareness	Information on removal of non- natives		Friends of Kooloonbung do this in Park however, need to educate residents of home removal of weeds including tobacco and Cocos palm
		Kooloonbung Friends educate at schools		
		Update information sheets on flying-fox disease		
		Tourism		
		Information on removal of non- native		Particularly those that are sources of bat food
		Signage at Kooloonbung Park		
		Ban barbed wire in residential areas and black monofilament netting		Promote wildlife friendly netting
		Priority – facts sheets on flying-fox disease risk		
		 Information on council website 		
		 Advice on tree trimming 		
		Medium – signage at Kooloonbung Creek - Wildlife educator		
		 Information on removal of non- natives 		
		Information on removal of non- native Advice on tree trimming or removal		Removal of blossoming trees close to housing Removal of roosting trees close to housing
		Information on removal of non- native	Signage in Kooloonbung	
		Advice on tree trimming or removal	Fact sheets on flying-fox disease risk	
			information on council website wildlife educator at schools or clubs	
			Signage already there	

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evel	Theme	Actions to prioritise	Actions to disregard	How or where to implement
			Signage not required	
			Wildlife educator	
	Property	priority		
	acquisition	VV		Must be at top of market value so as not disadvantage residents
	Level 1		Level 1 management actions mostly completed refer 2012 KCPoM	
	Create alternative habitat	V		Alternative camp corner of Lake Road and Ocear Drive – mostly paperbark vegetations
	Create alternative roost sites	Priority		Look at expanding planting at the Hatch as Alternative Roost Site
	Subsidies	Air-conditioning – who pays power bill?		
		High pressure hosing		but more to do with soun and important to have shock value
		Car / clothes line / pool covers priority		
		Air deodorisers, high pressure hosing, air -conditioning, double glazed windows all priorities		rate and electricity subsidies
		Car / clothes line / pool covers, Air deodorisers, high pressure hosing, air -conditioning, double glazed windows		
		High pressure hosing – consider for cleaning back patio		
		Subsides for all air- conditioning		
		Water subsidy for daily washdown		
		Water subsidy should be in place		
		High pressure hosing, air- conditioning, double-glazed windows		Would consider this a good option to try. Would allow us to breath at

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Level	Theme	Actions to prioritise	Actions to disregard	How or where to implement
				night. Think this would assist with noise control to help us get some sleep and could actually have visitors at our home
		Double-glazed windows would help with noise		
			Air deodorisers – impossible as the smell covers a huge area outside making living on back areas impossible	
			Car / clothes line / pool covers	
2	Buffers	Canopy-mounted sprinklers useful to shift bats near around homes		Near affected homes
		Canopy-mounted sprinkler; idle diesel generator motor; air compressor noise		
		Canopy-mounted sprinkler high priority seems to have worked in other areas		
		Canopy-mounted sprinkler		
		Canopy-mounted sprinkler, vegetation trimming and removal		Need to address tree choice. Less roosting trees and food sources in the park. There are huge areas of blossom down the road in the industrial area.
		Will consider canopy-mounted sprinkler, vegetation trimming, vegetation removal and acoustic barrier		
		Canopy-mounted sprinkler and vegetation removal		Bats within5 metres of back fence in tall paperbark trees
		Vegetation trimming - maybe		
			Vegetation removal not feasible otherwise compromise reserve values	
			Removal of a narrow band of trees would affect other wildlife	

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Level	Theme	Actions to prioritise	Actions to disregard	How or where to implement
			Barriers could affect movement of koalas	
		Acoustic barrier but shock value as with Royal Melbourne Botanical Gardens		
				Concerned that shadiest roost area left is along the rear of houses – other areas have lost canopy cover and therefore heat stress concern

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Revision No.	Revision date	Details	Prepared by	Reviewed by	Approved by
00	4/12/2018	18 PR3995 Kooloonbung Creek Emily Hatfield, Senior Jess Brac Camp Management Plan Wildlife Biologist Biologist		Jess Bracks, P Biologist	rincipal Wildlife
01	04/01/2018	······································		Julie Whelan, Senior Environmental Scientist	
02	06/03/2019	PR3995 Kooloonbung Creek Camp Management Plan.DR3			rincipal Wildlife
03	22/03/2019	PR3995 Kooloonbung Creek Camp Management Plan.DR4	Emily Hatfield, Senior Wildlife Biologist	nior Jess Bracks, Principal Wildlif Biologist	

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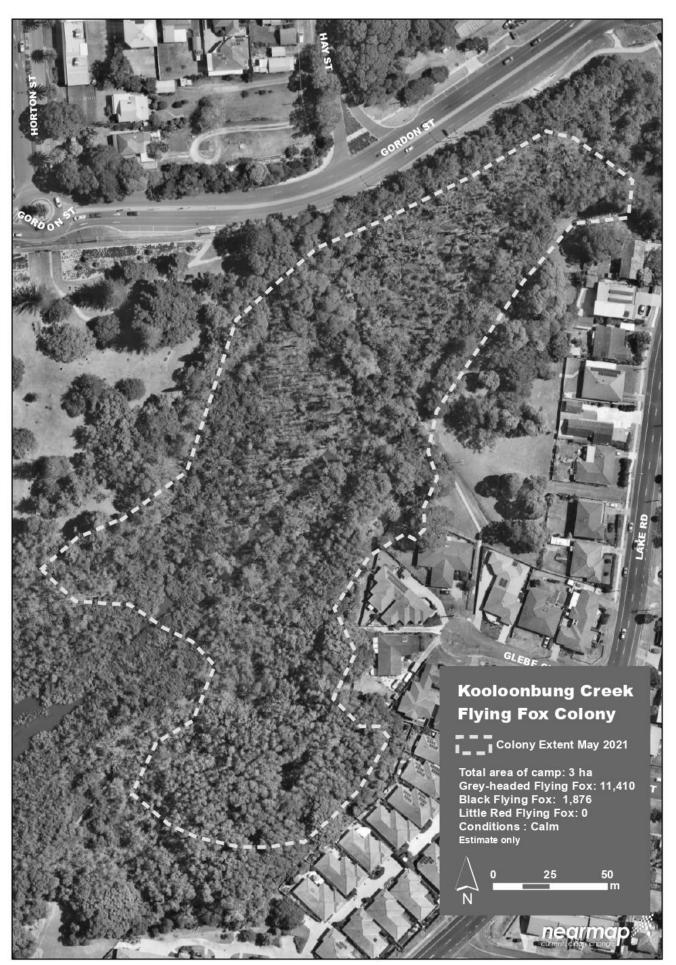
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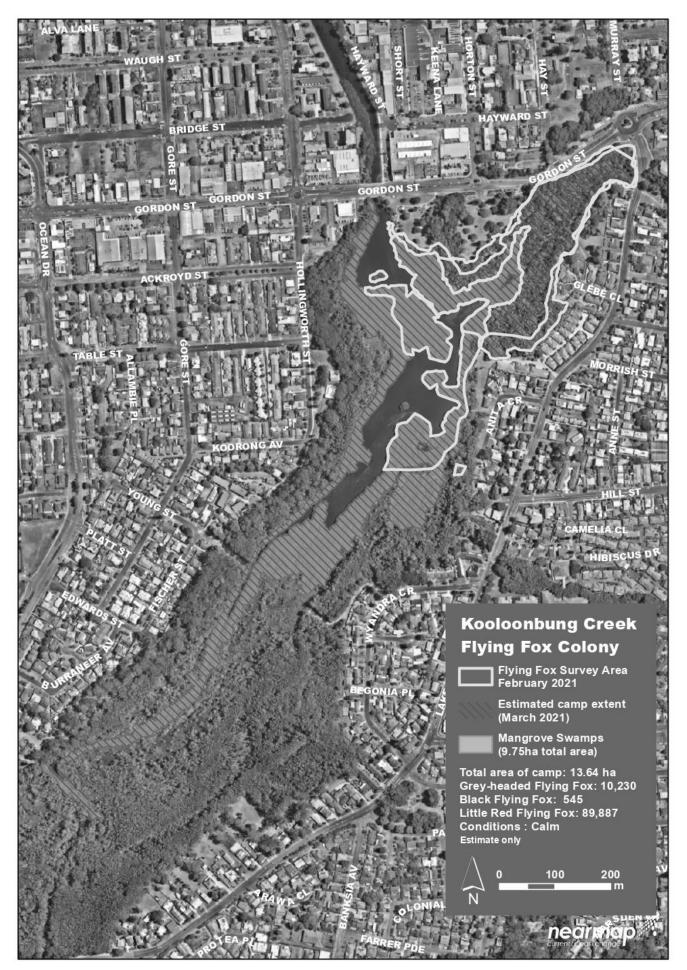
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Kooloonbung Creek Flying Fox Camp Noise Assessment

Information Brochure 2021

Background

The Kooloonbung flying-fox camp was established in the 1990s and is occupied by three different species: the Grey-headed Flying-fox (*Pteropus poliocephalus*), the Black Flying-fox (*P. alecto*) and the Little Red Flying-fox (*P. scapulatus*). The Grey-headed Flying-fox is a threatened species listed as Vulnerable to extinction under both the NSW *Biodiversity Conservation Act 2016* and the Commonwealth *Environment Protection and Biodiversity Conservation Act 1999*. This legal status provides protection to this species and their habitat.

The behaviour of flying-foxes means that the number of individuals present in a camp at any given time can change significantly. Camps often occur in favourable habitat and near food resources. Changes in availability and seasonality of these food resources can lead to changes in the number of individual flying-foxes present in a camp. Flying-fox camps may be permanently occupied by some individuals, but only seasonally or temporarily visited by others. For these reasons, the number of individual flying-foxes present in a camp can change quite significantly depending on the time of year. Counts of flying-foxes in the Kooloonbung Creek camp carried out over several years show that the number of individuals in the camp can range from around 1,000 flying-foxes over a relatively small area to around 170,000 flying-foxes spread over 77 hectares.

Flying-foxes can make a lot of screeching noises during the day and throughout the night. They are typically most noisy when they leave at dusk to feed and when they return at dawn to find a roost. The greatest noise impacts to residents tend to occur at dawn roosting times. During the day-time flying-foxes continue to make some noise, and are noisiest when stressed or scared, such as when they are disturbed by people (e.g. people mowing the lawn). At night-time they can also be noisy if there is a small group of flying-foxes feeding on fruiting or flowering trees, including palm trees.

What was the Purpose of the Noise Assessment?

The Kooloonbung Creek Flying-fox Camp Management Plan prepared in 2019 provides a number of management actions that aim to manage community concerns in relation to the camp whilst at the same time conserving flying-foxes and their habitat. Amongst other things, the Plan recommends a combination of property modifications and land use planning to manage noise impacts to residents of existing and proposed development near the camp.

The purpose of the noise assessment project was to quantify and map the noise impacts of the flying-fox camp and to develop recommendations for potential property modifications to manage noise impacts based on the extent to which individual properties are impacted. It is intended that the noise assessment provide a scientific foundation for decision making by Council and residents.

Noise Assessment Methodology

The noise assessment was carried out by a team of specialist acoustic engineers and involved the following steps:

- Monitoring of the actual noise levels around the camp over a period of seven days in February 2021, which is
 the time of year the camp is usually at its largest;
- Counts of the number of flying-foxes in the camp were also undertaken at the start and end of this period to verify the noise model. There were around 26,500 individuals on the 9 February and 57,000 on 18 February 2021;
- 3D computer-based noise modelling based on the measured noise levels, number of flying-foxes present, site topography and taking into account the location of buildings in the study area. Two scenarios were considered – a typical camp represented by the average camp size for February 2021 and a 'worst case scenario' adopting a camp size of 170,000 individuals;
- Identification of a suitable internal noise objective, being the noise level beyond which noise mitigation would be recommended;
- Generation of noise contour maps for each scenario showing the level of noise impact in different locations; and
- Identification of property modifications that would potentially be effective in reducing the flying-fox noise levels inside residential dwellings (in particular bedrooms) to meet the internal noise objective.

Noise Assessment Methodology (cont.)

Noise is the sound pressure level measured in decibels (dB), and overall noise includes sounds with frequencies in the audible hearing range. For humans, assessments normally consider the overall noise level for sounds between the 63 Hz and 8,000 Hz single octave bands. Humans are generally more sensitive to higher frequencies of noise, such as those made by flying-foxes (at 2,000-4,000 Hz).

There are no guidelines specific to assessing impacts of animal noise on humans, and therefore the noise assessment considered the NSW Department of Planning's Development Near Rail Corridors and Busy Roads -Interim Guidelines (2008). Residents have advised that the flying-foxes are noisiest at dawn and this was supported by the noise measurements, which found they were noisiest between 5:45AM and 6AM. For this reason, the criteria from the Guidelines relating to sleep areas was adopted as the internal noise objective for this assessment. The Guideline recommends that noise levels in sleeping areas be limited to 35 dBA where feasible so as to minimize the risk of occupants being woken from sleep.

	_		1
	120	Jet take off at runway edge	
Painful	110	Rock concert	
Faillu	100	225mm angle grinder at 1 metre	
	90	Heavy industrial factory interior	Table 1. Range
	80	Shouting at 1 metre	of typical Noise Levels, dBA
Noiov	70	Freeway at 20 metres	relative to 20 microPascals
NOISY	60	Normal conversation at 1 metre	
	50	A running refrigerator	
Quiet	40	Office air-conditioning, background noise level in a standard place of worship	
	20	Whisper, broadcast studio	
	10	Human breathing at 3 metres	
quiet	0	Threshold of typical hearing	
	Painful Noisy Quiet Very Quiet	Painful 110 100 90 90 80 Noisy 70 60 50 Quiet 40 Very 20 Quiet 10	Painful 110 Rock concert 100 225mm angle grinder at 1 metre 90 Heavy industrial factory interior 80 Shouting at 1 metre 70 Freeway at 20 metres 60 Normal conversation at 1 metre 50 A running refrigerator Quiet 40 Office air-conditioning, background noise level in a standard place of worship Very Quiet 10 Human breathing at 3 metres

Noise assessment results

Figures 1 and 2 show the results of the noise modelling. Figure 1 shows the results for Scenario 1, which corresponds to the average camp size during the noise monitoring period. Figure 2 (see overpage) shows the results for Scenario 2, which is the 'worst case scenario' of a maximum camp size.

The extent of the camp adopted in the noise model is shown in blue hatching on the figures. The colourcoded contours show the noise levels.

The results show that dwellings close to the parts of the camp with larger numbers of flying-foxes are being impacted by noise levels between 55-75 dBA.

It is noted that, based on a typical residential dwelling construction, noise levels as measured at the façade of a building may be up to around 20 dBA lower inside the building with the windows and doors closed. That is, if the noise levels at the façade of the building are 55dBA, the internal noise levels would likely achieve the internal noise objective of 35 dBA.

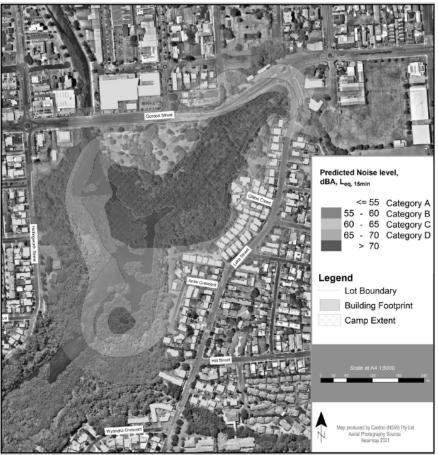
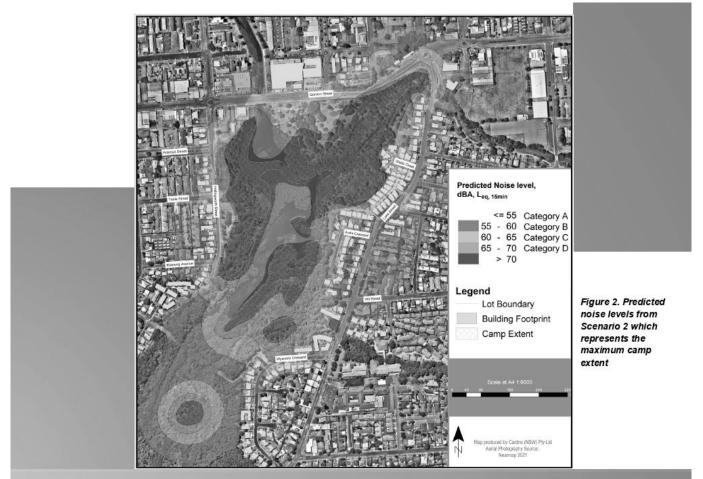


Figure 1. Predicted noise level from Scenario 1 which represents the average camp extent

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What can be done to reduce the impacts of noise on residents?

Property modifications can be undertaken to reduce the impact of noise inside a building. Due to the time of day the flying-foxes disturb residents, it was considered appropriate to target sleeping areas, as this is where the greatest benefit to residents can be achieved. These include:

- Window glazing, with thicker glazing achieving greater noise reductions;
- Using acoustic seals where noise could get in, such as around windows and doors; and
- Installation of insulation in the ceiling void.

To ensure the property modifications are effective, windows and doors should be closed, which may result in the need for mechanical ventilation (e.g. air conditioning) to provide fresh air. Dwellings with mechanical ventilation may have increased energy consumption and potentially higher electricity bills. There is also potential for any air conditioning units (or similar devices) to create additional noise nuisance to nearby residences. It may be necessary to shield any new air conditioners or locate them so that they cannot be heard inside neighbouring residences.

Based on the noise modelling results, the required property modifications to achieve the internal noise objective for different levels of external noise impact are provided in Table 2. The building treatment categories in Table 2 align with the noise mapping in Figures 1 and 2:

- Residences located within the orange or red contours are Category D;
- Residences located within the yellow zone are Category C;
- Residences within the green zone are Category B; and
- Residences in all other areas are Category A.

It is recommended that typical glazing in the range of 4-6 mm be avoided for buildings that are most affected by noise from flying-foxes. This is because it does not perform as well for noise frequencies similar to that made by flying-foxes. For buildings that are more impacted by noise, glazing of 10-10.38 mm thickness may provide better outcomes while avoiding more expensive options such as double glazing or upgrading of the external walls and ceilings.

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What can be done to reduce the impacts of noise on residents? cont.

Table 2. Recommended property modifications to reduce noise impact. The weighted sound reduction index (Rw), is the single number acoustic rating for sound transmission loss, determined by laboratory testing in accordance with ISO 717-1. It is used to compare the acoustic performance for different partitions. A higher Rw rating indicates better acoustic performance (that is, more noise reduction) than a lower Rw rating.

Category	External noise level	Glazing treatment	Building Façade Treatment		
	Existing residential development – Sleeping areas				
A	<55 dBA	No treatment needed	No treatment needed		
В	55-60 dBA	Rw 29 – 6mm float glass with acoustic seals	Seal gaps and holes		
с	60-65 dBA	Rw 31 - 6.38mm laminate glass with acoustic seal	Seal gaps and holes Insulation to ceiling void		
D	>65-70 dBA	Rw 35 - 10.38mm laminate glass with acoustic seals	Seal gaps and holes Insulation to ceiling void		
	Future residential development – Sleeping areas				
A	<55 dBA	No treatment needed	No treatment needed		
в	55-60 dBA	Rw 29 – 6mm float glass with acoustic seals	External wall system to achieve Rw 35 External roof/ceiling system to achieve Rw 35		
с	60-65 dBA	Rw 31 - 6.38mm laminate glass with acoustic seal	External wall system to achieve Rw 38 External roof/ceiling system to achieve Rw 38		
D	>65 dBA	Rw 35 - 10.38mm laminate glass with acoustic seals	External wall system to achieve Rw 40 External roof/ceiling system to achieve Rw 38		

How do I find out more information?

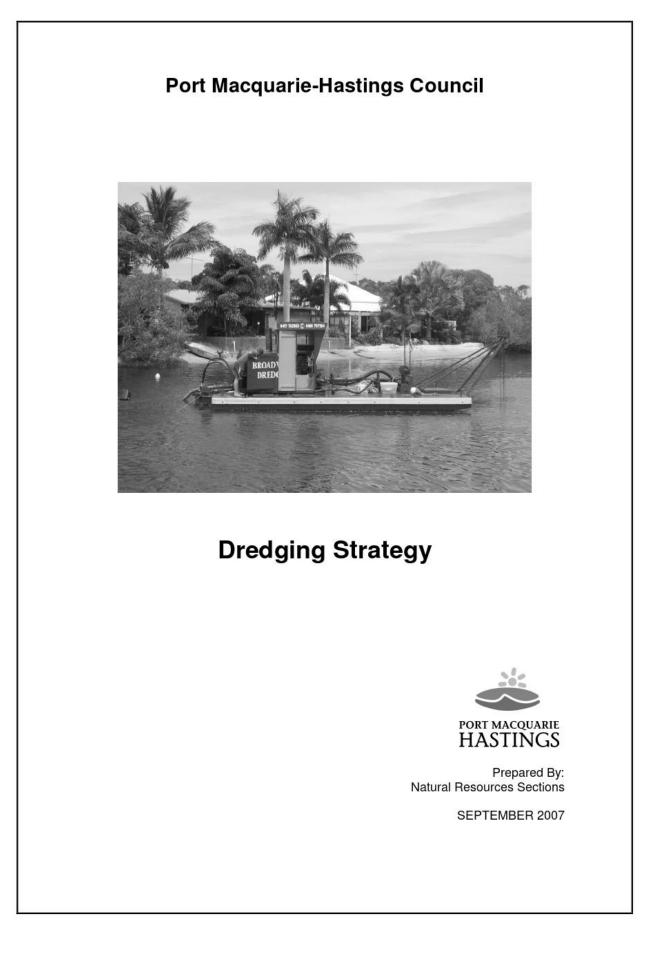
For more information about the Kooloonbung Creek flying fox camp noise assessment please contact Port Macquarie Hastings Council.



Contact information Phone: 02 6581 8111 Email: <u>council@pmhc.nsw.gov.au</u> Website: <u>www.pmhc.nsw.gov.au</u>



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Introduction

The estuaries of the Port Macquarie-Hastings (PMHC) local government area are iconic natural features that are vital to the area, supporting a diverse range of social, economic and environmental values. The estuaries of the PMHC area include the Hastings River, the Camden Haven River and Lake Cathie (an intermittently closed / open lake).

Management of these estuaries is guided by Estuary Management Plans prepared in consultation with the community, relevant agencies and adopted by PMHC. The coordination of dredging activities is one aspect within the EMPs that council is responsible for. This strategy seeks to clarify future dredging requirements for each estuary and to set a strategic direction for managing dredging within the PMHC area.

Why Is River Dredging Necessary?

The fundamental concept that will direct this strategy is the need for and purpose of river dredging. The principle needs for river dredging are:

- To maintain channels for the purpose of navigation.
- To maintain tidal flow in estuarine waters

Navigation requirements vary between estuaries and within estuaries given their different uses.

Sustainable Dredging and Estuary Processes

It is critical that any dredging activities do not have adverse impacts upon the aquatic ecosystem and the sedimentary processes of an estuary. For this reason, dredging must be planned and managed for the long-term in a sustainable way.

Dredging Objectives

The dredging philosophy that forms the corner stone of this Strategy is based upon maintaining the long-term sustainability of our estuaries. The following objectives have been identified to guide the implementation of this strategy:

- The purpose of dredging is for the maintenance of navigation and tidal flows.
- o Dredging needs to reflect best environmental practise.
- Dredging needs to minimise impacts on the overall sediment budget for each estuary.
- o Dredging needs to minimise impacts on the aquatic environment.

Sedimentation and Estuary Processes

The nature of shoaling and sedimentation in the Hastings River, Camden Haven River and Lake Cathie is documented in Estuary Processes Studies that support each Estuary Management Plan. Additionally, a hydrodynamic model exists for the Hastings River Estuary and sedimentation investigations have been carried using the hydrodynamic model.

The following is a general description of sedimentation in each estuary.

Hastings River Estuary

The Hastings River Estuary Processes Study estimates a total of 5,000m³ of sediments enter the estuary from the beach zone under annual average conditions. The study also identifies a number of key areas where sedimentation from fluvial sources and reworking of coastal sands is a management issue. These areas, together with the estimated sediment deposition rates are outlined below:

- Entrance/Harbour Area this area includes the inner bar, the Pelican Island spit and the Westport harbour area. The Estuary Processes Study estimates about 3,000m³ of marine sediment is deposited on Pelican Island, 1000m³ in the Back Channel and 1000m³ in the harbour area annually
- Northern Back Channel deposition of approximately 1,000m³/year of reworked coastal sands and marine sands (minor fluvial sediment component)
- Maria River Confluence deposition of approximately 5,000m³/year of reworked coastal sands and fluvial sediments (including sediments from bank erosion)
- Limeburners Creek specific estimate not provided but comprising reworked coastal sands and marine sands (minor fluvial sediment component)

The Hastings River Estuary Hydrodynamic Model was developed in 2006 to allow for detailed investigations into a range of estuary management issues. The model provides data on yearly sediment transports in the lower estuary taking into account wave conditions at the river entrance and is based upon 1999 hydrographic survey data.

Under conditions not dominated by waves at the entrance, the model predicts a net yearly export of sediment of 4,300m³. Under wave dominated conditions, the model predicts a net importation of 400m³. Previous studies (Webb et al, 1998) have indicated that the entrance is neither importing nor exporting large amounts of sand. The two mechanisms, export of sand due to tidal asymmetry and import due to wave action outside the entrance, must therefore be more or less in balance at the Hastings River estuary mouth (Cardeno Lawson Treloar, 2006).

The estimations of sediment transport through the entrance provided by the Hastings Estuary Processes Study and the Hastings River Hydrodynamic Model are generally comparable and provide a high level of confidence that nett sand importation through the entrance is not a significant issue.

The Hydrodynamic Model was used to predict the likely hydrodynamic and dredge plume impacts of a range of dredging scenarios. Dredge plume prediction were based on the use of a cutter suction dredge with offsite sediment management. The following summaries the results of this modelling:

- The Harbour area between the Broadwater (Canals) and the Town Green this scenario investigated dredging a channel 50m wide and 1,500m long and predicted negligible impacts on water quality or hydrodynamics.
- Limeburners Creek Entrance this scenario investigated dredging a channel 50m wide and 600m long and predicted post dredging sediment accretion in the upstream part of the site up about 1,200m³/year. Sedimentation rates over most of the site were negligible.
- Maria River Entrance this scenario investigated dredging a channel 300m wide and 500m long. These investigations highlight potential sedimentation in

the main channel at the confluence and for erosion of the Blackman's Point sand spit. However, in reality a channel width of 300m is excessive. Further investigations will be necessary to determine the impact of a dredging scenario incorporating a much narrower channel.

Camden Haven River Estuary

The Camden Haven Estuary Processes Study describes sedimentary processes and summarised these processes in a conceptual model. The study does not attempt to identify sedimentation rates for specific sites but estimates catchment sediment loads of about 4,700m³ per year and identifies a net marine sediment input within the lower estuary. The study and Estuary Management Plan identify a number of key areas where sedimentation is a management issue:

- Gogley's Lagoon Entrance sedimentation resulting from net importation of marine sands
- Upstream Camden Haven delta in Watson Taylor's Lake sedimentation comprising fluvial sediments
- Stingray Creek Delta in Queens Lake sedimentation resulting from upstream migration of marine sands

Lake Cathie / Lake Innes

The Lake Innes/Lake Cathie Estuary Management Study examines sedimentary processes within the estuary. This study also uses a conceptual model to describes sediment movement.

The Study identifies that sedimentation within the estuary is primarily associated with reworked coastal and marine sands and that catchment sediment loads are not significant in the context of shoaling. There a three main areas of significant shoaling including the Lower Cathie Creek between the Ocean Drive Bridge and the entrance, Cathie Creek upstream of the Ocean Drive Bridge and the flood tide delta at the manmade channel entrance to Lake Innes. The Estuary Management Plan identifies only one area as requiring management intervention:

 Lower Cathie Creek (east of the Ocean Drive Bridge) – This area is part of the active beach zone with approximately 20,000m³ of nett marine sand transport (import and export) annually depending upon the entrance conditions.

The Estuary Management Plans detail a range of dredging scenarios and management options for all estuaries. This information is an important input to this Strategy.

Dredging Sites

Table 1 shows the sites that have been identified for future dredging under this Strategy. These sites have been identified through a review of literature including Estuary Management Plans, through anecdotal evidence from staff, state agency representatives and the community and on the basis of future navigation requirements.

Site	Site Ref.
Camden Haven River Estuary	
Watson Taylor's Lake at entrance to Camden Haven River	1
Gogley's Lagoon entrance and channel	2
Queens Lake entrance	3
Hastings River Estuary	
Back Channel (upper reach)	4
Inner Bar	5
Limeburners Creek Confluence	6
Lower Maria River (1st left hand bend)	7
Maria River Confluence	8
Settlement Shores Canals	9
Town Green	10
Westport Harbour (north of Marina)	11
Lake Cathie Estuary	
Lower Cathie Creek (east of Ocean Dr Bridge)	12

These sites are shown graphically in Appendix A.

Dredging Priorities

Given the scope of this Strategy and cost implications of dredging works, dredging of individual sites will need to be prioritised. Accordingly, dredging sites have been assessed according to a range of key aspects and prioritised initially using a basic scoring process. A table detailing the identified sites and the prioritisation assessment is provided in Appendix B.

This assessment process is based upon known and anecdotal evidence. Scoring of current sedimentation status is limited by the absence of current detailed hydrographic survey data. The gathering of detailed hydrographic survey data will occur as part of future individual dredging operation planning and assessment phases.

Table 2 identifies the priorities that have been determined for dredging within the Port Macquarie-Hastings local government area:

Table 2 – Dredging Priorities in the Port Macquarie-Hastings area

High Priority Sites	Site Ref.
Hastings River - Limeburners Creek Confluence	6
Hastings River - Town Green	10
Camden Haven River - Gogley's Lagoon Entrance	2
Hastings River - Inner Bar	5
Medium Priority Sites	
Camden Haven River - Queens Lake Delta	3
Camden Haven River – Watson Taylor's Lake entrance to Camden Haven	1
River	
Hastings River - Westport Harbour	11
Hastings River - Settlement Shores Canals	9
Low Priority Sites	
Hastings River - Maria River Confluence	8
Hastings River - Lower Maria River	7
Lake Cathie - Lower Cathie Creek (d/s Ocean Dr Bridge)	12
Hastings River - Back Channel (upper reach)	4

It should be noted that despite the priorities above, dredging works may be carried out across a number of sites with different priorities for practical reasons and to take advantage of economies of scale for nearby sites.

Maintenance Dredging Frequency

It important to recognise that maintenance dredging is an ongoing process. The need for maintenance dredging is driven by natural sedimentary processes that impact upon navigation channels and other uses within the estuaries. Information gained through the Hasting River Hydrodynamic model and conceptual modelling for Lake Cathie and the Camden Haven River provides an indication of the frequency of dredging requirements at a number of the identified sites.

Table 3 provides an estimate of likely maintenance dredging frequencies at each site. Estimates have been provided in the absence of any documented information.

Table 3 – Maintenance Dredging Frequencies

Site	Site Ref	Estimated Dredging Frequency (years)
Camden Haven River Estuary		
Watson Taylor's Lake at entrance to Camden Haven River	1	+10
Gogley's Lagoon entrance and channel	2	10
Queens Lake entrance	3	+10
Hastings River Estuary		
Back Channel (upper reach)	4	5 - 10
Inner Bar	5	5 - 10
Limeburners Creek Confluence	6	5 - 10
Lower Maria River (1st left hand bend)	7	+10
Maria River Confluence	8	+10
Settlement Shores Canals	9	5
Town Green	10	5 - 10
Westport Harbour (north of Marina)	11	+10
Lake Cathie Estuary		
Lower Cathie Creek (east of Ocean Dr Bridge)	12	5 - 10

Spoil Management Options

The management of dredge spoil is a critical component of dredging operations. Spoil management has significant potential to impact upon dredging operations, dredging costs and sedimentary processes.

From an environmental management perspective, management of sediments within the estuary or near shore environment is clearly the most sustainable practice. This approach minimises the potential for impacts upon estuarine sediment budgets and adverse consequences including excessive scour during flood events, bank erosion and beach dune erosion. It also minimises costs associated with transport and disposal of spoil material.

On the basis of this practice, the identification of suitable dredge spoil disposal sites is required. Preference will be given to sites that provide an ancillary benefit, such as foreshore restoration sites or dune erosion sites.

Potential dredge spoil disposal sites in each estuary are identified along with dredging sites in Appendix A. These are conceptual locations only and will need to be refined in detail as part of future individual dredging operation planning and assessment phases.

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Dredging Methods

Methods of dredging will be determined as part of the detailed investigations for individual dredging operations. Dredging methods employed under this Strategy must:

- Be those most suitable for the specific site
- Reflect environmental best practise
- Be cost effective
- Meet statutory requirements

Dredging methods are likely to include:

- o Cutter suction dredge with pump to disposal site
- o Self loading cutter suction dredge with off-shore disposal (lower estuaries)
- Land based excavator (for minor works)

Statutory Requirements

There are a range of statutory provisions relevant to maintenance dredging activities. The following is a summary of the principle controls relating to maintenance dredging and how they are likely to impact upon activities under this strategy.

Environment Protection and Biodiversity Conservation Act 1999

The EPBC Act requires assessment and approval of actions that will potentially have a significant impact of matters of National Environmental Significance. Matters of National Environmental Significance include: world heritage areas; wetlands protected by international treaties; nationally listed threatened species and ecological communities; nationally listed migratory species; nuclear actions; and Commonwealth marine areas.

Appropriate assessment will need to be carried out at the environmental assessment stage of each activity.

Environmental Planning & Assessment Act and Regulations

The EP&A Act generally imposes requirements for controlling development under two schemes. Part 4 of the Act controls development that requires consent or is prohibited under an environmental planning instrument. Part 5 of the Act imposes requirements for assessing the impact of development that does not require consent. Maintenance dredging activities under this strategy require assessment under Part 5 of the EP&A Act. Port Macquarie-Hastings Council is the determining authority and is required by the Act to examine the environmental aspects of carrying out the activity.

Such an examination would take the form of a Review of Environmental Factors (REF). Should any dredging proposal be 'likely to significantly affect the environment', an Environmental Impact Assessment would be required.

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State Environmental Planning Policy 14 – Coastal Wetlands

SEPP 14 aims to preserve and protect costal wetlands in the environmental and economic interests of NSW. SEPP 14 applies to development that involves clearing, constructing a levee, draining or filling on the land mapped under the SEPP.

In respects of dredging activities the requirement to gain consent for 'clearing' will trigger SEPP 14 where seagrass is required to be removed as part of the dredging works.

<u>State Environmental Planning Policy 35 – Maintenance Dredging of Tidal Waterways</u> SEPP 35 enables public authorities to carry out maintenance dredging of tidal waterways without development consent. The SEPP specifies government agencies with which the public authority must consult when carrying out maintenance dredging. SEPP 35 applies to all maintenance dredging proposed by this strategy.

Hastings Local Environment Plan 2001

Hastings LEP applies to all land in the LGA. A number of different zones apply to dredging sites identified by this Strategy.

- Hastings River Sites unzoned SEPP 35 allows for maintenance dredging without consent
- Lake Cathie 7(f1) coastal protection SEPP 35 allows for maintenance dredging without consent
- Maria River confluence part 1(a1) rural part unzoned SEPP 35 allows for maintenance dredging without consent
- Limeburners Creek confluence unzoned SEPP 35 allows for maintenance dredging without consent
- Queens Lake entrance unzoned SEPP 35 allows for maintenance dredging without consent
- Camden Haven River Entrance at Watson Taylor's Lake 8(a) National Park & Nature Reserve – must be authorised by or under the National Parks and Wildlife Act 1974 i.e. consistent with the Plan of Management for the Reserve
- Gogley's Lagoon entrance unzoned SEPP 35 allows for maintenance dredging without consent. However, dredge area is within SEPP 14 Wetlands and therefore consent will be required for any dredging activity

Crown Lands Act 1989

Given that dredging activities will extend below the mean high water mark, above which Council has care and control, a licence would be required from the Department of Lands for maintenance dredging activities.

Protection of the Environment Operations Act 1997

This legislation controls environmental pollution and regulates scheduled activities carried out in NSW. In relation to dredging activities, the provisions of the Act would primarily relate to preventing water pollution as well as ancillary matters such as noise and air pollution. Dredging activities that involve the dredging of more than 30,000 m³ of material annually are Scheduled and require an Environmental Protection Licence.

Depending upon staging of dredging works, an EPL may be required for maintenance dredging at some sites.

Threatened Species Conservation Act 1995

The Threatened Species Conservation Act requires specific assessment of listed threatened species that occur on any proposed development site. A Seven Part Test

will be required under this legislation as part of the environmental assessment for individual dredging proposals.

Fisheries Management Act 1994

This legislation imposes requirements for Council to obtain permits for dredging or reclamation works. However, a permit is not required where the works are authorised under the Crown Lands Act or by any other relevant authority (excluding Council). Given that a Crown Lands licence will be required for all river dredging sites identified in this strategy, a dredging permit from NSW DPI will not be required.

The Act also requires a permit to be issued where marine vegetation is to be harmed.

Rivers & Foreshores Improvement Act 1948

This legislation requires a permit to be obtained for works within 40m of a river/foreshores. However, certain public authorities (including Council) are exempt from the requirements of the Act.

A summary of the applicability of statutory provisions to the sites identified under this Strategy is provided in Appendix C.

The Do Nothing Option

To ensure a balanced assessment of the maintenance dredging issue, the 'do nothing' option should be assessed. This is a relevant consideration given the cost implications and potential environmental impacts of maintenance dredging.

Table 4 is a summary of the potential risks of maintenance dredging that can be used to inform decisions.

	and a second s
Risk	Mitigation Measures
Water pollution	Employ best practise dredging methods
Adverse impacts on sedimentary	Keep sediments in the system and minimise maintenance
processes	dredging footprints
Loss of aquatic habitat and	Align channels to avoid seagrass disturbance
impact upon aquatic organisms	
High Costs	Seek government subsidies to reduce financial impacts

Table 4 – Potential Maintenance Dredging Risks

As indicated above, for each potential risk there are sound and realistic measures that can be implemented to mitigate risk and enable maintenance dredging to proceed in a sustainable manner.

Risks and potential disadvantages also need to be compared with the benefits of maintenance dredging. Most of the advantages associated with maintenance dredging are economic and social benefits as demonstrated by the following:

- o recreational boating access
- o continued viability of commercial boating operations and support industries
- continued viability of waterway associated tourism operations
- o maintenance of tidal flows in high priority oyster growing areas

In the context of the activities listed above, it is clear that a failure to maintain navigable waterways is likely to have an economic and social impact on the community. The following negative impacts are relevant:

loss of waterway access for recreational fishing and boating uses

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- negative tourism implications associated with diminished access for tourism vessel operators
- negative economic and tourism impacts resulting from reduced vessel stop overs
- negative economic impacts associated with loss of access on maritime industries and commercial fishing/aquaculture industries

On balance, the benefits of maintenance dredging outweigh the risks identified above and on this basis the 'do nothing' option can be discounted.

Funding

Maintenance dredging works are expensive to undertake. Detailed costing for individual dredging operations have not been carried out given the lack of hydrosurvey data and other relevant pre-planning information. At this stage preliminary costing for dredging works identified by the Strategy have been determined and the following table provides a broad guide to potential dredging costs at each site.

Table 5 – Estimate Maintenance Dredging Costs

Site	Site Ref.	Likely Dredging Cost / operation
Camden Haven River Estuary		
Watson Taylor's Lake at entrance to Camden Haven River	1	\$ 145,000
Gogley's Lagoon entrance and channel	2	\$ 130,000
Queens Lake entrance	3	\$ 145,000
Hastings River Estuary		
Back Channel (upper reach)	4	\$ 10,000
Inner Bar	5	\$ 325,000
Limeburners Creek Confluence	6	\$ 145,000
Lower Maria River (1st left hand bend)	7	\$ 300,000
Maria River Confluence	8	\$ 300,000
Settlement Shores Canals	9	\$ 470,000
Town Green	10	\$ 360,000
Westport Harbour (north of Marina)	11	\$ 120,000
Lake Cathie Estuary		•
Lower Cathie Creek (east of Ocean Dr Bridge)	12	\$ 280,000

These costs need to be considered in the context of likely maintenance dredging frequencies at each site.

Significant funding over the long-term will be necessary for sustainable maintenance dredging to be implemented in the Port Macquarie-Hastings LGA.

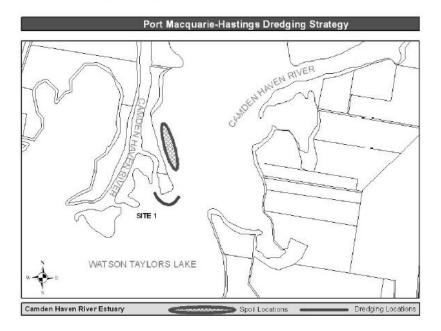
Principle funding sources will include:

- Council funds
- NSW Government Estuary Grants Program
- NSW Dept Lands Minor Ports

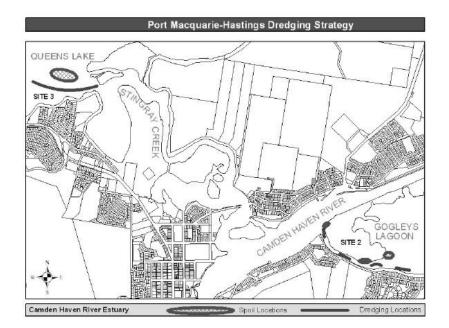
Significant contributions by government will be crucial to the implementation of dredging across the LGA. NSW Government funding would usually be sought on an individual project basis.

This Strategy will allow Council to incorporate future dredging works into its rolling works program, depending upon available funding, and liaise with government on funding opportunities to ensure dredging frequencies can be achieved.

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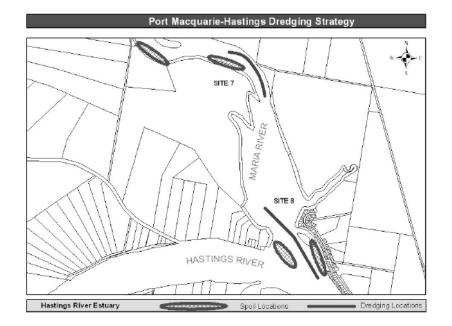


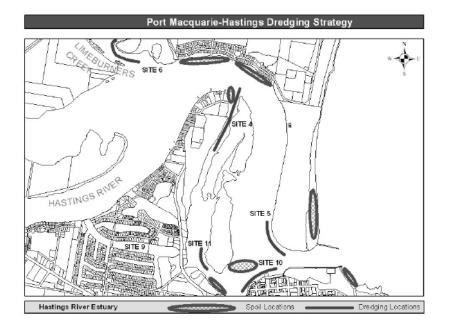
Appendix A - Dredging and Spoil Sites



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Appendix B – Site Priorities Assessment

Ref.	Estuary	Site	Data Sources	Sediment Type	Current Degree of Sedimentation	De	Rate of Sedimentation		Tidal Exchange Capacity	Score	Vavigation requirements		Dominant Navigation Jse	Score	Intensity of Use		Overall Score	Rank	Priority
1	Camden Haven River	Camden Haven River entrance at Watson Taylor's Lake	Camden Haven Estuary Processes Study	Fluvial Sediments	High	5	Low	1	Low 5		Moderate and S small vessels		Commercial & decreational	4	Moderate 3	3	21	5	Medium
2	Camden Haven River	Gogley's Lagoon Entrance	Camden Haven Estuary Processes Study	Marine Sands	Moderate-High	4	Moderate	3	Low 4		Small vessels, S byster punts		Commercial & S ecreational	5	High 5	5	24	3	High
3	Camden Haven River	Queens Lake Delta	Camden Haven Estuary Processes Study	Marine Sands	High	5	Low	1	Moderate 3	3 S	Small vessels 2		Commercial & 4 ecreational	4	High 4	4	19	5	Medium
4	Hastings River		Processes Study	Mixed Reworked Coastal Sands and Fluvial Sediments	Moderate	3	Moderate	3	Low 4	р	Small non- oowered /essels	1 F	Recreational	2	Low 1	1	14	13	Low
5	Hastings River	Inner Bar	Hastings Estuary Processes Study	Marine Sands	Low	2	Moderate	4	High 1	1 L	arge Vessels		Commercial & secreational	5	High 5	5	22	4	High
6	Hastings River	Limeburners Creek Confluence	Hastings Estuary Processes Study	Reworked Coastal Sands/ Marine Sands / minor fluvial sediments	High	5	High	5	Low 5		Small vessels, 3 Syster punts		Commercial & secreational	5	High §	5	28	1	High
7	Hastings River	Lower Maria River (1st left hand bend)		Mixed Reworked Coastal Sands (bank erosion) and Fluvial Sediments	Moderate	3	Moderate	3	Moderate 3		Moderate and S small vessels	3 F	Recreational	3	Moderate 3	3	18	9	Low
8	Hastings River	Maria River Confluence	Hastings Estuary Processes Study	Mixed Reworked Coastal Sands and Fluvial Sediments	Moderate	3	Moderate	3	Moderate 3		Moderate and S small vessels		Commercial & S ecreational	3	Moderate 3	3	18	9	Low
9	Hastings River	Settlement Shores Canals	Settlement Shores Canal Maintenance Plan	Mixed Reworked Coastal Sands and Fluvial Sediments	Moderate-High	4	Low	1	Moderate 3		Moderate and S Small vessels	3 F	Recreational	3	High 5	5	19	7	Medium
10	Hastings River		Hastings Estuary Processes Study	Marine Sands	Moderate-High	4	High	5	High 1	1 L	arge Vessels		Commercial & S ecreational	5	High 5	5	25	2	High
11	Hastings River	Westport Harbour (north of Marina)	Hastings Estuary Processes Study	Mixed Reworked Coastal Sands and Fluvial Sediments	High	4	Low	1	High 1	1 L	arge Vessels		Commercial & 4 ecreational	4	High t	5	20	6	Medium
12	Lake Cathie	Lower Cathie Creek (d/s Ocean Dr Bridge)	Lake Innes/Lake Cathie Management Study	Marine Sands	Moderate	3	High	5	Moderate (during 3 open entrance conditions)	p re	Small non- powered ecreational vatercraft	1 F	Recreational		Moderate 3 (Seasonal)	3	18	9	Low

ORDINARY COUNCIL 16/06/2021

Port Macquarie-Hastings Council Dredging Strategy

Estuary	Site	SEPP 35 Maintenance Dredging of Tidal Waterways	EP&A Act Part V Approval	Threatened Species Conservation Act - Seven Part Test	Licence under Crown Lands Act	Environment Protection Licence	Fisheries Management Act - Harm Marine Vegetation	SEPP 14 Coastal Wetlands	Development Consent	Consistency with National Parks & Wildlife Plan of Management
Camden Haven River	Camden Haven River entrance at Watson Taylor's Lake	*	*	*	*	+				*
Camden Haven River	Gogley's Lagoon Entrance			*	*		*	*	*	
Camden Haven River	Queens Lake Delta	*	*	*	*		*			
Hastings River	Back Channel (upper reach)	*	*	*	*					
Hastings River	Inner Bar	*	*	*	*	+				
Hastings River	Limeburners Creek Confluence	*	*	*	*					
Hastings River	Lower Maria River (1st left hand bend)	*	*	*	*					
Hastings River	Maria River Confluence	*	*	*	*					
Hastings River	Settlement Shores Canals		*	*			*			
Hastings River	Town Green	*	*	*	*	+				
Hastings River	Westport Harbour (north of Marina)	*	*	*	*		*			
	1						1	1	1	

* Applies

+ May Apply

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ORDINARY COUNCIL 16/06/2021



Leslie WILLIAMS MP Member for Port Macquarie



3rd November 2020

Mr Jeffrey Sharp Acting General Manager Port Macquarie-Hastings Council PO Box 84 PORT MACQUARIE NSW 2444

Dear Mr Sharp

RE: Dredging Lake Cathie Lagoon

I write in relation to the sand build-up in the Lake Cathie lagoon which is now noticeable since the Lake closed to the ocean last month.

I ask that Council give consideration to dredging the lagoon in the coming months for the purpose of beach nourishment on Illaroo Road. As well as replacing recent sand loss on the fore-dunes this action would also provide the benefit of increasing the water depth within the lagoon which I believe would be advantageous during predicted heavy rainfall events in Australia's sourth-eastern states as a result of La Nina. The enhanced water volume within the lagoon would also likely have positive impacts on the next lake opening which of course many residents will be hoping is prior to the upcoming Christmas holiday season.

The Coastal Zone Management Plan (CZMP) proposes "that Council will continue to undertake periodic dredging of Lake Cathie Lagoon and to place dredged/excavated sand from the Lake Cathie entrance on the beach adjacent to Illaroo Road. As an indication of the volume of sand that may be available in the Lake Cathie entrance, approximately 25,000 m3 was removed in 2005 and ongoing in 2014."

I am advised that Council currently has a Development Consent to dredge the Lake Cathie Lagoon for beach nourishment and that a Short Term Licence could therefore be issued without delay by Crown Lands to support this action.

As you are aware, the NSW Government has previously provided financial support to Port Macquarie-Hastings Council for dredging within the lake for passive recreation with dredged sands utilised for foreshore and beach nourishment on Illaroo Road. If Council favoured the dredging of the Lake Cathie Lagoon, I would certainly advocate for financial assistance from the Government to support this action.

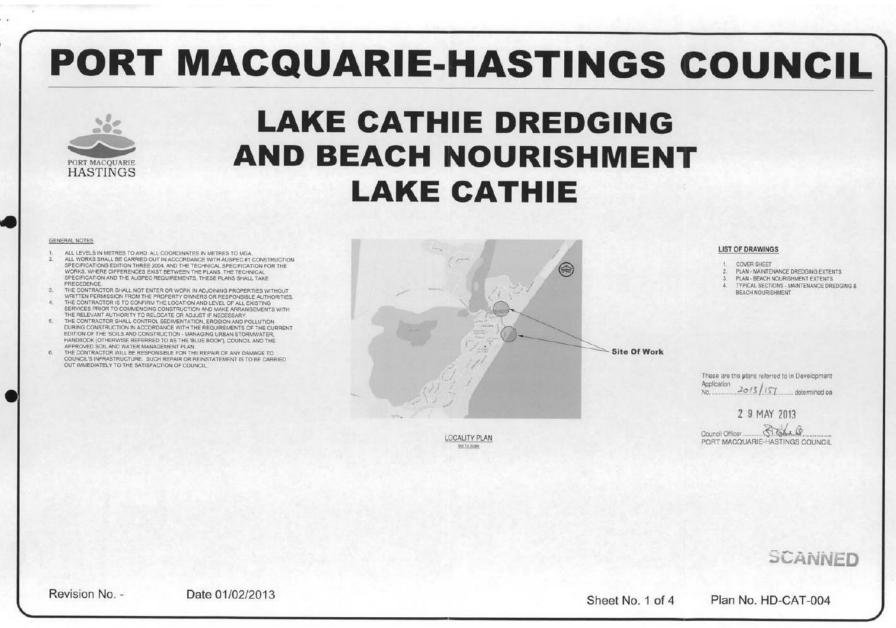
I would be pleased to have the opportunity to discuss this matter further but if you have any questions in the meantime don't hesitate to give me a call.

Yours sincerely

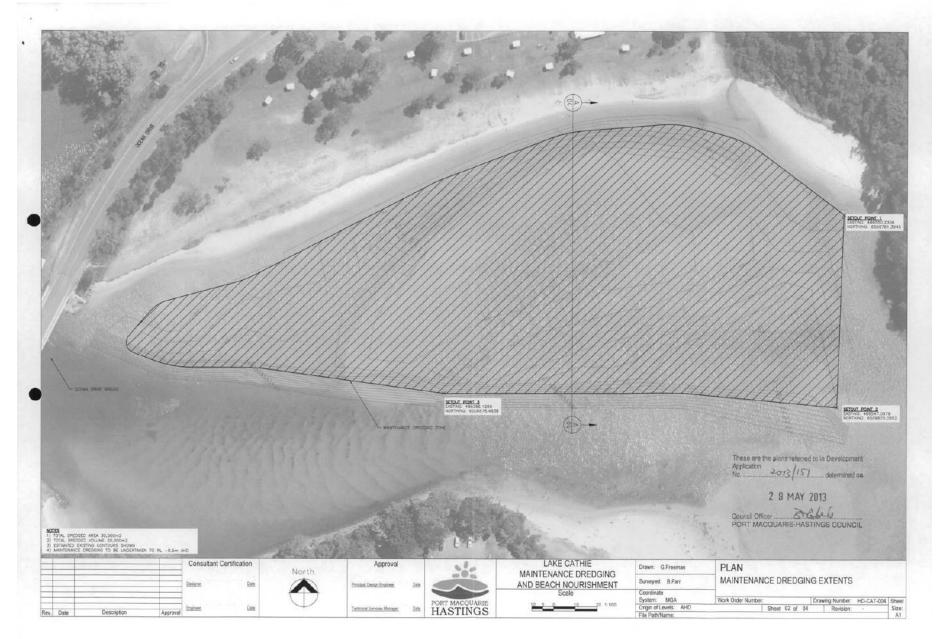
Leslie Williams MP Member for Port Macquarie Deputy Speaker

CC: Mayor and Councillors

Item 12.04 Attachment 2 Page 819

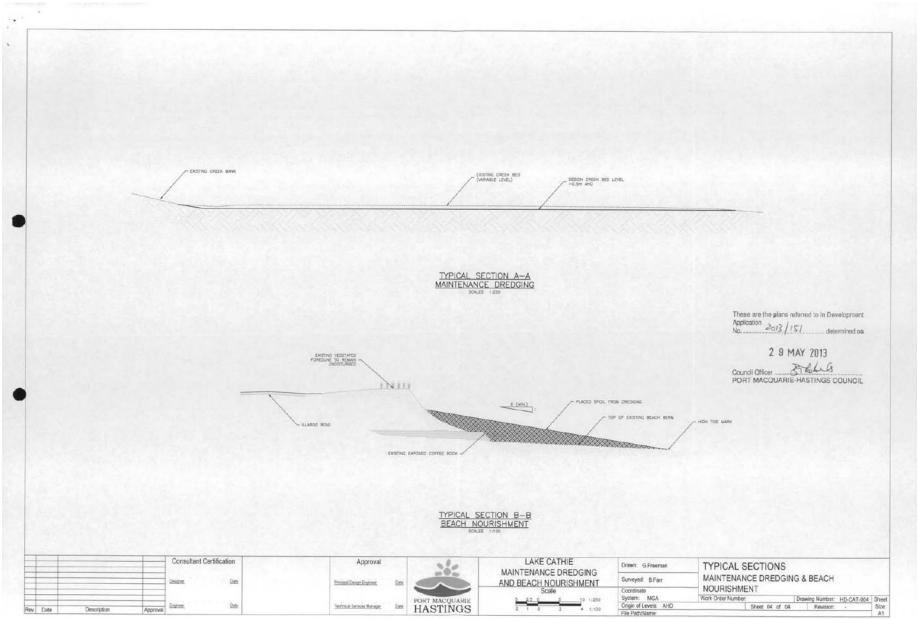


ATTACHMENT





ATTACHMENT





Our Ref: DOC21/57726 Your Ref: SF17/2022

> Chief Executive Officer Port Macquarie Hastings Council PO Box 84 Port Macquarie NSW 2444

Attention: Ms Blayne West

Dear Dr Allen

Re: Request for Biodiversity and Conservation Division Input – Assessment of Environmental Impacts – Lake Cathie Dredging for Beach Nourishment

Thank you for your letter dated 24 December 2020 about the potential dredging of Lake Cathie and subsequent nourishment of the nearby beach with dredged material, seeking comments from the Biodiversity and Conservation Division (BCD) of the Biodiversity, Conservation and Science Directorate in the Environment, Energy and Science Group of the Department of Planning, Industry and Environment. I appreciate the opportunity to provide input.

In order to determine the likely impact of the project, a detailed environmental impact assessment would be necessary. We suggest the Port Macquarie Hastings Council re-examines the approved Development Application (DA) relating to the periodic dredging of the lake and its supporting impact assessment, to ensure its scope included all possible impacts of the potential dredging.

Given it is unlikely that the beach nourishment component of the proposal was included as part of that DA, the council must determine the appropriate assessment and approval pathway. This would allow council to ascertain if the proposed beach nourishment is permissible, and if so, ensure the impacts of this component of the potential work are appropriately scoped, considered and assessed.

In revising the impact assessment, the following points should be considered, noting that this is not an exhaustive list of the potential considerations:

- 1. Features of the dredging and nourishment areas:
 - a. Destination beach type, characteristics and profile
 - b. Beach sediment transport processes for this beach compartment
 - c. Lagoon waterway area extent, use and characteristics
 - d. Geomorphology and bathymetry
 - e. Beach berm characteristics, natural height and profile
 - f. Lagoon bed sediment type, characteristics, extent and volumes and heavy metals, including checks for presence of acid sulfate soils and coffee rock
 - g. Seasonal availability of sediment for sand nourishment of the beach fronting Illaroo Road
 - h. Stratigraphy of bed material within lagoon set up
 - i. Depth to bedrock
 - j. Tidal dynamics when open
 - k. Marine sediment transport dynamics, pathways and mechanisms, including seasonal patterns/cycles of movement, key triggers and volumes available for dredging

Level 8, 24 Moonee Street, (Locked Bag 914), Coffs Harbour, NSW 2450 | Ph (02) 6659 8200 | dpie.nsw.gov.au

Page 2

- I. Presence of acid sulfate materials
- m. Coastal process interactions with the lagoon and beach.
- n. Coastal erosion hazard risk of Illaroo Rd
- o. Entrance opening interactions with lagoon shoaling
- p. Flood risk to lagoon when closed, including the catchment rainfall required to trigger an opening during dry and wet seasons
- q. Water quality character of the lagoon during a range of entrance and climatic conditions
- r. Beach nourishment approach and access
- 2. Estuarine vegetation
 - a. Seagrass
 - b. Saltmarsh
 - c. Mangroves
 - d. Macroalgae
 - e. Upstream and adjacent terrestrial vegetation
- 3. Aquatic and marine fauna
 - a. Fish
 - b. Mammals
 - c. Benthic macroinvertebrates
- 4. Terrestrial flora and fauna:
 - a. Shore and water birds, including seasonal movements and critical breeding feeding and nesting times
 - b. Dune vegetation
- 5. National Parks and Wildlife Service Estate
 - a. Lakes Innes Nature Reserve

Given the additional matters you raised in your letter relating to potential impacts on Crown Land and Aboriginal cultural heritage, we also suggest the council liaises directly with the Department's Crown Land Division and with Heritage NSW, the latter of which forms part of the Department of Premier and Cabinet.

The full list of our environmental assessment requirements that may need to be addressed is provided in Attachment 1. In preparing the environmental assessment, the proponent should refer to the relevant guidance material listed in Attachment 2.

If you have any questions about this advice, please do not hesitate to contact Ms Nicky Owner, Senior Conservation Planning Officer, at nicky.owner@environment.nsw.gov.au or 6659 8254.

Yours sincerely

24 February 2021

DIMITRI YOUNG Senior Team Leader Planning, North East Branch **Biodiversity and Conservation**

Enclosures:

Attachment 1 - BCD Recommended Environmental Assessment Requirements - Lake Cathie Dredging for Beach Nourishment Attachment 2 - Environmental Assessment Guidance Material

Attachment 1

Biodiversity and Conservation Division's Recommended Environmental Assessment Requirements

for

Lake Cathie Dredging for Beach Nourishment

Item 12.04 Attachment 4 Page 826 This page has been deliberately left blank.

Item 12.04 Attachment 4 Page 827 BCD Recommended EARs - Lake Cathie Dredging for Beach Nourishment

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A. The Proposal

The environmental assessment should fully and clearly describe the proposal, including any environmental impact mitigation measures, and identify all the processes and activities intended for the site during the life of the proposal.

The description of the proposal in the environmental assessment should, where relevant, include:

- 1. the location of the proposal and details of the surrounding environment;
- 2. appropriate land use zoning;
- 3. the size and type of the proposal and its operation;
- 4. the proposed layout of the site;
- 5. the staging and timing of the proposal;
- 6. the proposal's relationship to any other proposal.
- 7. all equipment proposed for use at the site;
- 8. chemicals, including fuel, used on the site and proposed methods for the transportation, storage, use and emergency management;
- 9. waste generation, storage and disposal;
- 10. the anticipated environment impacts of the proposal, both direct and indirect,
- 11. a plan showing the distribution of any threatened flora or fauna species and the vegetation communities on or adjacent to the subject site, and the extent of vegetation proposed to be cleared; and
- 12. ownership details of any residence and/or land likely to be affected by the proposal;
- maps/diagrams showing the location of residences and properties likely to be affected and other industrial developments, conservation areas, wetlands, etc. in the locality that may be affected by the proposal;
- 14. methods to mitigate any expected environmental impacts of the proposal; and
- 15. the anticipated level of performance in meeting required environmental standards.

B. Environmental Impacts of the Proposal

Impacts related to the following environmental issues should be assessed, quantified and reported on:

- Biodiversity
- NPWS Estate (land reserved or acquired under the National Parks and Wildlife Act 1974)
- Acid Sulfate Soils
- Flooding, Stormwater and Coastal Erosion
- Cumulative Impacts

The environmental assessment should address the specific requirements outlined under each heading below and assess impacts in accordance with the relevant guidelines mentioned. A full list of guidelines is at **Attachment 2**.

C. Biodiversity

- The environmental assessment must assess the impacts of the proposal on biodiversity values to determine if the proposal is "likely to significantly affect threatened species" for the purposes of Section 7.2 of the *Biodiversity Conservation Act 2016* (BC Act) as follows:
 - A. The environmental assessment must demonstrate whether the proposal is to be carried out in a declared area of outstanding biodiversity value.
 - B. If the proposal is not carried out in a declared area of outstanding biodiversity value, then the environmental assessment must demonstrate and document whether the proposal, if assessed as development, exceeds the biodiversity offset scheme threshold, as set out in section 7.4 of the BC Act and clause 7.1 of the *Biodiversity Conservation Regulation 2017* (BC Regulation), by determining whether the proposal involves:
 - I. The clearing of native vegetation of an area declared by clause 7.23 of the BC Regulation as exceeding the threshold, or
 - II. The clearing of native vegetation, or other action prescribed by clause 6.1 of the BC Regulation, on land included on the Biodiversity Values Map published under clause 7.3 of the BC Regulation.
 - C. If the biodiversity offset scheme threshold is not exceeded, then the environmental assessment must document *the test for determining whether* proposed development is likely to significantly affect threatened species or ecological communities as outlined in Section 7.3 of the BC Act, by preparing an ecological assessment that should include:
 - I. A field survey of the site conducted and documented in accordance with relevant guidelines, including:
 - a. Field survey methods for environmental consultants and surveyors when assessing proposed developments or other activities on sites containing threatened species <u>https://www.environment.nsw.gov.au/-</u> /media/OEH/Corporate-Site/Documents/Animals-and-plants/Threatenedspecies/field-survey-method-guidelines.pdf
 - b. Threatened Species Survey and Assessment Guidelines: Field Survey Methods for Fauna -Amphibians (DECC, 2009) <u>http://www.environment.nsw.gov.au/resources/threatenedspecies/09213a</u> <u>mphibians.pdf</u>
 - c. NSW Guide to Surveying Threatened Plants (OEH 2016) <u>https://www.environment.nsw.gov.au/-/media/OEH/Corporate-</u> <u>Site/Documents/Animals-and-plants/Threatened-species/guide-surveying-</u> <u>threatened-plants-160129.pdf</u>
 - d. "Species credit' threatened bats and their habitats <u>https://www.environment.nsw.gov.au/-/media/OEH/Corporate-</u> <u>Site/Documents/Animals-and-plants/Threatened-species/species-credit-</u> <u>threatened-bats-survey-guide-180466.pdf</u>

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e. Threatened Biodiversity Survey and Assessment: Guidelines for Developments and Activities - Working Draft (DEC, 2004), https://www.environment.nsw.gov.au/-/media/OEH/Corporate-Site/Documents/Animals-and-plants/Threatened-species/draft-threatenedbiodiversity-survey-guide.pdf

If a proposed field survey methodology is likely to vary significantly from the methods in the guidelines above, then the proponent should discuss the proposed methodology with the Biodiversity and Conservation Division prior to undertaking surveys for the environmental assessment, to determine whether the Biodiversity and Conservation Division considers the proposed methodology appropriate.

The results of recent (less than five years old) field surveys may be used. However, the results of previous field surveys should not be used if they have:

- been undertaken in seasons, weather conditions or following extensive disturbance events when the subject species are unlikely to be detected or present, or
- utilised methodologies, survey sampling intensities, timeframes or baits that are not the most appropriate for detecting the target subject species,

unless these differences can be clearly demonstrated to have had an insignificant impact upon the outcomes of the field surveys.

If the results of previous field surveys are used, then field surveys for any additional threatened entities listed under the BC Act since the previous field surveys took place, must be undertaken and documented.

The list of potential threatened species, populations, ecological communities, or their habitats for the site should be determined in accordance with:

- the Threatened Biodiversity Survey and Assessment: Guidelines for Developments and Activities - Working Draft (DEC, 2004), and
- the Department's Threatened Species website http://www.environment.nsw.gov.au/topics/animals-and-plants/threatenedspecies .and
- the Bionet Atlas of NSW http://www.environment.nsw.gov.au/wildlifeatlas/about.htm , and
- the Vegetation Information System (BioNet Vegetation Classification) http://www.environment.nsw.gov.au/research/Visclassification.htm , and
- other data sources (e.g. PlantNET, Online Zoological Collections of Australian Museums (http://www.ozcam.org/), previous or nearby surveys etc.) may also be used to compile the list.
- The following information as a minimum: 11
 - a. A description, spatial data files, and geo-referenced mapping of the study area, (overlays on topographic maps, satellite images and /or aerial photos, including details of map datum, projection and zone), showing all field survey locations, vegetation communities classified in accordance with the BioNet Vegetation Classification

(http://www.environment.nsw.gov.au/research/Visclassification.htm), key

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habitat features and reported locations of threatened species and ecological communities present in the subject site and study area.

- b. A description of survey methodologies used, including timing, location and weather conditions.
- c. Details, including qualifications and experience, of all staff undertaking the surveys, mapping and assessment of impacts as part of the EIS.
- d. Identification of national and state listed threatened biota known or likely to occur in the study area and their conservation status.
- e. A description of the likely impacts of the proposal on biodiversity values, including direct and indirect impacts and construction and operation impacts, with impacts quantified, wherever possible, such as the amount of each vegetation community or species habitat to be cleared or impacted, and/or the degree of fragmentation of a habitat connectivity.
- f. Identification of the avoidance, mitigation and management measures that will be put in place as part of the proposal to avoid or minimise biodiversity impacts, including details about alternative options considered and how longterm management arrangements will be guaranteed.
- g. A description of the residual impacts of the proposal.
- III. The 'test for determining whether proposed development or activity is likely to significantly affect threatened species or ecological communities, or their habitats' as outlined in Section 7.3 of the BC Act undertaken in accordance with the gazetted *Threatened Species Test of Significance Guidelines* (OEH 2018) available at: <u>https://www.environment.nsw.gov.au/-</u> /media/OEH/Corporate-Site/Documents/Animals-and-plants/Threatenedspecies/threatened-species-test-significance-guidelines-170634.pdf
- If the environmental assessment determines under 1 above that the proposal is likely to significantly affect threatened species, then in accordance with Section 7.7 of the BC Act the environmental assessment must be accompanied by a Biodiversity Development Assessment Report prepared in accordance with Part 6, Division 3 of the BC Act.
- 3. If the environmental assessment determines under 1 above that the proposal is unlikely to significantly affect threatened species, then the proposal should:
 - a. be designed to avoid and minimise impacts on biodiversity values to the fullest extent possible, and
 - b. include a biodiversity offset package to offset remaining direct and indirect impacts on biodiversity values, prepared in accordance with the Department's 13 offsetting principles available at <u>http://www.environment.nsw.gov.au/biodivoffsets/oehoffsetprincip.htm</u>:

Note:

For the *Commonwealth Environment Protection and Biodiversity Conservation Act 1999*, the EIS should identify any relevant Matters of National Environmental Significance and whether the proposal has been referred to the Commonwealth or already determined to be a controlled action.

D. NPWS Estate

The environmental assessment should address the following with respect to land reserved under the *National Parks and Wildlife Act 1974*.

 Where appropriate, likely impacts (both direct and indirect) of the proposal on any adjoining and/or nearby NPWS estate reserved under the National Parks and Wildlife Act 1974 should be considered, with reference to the *Developments adjacent to National Parks and Wildlife Service lands Guidelines for consent and planning authorities (DPIE 2020)* available at:

https://www.environment.nsw.gov.au/research-and-publications/publicationssearch/developments-adjacent-to-national-parks-and-wildlife-service-lands

Note: Proposals which may impact marine protected areas should be referred to the Regions, Industry, Agriculture and Resources Group in the Department of Planning, Industry and Environment to determine the assessment and approval requirements.

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E. Acid Sulfate Soils

The environmental assessment should address the following:

- The potential impacts of the proposal on acid sulfate soils must be assessed in accordance with the relevant guidelines in the Acid Sulfate Soils Manual (Stone et al. 1998) and the Acid Sulfate Soils Laboratory Methods Guidelines (Ahern et al. 2004).
- 2. The mitigation and management options that will be used to prevent, control, abate or minimise potential impacts from the disturbance of acid sulfate soils associated with the proposal and to reduce risks to human health and prevent the degradation of the environment must be described, including include an assessment of the effectiveness and reliability of the measures and any residual impacts after these measures are implemented.

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F. Flooding, Stormwater and Coastal Erosion

The environmental assessment should include an assessment of the following referring to the relevant guidelines in Attachment 2:

- 1. The potential effect of coastal processes and coastal hazards including potential impacts of sea level rise:
 - a. on the proposal; and
 - b. arising from the proposal.
- 2. Whether the proposal is consistent with any coastal zone management plans.
- 3. Whether the proposal is consistent with any floodplain risk management plans.
- 4. Whether the proposal is compatible with the flood hazard of the land.
- 5. Whether the proposal will significantly adversely affect flood behaviour resulting in detrimental increases in the potential flood affectation of other development or properties.
- 6. Whether the proposal will significantly adversely affect the environment or cause avoidable erosion, siltation, destruction of riparian vegetation or a reduction in the stability of river banks or watercourses.
- 7. Whether the proposal incorporates appropriate measures to manage risk to life from flood.
- 8. Whether the proposal is likely to result in unsustainable social and economic costs to the community as a consequence of flooding.
- 9. The implications of flooding over the full range of potential flooding, including the probable maximum flood, should be considered as set out in the NSW Government Floodplain Development Manual. This should include the provision of:
 - a. Full details of the flood assessment and modelling undertaken in determining any design flood levels (if applicable), including the 1 in 100 year flood levels.
 - b. A sensitivity assessment of the potential impacts of an increase in rainfall intensity and runoff (10%, 20% and 30%) and sea level rise on the flood behaviour for the 1 in 100 year design flood if applicable.
- 10. All site drainage, stormwater quality devices and erosion / sedimentation control measures should be identified and the onsite treatment of stormwater and effluent runoff and predicted stormwater discharge quality from the proposal should be detailed.

G.Cumulative Impacts

The environmental assessment should include an assessment of the following:

- 1. The cumulative impacts, including both construction and operational impacts, from all clearing activities and operations, associated edge effects and other indirect impacts on cultural heritage, biodiversity and NPWS Estate in accordance with the *Environmental Planning and Assessment Act 1979*.
- 2. The cumulative impacts, including both construction and operational impacts, of the proponent's existing proposals and other proposals and associated infrastructure (such as access tracks etc.) as well as the cumulative impact of the proposal in the context of other proposals located in the vicinity.

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Attachment 2 – Environmental Assessment Guidance Material

Title	Web address	
Relevant Legislation		
Biodiversity Conservation Act 2016	https://www.legislation.nsw.gov.au/#/view/act/2016/63/full	
Coastal Management Act 2016	https://www.legislation.nsw.gov.au/#/view/act/2016/20/full	
Commonwealth Environment Protection and Biodiversity Conservation Act 1999	http://www.austlii.edu.au/au/legis/cth/consol_act/epabca1999588/	
Environmental Planning and Assessment Act 1979	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+203+1 979+cd+0+N	
Fisheries Management Act 1994	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+38+19 94+cd+0+N	
Marine Parks Act 1997	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+64+19 97+cd+0+N	
National Parks and Wildlife Act 1974	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+80+19 74+cd+0+N	
Protection of the Environment Operations Act 1997	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+156+1 997+cd+0+N	
Water Management Act 2000	http://www.legislation.nsw.gov.au/maintop/view/inforce/act+92+20 00+cd+0+N	
Wildemess Act 1987	http://www.legislation.nsw.gov.au/viewtop/inforce/act+196+1987+ FIRST+0+N	
Biodiversity		
Biodiversity Assessment Method (OEH, 2017)	http://www.environment.nsw.gov.au/resources/bcact/biodiversity- assessment-method-170206.pdf	
Biodiversity Development Assessment Report	https://www.legislation.nsw.gov.au/#/view/act/2016/63/part6/div3/ sec6.12	
Guidance and Criteria to assist a decision maker to determine a serious and irreversible impact (OEH, 2017)	http://www.environment.nsw.gov.au/resources/bcact/guidance- decision-makers-determine-serious-irreversible-impact- <u>170204.pdf</u>	
Accreditation Scheme for Application of the Biodiversity Assessment Method Order 2017	https://www.legislation.nsw.gov.au/regulations/2017-471.pdf	

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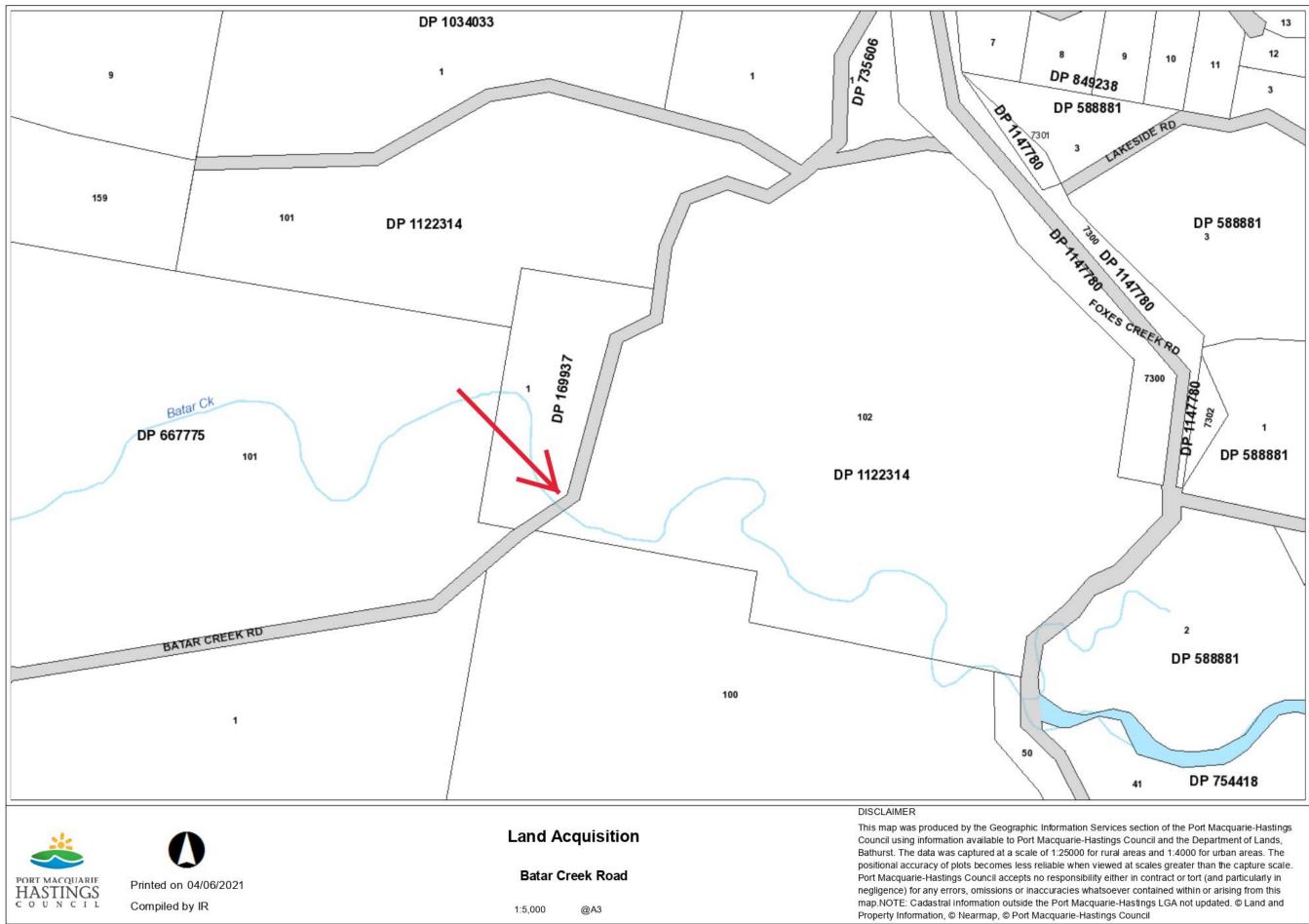
Attachment 2 - Environmental Assessment Guidance Material

Title	Web address	
	1	
Biodiversity conservation actions	www.environment.nsw.gov.au/resources/bcact/ancillary-rules-	
	biodiversity-actions-170496.pdf	
Reasonable steps to seek like-for-like	www.environment.nsw.gov.au/resources/bcact/ancillary-rules-	
biodiversity credits for the purpose of	reasonable-steps-170498.pdf	
applying the variation rules		
Threatened Species Website	www.environment.nsw.gov.au/threatenedspecies/	
NSW BioNet (Atlas of NSW Wildlife)	www.bionet.nsw.gov.au/	
NSW guide to surveying threatened	www.environment.nsw.gov.au/resources/threatenedspecies/1601	
plants (OEH 2016)	29-threatened-plants-survey-guide.pdf	
Threatened species survey and	www.environment.nsw.gov.au/threatenedspecies/surveyassessm	
assessment guideline information	entgdlns.htm	
BioNet Vegetation Classification - NSW	www.environment.nsw.gov.au/research/Vegetationinformationsyst	
Plant Community Type (PCT) database	em.htm	
Data Portal (access to online spatial data)	http://data.environment.nsw.gov.au/	
Fisheries NSW policies and guidelines	http://www.dpi.nsw.gov.au/fisheries/habitat/publications/policies,-	
	guidelines-and-manuals/fish-habitat-conservation	
List of national parks	http://www.environment.nsw.gov.au/NationalParks/parksearchato	
	<u>Z.aspx</u>	
Revocation, recategorisation and road	http://www.environment.nsw.gov.au/policies/RevocationOfLandPo	
adjustment policy (OEH, 2012)	licy.htm	
Developments adjacent to National Parks	https://www.environment.nsw.gov.au/-/media/OEH/Corporate-	
and Wildlife Service lands Guidelines for	Site/Documents/Parks-reserves-and-protected-	
consent and planning authorities (DPIE	areas/Development-guidelines/developments-adjacent-npws-	
2020)	lands-200362.pdf	
Water and Soils		
Acid sulphate soils		

Acid sulphate soils	
Acid Sulfate Soils Planning Maps via	http://data.nsw.gov.au/data/
Data.NSW	
Acid Sulfate Soils Manual (Stone et al.	http://www.environment.nsw.gov.au/resources/epa/Acid-Sulfate-
1998)	Manual-1998.pdf

Attachment 2 - Environmental Assessment Guidance Material

Title	Web address
Acid Sulfate Soils Laboratory Methods	http://www.environment.nsw.gov.au/resources/soils/acid-sulfate-
Guidelines (Ahern et al. 2004)	soils-laboratory-methods-guidelines.pdf
	This replaces Chapter 4 of the Acid Sulfate Soils Manual above.
Flooding and Coastal Erosion	
Reforms to coastal erosion management	http://www.environment.nsw.gov.au/coasts/coastalerosionmgmt.ht m
Floodplain development manual	http://www.environment.nsw.gov.au/floodplains/manual.htm
Guidelines for Preparing Coastal Zone Management Plans	http://www.environment.nsw.gov.au/resources/coasts/130224CZM PGuide.pdf
NSW Climate Impact Profile	http://climatechange.environment.nsw.gov.au/
Climate Change Impacts and Risk	Climate Change Impacts and Risk Management: A Guide for
Management	Business and Government, AGIC Guidelines for Climate Change
	Adaptation
Water	
Water Quality Objectives	http://www.environment.nsw.gov.au/ieo/index.htm
ANZECC (2000) Guidelines for Fresh	www.environment.gov.au/water/publications/quality/australian-
and Marine Water Quality	and-new-zealand-guidelines-fresh-marine-water-quality-volume-1
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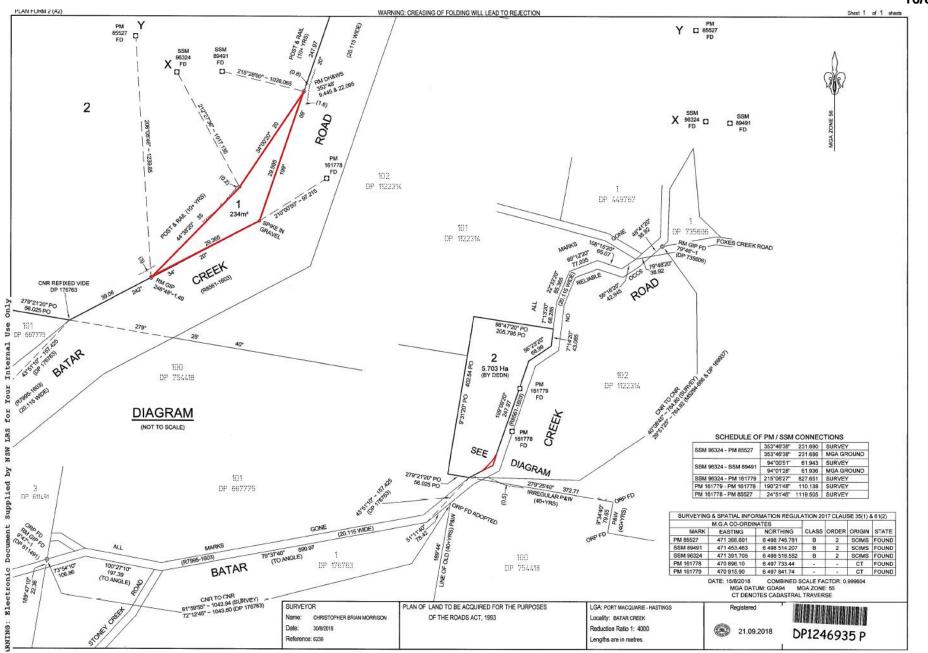


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